

GREAT WESTERN RAILWAY

GENERAL APPENDIX

TO THE

RULE BOOK

To come into operation on August 1st, 1936.






JAMES MILNE,
General Manager.

BRITISH RAILWAYS

(Western Operating Area)






Passenger & Freight Train Classification

On and from Monday, 5th June, 1950, a standard classification for passenger and freight trains will be introduced, and from that date existing headlamp and block bell codes shewn on pages 139 and 140 of the General Appendix to the Rule Book will be superseded by the following :—

Classification.	Engine Head-code (white lights or discs).	Description of Train.	Block Bell Code.	
			Beats on Bell or Gong.	How to be given.
A		Express passenger train, newspaper train or breakdown van train or snow plough going to clear the line or light engine going to assist disabled train...	4	Consecutively.
		Officers' Special train not requiring to stop in section	4	Consecutively.
		Express Diesel Car	8	4 pause 1 pause 3.
B		Ordinary passenger train, mixed train, or breakdown van train NOT going to clear the line, or loaded rail motor train†	4	3 pause 1.
		† Branch passenger train	4	1 pause 3.
		Ordinary passenger or parcels Diesel Car	9	5 pause 1 pause 3.
C		Parcels, fish, fruit, horse, livestock, meat, milk, pigeon or perishable train composed entirely of vehicles conforming to coaching stock requirements ...	5	1 pause 3 pause 1
		Express freight, livestock, perishable or ballast train, pipe-fitted throughout, with the automatic brake operative on not less than half of the vehicles ...	5	3 pause 1 pause 1.
		Empty coaching stock train (not specially authorised to carry "A" headcode), or empty rail motor train†	5	2 pause 2 pause 1.
D		Express freight, livestock, perishable or ballast train partly fitted with the automatic brake operative on not less than one-third of the vehicles ...	5	Consecutively.
E		Express freight, livestock, perishable or ballast train partly fitted with not less than four braked vehicles connected by vacuum pipe to the engine ...	5	1 pause 2 pause
		Express freight, livestock, perishable or ballast train with a limited load of vehicles NOT fitted with continuous brake	5	1 pause 2 pause

Continued over

Passenger and Freight Train Classification—continued.

Classification.	Engine Head-code (white lights or discs).	Description of Train.	Block Bell Code.	
			Beats on Bell or Gong.	How to be given.
F		Express freight, livestock, perishable, or ballast train NOT fitted with continuous brake	5	3 pause 2.
G		Light engine or light engines coupled ... Engine with not more than two brake vans	5 5	2 pause 3. 1 pause 1 pause 3.
H		Through freight or ballast train not running under class "C," "D," "E" or "F" headcode	5	1 pause 4.
J		Mineral or empty wagon train	5	4 pause 1.
K		Freight, mineral or ballast train stopping at intermediate stations † Branch freight train Freight, ballast or Officers' Special train, requiring to stop in section	3 3 7	Consecutively. 1 pause 2. 2 pause 2 pause 3.

† To be used only where authorised by the Operating Superintendent.

‡ The term "rail motor train" includes "auto train."

The classification of each train will be shewn in the appropriate column of the working timetables on and from the above date.

PADDINGTON STATION,
May, 1950.

GILBERT MATTHEWS,
Operating Superintendent.

5/50. 42,500.

BRITISH RAILWAYS (WESTERN OPERATING AREA)

Alterations and Additions to the General Appendix to the Rule Book

To come into operation on receipt.

The following instructions to be inserted on page 70:—

INSTRUCTIONS FOR RUNNING AND WORKING OF THE LENNOX-LOMAX EARTH AUGER EQUIPMENT.

1. The machine must only be used by the staff authorised by the Signal Engineer.
2. The machine, which is provided with a special match truck, is capable of self-propulsion at a speed of 10 miles per hour (in forward or reverse gear) and, except when operating, must have the match truck coupled. The machine is fitted with three types of brakes, viz., Hydraulic, Transmission and hand screw wheel. The match truck has a hand brake only.
3. When travelling under its own power the machine or the match truck, as the case may be, must carry a white headlamp and a tail lamp, which must be lighted as necessary. A red flag must be displayed on the rear vehicle by day. The machine must be equipped with red and green handsignal flags, not less than 12 detonators, a hand lamp (lighted when necessary) and a sprag. A portable telephone or "walkie-talkie" apparatus must also be available.
4. When boring operations are required to be carried out an absolute occupation of the line concerned must be arranged. The service on which the machine will be worked to the site of operations and the occupation required must be pre-arranged with the District Operating or District Traffic Superintendent concerned.
5. The machine must be worked to the agreed place near the site of work, or vice versa, by freight train carrying "F" or inferior headcodes and be marshalled next inside the Guard's brake van. The match truck may be leading or trailing. Should it be necessary to run the machine as a special train hauled by a locomotive a brake van must always be provided at the rear, in which a Guard must ride.
6. When the machine is proceeding under its own power to the site where it is required to work the match truck must always be attached and both the screw and chain couplings must be used for the purpose. Such movements must be confined to the shortest possible distance but if it is necessary for the machine to pass completely through one or more sections it must be signalled and dealt with as a Through Ballast Train. When running in a multiple-aspect signalling area automatic signalling must be suspended and the machine dealt with in accordance with T.C.B. Regulation 15. Where an Inner Home Signal is provided the "Is Line Clear?" signal must not be accepted until the Home signal can be lowered.
7. The machine must only be moved under its own power by the Driver who has been passed as competent by the Signal Engineer and he must be accompanied by a man who has been passed as competent by the Motive Power Department in (a) knowledge of the route and (b) protective duties of a Driver. A member of the crew of the machine who has been passed by the Operating Department as competent to carry out the protective duties of a Guard must always be present.
8. The machine must not be relied upon to actuate track circuits and Rule 55 must be observed in all cases by the man acting as Guard. When the machine is detained whilst waiting acceptance by the box in advance it must not draw forward to the signal controlling the entrance to the section ahead or to an Intermediate Block Home signal but must be held opposite the box. No train must be allowed to follow the machine towards the Intermediate Block Signal until "Train out of Section" has been received.

9. A good look out must be kept when approaching level crossings.
10. No movement must be made past stop signals otherwise than with the consent of the Signaller.
11. Protection at the site of boring operations must be in accordance with Rule 217
12. A portable telephone or "walkie-talkie" sets must be provided to enable contact to be maintained between the site of operations and the signal box in the rear.
13. When the machine is working, trains may pass on an adjoining line without restriction except as provided for in Clause 14.
14. Should boring between running lines be required to be carried out or should there be any possibility that the operations will obstruct the opposite or adjoining line, prior notification must be given to the Operating Department and a responsible member of the Operating Department must be present and the operations must not be commenced without his permission. Before giving such permission he must ascertain from the Signaller in the rear that no train is approaching on the opposite or adjoining line and the Signaller, before agreeing to obstruction of such line, must comply with the provisions of Block Regulation 13. The line affected must, in addition, be protected by a Handsignaller in accordance with Rule 217.
15. When the opposite or adjoining line is clear, the Signaller must be advised and such line must not be further obstructed until the provisions of Clause 14 have again been complied with.
16. At the conclusion of boring operations the machine and match truck will be removed from the section under the power of the machine. If the machine is returned to the signal box in rear, the Driver must bring it to a stand before reaching the detonators protecting the work. The conductor must proceed on foot to obtain the Signaller's Wrong Line Order authorising return to the signal box. The Signal Department person in charge must give the Signaller an assurance that the section is clear of obstruction.
(G.A.32.Op-10/56. OM12832)

S. G. HEARN,

October, 1956.

Chief Operating Superintendent.

Each member of the Staff receiving a copy of this Circular is required to read carefully and note the contents, and, if supplied with a copy of the General Appendix to the Rule Book to alter or cancel in ink the present instructions on the subject appearing therein, afterwards pasting the amendments in their proper places in the General Appendix to the Rule Book.

Station and Depot Masters are responsible for seeing that copies of the General Appendix to the Rule Book supplied to Signal Boxes, etc., under their supervision, are corrected in accordance with this Circular.

S.R. 30145/33 G.A.32.Op.

(This form must be detached and forwarded to the Head of Department.)

.....Station

.....1956

RECEIVED copy of Circular G.A.32.Op., dated October, 1956, containing alterations and additions to the General Appendix to the Rule Book.

PRIVATE.—For use of the staff concerned only

BRITISH RAILWAYS
(WESTERN OPERATING AREA)

NOTICE
OF
SPECIAL TRAIN
ARRANGEMENTS

EDINBURGH to FILTON JUNCTION
(Via SHREWSBURY, HEREFORD and SEVERN TUNNEL)

BRISTOL (T.M.) to PADDINGTON

FRIDAY, 6th NOVEMBER, 1953

THIS NOTICE, WHICH WILL BE DISTRIBUTED BY THE DISTRICT OPERATING SUPERINTENDENTS CONCERNED TO ALL STAFF AFFECTED IN THEIR RESPECTIVE DISTRICTS, MUST BE ACKNOWLEDGED TO THE DISTRICT OPERATING SUPERINTENDENTS IMMEDIATELY ON RECEIPT BY TELEGRAM AS FOLLOWS:—" ARNO FIFTY."

THE INSTRUCTIONS CONTAINED IN RAILWAY EXECUTIVE CIRCULAR O/RR, DATED DECEMBER, 1950, UNDER THE HEADING "SPECIAL INSTRUCTIONS TO BE OBSERVED IN CONNECTION WITH THE WORKING OF TRAINS DESIGNATED BY THE CODE WORD 'DEEPDENE'" MUST BE APPLIED TO THE TRAINS IN THIS NOTICE WHICH ARE MARKED "DEEPDENE."

TIME TABLE OF "DEEPDENE" TRAIN—EDINBURGH TO WESTERLEIGH STABLING POINT (VIA LONDON MIDLAND REGION, SHREWSBURY, HEREFORD AND SEVERN TUNNEL)—EARLY FRIDAY MORNING, 6th NOVEMBER, 1953.

The Engine to carry EXPRESS PASSENGER TRAIN HEAD CODE.

The Train will carry TWO TAIL LAMPS from Shrewsbury to Stabling Point. (Important—see paragraph 2 of R.E. Circular O/RR, dated December, 1950.) The Engine Head Lamps and the Tail Lamps must be lighted before leaving Shrewsbury.

The Train will run on the Main Line to Westerleigh West Junction; via the Down Platform Line at Hereford (Barr's Court) and Pontypool Road.

WORKING OF TRAIN ENGINE.

The Engine (No. 5073) to work the "Deepdene" Train forward from Shrewsbury Station to leave Coleham Shed, chimney towards Hereford, at 11.45 p.m. (5th November), and arrive Turntable Siding, Severn Bridge Junction at 12.00 night.

FORMATION (FROM ENGINE) LEAVING SHREWSBURY:—

FORMATION (FROM ENGINE) LEAVING SHREWSBURY.							
L.M.R. Stock.	{	BRAKE FIRST (Brake End Leading)	No.	5154	} Approximately 183 tons.
		SALOON (Kitchen End Leading)	..	..	..	45006
		SALOON (Principal Door Leading)	..	..	..	45005
		SALOON (Kitchen End Trailing)	..	..	..	45000
		VAN THIRD (Brake End Trailing)	..	..	..	26287
Length of Train (excluding Engine)			..	309 feet 10 inches.			

The distance from the centre of the footplate of the engine to the centre of the principal door of Saloon No. 45005 is approximately 152 feet 2 inches. The distance from the centre of the principal door of Saloon No. 45005 to the buffer face of the rear coach (Van Third No. 26287) is approximately 182 feet 1½ inches.

THE PERMANENT AND TEMPORARY SPEED RESTRICTIONS MUST BE STRICTLY OBSERVED.

Distances from Harlescott Crossing.		PRINCIPAL STATIONS AND INTERMEDIATE SIGNAL BOXES.		TIMES.	REMARKS.
Mls.	Chs.			a.m.	
—	—	Harlescott Crossing	.. pass	12.25	From L.M.R.
2	0	Crewe Junction	.. ..	—
2	7	SHREWSBURY (Up and Down Platform)	{ arr. 12.30 dep. 12.55	12.30 12.55	Change Engines and Guards. Gas and Water as required. L.M.R. Engine to be detached and run at once to Coleham Shed. W.R. Engine No. 5073 to work forward. For detailed instructions for dealing with the "Deepdene" Train at Shrewsbury, see Chester District Operating Superintendent's Notice.
2	61	Coleham	.. .. pass	12.58	
2	77	Sutton Bridge Junction	.. ..	—
6	33	Condover	.. ..	—
8	41	Dorrington	.. ..	—

TIME TABLE OF "DEEPDENE" TRAIN FROM EDINBURGH TO WESTERLEIGH STABLING POINT, FRIDAY, 6th NOVEMBER, 1953—

continued.

Distances from Harlecott Crossing.		PRINCIPAL STATIONS AND INTERMEDIATE SIGNAL BOXES.	TIME.	REMARKS.
Mls.	Chs.		a.m.	
11	30	Leebotwood pass	—	
—	—	Dudgeley Signals	—	Intermediate Block Signals—Down Distant and Down Home.
14	70	Church Stretton	1.21	
22	3	Craven Arms & Stokesay ..	1.31	
27	31	Bromfield	—	
28	43	Water Troughs	—	No Up trains must be in the section from Ludlow to Bromfield while the Royal Train is passing through the section from Bromfield to Ludlow.
29	50	Ludlow	1.42	
29	53	Ludlow Tunnel	—	
34	18	Woofferton	1.48½	
40	43	Leominster	1.57½	
44	74½	Dinmore Tunnel (Up and Down)	—	
51	33	Shelwick Junction	2.13	
52	29	Barr's Court Junction	2.15	
53	10	Hereford (Barr's Court) { arr. 2.18 dep. 2.23		To run via Down Platform Line. Engine to take Water.
56	36	Red Hill Junction .. pass	2.29	
57	40	Red Hill Tunnel	—	
59	62	Tram Inn	—	
62	14	St. Doveaux	—	
65	49	Pontrilas	2.42½	
70	58	Pandy	—	
73	7	Llanvihangel	2.55	
76	6	Abergavenny Junction ..	—	
77	8	Abergavenny (Monmouth Road)	3. 1	
79	63	Penpergwm	—	
82	32	Nantyderry	—	
85	1	Little Mill Junction ..	—	
86	44	Pontypool Road	3.14½	To run via Down Platform Line.
88	4	Panteg Junction	—	
89	22	Lower Pontnewydd	—	
90	49	Llantarnam Junction ..	3.20½	

TIME TABLE OF "DEEPDENE" TRAIN FROM EDINBURGH TO WESTERLEIGH STABLING POINT, FRIDAY, 6th NOVEMBER, 1953—

continued.

Distances from Barlescott Crossing.		PRINCIPAL STATIONS AND INTERMEDIATE SIGNAL BOXES.	TIMES.	REMARKS.
Mls.	Chs.		a.m.	
91	3	Llantarnam pass	—	
92	23	Ponthir "	—	
93	66	Caelecon "	—	
95	59	Maindee Junction North ..	3.29	
96	18	Maindee Junction East ..	3.31	
103	58	Water Troughs "	—	No Down train must be in the section from Undy Crossing to Magor while the "Deepdene" Train is passing through the section from Magor to Undy Crossing.
105	31	Severn Tunnel Junction ..	3.44	To run from Up Main to Up Tunnel line at Severn Tunnel Junction East.
106	20	Severn Tunnel West	3.46	
111	12½	Severn Tunnel East	4. 5	
111	15½	Ableton Lane Tunnel	—	
114	14	Patchway Tunnel	—	
115	73	Patchway "	4.16	
117	22	Stoke Gifford "	4.19	
119	10	Winterbourne "	4.23½	
120	40	Coalpit Heath "	4.27	
121	67	Westerleigh West Junction Signal Box ..	4.32	
122	15	STABLING POINT .. arr. (opposite the redundant Westerleigh North Junction Signal Box.)	4.37	For detailed instructions for dealing with the "Deepdene" Train at Stabling Point, see Bristol District Operating Superintendent's Notice.

TIME TABLE OF "DEEPDENE" TRAIN FROM WESTERLEIGH STABLING POINT TO FILTON JUNCTION STATION, FRIDAY, 6th NOVEMBER, 1953.

The Engine to carry **EXPRESS PASSENGER TRAIN HEAD CODE.**

The Train will carry **TWO TAIL LAMPS** from Stabling Point to Filton Junction Station. (Important—see paragraph 2 of R.E. Circular O/RR, dated December, 1950.) The Engine Head Lamps and the Tail Lamps must be lighted before leaving Stabling Point.

WORKING OF TRAIN ENGINE.

The Engine (No. 5000) to work the "Deepdene" Train from Stabling Point to Filton Junction Station, to leave Bath Road Shed, Tender leading, at 3||50 a.m. for Stabling Point, arrive 4||43 a.m. To be attached to Van Third No. 26287 and steam heat the train until departure at 10.0 a.m.

FORMATION (FROM ENGINE) LEAVING STABLING POINT :—

L.M.R. Stock.	{	VAN THIRD (Brake End Leading)	No. 26287	} Approximately 183 tons.
		SALOON (Kitchen End Leading)	" 45000	
		SALOON (Principal Door Trailing)	" 45005	
		SALOON (Kitchen End Trailing)	" 45006	
		BRAKE FIRST (Brake End Trailing)	" 5154	
		Length of Train (excluding Engine)	309 feet 10 inches.	

The distance from the centre of the footplate of the engine to the centre of the principal door of Saloon No. 45005 is approximately 206 feet 7½ inches. The distance from the centre of the principal door of Saloon No. 45005 to the buffer face of the rear coach (Brake First No. 5154) is approximately 127 feet 8½ inches.

THE PERMANENT AND TEMPORARY SPEED RESTRICTIONS MUST BE STRICTLY OBSERVED.

Distances from Stabling Point.		PRINCIPAL STATIONS AND INTERMEDIATE SIGNAL BOXES.	TIMES.	REMARKS.
Mls.	Chs.		A.M.	
—	—	STABLING POINT .. dep.	10. 0	For detailed instructions for dealing with the "Deepdene" Train at Stabling Point, see Bristol District Operating Superintendent's Notice.
—	28	Westerleigh West Junction Signal Box .. pass	10. 3	
1	55	Coalpit Heath	10. 6	
3	5	Winterbourne	10. 9	
4	73	Stoke Gifford	10.14	
6	4	FILTON JUNCTION STATION (Down Main Platform) .. arr.	10.20	For detailed instructions for dealing with the "Deepdene" Train at Filton Junction Station, see Bristol District Operating Superintendent's Notice.

WORKING OF EMPTY L.M.R. STOCK FRIDAY, 6th NOVEMBER, 1953.

FILTON JUNCTION STATION TO DR. DAY'S SIDINGS, BRISTOL.

THE PERMANENT AND TEMPORARY SPEED RESTRICTIONS MUST BE STRICTLY OBSERVED.

PRINCIPAL STATIONS AND INTERMEDIATE SIGNAL BOXES.	TIMES.	REMARKS.
Filton Junction Station (Down Main Platform)	10.20 RL	To carry "A" Head Code and be signalled accordingly. To be given a clear run. To carry one Tail Lamp from Filton Junction Engine No. 500. For detailed instructions for dealing with the Empty Train at Filton Junction Station, see Bristol District Operating Superintendent's Notice.
Stapleton Road pass	10.34	
Dr. Day's Sidings arr.	10.40	For detailed instructions for dealing with the Empty Train at Dr. Day's Sidings, see Bristol District Operating Superintendent's Notice.

The L.M.R. coaches (five) to be conveyed "light" formed next engine of the 1.50 p.m. ordinary train from Bristol T.M. to Paddington. Load 12 = approximately 415 tons Bristol to Reading, and Load 11 = approximately 380 tons Reading to Paddington.

Class "A" Head Code.

Old Oak Common Yard
Old Oak Common East
North Pole Junction ..
Kensington (Olympia)
North Pole Junction ..

6th November.
N Reverse.
For Wolverton, via Willesden Junction

L.M.R. Engine and Guard to work forward from Kensington (Olympia).

L.M.R. Van Third No. 26287 to be gangwayed to Main Train at Dr. Day's Sidings, but corridor doors to be kept closed during journey from Bristol T.M. to Paddington and Old Oak Common.

The following coaches (2) to be conveyed next behind Buffer Car at 1.50 p.m. Bristol (T.M.) to Paddington not to work on this date. Load 12 = approximately 415 tons Bristol to Reading, and Load 11 = approximately 380 tons Reading to Paddington.

For detailed arrangements see Bristol and Filton District Operating Superintendent's Notices.

**TIME TABLE OF 4.15 p.m. BRISTOL T.M.) TO PADDINGTON TO RUN AT
AMENDED TIMES UNDER "DEEPDENE" INSTRUCTIONS BRISTOL T.M.)
TO PADDINGTON FRIDAY, 6th NOVEMBER, 1953.**

The Engine to carry EXPRESS PASSENGER TRAIN HEAD CODE.

The Train will carry TWO ORDINARY TAIL LAMPS from Bristol (T.M. to Paddington. Important see paragraph 2 of R.E. Circular O RR, dated December, 1950. Tail Lamps will be carried. The Engine Head Lamps and the Tail Lamps must be lighted before leaving Bristol (T.M.).

The Train will run on the Main Line throughout the journey.

WORKING OF TRAIN ENGINE.

The Engine will work the Deepdene Train from Bristol (T.M.) to Paddington to leave Bristol (T.M.) at 4.15 p.m. for Malaga Vale to be attached to coaches for steam heating prior to departure with stock at 3.45 p.m. for Bristol (T.M.) arrive 3.40 p.m.

FORMATION (FROM ENGINE) LEAVING BRISTOL (T.M.) :-

For Royal Parks	{	BRAKE COMPO* (Brake End Leading)	No. 7377	
		H SALOON	9001	
		KITCHEN End Leading		
		VAN THIRD (Brake End Leading)		
		FIRST		
Regular Coaches.		FIRST		
For Ordinary Passengers.		BUFFET CAR		
		THIRD		
		THIRD		
		THIRD		
		THIRD		
		VAN THIRD (Brake End Trailing)		

Bristol
(T.M.)
to
Paddington.

* Tables to be fitted in all compartments

H—To be gangwayed to Main Train, but corridor doors to be kept locked.

NOTE The Regular Bristol to Reading Slip Coach not to be attached. Train to call specially at Reading to set down and to carry Two Ordinary Tail Lamps throughout. Clause 31 of the Slip Carriage Working Instructions will not apply.

Load 13 Coaches = approximately 455 tons (not to be exceeded).

The length of the train at the top end of the engine to the centre of the principal door of Saloon No. 10001 is approximately 150 feet 7 inches

**4.15 p.m. BRISTOL T.M.) TO PADDINGTON (AMENDED TIMES.
THE PERMANENT AND TEMPORARY SPEED RESTRICTIONS MUST BE
STRICTLY OBSERVED.**

Stations	PERSONAL STATIONS AND INTERMEDIATE SIGNAL BOXES.	TIMES.	REMARKS.
M. Chs.	BRISTOL (T.M.) No. 9 Platform	.. dep. 4.15	For detailed instructions for dealing with the "Deepdene" Train at Bristol T.M. see Bristol District Operating Superintendent's Notice
1	62 North Somerset Junction pass	..	
1	31 Bristol East Depot	.. ..
1	St Andrew's Tunnel	..	
1	60 Fox's Wood Tunnel No. 2	..	
2	3½ Fox's Wood Tunnel No. 3	..	
3	63 Water Troughs .. ..	..	No Down train must be in the section from Keynsham West to Fox's Wood whilst the "Deepdene" Train is passing through the section from Fox's Wood to Keynsham West
4	46 Keynsham .. ..	..
6	43 Saltford Tunnel .. ..	..
7	0 Saltford .. ..	..

TIME TABLE OF 4.15 p.m. BRISTOL (T.M.) TO PADDINGTON AMENDED TIMES, FRIDAY, 6th NOVEMBER, 1953—continued.

Distances from Bristol (T.M.)		PRINCIPAL STATIONS AND INTERMEDIATE SIGNAL BOXES.	Times.	REMARKS.
Mls.	Chs.		p.m.	
9	13	Twerton Tunnel .. pass	—	
10	35	Oldfield Park	—	
10	79	Bath Goods	—	
11	38	Bath Spa .. { arr. 4.32 dep. 4.36		
11	75	Sydney Gardens W. Tun. pass	—	
12	0½	Sydney Gardens E. Tun. ..	—	
13	59	Bathampton	—	
16	60½	Middle Hill Tunnel	—	
17	30	Box Tunnel	—	
24	32	Chippenham	4.00	
35	48	Wootton Bassett		The 4.15 p.m. Chippenham to Swindon must work punctually to Wootton Bassett, and after detraining passengers at Wootton Bassett, it must be back shunted to the Up Avoiding Line at Wootton Bassett. When the train has come to a stand inside the catch point the latter must be restored to the open position. The 4.35 p.m. Chippenham to Swindon must not be allowed to leave Wootton Bassett East until the "Train out of Section" signal has been received by Wootton Bassett East from Hay Lane for the "Deepdene" Train.
39	06	Sturley Signals		The 4.15 p.m. Wootton Bassett to Swindon must work punctually to Sturley Signals, and after detraining passengers at Sturley Signals, it must be back shunted to the Up Avoiding Line at Sturley Signals.
41	4	Swindon Junction	5.13½	The 4.40 p.m. Ministry to Swindon must work punctually and run to No. 8 Platform at Swindon. The 3.50 p.m. Cheltenham to Paddington to run to No. 5 Platform at Swindon with Points Nos. 83 & 111 set for the 4.40 p.m. Ministry to Swindon. The 4.40 p.m. Ministry to Swindon to Calne } Must work punctually. The 4.40 p.m. Ministry to Swindon to Chippenham } The 4.40 p.m. Ministry to Swindon to Melksham }
61	66	Steventon	5.32½	
64	52	Foxhall Junction	ML	The 12.0 noon Minehead to Paddington } Must work punctually throughout. The 1.00 p.m. Weymouth to Paddington } The 11.10 a.m. Milford Haven to Paddington } The 4.20 p.m. Swindon to Paddington }
65	18	Didcot	5.35½	
69	71	Cholsey & Moulsoford	—	
71	61	South Stoke Signals	—	Intermediate Block Signals (Colour Light) Up Distant and Up Home.
73	48	Goring & Stratley	—	No Down Train must be in the section from Pangbourne to Goring & Stratley whilst the "Deepdene" Train is passing through the section from Goring & Stratley to Pangbourne.
74	48	Water Troughs	—	
75	11	Basildon Signals	—	Intermediate Block Signals—Up Distant and Up Home.
76	65	Pangbourne	—	
78	18	Purley Signals	—	Intermediate Block Signals—Up Distant and Up Home.
79	57	Tilehurst	—	
81	30	Reading West Junction	—	

TIME TABLE OF 4.15 p.m. BRISTOL (T.M.) TO PADDINGTON (AMENDED TIMES, FRIDAY, 6th NOVEMBER, 1953 *continued*

Distances from Bristol (T.M.).		PRINCIPAL STATIONS AND INTERMEDIATE SIGNAL BOXES.		TIMES		REMARKS
Mls.	Chs.				p.m.	
—	—	Reading Main Line West pass			—	
52	30	Reading General { arr.		5.53		To set down passengers only. The 4.40 p.m. A. & P. train to Reading must work punctually. The 4.40 p.m. Swindon to Reading train must work punctually at advertised time, 5.30 p.m. The 5.20 p.m. Didcot to Reading must work punctually and be cleared promptly from the Up Relief Line Platform at Reading General. The 3.50 p.m. Cheltenham to Paddington to run via the Relief Line from Reading Main Line West and be dealt with at the Up Relief Line Platform at Reading General. To run via the Main Line from Reading Main Line East.
		Up Main Line Platform { dep		5.55		
87	27	Twyford	pass	6.24		
94	9	Mardenhead	"	6.9		
99	72	Slough	"	6.14 1/2		The 4.20 p.m. Swindon to Paddington to leave Slough at advertised time, 5.55 p.m.
105	10	West Drayton		—		The 5.45 p.m. Paddington to High Wycombe must work punctually to West Drayton. To run via the Relief Line from West Drayton East, preceding the 6.33 p.m. Paddington.
						The 4.15 p.m. Reading to Paddington must work punctually.
104	22	Southall		6.23		Between Southall and Ealing Broadway Absolute Clock Working by Bell. The 4.15 p.m. train preceding the "Deerpene" Train and for the "Deerpene" Train itself in accordance with the "Deerpene" Train for the "Deerpene" Train on double lines where Block Apparatus is not provided in Multiple Aspect Signalling Areas—Notice L.K. 245. The 5.40 p.m. Slough Depot to Paddington (Suburban) to run via the Relief Line to Ealing Broadway, leaving following the 5.40 p.m. Uxbridge and preceding the 6.12 p.m. Auto Greenford to Ealing Broadway.
112	62	Ealing Broadway ..	"	—		
115	43	Old Oak Common East..	"	—		
117	8	Westbourne Park ..	"	6.31		
118	58	PADDINGTON (No. 8 Platform)	arr.	6.35		For details of the "Deerpene" Train see Notice L.K. 245. For details of the "Deerpene" Train see Notice L.K. 245. Operating Superintendent's Notice.

TRAIN OPERATING.

The Train Operating Superintendent must be reported from the local reporting points to the District Operating Superintendent, who must take steps to see that the information is passed forward to the Operating Superintendent's Office, Paddington.

GILBERT MATTHEWS,
Operating Superintendent,
PADDINGTON STATION.

3rd November, 1953.
(1.39,626 G.)

BRITISH RAILWAYS

(WESTERN OPERATING AREA.)

DIESEL ELECTRIC ENGINE P.W.M.650.

The Civil Engineer's Diesel Electric Engine No P.W.M.650 may operate under the following instructions:—

- | | |
|--|--|
| General | (1) It is Engineering Department plant and is to be used for Engineering purposes only under the general supervision of the Crane Relaying Inspector. |
| | (2) It will be operated by a man appointed by the Engineering Department who will be instructed by a Motive Power Department Headquarters Inspector in rules applicable to a driver employed by the Motive Power Department so that when operating in sidings he can follow the normal procedure applicable thereto. |
| | (3) It will operate track circuits. |
| | (4) The maximum permitted speed under its own power is 20 m.p.h. and when conveyed with gear disengaged 25 m.p.h., and in neither case may it travel more than 25 miles without intermediate stop for examination. |
| Movement and Operating Instructions | (5) Machine P.W.M.650 normally is only permitted to work on lines completely occupied by the Civil Engineer or in Engineering Department Sidings and in such circumstances the Engineering Department Operator will operate to the instructions of the groundman or supervisor who will be required to see the track is clear and points correctly set for the movement. In the above circumstances a Motive Power Department conductor is not required. |
| | (6) (a) It may be permitted to travel under its own power on running lines and sidings by prior arrangement with the Operating and Motive Power Departments, but must in all such instances, in addition to the Engineering Department Operator, be accompanied by a conductor provided by the Motive Power Department who will be responsible for carrying out all protective rules. |
| | (b) As a light engine under its own power in charge of an Engineering Department Operator with a Motive Power Department conductor. To be dealt with as a light engine and signalled "G" headcode (2-3). |
| | (c) Working in charge of an Engineering Department Operator, with a Motive Power Department conductor, under its own power, an Engineering Department train with freight brake van and guard. To be signalled as a ballast train under "H" headcode (1-4) conditions. |
| | (d) Under its own power, coupled to a track relaying crane to an occupied section in charge of an Engineering Department Operator with a Motive Power Department conductor. A freight brake van and guard to be provided unless otherwise decided by the Engineering Department representative in charge. To be signalled as a ballast train requiring to stop in section "K" headcode (2-2-3). |
| | (e) Hauled by a steam locomotive, with the gear disengaged with freight brake van and guard. To run under "H" headcode conditions and be signalled (1-4). To be accompanied by an Engineering Department representative. |
| | (f) When forming part of an ordinary freight train it must be marshalled next in front of the rear freight brake van. To be restricted to "H" and inferior headcode trains and signalled accordingly. To be accompanied by an Engineering Department representative. |
| | (g) When forming part of an Engineering Department special train at the discretion of the Engineering Department representative in charge, may be marshalled in any position and to be accompanied by an Engineering Department representative. |

- (7) On a Permissive Line when running under its own power, the next following train admitted to such line must be brought to a stand at the Signal Box and the driver verbally told to proceed with caution and that he must take care not to buffer up to the machine.
- (8) When in a siding unattended it is to be left with gear disengaged with the brake on and the switches are to be set and clipped to prevent any movement to the siding, or a wheel stop placed in position in order to protect the plant from contact with any shunting movements in the siding.
- (9) All shunting movements with the machine must be carried out with care and in every case the machine must be accompanied by an Engineering Department representative.

M. G. R. SMITH.
Civil Engineer.

W. N. PELLOW.
Motive Power Superintendent.

GILBERT MATTHEWS.
Operating Superintendent.

B.R.361, 5.

PADDINGTON STATION.
August, 1953.



BR 87223

THE RAILWAY EXECUTIVE
(BRITISH RAILWAYS)

28/6
15/5/50

PROCEDURE TO BE ADOPTED

and

STANDARD INSTRUCTIONS

in connection with

TRAIN AND TRAFFIC CONTROL

APRIL, 1950.

THE RAILWAY EXECUTIVE

(BRITISH RAILWAYS)

INTRODUCTION

In the interest of uniformity throughout the Regions forming British Railways and in conformity with the integration of practices which is one of the essential features of nationalisation, consideration has been given to the adoption of a standard procedure for Train and Traffic Control and this Booklet has been compiled which sets out the basic principles to be observed in the endeavour to maintain efficient train and traffic operating. The fundamental principles outlined will be supplemented by Instructions local to the District and these will be issued by the respective Regional Operating Superintendents.

The primary duty of a Control Office is to ensure that effect is given to the principles defined. As regards train control, there would be no necessity for a Control organisation if the service given completely followed the service scheduled, but all practical railwaymen appreciate that this ideal can never materialise, but it is hoped that as the re-organisation and integration of the railway service continues, we shall, at least, get nearer to this objective.

The Control organisation represents a service to provide for "a place for everything and everything in its place"; the former calls for accurate and practical timing and diagramming and the latter for a good organisation primarily to ensure adherence to the pre-arranged plan but also, in the event of untoward incidents, to take such action as will ease the immediate difficulties until the service is once more back to normality and to provide for adjustments in the booked service to meet fluctuations in flows of traffic.

The danger in the past has been the tendency for the Control Organisation to degenerate into a recording agency and insufficient time has been given to the study of the train service and its regulation both from the train and traffic viewpoint, in particular, regard should be paid to the clearance of yards and the most effective use of motive power and train crews.

Whilst records must be maintained, this is not the primary function of the Control Office. The records of the daily performance throughout the week in a particular district are now readily available by use of the weekly card and this record should be studied day by day with a view to special steps being taken to eradicate any consistent bad working.

The Control organisation, which will operate continuously on weekdays and, when necessary, on Sundays, is, in effect, the "watch dog" of the District Superintendent; this Officer, who should be regarded as the Chief Controller, must be served faithfully by three officials designated "Deputy Chief Controllers" directly responsible to their District Officer and deputising for him during their shifts of duty.

The principal duties of the staff operating the District Control Office are as follows:—

(i) Deputy Chief Controller

The senior man in charge of each shift in the Control is called the Deputy Chief Controller.

The duties of the Deputy Chief Controller are to ensure that in accordance with the District Operating Superintendent's direction, the objects of control as defined in the Standard Control Instructions are achieved, to supervise the work of the Control staff, and to deal personally with all important matters.

(ii) Section Controllers

Each District is sub-divided into suitable territorial sections, due regard being paid to the intensity and flow of traffic and the avoidance of unnecessary exchange of information between sections.

It will be the practice to divide control offices into sections of a size which can be adequately administered by a Section Controller who does his own recording.

The Section Controller should fulfil all the defined train and traffic control functions on his section, collaborating with the Trainmen's Relief Controllers about the matters which are their concern, and referring, when necessary, to the Deputy Chief Controller on important matters.

(iii) Trainmen's Relief Controllers

The Controllers responsible for watching the hours of trainmen, providing relief when necessary, and arranging the supply of men for special working, are designated "Trainmen's Relief Controllers."

In many districts the Trainmen's Relief Controllers will deal also with the district responsibility for unbalanced power; where there is a large amount of this work the appointment of a Motive Power Controller may be necessary.

(iv) Assistant Controllers.

In most districts Assistant Controllers are appointed to do the routine work, such as the exchange of train information with other districts, recording the particulars of engines and trainmen, the collection from the Section Controllers and the transmission to Headquarters of traffic stock summaries and selected train running records, preparing and filing train cards and records and relieving for meal breaks.

(v) Office Relief Controllers

The number of staff under this heading will be determined by the number of positions to be covered and the calculated needs for relieving the various grades to cover holidays, rest days, sickness and vacancies.

These instructions define the standard principles and practices that are to be followed, and it is hoped that all concerned will do their utmost to make the Control Organisation a real and vital force in the operation of British Railways. At the same time the Control staff must appreciate that the measure of their success will largely depend upon the degree of co-operation which they receive from the outside staff. To achieve this co-operation and the enthusiasm of the outside staff to the maximum degree, the Control staff will, in normal circumstances, leave the initiative in the hands of the outside staff, so that the fullest use may be made of their skill, ability and experience. The outside staff, will, in turn, look to the Control staff for guidance and leadership.

Int. Barrington-Ward.

222, Marylebone Road,
LONDON, N.W.1

APRIL, 1950

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TRAIN AND TRAFFIC CONTROL INSTRUCTIONS

OBJECTS OF CONTROL

1 The fundamental principle of efficient train and traffic operating is that the scheduled timings of trains in the working time tables, notices etc be maintained, the booked workings for Guards, Enginemen and engines be adhered to, and that the instructions relating to the classification, marshalling and loading of freight trains be complied with.

2. The main objects of Control are to maintain the booked arrangements to the maximum possible extent, to guide the working back to normal when out of course, and to modify the arrangements when necessary to meet fluctuations in traffic.

3 The Control in conjunction with other Operating and Motive Power staff engaged in train and traffic working will have the following general aims. --

- (a) To ensure the expeditious working of traffic including empty stock.
- (b) To plan and organise the current working of Passenger and Freight trains so as to avoid delay.
- (c) To obtain the maximum work from engine power and trainmen by --
 - (i) Punctual working.
 - (ii) Using the fewest locomotives possible.
 - (iii) Securing the maximum authorised loading.
 - (iv) Incurring the minimum amount of light mileage or unrequired assistance.
 - (v) Releasing engines promptly after completion of work.
 - (vi) Making the best use of unbalanced engines.
- (d) To regulate the working of trainmen to ensure economical working and avoid excessive hours.

4. All staff associated with the working of trains and the movement of traffic must carry out instructions given from the Control and must co-operate at all times to the fullest extent, by information, consultation and suggestion to overcome difficulties.

5 Station Masters, Yard Masters, Supervisors and the Staff under them are not relieved of their responsibility and must use their initiative in carrying out laid down arrangements.

6 Departures from scheduled arrangements and the provision of power and trainmen, for any purpose other than booked, must be made through the Control.

TRAIN CONTROL

7 The detailed regulation of trains in running must be carried out by the Signalmen (or Regulators) from information received in regard to the actual running, the working at their signal boxes and their knowledge of the requirements of the line ahead, taking into consideration the margins available for each type of train.

Signalmen will when necessary receive instructions from the Control as to ultimate requirements, thus assisting them in carrying out the detailed regulation. In certain circumstances, however, the Control Staff will give definite instructions to the Signalmen on questions of regulation.

Signalmen or others responsible for regulation must consult the Control in all cases of difficulty.

8. When necessary and in conjunction with other Control Offices concerned, the Control will make arrangements for the alteration or cancellation, of booked freight trains and the running of special trains, due regard being had to requirements for balancing or return working.

In emergency the Control will modify the working of passenger trains.

The Control will be expected to make suggestions for altered working and, to assist in this direction, current copies of the graphical train diagrams will be made available for the use of the Control.

9. The loading and movement of Freight trains, including Departmental trains, and the movement of specified Passenger trains falling within the following categories must be reported to the Control as shown in the local instructions:—

- (a) Certain main line passenger trains.
- (b) Important connecting passenger trains.
- (c) Suburban trains operating in particular localities.

10. Late starts, delays, loss of time in running with the cause, must be reported currently to the Control by the points at which delay takes place except where a modification or exemption is granted. When engines are late off shed, steps must be taken in collaboration with the Motive Power Depot to adjust the immediate arrangements and subsequent working as may be deemed necessary.

11. All orders for special Departmental trains and light engines will be received in the Control. Special power may be unnecessary, in which event the Control must arrange suitable transits by booked services in agreement with the Department concerned. When Departmental trains are working on the Line, the Control must co-operate with the Signalmen or other Staff in order that the maximum facilities may be given.

TRAFFIC CONTROL

12 The detailed position of traffic on hand must be telephoned to the Control in accordance with local instructions.

13 When traffic is stabled at places not specified in the local instructions for reporting traffic on hand, the person stabling the traffic should advise the Control of any wagons containing perishable or other important traffic. In addition, the Station Master or other person in charge must, at intervals, direct the attention of the Control to such traffic until it is cleared.

14 The Control must be advised also, at other than the times specified, of exceptional quantities of freight traffic passing or about to pass, and when it is seen that there will be a surplus or shortage of traffic for any of the booked services so that the necessary arrangements can be made.

Urgent special freight traffics, such as livestock, meat and fruit, for which no previous arrangements have been made must be advised to the Control so that suitable services can be arranged.

15 When it is necessary to stop or restrict temporarily the flow of any particular traffic, the Control will make the appropriate arrangements.

16 The Control must be advised of exceptional numbers of passengers for regular or special services.

USE OF MOTIVE POWER

17 The fundamental principle of the use of motive power is the preparation and maintenance of efficient engine diagrams. Adherence to the diagrammed workings to the maximum possible extent on the part of the Motive Power Depots and Controls is essential.

18 Motive Power Depots will give immediate advice to Control of anticipated unavoidable departures from diagrammed engine workings and Control must advise the Motive Power Depot as soon as it can be foreseen that a special engine is required, or a booked engine will be cancelled, or that an engine cannot reach the Motive Power Depot in time to be got ready for its next booked working. Full consultation between the Motive Power Depots and Control must take place in all matters of this nature, in particular no work must be added to the booked diagrammed work of an engine without prior agreement with the Motive Power Depot.

Cancellations of freight trains owing to non availability of locomotives, also the total number and extent of late starts owing to late provision of locomotives must be recorded by the Control currently for each 24 hours.

19. Engines must not be used on workings allocated to any Depot other than that to which the engine belongs without authority from the Control

20. Unbalanced engines must be closely supervised by the Control and it will be their responsibility to arrange in advance for such engines to be returned to their home depots, as quickly as possible, in the most productive and economical way

21. Engines belonging to a Motive Power Depot in one operating district working into another district must be treated as 'foreign' and their use when unbalanced must be at the directions of Headquarters Control to whom the position must be reported by the District Controls.

Engines belonging to a Motive Power Depot in a particular operating district and working within that district should be treated as 'local' and their use when unbalanced must be at the direction of the District Control except where instructions are issued to the contrary.

22. Unbalanced engines on hand must be reported to the Control currently; for those under repair, an advice as to when they will be ready for work must be given as soon as this is known.

At a stipulated time each day Motive Power Depots must advise the Control the power position which must include information in regard to engines away from the Depot without booked or pre-arranged return working, unbalanced engines on hand, and other Depots' engines on hand under repair.

23. At Headquarters and in Districts where the volume justifies it, the system of recording unbalanced engines and the use made of them should be by card index. In Districts where the volume of work does not warrant such a system the necessary records will be made on suitable forms.

CONTROL OF TRAINMEN

24. It is the responsibility of the Control to see that Enginemmen and Guards do not incur excessive losses. Particulars of the booking on times and home stations of trainmen reported by Motive Power and Goods Guards Depots must be recorded by the Control. Similar details must be given as to trainmen available for relief purposes, spare or becoming spare.

The Control will use the available staff to the best advantage by anticipating requirements and will record the use made of the relief staff

Trainmen when relieved must be informed where practicable by what means they must proceed to the signing off point.

Particulars of trainmen travelling as passengers to another district must be advised to the Control concerned.

DISTRIBUTION OF FREIGHT BRAKE VANS

25. The distribution of freight brake vans must be based on a planned allocation arrived at by balancing incoming and outgoing vans for every yard and station where vans are required for freight train working.

The allocation represents the minimum number of freight train brake vans required to be on hand at any yard at 8.0 a.m. each day to cover the working for the next 24 hours after an allowance has been made for the number of brake vans to arrive on incoming trains (with marginal allowance for late running based on the average working) and for special and departmental trains.

The total of the yard and station allocation is the allocation for the District at 8.0 a.m.

26. The actual number of freight brake vans on hand at all points, whether there is an allocation or not, must be recorded at 8.0 a.m. daily including those due to arrive by 8.0 a.m. but running late, provided it is known they are en route but excluding the following :—

- (i) Crippled brake vans.
- (ii) Brake vans which should have left by 8.0 a.m. but whose departures have been delayed.
- (iii) Brake vans on trains at intermediate points on their journey.

The number of brake vans recorded on the foregoing basis and the number short of, or surplus to, the 8.0 a.m. allocation, must be reported to the Control.

27. The District Officer will arrange with the principal C & W Shops to report daily at 8.0 a.m. the number of freight brake vans which it is anticipated will be ready for use that day.

28. The daily freight brake van position for the District, which will be arrived at by taking the number of brake vans on hand and comparing it with the allocations, must be telephoned promptly to Headquarters Control.

The shortage or excess of brake vans must be adjusted between yards by Control and between Districts by Headquarters; all disposal instructions so given must be fully and promptly carried out.

When instructions are received from Headquarters that freight brake vans must be sent to another District, the number of vans forwarded must be recorded together with particulars of despatch.

29. Cancellation of freight trains also the total number and extent of late starts waiting for goods brake vans during the previous twenty-four hours must be recorded.

RELEASE OF COAL AND COKE FOR SHIPMENT

30 Except in those cases where free movement from pit to port is authorised, coal and coke for shipment must not be worked forward until released by the District Control.

31 The designated responsible person at the port, after ascertaining the requirements of the shipping agents and after taking into account the availability of vessels and the amount of coal already on hand to meet demands, should advise the District Control of the release with the following information : -

- (i) Originating colliery or works.
- (ii) Description and tonnage of coal or coke.
- (iii) Name of vessel or shipping agent.
- (iv) Date and time required at port.

32 The District Control will release the traffic from the originating points in that District and arrange for it to be worked forward to the port.

Traffic arising in a district other than the one in which the port is located will be released by the District Control advising the appropriate originating District Control.

A record must be kept of the releases and of the time each consignment is worked from the originating point and the service given.

GENERAL INSTRUCTIONS

33. Prompt attention to the telephones and accuracy and conciseness of messages to and from the Control are essential.

On telephone circuits not reserved exclusively for control purposes, messages to and from the Control must have precedence over all others excepting those between Signalmen in regard to the running of trains.

34 Adverse weather conditions such as fog or snow, likely to give rise to difficulty, and subsequent developments must be reported to the Control. Stations calling out fogmen are to advise the time they are called, on duty, and recalled.

Accidents, engine failures or other occurrences causing, or likely to cause, interruption to train or traffic working must be reported immediately to the Control who in turn must advise Headquarters Control.

In the event of accidents, adverse weather conditions or other occurrences making it necessary the Control must arrange to curtail, divert or cancel trains or reduce loads to minimise, to the greatest extent possible, the adverse effect upon the working.

Control must also inform all stations and neighbouring Districts affected so that intending passengers can be acquainted. Alternative road services must be arranged where necessary.

CONTROL PROCEDURE

35. The running and working of freight trains must be recorded on weekly cards of the approved design except for freight trains which have not a regular daily working when a one day card may be used.

In certain cases, working of a local character, usually confined to short distance working within one control section, may be recorded on daily sheets.

The movement of passenger trains, where reported, must be recorded on daily sheets of the approved design.

36. The position of freight trains should be indicated currently on the geographical boards provided.

37. In the case of inter-district trains the necessary particulars including those relating to the engine number and trainmen, must be passed promptly to the next Control.

38. Reports as to traffic stocks, line position and the general situation in each District must be given periodically as required.

The Headquarters Control will co-ordinate the working between districts giving decisions on matters affecting inter-district working and will take the initiative in overcoming difficulties which arise, including serious difficulties local to a district.

The District Control must therefore consult Headquarters Control freely and obtain their consent before making alterations to inter-district train working, such as alterations to timing, marshalling and classification, cancelling of booked trains and the running of specials.

In the case of alterations to regular inter-district freight trains or the running of special inter-district freight trains between two neighbouring districts these can be arranged between these districts and the Headquarters Control be kept advised.

It will not be necessary to advise Headquarters Control in respect of alterations, etc. to the running of local trip trains over district boundaries.

39. Control must arrange through Headquarters Control, services for live-stock and other important traffic when the inter-district booked services do not meet requirements.

40. Any stop or restriction on traffic must be arranged through Headquarters Control.

BRITISH RAILWAYS
(WESTERN OPERATING AREA)

Alterations and Additions to the General Appendix to the Rule Book

To come into operation on receipt.

Reference to the following to be made on page 2:—

COLOUR LIGHT DISTANT SIGNALS.

Where a colour light Distant signal is placed below a semaphore stop signal no light will be exhibited in the Distant signal when the semaphore arm above it is in the danger position, but a yellow or green light will be exhibited when the semaphore arm is in the clear position.

At night no green light will be exhibited by the semaphore signal when placed to the clear position.

(G.A.30 Op.—9/45 L.65733/33)

ADDITIONS TO THE STANDARD RULES—Page 14.

The following to be inserted on page 14:—

Signal Post Signs.

All concerned to please note the following new signal post signs will be exhibited where applicable —

Letter "T" in black on white diamond sign.

Letter "T" in black on white rectangular plate.

Designation of signal in black figures on white rectangular plate.

Fixed on signal to rear of which track circuit is provided and a telephone to signal box is provided in addition.

Telephone to signal box provided (no track circuit).

Provided in colour light signalling areas and for all colour light Intermediate Block Signals.

(G.A.30 Op.—9/54 L.72404/283)

Rule 35 clause (c).

The instruction shown under this heading to be amended to read:—

On the Western Region this clause will only apply to the Multiple Aspect Signalling area in the London District.

(G.A.30 Op.—9/54)

Rule 133.—The amplification of this rule to be amended to read:—

Rule 133.—Tail lamps of passenger, empty stock, perishable, parcels trains and "C" Headcode freight trains assisted in the rear uncoupled need not be removed before ascending the incline

(G.A.30 Op.—9/54 L.K.1/-)

Rule 149. exception vii.—Propelling Ballast Trains.—page 20.

The second paragraph of this instruction to be amended to read:—

The restriction in regard to propelling on falling gradients steeper than 1 in 200 may be withdrawn in the case of Engineers light inspection trains completely composed of vacuum stock coupled throughout also vacuum fitted Hopper ballast wagon trains equipped with fully vacuum fitted or piped brake vans, provided the Engineer has occupation of the line and also that there is a brake van at the end which is leading with someone riding in it who can apply the hand brake and/or the vacuum brake.

(G.A. 30 Op.—9/54 L.K.1/11729/365)

The following to be inserted as the fourth paragraph:—

Weed cutting trains which are not fully vacuum fitted throughout or on which there are less than four fully vacuum fitted vehicles attached to the engine must not be propelled on falling gradients steeper than 1 in 260.

G.A.30 Op.—9/54 L.K.1/10661/417)

Rules 189 to 208 and 217 Arrangements during Permanent Way Operations and Signal Alterations.—page 21.

The following to be inserted as the third paragraph of the instructions under heading "Between Trains' Occupations":—

"A ballast train must not be set back into a section where intermediate block signals are provided."

(G.A.30 Op.—9/54 L.K.1/11874/420)

Rules 215-217 and 234—page 21:—

"Clause (h) of Rule 234" amend to read "Clause (i) of Rule 234."

(G.A.30 Op.—9/54)

Rule 240.—Conveyance by Goods Train of Explosives and Dangerous Goods.—page 22.

The amplification of Clause (9) to be cancelled. See standard Rule 240 clause (9).

The General Note shown in supplement G.A.B to be cancelled—See standard Rule 240—last paragraph.

Clause (10). The existing amendment to be deleted and the following substituted:—

Rule 240.—Clause (10) The restriction on the number of vehicles containing explosives which may be conveyed by any one train at any one time to a maximum of five does not apply in the case of explosives conveyed on account of:—

(i) The Government (Admiralty, War Office, Air Ministry and Ministry of Supply)

(ii) A Trader for transit to a Government establishment.

(iii) A Trader for transit to a Trader on account of the Government.

The marshalling arrangements laid down in the Rule must, however, be observed

(G.A.30 Op.—9/54 L.K.1/E)

Reference to the following to be made on page 73:—

TRAINS NOT COMPLETELY WITHIN FIXED SIGNALS.

(a) In cases where—

(i) an outlet signal is provided to control movements from a siding to a running line, and

(ii) a shunting signal is provided to control set-back movements

Drivers must regard such signals as controlling the movement, although the engine may be standing on the wrong side of the signal, and must not move their engines until they are satisfied the signal is off. Where, however, the Driver cannot see the signal and the movement is accompanied by a Shunter, the latter must advise the Driver when the signal is lowered.

If for any reason the signal cannot be lowered, the Shunter or person in charge will be responsible for ascertaining from the Signaller that it is in order for the movement to be made and for satisfying himself that the points are in the proper position. In the case of a light engine unaccompanied by a Shunter, this duty must be carried out by the Fireman.

(b) On platform lines or other running lines when an engine is ahead of the signal controlling the starting of trains owing to the length of the train, a Driver must regard that signal as controlling his movement. When the Driver cannot see such signal, or back indication where provided, or the signal cannot be lowered owing to the engine occupying a track circuit or other apparatus which prevents the signal being lowered, the Driver must not proceed until he receives a green handsignal from the Signaller or verbal intimation to do so from the person acting under the instructions of the Signaller. The Driver must not be authorised to proceed until it has been ascertained that any points concerned have been correctly set.

In cases, however, where Absolute Block Working is not in operation and the signal has been lowered to enable the train to draw forward for station duties the signal must be placed at Danger in accordance with Rule 68a (i) and (iii). In such circumstances the signal must not be regarded as controlling the further movement of the train. If it is necessary for the Signaller to allow a conflicting movement to take place ahead of the standing train he must not do so until the Driver of such train has been advised of what is about to be done. After the conflicting movement has been completed and when the train is ready to continue its journey, the Driver must not proceed until he has received a verbal intimation to do so from the Signaller or the person acting under the Signaller's instructions, in addition to the Guard's "right away" signal.

(c) When the fixed signals referred to in clauses (a) and (b) lead to more than one running line, the Driver should satisfy himself by observation which line he is travelling over, but the person in charge of the movement mentioned in clause (a) or the Signaller or person acting under his instructions as mentioned in clause (b) must, whenever practicable, also inform the Driver over which line he is about to travel.

(G.A.30 Op.—9/54 476/E)

Reference to the following to be made on page 74:—

LIMITED CLEARANCE—WARNING TO STAFF.

The attention of all concerned is directed towards the need for exercising care when working at places where there is a restricted clearance between the running line or siding and adjacent structures, or between running lines and/or sidings. This applies to Footplate Staff and Guards as well as to staff working on the ground.

At certain places the limited clearance may be indicated by a Red and White chequered board bearing the words "Warning—Limited Clearance."

(G.A.30 Op.—9/54 L.K.I/10483/364)

DEFECTIVE SIGNALS AND POINTS. —Page 74.

Delete the instructions under the above heading and substitute the following:—

Defective Signals and Points.

If it is found that any signals or points do not respond to the working of the controlling lever or that it is not possible to correctly set up a route the Signaller must first replace the levers and operate them again, provided it is safe for him to do so. If this is not successful it is probable that the cause is due to some hold up in the mechanism, such as a stone in the points or other obstruction in the wire or rodding. The Signaller should then endeavour to locate the fault and, if possible, remove the obstruction.

If the Signaller is unable to leave his box for the purpose of tracing the fault he should obtain assistance from a member of the station or yard staff.

Should the Signaller be unable to trace or remove the obstruction he must send for the Lineman.

No attempt should be made by the Signaller to interfere in any way with electrical signalling apparatus.

(G.A.30 Op.—9/54 476/E)

EMERGENCY OPERATION OF ELECTRIC POINT MACHINES.—Page 75.

The following to be inserted as the fourth and fifth paragraphs:—

The crank handle must not be restored to the circuit controller if the Signaller has given permission for a train to pass over the points, until such train has cleared the points.

When the fault has been rectified, and the points set in a position corresponding to the lever in the frame, i.e. normal or reverse, the crank handle must be placed in the circuit controller and a test made to ensure that the points are working correctly.

(G.A.30 Op.—9/54 O.M.12594)

AUTOMATIC TRAIN CONTROL SYSTEM IN USE ON THE G.W.R.—page 80.

The above heading amended to read:—

AUTOMATIC TRAIN CONTROL SYSTEM IN USE ON THE WESTERN OPERATING AREA.

The second paragraph of Clause 12 to be amended to read —

The automatic train control system is in operation on the undermentioned sections of the line (double line sections except where otherwise indicated):

Paddington and Didcot (Four lines)

West Ealing and Greenford

Henley Branch.

Reading and Penzance via Westbury.

Didcot and Newbury.

Thingley Junction and Bradford Junction.

Bathampton and Westbury.

Castle Cary and Weymouth.

Newton Abbot and Paignton.

Didcot and Taunton via Bath.

Swindon and Severn Tunnel Junction via Gloucester.

Wootton Bassett and Bristol and Severn Tunnel Junction via Badminton.

Severn Tunnel Junction and Fishguard.

Skewen and Llandilo Junction. (Swansea District Line.)

Old Oak Common and Saltney Dee Junction via Birmingham.

Didcot and Aynho Junction.

Oxford and Wolverhampton via Worcester.

Worcester and Newport.

Fairford Branch. (Single Line.)

Handsworth Junction and Stourbridge Junction.

Tyseley and Gloucester.

Shrewsbury and Hereford.

(G.A.30 Op.—9/54 LKI/11041/232)

The following to be added at the end of these instructions on page 82:—

Action to be taken if bell indication received when the Distant Signal is at 'Caution.'

Should a Driver receive the bell indication "Proceed" at a ramp, but find that the Distant signal controlling the ramp is at "Caution" he must stop at the Signal Box to which the Distant Signal applies and inform the Signaller who must advise the Signaller at the Signal Box in rear in order that subsequent trains passing through the section may be stopped and the Drivers advised that the A.T.C. ramp is defective. The Signaller first advised of the failure must promptly call the Lineman and the District Operating Superintendent or District Traffic Superintendent must be notified immediately.

The Distant Signal concerned must be maintained at "Caution" until the Signaller to whom the failure was first reported is advised by the Lineman that the ramp is in order when he must notify the Signaller at the Signal Box in rear that normal working may be resumed.

An entry must be made by each Signaller in his Train Register Book shewing the time the defective A.T.C. ramp is reported. The Signaller at the Signal Box to which the Distant Signal applies must upon being advised that the failure has been rectified notify the Signaller at the Signal Box in the rear. Each Signaller must then enter the time in the Train Register Book, and the entry in the Train Register Book at the Signal Box to which the Distant Signal applies must be countersigned by the Lineman.

(G.A.30 Op.—9/54 O.M./12652).

TRACK CIRCUITS.—Pages 82-83.

The following to be added at the end of these instructions:—

Sand—use of by Enginemen.

Enginemen must, as far as practicable, avoid the use of sand when standing on or passing over track circuits, lock bars, points or crossings. If, however, it is essential that sand be used in these circumstances the minimum quantity necessary should be used.

(G.A.30 Op.—9/54.)

The "General Instructions" at the end of these regulations amended to read —

General Instructions.

1. Signallers must watch the action of track circuit indicators each time a train or engine passes on to or off that portion of the line to which each indicator applies, and if it fails to give a correct indication, immediately advise the Lineman.

If an indicator fails to show when a train or vehicle is on the track circuit the Lineman must be advised by the most expeditious means, and arrangements made for the vehicles to be examined as soon as possible and, except as shown below, they must be taken out of traffic and held until such examination has been made.

Should the vehicle or vehicles be loaded and undue delay would be caused to the contents, or in the case of an empty vehicle specially required for traffic, a wre must be sent to the destination or exchange station requesting that an examination be made there.

In every case of failure of a vehicle to operate a track circuit a detailed report must be sent to the District Operating Superintendent or District Traffic Superintendent from the Station at which the failure occurred.

An entry must be made in the Train Register Book of all irregularities in the working of track circuits.

2. In case of emergency during the time a track circuit is out of order a lock may be released by the Lineman after an understanding with the Signaller, to allow a signal to be restored to normal, or points moved to either normal or reverse, on the written authority of the Station Master, Assistant Station Master or District Inspector.

An entry must be made in the Train Register Book and signed by the Signaller and Lineman.

3. In no circumstances must a lock be released to allow a signal to be lowered or a block instrument be released to allow "LINE CLEAR" to be given when the track circuit controlling either is out of order.

4. Referring to Rule 83 if, in consequence of a derailment, permanent way operations, signaling alterations or other exceptional causes, one or more track circuits are occupied for an extended period, certain movements cannot be signalled even though they would be clear of the obstruction, the Lineman at the request, IN WRITING, of the Station Master, Assistant Station Master or District Inspector may, after satisfying himself that the obstruction is clear of the movement to be made, give the necessary release. The Signaller must make use of the lever collars or other similar devices where provided and points both facing and trailing which are locked by the track circuit concerned must be secured by clip or scotch until the movements are completed.

A separate release must be given for each Movement.

After each movement has been completed the Lineman must restore the full track circuit controls. The time and details of the release and the restoration of the controls must be entered in the Train Register Book on each occasion and signed by the Signaller and Lineman and also the Station Master, Assistant Station Master or District Inspector as the case may be.

5. Enginemen must not throw cinders on track circuited portions of the lines.

6. An Engineer's trolley motor trolley or vehicle must not be relied upon to operate track circuits and protection must be provided in accordance with the Rules or Instructions applicable.

7. Before a trolley is placed upon, or run over, any portion of a line that is track circuited, the permission of the Signaller in whose box the track circuit is indicated must be obtained.

(G.A.30 Op.—9/54 LK1/-)

Reference to the following to be made on page 83:—

Facing Points controlled by Track Circuits.—Emergency Release.

When track circuits are provided in lieu of Facing Point Lock Bars, in certain cases emergency releases will be provided and the following instructions must be carried out in the event of a failure of the track circuit:—

1. When the track circuit fails and the facing point lock lever cannot be restored to normal, the Emergency Release provided in the signal box must be used and the following instructions must be carried out by the Signaller in the order shown:—

(a) Satisfy himself by personal observation, or by assurance from a responsible person, that no track circuit controlling the Facing Point Lock lever is fouled by any vehicle and that it is in order for the facing points to be reversed or restored to normal as the case may be.

(b) Break the glass on the front of the box containing the release plunger.

(c) Operate plunger and restore F.P.L. lever to normal, thus releasing the points.

(d) The facing points can then be operated normally and when the F.P.L. lever is reversed the appropriate signal, if not locked by the track circuit which has failed, can be lowered, provided the detection is intact.

(e) An entry must be made in the Train Register Book with the time whenever the glass is broken. The entry must be signed by the Signaller, who must promptly report the circumstances to the Station Master and Lineman.

(f) While the track circuit lock is out of use it will be necessary to press the emergency plunger on each occasion when the F.P.L. lever is to be restored to normal.

(g) While the track circuit remains out of use a Handsignaller or competent man must be appointed as required by Rule 77(e).

(h) When the track circuit is out of use the Signaller must, before restoring the F.P.L. lever to normal, satisfy himself by personal observation, or by assurance from the Handsignaller or competent man, that any train or engine signalled to pass over the facing points has passed clear of same.

2. The Station Master must specially report to the District Operating Superintendent or District Traffic Superintendent every occasion on which the glass front has been broken and must see that it is renewed, that normal working is resumed and that the paper label fixed to the new glass bears the Lineman's signature and the date replaced. An entry must be made in the Train Register Book showing the time the track circuit is again in order and normal working resumed, and this must be signed by the Lineman.

3. The Emergency Release Plunger is for use in connection with the failure of the track circuit only.

(G.A.30 Op.—9/54 L66326/145)

"THREE SHOT" DETONATOR MACHINE WORKED IN CONJUNCTION WITH TRAILING POINTS.—Page 86.

The third paragraph of the above instructions to be deleted and the following substituted —

The Station Master must see that three fresh detonators are placed in the machine every month and a note to the effect that the detonators have been changed, must be made in the Train Register Book. Where the change is not made by the Signaller the person appointed to change the detonators must countersign the entry. The old detonators taken out of the machine in this way must be returned by the Station Master to the Stores Department on the 1st June and 1st December each year with the name of the sending station.

(G.A.30 Op.—9/54 LK1/10730/363E)

SLIP CARRIAGE WORKING.—Page 91.

The following additional paragraphs to be added to Clause 4.

(d) The rear vehicle of the main train to which the slip coach is to be attached and from which it is to be slipped must always be a Western Region or B.R. Standard stock type vehicle. Other Region's vehicles are prohibited from working as the rear vehicle of the Main Train and if it is necessary for such vehicle to be conveyed on the train it must be marshalled inside the rear Western Region or B.R. standard vehicle of the Main Train or arrangements must be made for the train to stop at the Slipping Station.

- (e) When a B.R. Standard coach is the last vehicle on the Main Train, a special gangway door, special steam pipe safety chain bracket, and coupling safety clip for use on the buckeye draw hook, must be used for the attachment of the slip portion.

(G.A.30 Op.—9/54 LK1/8247/2/Gen.)

REGULATIONS FOR WORKING THE VACUUM BRAKE.—Page 98.

Clause 1 (c) of these instructions to be amended to read —

The vacuum train pipes at the ends of "fitted" vehicles are painted red, the vacuum train pipes at the ends of "piped" vehicles are painted white.

(G.A.30 Op.—9/54 RI/5662)

INSTRUCTIONS TO GUARDS AND TICKET COLLECTORS ON CORRIDOR TRAINS.—Page 115.

The existing paragraph 1 of Clause 1 to be deleted and the following substituted —

Corridor and gangway doors should be left unlocked so as to provide free access through the train, except as shown below:—

Locking of
corridor and
gangway doors

- (1) Gangway doors at the extreme ends of the trains. Care must be taken to ensure that when vehicles are detached from a train en route that the gangway doors at the point of detachment are locked.
- (2) Brake Vans or Luggage Vans at the extreme ends of the train. Where, however, a guard is riding in the van or it is empty, the doors should be unlocked. Should the guard have occasion to leave his van whilst the train is in motion he must lock the door.
- (3) Brake Vans or Luggage Vans intermediate in the train. Where, however, there is a restaurant or buffet car on the train or a guard is riding in the van or the latter is empty, the doors should be unlocked.
- (4) Where the gangway connections cannot be made.
- (5) The gangway doors at both ends of sleeping car accommodation on trains. Where, however, it is necessary to admit passengers to their berths or to give access to a restaurant car during the time the restaurant service operates, the doors should be unlocked.

In laying down the marshalling of trains, arrangements should be made, if possible, to avoid a passenger carrying vehicle being isolated from the remainder of the train by being marshalled between the brakevan in which a guard is not riding and the sleeping car accommodation. In cases where, in the interests of the working, this is not desirable the door leading to the sleeping car must be left unlocked.

The Guard will be responsible for carrying out these instructions but Travelling Ticket Staff, where provided, should assist. In the case of Sleeping Cars, the Sleeping Car Attendant will be responsible.

The first paragraph of Clause 7 amended to read:—

In the event of it becoming necessary to obtain access to the locked lavatory in a coach of the former G.W.R. design in the case of an emergency, this can be done by removing the indicator plate, which is secured by four screws, when the end of the spindle becomes exposed. The spindle will take the standard gas key and, by using the same, the door can be readily opened.

The following to be added as the third paragraph of Clause 7:—

When it is necessary to release a lavatory door lock (with indicator set at "Engaged" belonging to a British Standard Carriage, the three screws which secure the indicator plate should be withdrawn, the plate removed, and the indicator driving pin will then become exposed. Upon sliding this pin towards the lock handle it will be possible to gain access by turning the lock handle.

(G.A.30 Op.—9/54 LK1 6624/Gen. E.)

DAMAGE TO CARRIAGE WINDOWS, ETC.—Page 117.

The following to be substituted for the list of amounts that must be collected from persons responsible for the breakage of windows or carriage fittings, if possible at the time of the incident, otherwise the identity and address of the person(s) should be established and reported.

Old Type Coaches		s.	d.		s.	d.
Top side light		6	0	Large light in corridor of saloon coaches under 34 inches wide...	35	0
Ordinary door light		16	0	Large light in corridor of saloon coaches, 34 inches wide and not exceeding 50 inches wide ...	52	6
Corridor door light		14	0	Large light in corridor of saloon coaches over 50 inches wide	56	6
Ordinary quarter light		22	0	Ventilators, large in Auto cars	86	0
Corridor quarter light		17	0	Ventilators, small in Auto cars	52	6
Lavatory drop light ...		8	6			
Lavatory fixed light		10	6			
Quarter light "Smoking"		22	0			
Door light frame including the lights		69	6			

New Type Coaches							
	s.	d.			s.	d.	
Door light frame (with glass) ...	71	6		Window under ventilator, compartment side ...	52	6	
Fixed window, corridor side				Window side of ventilator, compartment side ...	10	0	
First Class ...	72	6		Fixed window, corridor partition, First and Third Class ...	16	6	
Fixed window, corridor side, Third Class ...	62	6		Door window, sliding door, corridor partition ...	19	6	
Drop window, corridor side, First and Third Class ...	17	6		Window, sliding ventilator, compartment side ...	10	0	
Door drop window ...	19	0					
Blinds, First Class							
Door 19½ inches ...	12	0		Rack netting, First Class ...	28	0	
Quarter light ...	9	0		Rack netting, Third Class ...	27	0	
Sliding door ...	10	6		Wash basin, old type ...	75	0	
				Wash basin, N.P. without pedestal ...	78	6	
Blinds, Third Class				Pedestal N.P. without wash basin	72	0	
Door 19½ inches ...	12	0		Pedestal and wash basin N.P. complete ...	144	0	
Quarter light ...	10	6		Cushions, Third Class corridor	138	0	
Sliding door ...	10	0		Cushions, Third Class, non corr.	220	0	
Carriage Fittings				Cushions, First Class corridor 6 per compartment ...	74	6	
Curtains, Third Class ...	26	6		Cushions, First Class corridor 4 per compartment ...	128	0	
Curtains, First Class ...	25	6					
Electric light bulbs ...	2	0					
Window straps, First Class ...	12	6					
Window straps, Third Class ...	5	6					
Mirrors, Lavatory O.P. (inc. frame)	33	0					
Mirrors, Lavatory O.P. (Glass only)	8	0					
Mirrors, N.P. ...	17	6					
Mirrors, Compartment ...	19	0					

The same amount should also be charged in the event of breakage of windows and carriage fittings in other Region's stock working on the Western Region. (G.A. 30 Op.—9/54 C.S.O.—C.B.D.)

STEAM HEATING OF PASSENGER TRAINS—Page 117.

Clause 1 of the existing instructions to be cancelled and the following substituted:—

1 The following dates are those which normally must be followed in the application and discontinuance of steam heat for passenger trains:

Commencement.

(i) All steam heater pipes to be fitted and coupled for use by 25th August.

(ii) Heat to be applied:

Sleeping Car trains

Other Express trains while running after 5.0 p.m. and before 10.0 a.m.

1st September

All other passenger trains

1st October

Discontinuance.

(i) Heat to be discontinued.

All trains except as shown below

Sleeping Car trains

Other express trains while running after 5.0 p.m. and before 10.0 a.m.

1st May

15th June

(ii) Pipes to be removed as soon as possible after

15th June

The foregoing dates for the application and discontinuance of steam heating are to be regarded as a general guide but guards and others concerned must use their discretion in the event of unusual climatic conditions. (G.A. 30 Op.—9/54 T33320 G/2)

Reference to the following to be made on page 121:—

FIRES IN PASSENGER TRAINS.

If any member of the staff, whether in the employ of the British Transport Commission or the British Transport Hotels and Catering Services, becomes aware of a fire on the train on which he is travelling he should take appropriate steps to extinguish it. If, however, he is unable to do so promptly he must arrange for the train to be stopped immediately so that suitable action can be taken.

After the train has been stopped the services of a Carriage and Wagon Examiner should be obtained, if possible. Attention is also drawn to the fact that if the fire occurred in a roof with a ceiling, it may be necessary to remove part of the ceiling to ensure that the fire is properly extinguished. When the train proceeds on its journey a member of the staff should, if possible, travel in the affected compartment or carriage for the purpose of observation and he should be prepared to deal with any subsequent outbreak of fire. Should a member of the staff not be available to ride in the compartment or carriage, the Guard should examine the affected compartment or carriage as frequently as possible.

Passengers should not be permitted to travel in the vehicle concerned until it has been passed fit for service by a Carriage and Wagon Examiner. (G.A. 30 Op.—9/54 LK1/11343/365E)

EMERGENCY APPLIANCES, &c., IN PASSENGER TRAINS.—Page 121.

The instructions under this heading to be cancelled and the following substituted:—

EMERGENCY APPLIANCES, &c., IN PASSENGER & PARCELS TRAINS.

Emergency appliances and first aid requisites are provided in vehicles on passenger and parcels trains for use in the event of fire or accident in accordance with the following:—

Description	Where provided
HAND FIRE EXTINGUISHERS	<p>Sleeping Cars Two water CO₂ pressure extinguishers, one at each end of corridor, or alternatively the two extinguishers in a recess at one end of the corridor.</p> <p>Restaurant Cars</p> <p>Twin Units ... One foam and one CO₂ gas extinguisher in kitchen. One water/CO₂ pressure extinguisher in corridor at end of kitchen and one water/CO₂ pressure extinguisher in corridor at end of dining car.</p> <p>Single Units ... One foam and one CO₂ gas extinguisher in kitchen, and one water/CO₂ pressure extinguisher in corridor at end of car.</p> <p>Corridor Brake Vans Standard Stock—One water/CO₂ pressure extinguisher and two buckets in brake compartment. One water/CO₂ pressure extinguisher in corridor. Non-Standard Stock—One water/CO₂ pressure extinguisher in brake compartment.</p> <p>Other Corridor... Vehicles One water/CO₂ pressure extinguisher or W.R. No. 4 "Hand Cup" extinguisher at end of corridor.</p> <p>Non-Corridor ... Brake Vans Standard Stock—One water/CO₂ pressure extinguisher and two buckets in each brake compartment. Non-Standard Stock—One water/CO₂ pressure extinguisher in each brake compartment.</p> <p>Diesel Cars ... One water/CO₂ pressure extinguisher and one W.R. No. 5 "Vessel and Syringe" C.T.C. extinguisher.</p> <p>Post Office ... Vans Two W.R. No. 4 "Hand Cup" extinguishers and two buckets of sand.</p>
SET OF TOOLS &c.	One case in the guard's compartment of each passenger brake vehicle.
FIRST AID REQUISITES	One cabinet in the guard's compartment of each passenger brake vehicle.

Note—The fire extinguishers and tools must only be used in the case of fire, or in the event of a train accident.

Special Instructions to Station Masters, Inspectors and others.

1. In the event of an accident to a train conveying passengers, it is of the first importance, after protection as required by the Rules, that immediate steps be taken to relieve any who may have sustained injury, and to adopt all precautionary measures which the circumstances may render desirable in the direction of limiting the effects of the occurrence. The following further important points which are supplementary to the instructions of the Rule Book must be borne in mind:—

(a) Examine train to see if there is any sign of fire from live engine coal or broken gas cylinders or connections, and if so take prompt steps to extinguish it by means of the fire extinguishers provided on the train, or any other available means.

Immediately call for assistance from the nearest Fire Brigade, who should, if possible, be informed the exact situation of the train and the means of access. The correct method for calling the brigade by telephone is included in the Fire Notice exhibited at every station, yard and signal box.

(b) Ascertain where the greatest damage has occurred and take instant steps to release any passengers who may be entangled in the wreckage.

Hand out tools and other appliances provided in the brake compartments to the most experienced men available.

(c) Ensure prompt telegraph or telephone communication with the local police officer and with the nearest likely places for doctors, nurses, ambulances, refreshments, etc., according to circumstances.

Obtain the assistance of all available first aid men and make the best possible use of the first aid cabinets provided in the brake compartments.

If a serious train accident occurs in the vicinity of a station, the station master or person in charge must despatch immediately as many staff qualified to render first aid as possible, with equipment.

(d) Enlist the services of any uninjured and willing passengers or other persons to convey messages, and to assist generally, under the direction of the railway staff.

(e) Use cushions taken from the coaches in such a manner as to form comfortable couches for any injured persons until such persons can be removed.

(f) Advise the nearest station master and other officials, also the permanent way men, at the earliest possible moment.

(g) Should gas be escaping from a broken pipe, bend the pipe away from timber or inflammable wreckage clear of any source of ignition and, if practicable, in the direction to which the wind is blowing. If possible, the pipe should be hammered flat to prevent the escape of gas.

Should the gas be ignited at a broken pipe end and it is not possible to flatten it, remove all inflammable material from the vicinity of the flame. If the end of the pipe is not accessible the adjacent woodwork should be wetted. Provided persons are not trapped in the wreckage, the flame may be extinguished by directing a stream of liquid from a water CO₂ pressure extinguisher to the end of the pipe in the SAME DIRECTION as the issuing flame. Naked lights should not be allowed in the vicinity until the gas has been dispersed.

2. Fire Extinguishers

The W.R. No. 1 pressure extinguisher is fitted with a quick-acting press valve, which is controlled by thumb pressure and closes automatically to reserve the liquid.

The following proprietary extinguishers are provided in passenger vehicles for use as shown:—

Name	Type	Use
Conquest 55	Water/CO ₂ pressure	All fires except electrical, petrol and oil
Firesnow	do.	do.
Waterloo	do.	do.
Phomene	Foam	Petrol, oil and fat fires.
Kidde (Lux)	CO ₂ gas	Electrical, petrol, oil and fat fires
Pyrene CO ₂	do.	do.

Instructional Booklet.

In addition to the instructions shown on each appliance, booklet B.R. 7006—"Portable Fire Extinguishers on Trains" illustrates the various types of extinguishers and their methods of operation.

All train staff must be in possession of this booklet and be fully acquainted with its contents.

Seals on Extinguishers.

Lead seals marked "B.R.(W)" and "S.F.B." are provided on W.R. No. 1 water, CO₂ pressure, W.R. No. 4 "Hand Cup," C.T.C. and CO₂ gas extinguishers.

Arrangements for Instructing Train Staff

Guards, ticket collectors, sleeping car attendants, restaurant car attendants and travelling carriage cleaners will be instructed in the use of each type of extinguisher, as may be appropriate according to the types they are likely to encounter. Where possible they will have actual experience in handling the appliances.

The periodical examination of trainmen in the rules and regulations must include the use of fire appliances.

Train staff must not on any account test the appliances by operating them and drawing off small quantities of liquid. When seals are provided they must not be broken unless the extinguishers are required for a fire or when permission for doing so is given by an authorised instructor.

3. Tool Cases.

Description.

The tool cases are painted red with the words "FOR EMERGENCY USE ONLY" in gilt lettering, black edged, on the front of the cases and a rectangular piece of reinforced glass is inset in the lid to enable the contents to be seen. The lid is taped and sealed at the top and bottom on one side, and in the event of an accident it is only necessary to cut or break the tape in order to open the lid and obtain the tools and appliances.

Contents of Cases.

Each tool case in Western Region stock contains the following articles:—

- 1 Saw, hand, 26 inches, rough tooth.
- 1 Saw, hand, 22 inches, rough tooth.
- 1 Felling Axe.
- 1 Axe, hand, small.
- 2 Crowbars, 3ft. 5in. and 2ft. 5in. long respectively.
- 1 Extension Tube.
- 1 Chain, 12ft. 0in. long, with hook at one end and ring at the other.
- 1 Spade.
- 1 Set of Splints (4 to set).
- 1 Metal Sleeve (for extension of splints)

Tool cases in B.R. standard stock contain the following articles:—

- 1 Saw, hand, large.
- 1 Saw, hand, small.
- 1 Felling Axe.
- 1 Axe, hand, small.
- 1 Crowbar, 3ft. 0in. long.
- 1 Crowbar, 4ft. 6in. long (in clips on left-hand side of tool case).
- 1 Crowbar Extension Tube.
- 1 Sledge Hammer.
- 2 Steel Wedges.
- 1 Coil of Rope, approx. 13 yards long and 1in. diameter.
- 1 Coil of Rope, approx. 12 yards long and ½in. diameter.
- 1 Inspection Lamp, electric, fitted with 200ft. flexible lead and plug.
- 2 Lamps, hand, paraffin.

4. Examination and Maintenance of Fire Extinguishers and Tool Cases.

Appliances to be in proper places with Seals intact.

The Chargeman Carriage & Wagon Examiner or other authorised member of the Carriage & Wagon Engineer's staff at the various depots will be held responsible for seeing the fire extinguishers and tool cases are in their proper places and that seals (where provided) are intact.

Replacement Extinguishers and replacements for Tool Cases.

Should an extinguisher be damaged or missing or be found with a broken or missing lead seal when it should have one, such extinguisher must at once be replaced. A supply of spare extinguishers and tool case replenishments will be kept on hand for renewal purposes at the following depots:—

Fire Extinguishers.

Aberdare H.L.	Oxford
Aberystwyth	Paddington
Barry	Penzance
Birmingham, S.H.	Plymouth
Bridgend	Pontypool Road
Bristol (Dr. Days)	Pontypridd (To draw on Cardiff Q. St.)
Caerphilly (To draw on Cardiff Queen St.)	Pwllheli
Cardiff General	Reading
Cardiff Queen Street	Ruabon
Carmarthen	St. Blazey
Cheltenham St. James	St. Erth
Exeter	Severn Tunnel Junction
Goodwick	Slough
Helston	Southall
Hereford	Stratford-upon-Avon
Leamington	Swansea High Street
Liskeard	Swindon
Marylebone	Taunton
Neasden Shops	Trowbridge
Neath	Truro
Newport (Fbbw) Carriage Shed	Tyseley
Newquay	Westbury
Neyland	West London
Old Oak Common	Wolverhampton Cannock Road
Oswestry	Wolverhampton L.I.
Oswestry Shops	Worcester
	Wrexham

Tool Cases.

Aberystwyth
Barry
Bridgend
Bristol
Cardiff General
Carmarthen
Cheltenham St. James
Fishguard & Goodwick
Hereford
Marylebone
Neasden Shops
Neath
Newport (Ebbw) Carriage Shed

Neyland
Old Oak Common
Oswestry
Penzance
Plymouth
Pwllheli
Swansea High Street
Taunton
Tyseley
West London
Wolverhampton Cannock Road
Wolverhampton L.L.
Worcester

Duties
of Guards,
Sleeping Car
Attendants
and
Restaurant Car
Attendants

Guards in charge of trains must satisfy themselves that extinguishers are installed in accordance with the introductory paragraph to these instructions and that the seals on extinguishers (where provided) and tool cases are intact. In the case of restaurant and sleeping cars the attendants will be responsible for examining the extinguishers and reporting defects, etc., to the guard.

Duties of
Carriage
Cleaning Staff

Carriage cleaning staff observing deficiencies, damage or broken seals when carrying out interior cleaning must advise the Carriage & Wagon Engineer's staff, so that replacements can be installed with a minimum of delay.

Fire
Appliances
damaged or
missing—
Guards to
report

The guard will be responsible for reporting to the Carriage & Wagon Examiner on duty at the station where the train terminates its journey any cases of extinguishers being damaged, missing or having broken seals. Where restaurant or sleeping cars are formed in the train the guard must confer with the attendants.

In cases where the guard does not proceed with the train to its destination he must report any defects, etc., to the guard who relieves him, and the latter will be responsible for advising the Carriage & Wagon Examiner at the terminating station.

Any such report made by the guard must be recorded on his journal and the District Operating or District Traffic Superintendent in whose office the journal is filed must immediately pass an extract of the report to the District Outdoor Carriage & Wagon Engineer. The latter must then report any special matters or defects in connection with fire appliances to the Carriage & Wagon Engineer, Swindon.

Replacing
Extinguishers

If an extinguisher is damaged or the seal provided is broken or missing it must at once be exchanged and the defective appliance returned in one of the crates provided to the Fire Station, M & E Engineer's Department, Swindon, the Chargeman, Carriage & Wagon Examiner or other authorised member of the Carriage & Wagon Engineer's staff at the depots mentioned being responsible for this duty.

5. First Aid Cabinets.

Contents
of Cabinets.

The First Aid Cabinet is marked "BR(W) FIRST AID No. 3" and contains the following requisites:—

Sterilised Dressing (small)	2
" " (medium)	2
" " (large)	2
Mines Dressing (large)	6
" (Medium)	3
Sterilised Cotton Wool, ½oz. packet	3
Antiseptic No. 5—2oz. bottle	1
Sal Volatile—2oz. bottle with screw cap (also to be used as smelling salts)	1
Triangular bandages (individually wrapped)	9
Roller bandages 2ins. x 2 yards	6
Safety pins—set of 6	1
Splints, with junctions—set	1
Lint—white, ½oz. packet	3
Eye drop No. 1A (castor oil)—½oz. bottle	1
Card of Instructions	1
Report Form	1

Use of
Cabinet.

The First Aid Cabinet is primarily intended for use in "Train Accidents."

How sealed.

The lid of the cabinet is sealed but the seal may be broken in event of emergency as indicated above, when the contents will be obtainable.

6. Examination and Maintenance of Train First Aid Cabinet.

Responsibility.

Station and Yard Masters will be held responsible for ensuring that the examination, replenishment and sealing of First Aid Cabinets is carried out by the Carriage Cleaning Staff.

Cabinets requiring attention.

Should a First Aid Cabinet be damaged or missing, or found with a broken or missing seal, it must be replaced or replenished without delay and a supply of spare First Aid requisites will be maintained for this purpose at certain Stations and Depots. When a First Aid Cabinet has been replenished and sealed a small coloured adhesive label must be affixed to the lid to indicate when the contents were last inspected. The labels will be forwarded annually, without requisition, to District Officers by the Stationery Department and will be used in the following sequence:—

Colour	For use during 3 months commencing
Pale Blue	January 1st
Brick	April 1st
Apple Green	July 1st
White	October 1st

7. Train First Aid Cabinets—Replenishment Cupboards.

Stations at which Replenishment Cupboards are provided.

Operating District	Station or Depot	Grade	Operating District	Station or Depot	Grade
LONDON	Didcot ...	C	CARDIFF	Barry ...	C
	Henley-on-Thames ...	C		Bridgend ...	B
	Old Oak Common ...	A		Caerphilly ...	C
	Oxford ...	C		Cardiff (Bute Road)	C
	Paddington ...	B		Cardiff (Carriage Sdgs)	A
	Reading ...	B		Cardiff (General)	C
	Slough ...	C		Cardiff (Queen St.)	C
	Southall ...	C		Dowlais (Caeharris)	C
	West London ...	A		Llantrisant ...	C
BRISTOL	Bristol (Dr. Days) ...	A		Maerdy ...	C
	Chippenham ...	C		Penarth Town ...	C
	Frome ...	C		Pontypridd ...	C
	Swindon ...	B		Porthcawl ...	C
	Trowbridge ...	A		Rhymney ...	C
	Westbury ...	C		Tondu ...	C
	Weston-Super-Mare	B		Treherbert ...	C
	Weymouth ...	B	SWANSEA	Aberavon Town	C
EXETER	Yeovil (Pen Mill) ...	C		Carmarthen ...	B
	Exeter (St. Davids)	C		Fishguard Harbour	C
	Kingswear ...	B		Llanelli ...	C
	Paignton ...	C		Neath (Carriage Sdgs)	B
	Newton Abbot ...	B		Neath (General)	B
PLYMOUTH	Taunton ...	B		Neyland ...	C
	Bodmin ...	C		Pembroke Dock	C
	Helston ...	C		Swansea (High St.)	A
	Kingsbridge ...	C		Swansea (Victoria)	B
	Laira ...	C	WORCESTER	Whitland ...	C
	Liskeard ...	C		Evesham ...	C
	Newquay ...	C		Kidderminster ...	C
	Penzance ...	B		Worcester (Shrub Hill)	B
	Plymouth (Millbay)	A	BIRMINGHAM	Banbury ...	C
	St. Blazey ...	C		Birmingham (Snow Hill)	B
GLOUCESTER	St. Ives ...	C		Leamington Spa ...	C
	Truro ...	B		Stourbridge Jct.	A
	Cheltenham (St. James)	C		Stratford-upon-Avon	C
	Gloucester (Central)	C		Tyseley (Carriage Sdgs)	A
	Lydney ...	C		Wolverhampton	B
	Ross-on-Wye ...	C		(Cannock Road)	B
				Wolverhampton (L.L.)	A

Operating District	Station or Depot	Grade	Operating District	Station or Depot	Grade
NEWPORT	Aberbeeg ...	C	CHESTER	Hereford ...	B
	Aberdare (High Level)	C		Ruabon ...	C
	Abergavenny Junction	C		Shrewsbury ...	B
	Ebbw Jct. (Carriage Shed)	A		Wellington (Salop)	C
	Merthyr ...	C		Wrexham ...	C
	Newport (High Street)	C	CENTRAL WALES	Aberystwyth ...	C
	Pontypool Road ...	C		Brecon ...	C
	Severn Tunnel Junction	C		Oswestry ...	C
	Tredegar ...	C		Pwllhell ...	C

Replenishment Cupboards—Details of Stock to be held.

Details of the stock to be held should be pasted on the inner side of the replenishment cupboard door and the stock must be maintained as near as possible to the quantities stated.

Excessive stocks must not be held.

Spare empty cabinets when required should be obtained from the nearest Carriage Repair Depot.

Replenishments to be ordered on form "BR.9016."

Standard Item Number	Description of Material	Stock Unit	Stock permitted at Stations		
			Grade of Station		
			'A'	'B'	'C'
102-204	Bandages Roller 2in. x 2 yards ...	doz.	4	2	1
102-207	Bandages Triangular—Wrapped ...	each	72	36	18
102-211	Blocks Wood ...	"	1	1	1
	Dressings Sterilised				
102-222	Compressed, Mines type, large ...	"	48	24	12
102-223	Compressed, Mines type, medium ...	"	24	12	6
102-224	Ordinary, Finger ...	"	16	8	4
102-225	Ordinary, Large, wound ...	"	16	8	4
102-226	Ordinary, Medium, wound ...	"	16	8	4
102-228	Drops Eye, No. 1A (castor oil) ...	"	8	4	2
102-230	Forms 5042 (Guard's Report) ...	"	24	12	6
102-237	Lint, White ½oz. packets ...	doz.	2	1	½
102-238	Lotion, Antiseptic, 2oz. bottles ...	each	8	4	2
102-245	Pins, safety (sets of six) ...	set	8	4	2
102-250	" " " 2oz. bottles ...	each	18	4	2
102-253	Seals—Lead ½in. ...	each	150	100	50
102-255	Splints—with joints ...	set	3	2	1
102-259	Tape, Sealing ...	doz. yds.	4	2	2
102-262	Wool, Absorbent ...	each	24	12	6
	Cards of Instructions ...	"	8	4	2

Duties of Guards.

8 Guards in charge of trains must satisfy themselves that the seals of the First Aid Cabinets are intact and if any of the equipment is damaged or missing, or the seals broken or missing, must report the matter to the person in charge at the station at which the train terminates its journey, who must advise the Carriage Cleaning Staff that a First Aid Cabinet requires attention.

In cases where a Guard does not proceed with the train to its destination he should report any defects to the Guard who relieves him and the latter will be responsible for advising the person in charge as stated above.

Entry on Guard's Journals

Any such report must be recorded on the Guard's journal and the District Operating Superintendent or District Traffic Superintendent in whose office the journal is filed must ensure that the necessary replenishment and re-sealing is carried out.

Record of
use

In the event of a First Aid Cabinet having been used the Guard must make an appropriate entry on the form provided and leave the form inside the Cabinet for the information of the Carriage Cleaning Staff at the terminating station or depot.

9. Pilferage from First Aid Cabinets.

Numerous losses from First Aid Cabinets in trains have been brought to notice and there is reason to believe that these occur during the night or whilst the vans are standing in isolated sidings.

Guards when taking charge of trains should satisfy themselves that the Cabinets are in order and call attention of the Station or Yard Master to any irregularity, which should also be reported on the journal.

Station and Yard Masters should report all irregularities to the District Operating Superintendent or District Traffic Superintendent and, if necessary, the B.T.C. Police.

The co-operation of the staff is desired to prevent pilferage from First Aid Cabinets.

(G.A.30 Op.—9/54 A1/7 1163/102 S2/AMB/7)

GENERAL INSTRUCTIONS TO BE OBSERVED IN CONNECTION WITH AUTO-CAR SERVICES.—Pages 128-131.

The following to be substituted for the table shown in paragraph 19 on page 130:—

Gradient	Auto Engines	
	2-4-0	2-6-2
	$\frac{0-4-2}{T}$	$\frac{0-6-0}{T}$
	Tons	Tons
1 in 40	72	90
1 in 50	96	120
1 in 60	120	150
1 in 80	144	180
1 in 100	168	210

(G.A.30 Op.—9/54 T.D./P.R.P.)

WORKING OF DIESEL CARS.—Page 132.

The following additional paragraph to be inserted after the instructions headed "Use of emergency coupling—Cars 1 to 17 inclusive":—

Tail Traffic.

Tail traffic may only be hauled by Diesel Cars numbered 18 upwards.

The maximum tail tonnage in all circumstances must not exceed 60 tons in the case of the Twin Diesel Units formed with an intermediate coach, the maximum tail tonnage must not exceed 30 tons.

The tail load must be restricted to 30 tons where there is a rising gradient of 1 in 60 or steeper, except where the diesel car is used for Engineering Department tunnel inspections when the restriction applies on rising gradients of 1 in 40 or steeper.

When steam heating is in use not more than one passenger carrying vehicle may be attached to the Diesel Car or Twin Diesel Units.

(G.A.30 Op.—9/54 LK1/12009/372 T.D.288)

The following to be inserted at the end of the clause headed "Instructions to Guards" —

All passenger Diesel Cars, with the exception of Diesel Car No. 1, may carry additional passengers up to a number not exceeding half the normal seating capacity.

Exceptions :

- The normal seating of Diesel Car No. 1 (69 passengers) must not be exceeded
- The loading of Diesel cars passing through the Severn Tunnel must not exceed the seating capacity.
- The maximum permissible loads in Luggage compartments of Diesel Cars must not exceed the maximum indicated on the Notice exhibited in the Car.

(G.A.30 Op.—9/54 T.39,119 G/1)

The following to be added after the word "run" in line two of the last paragraph of these instructions (See G.A.25):—

" or if a Diesel Car is required to work in a service which is normally scheduled to be worked by a train or auto-car on a route over which Diesel Cars are authorised to be worked "

(G.A.30 Op.—9/54 LK1/8847/372)

DIAGRAM SHOWING CLASSIFICATION, HEAD LAMPS AND CORRESPONDING BELL SIGNALS.—Page 139.

Insert § against D headcode trains and the following at the foot of page 140.

§ The proportion of vehicles on which the automatic brake must be operative to be based on the equivalent load, Class 3 traffic.

(G.A.30 Op.—9/54 E.84669 H(2-C)).

Engine Head Codes.

The following to be added to the "description of Trains" carrying "E" Head Codes:—

Beats on Bell	How to be given
5	1 pause 2 pause 2

Weed killing trains when both running and spraying

(G.A.30 Op.—9/54 LK1/10661/417E).

TIMING OF EMPTY COACHING STOCK TRAINS.—Page 141.

The following to be added at the end of the 2nd paragraph:—

"on sections of the line where the ruling gradient is less than 1 in 100 rising."

(G.A.30 Op.—9/54 T.38964 G/7).

Reference to the following to be made on page 141:—

WORKING OF GAS TURBINE ENGINES.

When a gas turbine engine is required to work over a section of line where it is not normally scheduled to run, prior advice must be issued to all concerned, including the staff of other Departments, e.g., Permanent Way men, of the intention to make such movement.

When the scheduled working of a gas turbine engine has been suspended for a short period, i.e., not exceeding seven days, in connection with repairs, etc., it will not be necessary for a special advice of resumption of normal working to be issued to all concerned.

When the scheduled working of a gas turbine engine has been suspended for a period in excess of seven days a notice to all concerned must be issued before normal working is resumed.

If it is absolutely necessary for a gas turbine engine to work over a section of line where it is not normally scheduled to run, or if a gas turbine engine is required to work in a service which is normally scheduled to be worked by a steam locomotive on a route over which the gas turbine engines are authorised to be worked and a printed or stencilled notice cannot be issued in sufficient time to ensure that 48 hours' notice is given to all concerned, the Drivers of such engine must be notified of the circumstances and must then sound the siren when entering and emerging from tunnels, also when approaching curves, level crossings, barrow crossings, overbridges, gangers' huts and other buildings adjacent to the line upon which the gas turbine engine is run.

(G.A.30 Op.—9/54 LK1/8847/372)

ENGINE WHISTLES.—Page 141.

The table and instructions under this heading to be deleted and the following substituted:—

STANDARD CODE OF ENGINE WHISTLES

The following code of engine whistles applies at all stations, junctions and sidings, not otherwise specially provided for in the local Code of Engine Whistles shown in the table following the standard codes.

In order to avoid annoyance to passengers at stations and residents in the neighbourhood of the Railway Drivers are requested not to make more frequent use of the engine whistles than is absolutely necessary to ensure safe and efficient working in compliance with the Rules and Regulations.

Note: The term "Slow line" includes Relief line.

Description	Whistles
*Main or Fast lines	1 long
*Line next to Main line (Slow or Goods)	2 long
*Line next to Slow or Goods ...	3 long

(One additional long whistle to be given for each additional line farther away from the Main line.)

*These codes to be given when approaching signals at Danger or when necessary to indicate when ready to proceed on same line.

Approaching Geographical Junctions and requiring to proceed through Junction.

†On Main line and requiring to proceed to left ...	1 long 1 short
†On Main line and requiring to proceed to right ...	1 long 2 short
†On Slow or Goods line and requiring to proceed to left ...	2 long 1 short
†On Slow or Goods line and requiring to proceed to right ...	2 long 2 short
†The appropriate route code whistle to be given at Signal Boxes enumerated in the local Appendices.	
To or from Goods line or Slow line or Loop and Main line	5 short
To cross from Main to Main ...	4 short
To or from Bay or Platform lines ...	1 crow 1 long
Down Main or Fast, Slow or Goods or Loop to Down Sidings	1 crow
Down Main or Fast, Slow or Goods or Loop to Up Sidings	2 short pause 3 short
Up Main or Fast, Slow or Goods or Loop to Up Sidings	3 short pause 1 short
Up Main or Fast, Slow or Goods or Loop to Down Sidings	3 short pause 2 short
Up Sidings to Down Sidings or vice versa	3 short pause 3 short
Train ready to leave Sidings	2 short pause 1 short
Shunt from Sidings to Main Line	2 short pause 2 short
To or from Loco.	2 short
Express trains requiring fresh engine at next stopping place	3 crows
Fire on lineside	1 crow 1 long 1 crow
*To be repeated when passing next Permanent Way men, Station Signal Box or Crossing Keeper's hut.	
Engine requiring water	1 long pause 3 short
To indicate light engine is clear of points which require to be turned	short
To indicate that train or light engine has been shunted clear of points leading from one running line to another—(Rule 69)	1 crow 1 short
To indicate that train or light engine has been shunted clear of all running lines—(Rule 69)	3 short
Before starting train assisted by engine in rear—(Rule 133 clause c)	2 crows

(G.A.30 Op.—9/54 LKI 9593 E).

WORKING OF ENGINES IN STEAM COUPLED TOGETHER.—Page 142.

The instruction under heading "(B)" Over Royal Albert Bridge, Saltash, to be cancelled and the following substituted:—

- (1) The maximum speed of all engines passing over the structure is 15 m.p.h.
- (2) The following 'Red' engines may be assisted by any of the 'Blue' or 'Red' engines shown—

"Red"	Assisting Engine
4-6-2 Standard Cl.7	49xx, 59xx, 69xx, 79xx
4-6-0 "Castle"	68xx
49xx	78xx
68xx	43xx, 53xx, 63xx, 73xx, 93xx
	41xx, 51xx, 61xx, 81xx

- (3) The following 'Red' engines may be assisted by any of the 'Blue' engines shown—

1000 Class	78xx
40xx	43xx, 53xx, 63xx, 73xx, 93xx
	41xx, 51xx, 61xx, 81xx

Both groups of engines may also be assisted by engines of the 'Yellow' and 'Uncoloured' classes.

Assisting tender engines with a leading bogie may be coupled either in front of the train engine or between the train engine and the train. In all other respects the general instructions for assisting or double heading of trains apply.

(G.A.30 Op.—9/54 TD95/41)

INSTRUCTIONS TO BE OBSERVED WHEN ARRANGING THE ASSISTING OR DOUBLE-HEADING OF TRAINS.—Page 144.**2. Passenger Trains.**

The following to be added as paragraph (iv) to Clause (b) —

- (iv) Engines of the 41xx and 51xx 2-6-2T types and of the 43xx 2-6-0 type may assist in front of any authorised engine between the following points:—

Par-Newquay
Newquay-Par.

(G.A.30 Op.—9/54 TD.95/166).

Reference to the following to be made on page 145:—

WORKING OF LOCOMOTIVES WITH TENDER LEADING.

Tender locomotives must not exceed a speed of 45 m.p.h. when running with the tender leading either when attached to a train or when running light. (G.A.30 Op.—9/54 LKI E)

RESTRICTIONS ON THE WORKING OF WESTERN REGION WIDE STOCK —Pages 150-154.

Restrictions as to Working of Stock over Certain Branch Lines. Page 150.

The following to be added to the present restrictions respecting stock 73ft. 0in. long by 9ft. 0in. wide

Branch	Station	Restriction
Severn Valley Line	Bridgnorth	Must not come back from Down Main Line into the Down Siding at the Highley end of the Station. (This also applies to stock 66ft. 8in. long by 8ft. 11in. wide) (G.A.30 Op.—9/54 R.6/-)

Stock 73ft. long by 9ft. wide.—Page 151.

DELETE 'Standish Junction' in the second line and substitute 'Abbots Wood Junction'
INSERT after "Yate Section"—"via Cheltenham and Gloucester South Junction only."

(G.A.30 Op.—9/54 R.6/5515).

The last sentence to be amended to read:—

and on the Central Wales Section may be worked only between Whitchurch and Aberystwyth, Aberystwyth and Carmarthen, Dovey Junction and Pwllhel (speed restriction of 25 m.p.h. through tunnels Aberdovey to Dovey Junction)

(G.A.30 Op.—9/54 R.17/355W.)

Stock 66ft. 8in. long by 8ft. 11in. wide.—Page 151.

The entry under heading 'L.M.S. Railway Lines prohibited' in respect of Swansea Victoria Station to be deleted.

(G.A.30 Op.—9/54 R.6/-)

Stock 63ft. 6in. long by 9ft. 5½in. and 9ft. 7in. wide.—Page 151.

The entry under heading 'Central Wales' in respect of Pwllhel to Dovey Junction to be amended to read:—

Pwllhel to Dovey Junction (Speed restricted to 25 m.p.h. through tunnels Aberdovey to Dovey Junction).

(G.A.30 Op.—9/54 R.17/355W.)

Stock 60ft. long by 9ft. wide.—Page 153.

The following to be added to the entry under heading 'L.M. & S. Company (Western Section)'

Swansea Victoria and Craven Arms (provided the length over buffers does not exceed 63ft. and the width over projections does not exceed 9ft. 4in.) subject to the following prohibitions:—

Swansea Victoria—Wash road in the Carriage Sheds.

Builth Road—Bay Platform.

(G.A.30 Op.—9/54 R.6/-).

SHUNTING INSTRUCTIONS—SHUNTING ETC. IN STATIONS AND STATION YARDS.—Page 155.

The following to be substituted for the fifth paragraph of these instructions:—

When coaching stock is propelled on running lines or from running lines to sidings, the Guard or Shunter should ride on the leading vehicle or the nearest suitable vehicle, keep a good lookout and be prepared to hand signal to the Driver or Fireman. If this is impracticable the Guard or Shunter should place himself in such a position on the ground that he can plainly signal to the Driver.

(G.A.30 Op.—9/54 O.M.12725).

CONTROL AND DISTRIBUTION OF ROLLING STOCK.—Page 166.

The instructions in G.A.9 under this heading to be cancelled and substituted by the following:

The existing instructions for the Supply and Distribution of Freight Stock are shown in booklets BR 87237 and 87238 "Standard instructions to Stations and Standard instructions to District Officers" respectively.

(G.A.30 Op.—9/54 R.).

Reference to the following to be made on page 168:—

DRIVERS' TICKETS.

It is very important that Drivers' Tickets are correctly and fully completed as the information is the basis of Railway Statistics called for by the Transport Act of 1947. There is evidence to show that Column 13 'Description of Trip' is not being accurately compiled in that 'loaded' or 'empty' freight trains are not being properly described. It is important to remember that if there are one or more loaded wagons on a train, that train must be regarded as a 'loaded' train.

Guards are responsible for informing Drivers as to the "Description of Trip" (Column 13 of Drivers' Ticket) and the number of wagons and the equivalent load (Column 21 of Drivers' Ticket). It is imperative that care should be taken to see that the Driver is properly informed as to —

- (a) Whether the train is "loaded" or "empty".
- (b) Number of vehicles and equivalent number of wagons at starting point.
- (c) Any alterations to load en route. (G.A.30 Op.—9/54 E.).

CONVEYANCE OF PASSENGER TRAIN STOCK ON FREIGHT TRAINS.—Page 168.

The following to be substituted for clauses 1, 3 and 4:—

1. Coaching Stock exceeding 65ft in length may be conveyed on a through freight train, with 'F' or inferior headcode only, subject to being formed next in front of the rear Goods Brake Van, with an additional Goods Brake Van or 5-ton open Goods (TUBF) wagon or other vehicle 24ft. over headstocks or longer, formed next in front of the coaching stock.

Freight trains conveying such coaching stock must not enter sidings not normally used for such stock, and the stock must not be conveyed on trains passing by routes over which the stock is prohibited except when notified by the Operating Superintendent giving Civil Engineer's Conditions of passage.

This instruction does not apply to stock passing from private building works, particulars of which are notified by the Operating Superintendent as out of gauge or exceptional loads with the applicable conditions of passage.

3. If essential for traffic reasons 8-wheel passenger train vehicles when conveyed by freight trains must be formed immediately in front of the brake van except in the case of local trains and pilot trips conveying not more than equal to 25 wagons of Class 1 traffic, when they may be placed in any position on the train.

4. If essential for traffic reasons 4 and 6-wheel passenger train vehicles may be formed in any position on freight trains.

(G.A.30 Op.—9/54 Ex.5299/52)

The following to be added as clause 6 —

6. Empty Parcels Vans Siphons 'F', 'G', 'H' or 'J' must not be conveyed on freight trains except where essential for traffic requirements on Branch Line services as arranged by the District Officer.

(G.A.30 Op.—9/54 E.1088H (2-C) R.8/-)

INSTRUCTIONS FOR WORKING BALLAST TRAINS.—Page 169

The instructions under this heading to be cancelled and the following substituted:—

INSTRUCTIONS FOR WORKING OF CIVIL ENGINEERING DEPARTMENT BALLAST AND MATERIAL TRAINS.

Additional instructions relating to the working of fully fitted Ballast trains are shown under separate heading "Fully fitted trains with Ballast loaded in Engineering Department Hopper Wagons and Plough or Freight Brake Van from Quarries to unloading sites and return of empty Hoppers."

Arrangements for running

The District Operating Superintendent or District Traffic Superintendent from whose District the trains will start will make all arrangements for the running, and advise the other District Operating Superintendents concerned. In his requirements, the District Engineer must indicate the approximate duration of the Engineering Department work at site or sites to enable the Operating Department to arrange relief of trainmen when necessary.

Any emergency arrangements after office hours will be made by the respective Control Offices.

Engine Power

The District Operating Superintendent will arrange with the District Motive Power Superintendent for the provision of the necessary engine power.

When making application for the running of these trains or engines in connection with relaying, reconstruction of bridges, or other Engineering Department work, the District Engineer must advise the District Operating Superintendent or District Traffic Superintendent (giving type of engine or engines if necessary), stating whether during any interval, the engines may be utilised for other purposes, and, whenever possible, name the time when it is expected an engine will be available to return from the site of the work.

Trains picking up men for unloading.

When these trains are required to call at places en route to pick up men for unloading the District Operating Superintendent or District Traffic Superintendent must be advised at the time arrangements are made for the running of the train.

Instructions to Guards, and Signalman as to site for unloading.

The section or station and mileage of the site with a description of the line at which such a train is to be worked must be shown in the Civil Engineer's weekly ballast programmes, Speed and Engineering Notice, or other notice, and the guard of the train must be instructed accordingly. Where it is necessary to unload some or all the material on another site and time will not admit of the Guard being notified in the usual way, information must be given by the Permanent Way Inspector or Ganger to the Signalman at the commencement of the section affected, and he in turn must inform the Guard of the train accordingly.

Ballast not to be deposited on points, rodding wires, connections, or A.T.C. Ramps.

When ballast or other material is being unloaded, great care must be exercised to see that it is not deposited on point rodding wires or connections, or automatic train control ramps.

Wagons put off for repairs.

When an empty wagon is put off for repairs it should be labelled by the Guard to the home station, and when a loaded wagon is detached for repairs it is to be labelled to the destination of the train.

Should any loaded wagons be put off for repairs, etc., the Guard must give particulars in writing to the man responsible for unloading at destination, in order that he may be aware that the complete train has not arrived, thus obviating the risk of error in the certification of the advice note.

The guard must show on the back of the engine ticket (form No. 2009), Engineering Department service vehicles, wagons put off for repairs, etc., giving the name of station and wagon numbers, and stating if loaded or empty and how labeled.

Guards to examine trains.

Guards must examine their trains immediately after unloading or on taking charge, and before proceeding to or leaving a Contractor's temporary road, and in the event of a wagon having been damaged, a report of the circumstances must be sent to the District Operating Superintendent or District Traffic Superintendent.

Reversing on incline

Trains which have to reverse on a steep incline must be provided with a brake van at each end. The provisions of Rule 151 must be observed.

Propelling on incline

The amplification of Rule 149, exception (vii) as shown on page 20 of the General Appendix to the Rule Book to apply.

Marking of wagons

The marking of Engineering Department wagons is not to be altered without authority from the Civil Engineer.

Guard's Journal

The Guard of each train must send a sectional journal on form B.R.87210 to the Operating Superintendent of each District through which his train passes.

Journals need not be rendered to the Engineering Department.

Engine Ticket

The Guard of each train must prepare engine ticket (form No. 2009), including on the back thereon particulars of the loading of the train, and immediately the last journey for the day is completed, it must be forwarded to the District Engineer in whose District the train originated.

The Guard must also give an engine ticket (form 2009A) to each Driver for the time the Driver was on the train.

Each Driver must obtain from the Guard an engine ticket and attach it to his daily record.

Guards are responsible for rendering an engine ticket to the Driver after the completion of the work, covering the whole of the time the engine has been in traffic. The Driver must communicate with the Guard after completion of the work in order to obtain this engine ticket.

Engines returning home.

When engines which have been used for working loaded trains are not required on the return journey and return home light, the time will be debited to the Engineering Department, but should they be utilised for ordinary traffic working no charge will be made.

Guard to inform Driver formation and number of wagons.

Before starting, the Guard must inform the Driver in charge the total number of wagons on the train, and what proportion of it is composed of vacuum fitted wagons, the brakes of which can be applied from the engine. **VACUUM-FITTED VEHICLES MUST BE FORMED NEXT TO THE ENGINE.**

Vacuum pipes out of use.

On arrival at site when these vehicles are disconnected the Guard must see that any loose vacuum pipes are placed on the stop plugs provided.

Headcodes to be carried

Engineering Department trains must carry "F" headcodes except in the case of trains fully or partially vacuum fitted which should carry "C," "D" or "E" headcodes respectively.

Stopping at
Stop Boards
to pin down
brakes, and at
foot of
inclines

ALL TRAINS WITH THE EXCEPTION OF THOSE CARRYING 'C' HEADCODE, MUST COMPLY WITH THE INCLINE INSTRUCTIONS AS SHOWN ON PAGES 178 AND 180 OF THE GENERAL APPENDIX TO THE RULE BOOK, MUST STOP AT STOP BOARDS AND PIN DOWN BRAKES AS REQUIRED IN ACCORDANCE WITH THE "GENERAL INSTRUCTIONS FOR WORKING INCLINES."

Co-operation
between
Operating and
Engineering
Departments

It is of importance that close co-operation should exist between the District Operating Superintendents and District Engineers so that Engineering Department trains shall be scheduled at a time when they will cause least interference with the ordinary booked train services. This is especially necessary where the trains have to work on, or pass over, single lines.

Running of
trains during
Summer

On main passenger train routes the running should be curtailed as far as possible throughout the period of summer traffic (during Bank Holiday periods, etc.) especially on Mondays, Fridays and Saturdays.

(G.A.30 Op.—9/54 E.8267 H(12-C.)).

Reference to the following to be made on page 171.

FULLY FITTED TRAINS WITH BALLAST LOADED IN ENGINEERING DEPARTMENT HOPPER WAGONS AND PLOUGH OR FREIGHT BRAKE VAN FROM QUARRIES TO UNLOADING SITES AND RETURN OF EMPTY HOPPERS.

Loaded trains must run under the following conditions:—

- (i) Carry "C" headcode.
- (ii) Maximum speed—50 m.p.h.
- (iii) Engine provided to have not less than 5ft. 8in. diameter coupled wheels.
- (iv) Vacuum brake on all wagons to be operative, also plough or freight brake van to be fitted or piped.
- (v) It will not be necessary to observe Incline Instructions.
- (vi) Maximum load not to exceed 22 20-ton loaded Engineering Department hopper wagons. On rising gradients the standard loading for Class I traffic shown in the Service Time Tables must not be exceeded. Engineering Department 20-ton hoppers loaded with ballast to be calculated as "Two equals Three 10-ton wagons of Class I traffic."
- (vii) Instantanor couplings must be in short position.

Procedure to
be adopted
in arranging
trains.

The Civil Engineer will issue a weekly programme to all concerned on Tuesdays showing the requirements for fully fitted trains of ballast to run from the following Monday to Sunday (both days inclusive).

The District Operating Superintendent or District Traffic Superintendent concerned with the despatch of the ballast must confer with the District Engineer and agree departure time of trains, according to the unloading point for all the trains listed and advise other District Operating or Traffic Superintendents concerned by telegram on Wednesdays full details of the trains arranged for the following week.

The District Engineer involved with the unloading of the ballast to consult the District Operating Superintendent or District Traffic Superintendent in whose District the unloading will take place, on Wednesdays and agree at what time this can be accomplished. The District Operating Superintendent or District Traffic Superintendent will order a fresh engine, if this is necessary, to undertake the work of discharging the ballast and bringing back the empties and arrange departure time for a special train conveying the empty hoppers back to the Quarry.

Train
Schedules

Standard schedules will be laid down for these trains, where practicable, and must be adhered to. In the case of journeys where a standard schedule cannot be applied, throughout timings must be pre-arranged in the case of loaded trains to arrive at unloading site by 7.30 a.m. unless otherwise specified in the Civil Engineer's weekly programme.

District
Operating
Notices

Details of the loaded and empty trains required to run including timings must be issued in the District Operating or Traffic Superintendent's notices, copies of which must be sent to the Operating Superintendent's T.D. Section.

Engine Power
and trainmen

In general where the transit is within 100 miles of the quarry, an engine and trainmen must be diagrammed to cover the loaded and empty journeys throughout, although in the case of branch lines it may be necessary to utilise a smaller type of engine from the junction in which case the engine used to convey the ballast from the Quarry, or nearby marshalling yard, must be available for working back the empty hoppers to the Quarry.

In the case of longer distance transits or where it is considered guaranteed arrivals at unloading sites within reasonable limits are uncertain, it is desirable that ballast should be despatched from Quarries as soon as possible after loading has been completed and be stabled at a convenient point near to unloading site. The Operating Superintendent will diagram engine and trainmen to work to stabling point and back to the Quarry.

A fresh engine and trainmen will be diagrammed to carry out the unloading of the ballast unless the District Engineer has made other arrangements.

Prior arrangements must be made in order to ensure that enginemen and guards are available to work the loaded and empty trains at the recognised changing points en route.

Return of empty hoppers to Quarries.

Owing to limited supply of hopper wagons for ballast traffic, it is important no delay occurs in returning these wagons, when empty, to Quarries in order that the programmed supplies of ballast may be maintained, and movement must be undertaken by special trains.

Empty hopper trains

Empty trains may run under "C" headcode freight train conditions not exceeding 50/20 ton hoppers. Engineering Department hopper wagons being calculated on the basis of 20/21 ton wagons shown in the Service Time Tables.

WORKING OF HOPPER BALLAST WAGONS AND PLOUGH.

Guard's duties in working hopper trains.

Whenever practicable, a loaded hopper ballast train must be worked by a Guard who from previous instruction and experience, is qualified to take charge.

Working of plough to be undertaken by Guard

The working of the hopper ballast plough for spreading the ballast is to be undertaken by the Guard, under direction of the Permanent Way Inspector or Sub-Inspector.

Where ploughs are not to be used

The hopper ballast plough is not to be used in station yards.

Material carried by hopper trains

Hopper wagons are only to be loaded with material as authorised by the Civil Engineer and the plough must not be employed for spreading material other than crushed slag or stone, gravel and ashes suitable for top ballast.

Discharging ballast from hopper trains

When required to be unloaded on lines already opened for traffic, the Permanent Way Inspector of the district will give instructions where the ballast is to be discharged, but the Guard of the train and the Ganger of the length must satisfy themselves that there are no crossings, catch points, guard rails, signal wires, locking gear rods, detonator machines, automatic train control ramps, level crossings, or other works with which the plough can possibly come in contact on the lengths of line to be ballasted.

Responsibilities of Guards in charge of hopper trains

The Guard will be held solely responsible for the proper working of the train and for satisfying himself that:—

- (a) Hopper wagons and plough vans are in good order and examined at the usual examining points en route.
- (b) When not engaged in unloading, the plough is in its normal position by being raised to the fullest extent, and the two keys inserted and secured by padlock.
- (c) When starting on a journey with a train which has to be unloaded in a tunnel, the plough is in the right direction, also that the hoppers are arranged so that all the levers are one side of the train, and that where unloading has to be performed in a double line tunnel, all levers face the 6-ft. way.
- (d) Upon arrival of the train at the place where it is intended to unload, and after it has been placed in position on the plough is lowered to the rails, but not screwed hard down. That the French keys in the centre and right-hand levers of the wagons are then removed and the key of the left-hand lever kept in.

That the hopper wagon next to the engine is discharged first, the door being opened by removing the key from the left-hand lever, and the engine started at a speed of not more than four miles per hour. Before the whole of the contents of the first wagon to be discharged are run out, that the hopper door of the next wagon to it is lowered, and so on through the train.

The Permanent Way Inspector or Ganger will assist in the discharge of the train, one being on one side of the train and the Guard on the other.

- (e) When the contents of the whole train have been spread, and before resuming the journey, the plough is cleaned with the scrapers which are kept in the plough van, then raised to the fullest extent and secured by means of the keys and padlock.
- (f) Before proceeding with the empty train the doors of the hoppers of the wagons are closed and secured with the French keys which are to be properly fixed in the three levers, the keys being inserted in the holes from the left-hand side.

Lubrication

The lubrication, which includes doors, pins and slides of hopper ballast wagons and plough vans will be done by the C. & W. Engineer's Department staff at the loading points.

Closing doors of hopper wagons

Should any difficulty be experienced in closing the doors of hopper wagons, force must not be used, but the doors swung back against the stop on the so ebar to dislodge any fine ballast from the ledge on the "Z" iron.

(G.A.30 Op.—9/54. E.82672H. (12-C.).

WORKING OF VACUUM AND PARTLY VACUUM FITTED FREIGHT TRAINS—page 172**(A) Freight Trains carrying "C" Headcodes.**

Clause 4 amended to read:—

- 4 Piped or fitted freight train brake van must be provided. (G.A.30 Op.—9/54. E.1562.W.).

WORKING OF VACUUM AND PARTLY VACUUM FITTED FREIGHT TRAINS —Page 173

The following to be inserted immediately after note "Z" —

Note. For the purpose of giving effect to this instruction the minimum number of wagons to be coupled with the vacuum pipe to the train engine to be based on the equivalent load for Class 3 traffic, e.g. should a load of 44 wagons of Classes 2 and 3 traffic be equal to 60 wagons of Class 3 traffic the minimum number of wagons to be coupled with the vacuum pipe to the engine to be 20 and not 15.

(G.A.30 Op.—9/54. E.84669H. (2-C.)).

Reference to the following to be made on page 174:—

RUNNING OF SPECIAL FREIGHT TRAINS.

When an Inter-District Special freight train is required (other than those pre arranged by notice or other means) the request to be passed by the originating District Control to Headquarters Control, Paddington, with details of headcode, load to be conveyed and time the Special train is required to start. The Headquarters Control to make the necessary arrangements with the other District Controls including points at which Carriage and Wagon examinations to be carried out and agree or otherwise with the District Control making the application whether or not the Special train can run.

As long notice as possible, minimum 4 hours, must be given to the Motive Power Department to enable preparation to be made in the provision of power. A serial number will be given by Headquarters Control to all Special freight trains agreed and this number to be quoted in all messages, telephonic or telegraphic, sent in connection with the train concerned.

(G.A.30 Op.—9/54. E.81019 H(7-B.)).

WORKING OF FREIGHT TRAINS.—Page 174.

The instructions under the above heading to be cancelled and the following substituted —

EXAMINATION OF FREIGHT TRAINS.

Unless otherwise specially agreed between the Operating Superintendent and the Carriage & Wagon Engineer, freight trains must not run more than the following distances shown under the various headcodes without stopping for Carriage and Wagon examination

Class of Freight Train	Maximum Distance in Miles, train is allowed to run without full Carriage & Wagon Examination	Type of Axle Box
"C"	160	Oil Axle Box
"D"	125	Oil Axle Box.
"E"	125	Oil Axle Box.
"E"	85	Grease Axle Box.
"F" and below.	85	Oil and Grease Axle Box. Wagons (loaded or empty).
"F"	125	Oil or Grease Axle Box empty wagons only*

* Full train loads of empty wagons capable of being run under "C", "D" or "E" headcode conditions to be so arranged.

(G.A.30 Op.—9/54. E.81019. H(7-B.)).

GENERAL INSTRUCTIONS FOR WORKING INCLINES.—Page 178.**Descending Inclines.**

Clause 15 amended to read:—

15. All ballast and freight trains, including those consisting partly of vacuum stock but excluding those which are fully vacuum fitted, must stop at the foot of inclines as well as at the top

(G.A.30 Op.—9/54. E.82672. H.(12-C.)).

BRAKING VACUUM-FITTED AND PARTLY VACUUM-FITTED FREIGHT TRAINS DOWN INCLINES.—Page 180.

Clause 1—The words "Guard's Brake Valve" to be substituted for "Brake Setter" in the last sentence of this clause.

(G.A.30 Op.—9/54. E.1562W.)

BRAKING VACUUM-FITTED AND PARTLY VACUUM-FITTED FREIGHT TRAINS DOWN INCLINES.—Page 181.

Clause 5 amended to read:—

5. These instructions do not apply to Ballast Trains partly composed of vacuum fitted stock which must work under the Incline Instructions.

(G.A.30 Op.—9/54. E.8267 H.(12-C.)).

Reference to the following to be made on page 182:—

REGULATIONS AS TO PRIVATE OWNERS' WAGONS.

1. The Owners of all new or rebuilt vehicles intended to work upon the Western Region must, before they are brought into use, communicate with the Carriage & Wagon Engineer, Swindon, so that he may have them inspected without unreasonable delay, and if built, or rebuilt, in accordance with the Railway Clearing House Standard Specification and addenda thereto, register plates, as described in the Specification, shall be forthwith affixed to each.

2. The name and address of the Owner or Lessee, the wagon number, and the tare, shall be painted or stencilled on both sides of the wagon, the maximum load must also be clearly indicated on both sides of the wagon.

When wagons are let on hire the Lessee will, for the purpose of these Regulations, be regarded as the Owner.

Provided that when the hire is for a term of not less than three months, the name and address of the Lessee shall be painted or exhibited on a board or plate on both sides of the wagon, and that when the hire is for less than three months, the name and address of the Lessee shall either be so painted, or exhibited on a card (other than the wagon label), on both sides of the wagon.

3. The Owners or Lessees, as the case may be, shall keep their wagons in good working condition, and shall have them properly lubricated and examined and put into good repair before being tendered to the Western Region for transit.

4. The British Transport Commission may remove the register plates from any wagon if wheels, axles, or other materials of less dimensions or strength than those provided for by the Railway Clearing House Standard Specification are afterwards substituted in contravention of the conditions of the said Specification.

5. If any material defect shall be observed, which for the proper and safe working it is necessary to repair before the wagons are allowed to proceed further, the British Transport Commission may, with the consent of the Owners, make such repairs, and charge them with all expenses incurred in effecting the same.

6. In pursuance of the rules made by the Board of Trade under the provisions of the Railway Employment (Prevention of Accidents) Act, 1900, when it is necessary in the ordinary course of business that any label or directions to destination or consignee shall be placed upon any railway wagon, such label or directions shall be placed on both sides of such wagon, and no Private Owner's Wagon will be accepted for conveyance by the Western Region unless so labelled or directed on both sides.

All wagon Owners, representatives of wagon Companies and their repairers, when labelling defective wagons at railway stations and depots, shall clearly set forth on the labels the station or siding from and to which the wagons are required to travel, and hand in a proper Consignment Note or written forwarding instructions.

The British Transport Commission agrees, without prejudice, to dispense with a Consignment Note or written forwarding instructions in the case of wagons painted with a yellow star in accordance with the provisions of the Commuted Empty Haulage Scheme, labelled to Repair Works.

Note—Not applicable to wagons working in Scotland.

7. When wagons, for the purpose of repair are required to be shunted into and out of Sidings belonging to the British Transport Commission and on into and out of premises in the occupation of Private Wagon Repairers, a charge of 2s. 8d. per wagon will be made for such services, except where a higher charge is now made, in which case such higher charge shall be the maximum charge under these regulations.

Siding rent will be chargeable to the owner, or his agent, in respect of standing room for any wagon detained at a station or siding for repairs at the rate of 1s 5½d. per wagon per day, which will be calculated from the expiration of three days, exclusive of the date of the advice note, Sundays and Bank Holidays from the time the wagon is placed at the disposal of the Owner or Repairer, and to terminate when the wagon is labelled for despatch after repair.

The foregoing charges do not apply when the owners are parties to the Commuted Shunting and Siding Rent Charge Scheme, in which case the wagons are plated or stencilled "C.C."

8 Any authorised servant of the British Transport Commission may detain any wagon which may appear to him unfit to run until it has been put into proper repair and passed by an authorised person of the Western Region Carriage & Wagon Engineer's Department.

9 The British Transport Commission will not be responsible for any damage to Private Owners' Wagons left unprotected in an imperfect state by the Owners, nor for any injury that may occur to wagon repairers, who will be required to execute an indemnity before they are allowed to work on British Transport Commission's premises.

10. Private Owners' Wagons running over the British Transport Commission lines must not, apart from a reasonable description of the contents of the wagon, be used for advertising purposes, but the British Transport Commission will not object to a description (to be approved by them) of a product of the Owner's manufacture being painted thereon.

11. Nothing contained in these Regulations shall prejudice or affect any responsibility to each other of the actual Owners or Lessees of Wagons and the British Transport Commission.

(G.A.30 Op.—9/54. E.86585 H.10-F.).

WAGONS LABELLED TO PASS ON SLOW TRAINS. Page 184.

The instructions under this heading to be cancelled.

(G.A.30 Op.—9/54 E.86585H.).

RAILWAY COMPANIES' OR PRIVATE OWNERS' WAGONS LABELLED FOR REPAIRS. —Page 184.

The instructions under this heading to be cancelled and the following substituted —

WAGONS LABELLED FOR REPAIRS.

Wagons fitted with oil axleboxes and bearing green "FOR REPAIR" labels, indicating defects other than those appertaining to axleboxes, axleguards, wheels and axles may be conveyed by trains running under "D" or inferior headcodes.

Wagons fitted with grease axleboxes and bearing green "FOR REPAIR" labels, indicating defects other than those appertaining to axleboxes, axleguards, wheels and axles may be conveyed by trains running under "F" or inferior headcode conditions.

Wagons bearing green "FOR REPAIR" labels indicating a defective axlebox, axleguard, wheel or axle, the nature of which does not call for the wagon to be loaded upon another vehicle, should only be conveyed by trains running under "H" or inferior Headcode conditions

(G.A.30 Op.—9/54. E.86585H.)

TRANSIT OF STEAM AND HAND TRAVELLING CRANES.—Page 186.

The following to be added as clause (c):—

A special train conveying a 45 ton steam crane complete with match truck and Stokes bogies, which are piped, when travelling within the Western Operating area to or from the site of Engineering Department Operations, may carry 'B' headcode, provided the Crew's vans are also piped and the other vehicles comprising the train are fully vacuum fitted. Such trains must not exceed a speed of 45 m.p.h. at any point.

G.A.30 Op.—9/54. LK1/12204/417. EX.3509/53.).

PREVENTION OF ACCIDENTS.—Page 266.

The following to be added to paragraph 5 respecting the whitening of platform edges.—

"The edges of the ramp must not be whitened."

(G.A.30 Op.—9/54. LK1/9678 Gen.).

INSTRUCTIONS FOR REQUISITIONING, STORAGE AND DISTRIBUTION OF PETROLEUM FOR TRAFFIC AND GOODS DEPARTMENTS.—Page 280.

Amend the word Extension in the last line of the second paragraph to read "Extinguisher"
(G.A.30 Op.—9/54).

GLASSES FOR LAMP CASE INTERIORS.—Page 285.

Delete the entry in respect of Route Indicator and Permanent Speed Indicator Lamps and insert the following:—

Route Indicator Lamps
4 in. x $5\frac{1}{2}$ in.

Permanent Speed Indicator Lamps
Front glasses $5\frac{1}{2}$ in. x 5 in.
Side glasses 3 in. x $2\frac{1}{2}$ in.

(G.A.30 Op.—9/54 LK1/10676/26).

HANDLING OF MOTOR CYCLE TRAFFIC.—Page 288.

The following to be inserted as clause (e):—

Any persons where special motor cycle loading boards are provided they must always be used for loading or unloading motor cycles into or out of vans. The boards must be carefully handled and after use they must be returned to their storage point.

(G.A.30 Op.—9/54. A2/154/55.).

EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES.—Page 293.**Instructions to Staff respecting Working and Transit.**

The following to be added as clause 13a:—

Staff

Staff must see that they do not expose themselves to danger during lifting operations by standing where the load may fall, be lowered on to them, or strike them during motion, or where they may be struck by rotation of crane handles which, through some defect, cannot be removed or disconnected as required by Clause 10. When with moving chains or slings clear of loads, care must be taken to ensure that they do not catch in the load and cause it to overturn.

Staff must not ride on a hook or load, nor interfere with a load except to guide or prevent it swinging.

(G.A.30 Op.—9/54. C.R.O.—W.36027/5).

Chains—Annealing and Testing.

Amend the first sentence of clause 56 to read:—

Form 2432 must always be rendered in duplicate when a chain is forwarded for testing

(G.A.30 Op.—9/54. C.R.O.—W.36027/5).

Amend the first sentence of clause 57 to read:—

Loose lifting tackle required for temporary use should be requisitioned on form 2432 rendered in duplicate and as necessary, and the numbers quoted as appearing in the diagram of 'Standard Lifting Tackle.'

(G.A.30 Op.—9/54. C.R.O.—W.36027/5).

CLEANING, TRIMMING AND LIGHTING OF TRAIN LAMPS AND MODE OF WORKING.—Page 310.

The first paragraph of these instructions to be amended to read:—

Side, Tail and Hand Lamps.—Only petroleum must be used in these lamps, great care must be taken in trimming the lamps to ensure that an adequate quantity is poured into the tank so that the tanks are not filled above the level of the bottom of the burner collar. The tanks must be wiped dry before being placed in the lamp, every part of which must be well cleaned especially the burners and vent holes.

(G.A.30 Op.—9/54 LK1/10688/229).

**GUARDS AND ENGINEMEN TRAVELLING FROM POINT TO POINT TO RELIEVE
TRAINMEN AND TRAINMEN PROCEEDING TO THEIR HOMES AFTER BEING
RELIEVED en route FOR REST, ETC.—Page 338.**

The instructions under this heading to be deleted.

(G.A.30 Op.—9/54 S.57368 M.).

September, 1954.

GILBERT MATTHEWS,
Operating Superintendent.

Each member of the Staff receiving a copy of this Circular is required to read carefully and note the contents, and, if supplied with a copy of the General Appendix to the Rule Book to alter or cancel in ink the present instructions on the subject appearing therein, afterwards pasting the amendments in their proper places in the General Appendix to the Rule Book.

Station and Depot Masters are responsible for seeing that copies of the General Appendix to the Rule Book supplied to Signal Boxes, etc., under their supervision, are corrected in accordance with this Circular.

B.R. 30145/29 G.A.30.Op.

(This form must be detached and forwarded to the Head of Department.)

..Station.

.....1954

RECEIVED copy of Circular G.A.30 Op., dated September, 1954, containing alterations and additions to the General Appendix to the Rule Book.

BRITISH RAILWAYS

(WESTERN OPERATING AREA)

Alterations and Additions to the General Appendix to the Rule Book

To come into operation on receipt.

ADDITIONS TO THE STANDARD RULES.

Rule 215—Use of Trolleys where track circuiting is provided.—Page 21.

The third paragraph under this heading to be deleted.

(G.A.31—7/56. O.M.12789)

INTERMEDIATE BLOCK SIGNALS—Pages 71 and 72.

The following to be inserted as Clause (h) paragraph on page 72 :—

(h) Trolley Working.

Where Intermediate Block Signals are provided before a trolley is placed upon or run over a track circuit, the permission of the Signaller to whose box the track circuit is connected must first be obtained. In such cases a telephone is provided which may be used by the Ganger to communicate with the Signaller.

Where the overlap track circuit of the Intermediate Block Home Signal does not extend to the Home Signal of the Signaller's box in advance and a trolley is required to be placed on the overlap track circuit of the Intermediate Block Home Signal, the Handsignaller when going back to protect such trolley must if he should reach the Intermediate Block Home Signal telephone to the Signaller and after acquiring the trolley's position continue to go back protecting the trolley until reaching the Home Signal as required by the Rule. He must then act in accordance with Rule 215 Clause (d).

When the trolley proceeds the Handsignaller upon following must when he comes to the telephone at the Intermediate Block Home Signal advise the Signaller that the trolley has proceeded. The Signaller must be similarly advised if the trolley is removed from the line before the Handsignaller has passed in advance of the Intermediate Block Home Signal.

The Signaller upon being advised by the Handsignaller must keep the Intermediate Block Signal in the "On" position until such time as he receives information that the trolley has either gone forward to a point $\frac{1}{2}$ mile in advance of the Intermediate Block Home Signal or has been removed from the line.

(G.A.31—7/56 O.M.12789)

PUBLIC LEVEL CROSSINGS LOCKED BY PADLOCK.—Page 73.

The entry under this heading to be deleted.

(G.A.31—7/56 O.M.12763)

DETONATORS, PERIODS KEPT IN STOCK, PAINTING OF DETONATORS.—Page 84

Reference to "Rules 56 to 58" in the first paragraph of these instructions to be deleted and substituted by Rule 58.

(G.A.31—7/56 L.K.I/12963/418)

WATER TROUGHS, G.W. AND JOINT LINES.—Page 89.

The following to be added :—

Water Troughs, Clearance of Ice.

1 The Length Ganger will be responsible for keeping ice broken and cleared from the troughs.

2 The Length Ganger must use his discretion when the ice thickens as to when the troughs should be closed, but must not allow the ice to become more than 3 in. thick before arranging for the troughs to be closed. If the temperature is so low that the water freezes as quickly as it is cleared the troughs must be closed.

3 If ice collects in the four foot to a depth of 1 in. due to spillage the troughs must be closed.

4 When the Length Ganger finds it necessary to close the troughs as set out in Clause 2 he must advise the nearest Signaller who will notify the District Controller. In turn the District Controller will immediately notify the following :

Chief Mechanical and Electrical Engineer's Outdoor Machinery District Mechanical Foreman or Local Mechanical Chargehand and responsible Water Fitter whichever is most convenient.

Motive Power Depots.

District Motive Power Superintendent.

Headquarter's Control.

Chief Mechanical and Electrical Engineer's Works Manager.

District Engineer.

The Length Ganger must also advise his Permanent Way Inspector.

5 When a thaw sets in after prolonged frost the Chief Mechanical and Electrical Engineer's Outdoor Machinery District Mechanical Foreman or Local Mechanical Chargehand or responsible Water Fitter, whichever is most convenient, after consulting with the Ganger, will say when normal taking of water can be resumed, and will advise the nearest Signaller and arrange for the water to be turned on. The Signaller must, in turn, advise the District Controller, and he will notify the Departments listed in Clause 4 that normal working of the trough can be resumed.

6. The District Operating Superintendent to notify promptly the Chief Operating Superintendent, who will issue the necessary instructions in regard to the taking of water while the water trough is out of use.

(G.A.31—7/56 L.K.I/12351/420)

ROBBERIES FROM LUGGAGE VANS.—Page 117.

Paragraph 2 of the instructions under this heading to be deleted and substituted by the following :—

2. The Guard in charge must see that the inside sliding doors of luggage compartments, where such are provided, and the steel grilles of luggage compartments fitted in British Railways Standard Stock are kept closed and locked whilst the train is travelling.

(G.A.31—7/56—T.50, 639G/7 L.K.1/13138/420)

EMERGENCY APPLIANCES, ETC., IN PASSENGER AND PARCELS TRAINS.—Page 121.

The following to be inserted after "Diesel Cars" in the table in the first paragraph :—

New Type Diesel Cars.

Power Cars.—One water, CO₂ pressure extinguisher in the brake compartment and two CO₂ gas extinguishers in the driving cab.

Note.—A chlorobromomethane (C.B.) extinguishing system is fitted to each diesel engine with detectors to give the driver warning of fire and indicate which motor is affected.

Trailer Cars.—One water CO₂ pressure extinguisher at one end of the corridor.

(G.A.31—7/56 A.1/72034/102)

7. Train First-aid Cabinets—Replenishment Cupboards.

The entry 463 at LONDON—Old Oak Common should be amended to read "A x 3."

(G.A.31—7/56 52/AMB/7)

WORKING OF DIESEL CARS.—Page 132.

The following to be added at the end of the second paragraph under the heading "Instructions to Signalmen." :—

"The Train, Entering Section, signal for the Diesel Car must be sent when the car leaves the signal box controlling the Intermediate Block Signal.

(G.A.31—7/56 L.K.1/12510/372)

Reference to the following to be made on page 141 :—

STANDARD WHISTLE CODE.

Drivers requiring Ass start Engine or Fresh Engine en route.

In order to minimise delays to trains because of partial or total engine failure, the attention of all Drivers and Signalmen is directed to the standard instructions in the Sectional Appendices which provide for the use of the following whistle code

Express trains requiring a fresh engine at next stopping place — 3 crows.

The sounding of this whistle code will denote to Signalmen that another engine is required at the next station, if available there, or at the next Motive Power Depot, and

Signalmen must immediately pass forward an advice, giving title and head code of train in difficulty to the appropriate Control Office who will decide what action is necessary and issue instructions accordingly. In cases where a fresh engine is required from a Motive Power Depot, an advice must be passed forward by the Control without delay, and, on receipt of such an advice, Motive Power Depot staff must take steps at once to provide the required fresh engine.

(G.A.31—7/56 T.38, 113 G 1 7.38, 417.G 1 M 1953)

WORKING OF ENGINES IN STEAM COUPLED TOGETHER.—Page 142.

The instruction under heading (B) "Over Royal Albert Bridge, Saltash" (see Supplement G.A.30) to be cancelled and the following substituted:—

- (1) The speed of all engines passing over the structure must not exceed 15 m.p.h.
- (2) Not more than two permitted engines may work coupled together.
- (3) The instructions shewn on pages 144 and 145 of the General Appendix to the Rule Book in respect of the assisting or double heading of trains must be observed.

(G.A.31—7/56. T.D 95/41)

RESTRICTIONS ON WORKING OF WESTERN REGION WIDE STOCK Page 151

66 ft. 8 ins. long by 8 ft. 11 ins. wide.

Delete the following entry from the list of lines prohibited:—

Brynamman Branch.

(G.A.31—7/56 R6/-)

Passing Restrictions.

The last paragraph of these Restrictions dealing with the Newport Engineering District as listed in G.A.26 Op. to be deleted and the following inserted:—

These coaches are prohibited from passing other coaching stock and out of gauge loads between the following points in the Newport Engineering District:—

Maesycwmmwr Station (inclusive) to Pengam Station.

The existing restrictions so far as other Regions are concerned will continue to apply as already shown in G.A.18

63 ft. 6½ ins. long by 9 ft. 3 ins. wide.—Page 152.

Delete the following from the list of lines over which this stock is prohibited:—

Vale of Glamorgan.

(G.A.31—7/56 R.6/3926)

Reference to the following to be made on Page 158:—

BRAKE STICKS

All brake sticks on hand at stations and in yards must be examined during the first week of June and December and any which shew signs of having become defective must be returned to Swindon Stores. Requisitions for replacements must be submitted through the usual channels.

(G.A.31—7/56—L.K.1/13121/420)

"INSTANTER" PATENT COUPLER.—Page 158.

The paragraph following figure 2 to be amended to read:—

The coupling can be used for shunting operations as an ordinary loose coupling as shewn in figure 2. When the centre link is required to be placed into the short or close-coupled position the following procedure to be observed.

(G.A.31—7/56 L.K.1/9003/Gen.E.)

CONVEYANCE OF PASSENGER TRAIN STOCK ON FREIGHT TRAINS.—Page 168

The following to be added as paragraphs 7 and 8:—

- 7 Eight wheel ex-passenger stock converted as tool vans, mess vans, etc., when conveyed on freight trains must be formed immediately in front of the brake-van or with other 8-wheel vehicles so formed.
- 8 Four or 6-wheel ex-passenger stock converted as tool vans, mess vans, etc., may be formed in any position on freight trains, but unless piped or fitted with the automatic vacuum brake they can not be conveyed on fully fitted trains nor in the fitted portion of a partially fitted train.

(G.A.31—7/56 L.K.1/12693/232)

WORKING OF VACUUM AND PARTLY VACUUM FITTED FREIGHT TRAINS.—Page 172.**B—Freight Trains carrying "D" headlamps.**

The Note following Clause (1) to be deleted.

(G.A.31—7/56—E.2109H. (1-B))

STATION INSTRUCTIONS—Pages 262-344.**Cleaning Petroleum Lamps—Page 281.**

The instructions under this heading to be deleted.

(G.A.31—7/56 B.R. 29611)

Long Burning Signal Lamps—Pages 282-285.

The instruction under this heading to be deleted and substituted by the following —

LONG BURNING SIGNAL LAMPS—EQUIPMENT.

The undermentioned articles may be ordered by Signal Lampmen from the Stores Department on Requisition Form No. 224 through the Station Master at their Home Station, and arrangements should in future be made accordingly:—

Glasses for Lamp Case Interiors.**Standard Pattern Interior Circular Tank.**

Front, $4\frac{3}{4}" \times 4\frac{1}{2}"$.

Back, $3\frac{1}{2}" \times 2\frac{1}{2}"$

Old Pattern Interior Shallow Circular Tank.

Back and Front, $3\frac{7}{8}" \times 3\frac{1}{8}"$.

Semaphore 9" and 11" Cases.

Back and Front, $5\frac{1}{2}" \times 5"$. (Bottom corners cut away.)

Repeater 9" and 11" Cases.

Back and Front, $4\frac{1}{2}" \times 4\frac{1}{2}"$.

Route Indicator Lamps.

$4\frac{7}{8}" \times 5\frac{1}{4}"$.

Permanent Speed Indicator Lamps.

Front glasses, $5\frac{1}{2}" \times 5"$.

Side glasses, $3" \times 2\frac{1}{4}"$.

Taff Vale Section "B" Type.

$5\frac{1}{2}" \times 4\frac{1}{2}"$ thin.

Taff Vale Section "C" Type.

$4\frac{7}{8}" \times 4\frac{1}{2}"$ thin.

Barry Section Signal.

Curved, $5\frac{1}{2}"$ high \times $4\frac{1}{2}"$.

Barry Section Disc.

Curved, $4\frac{1}{2}"$ high \times $2\frac{1}{2}"$.

Rhymney Section.

$2\frac{1}{2}" \times 3\frac{1}{2}"$ thin.

Rhymney Section.

Elevated Disc, $5\frac{1}{2}" \times 3\frac{7}{8}"$ thin.

$5\frac{1}{2}" \times 3\frac{1}{2}"$
 $5\frac{1}{2}" \times 3\frac{1}{2}"$

B. & M. Section Signal.

$5\frac{1}{2}" \times 5\frac{1}{2}"$.

Wicks for Ordinary and Disc Long Burning Lamps.

Ditto for Standard "Serex" (T.V.) Lamps.

Feeding, Round, 11" long

Wicks for Repeater Long Burning Lamps.

Burning, $12" \times \frac{1}{4}"$ wide.

Feeding, $6" \times \frac{1}{4}"$ wide.

Tins, Safety, for Methylated Spirit**Scissors, Lamp.****Tins, Pocket, wick.****Polish, Metal, for cleaning reflectors.**

Special attention is directed to the fact that metal polish instead of whitening may be ordered for cleaning lamp reflectors

Leather washers should be used on the screw stoppers of these lamps, and if any are required at any time they can be requisitioned by Station Masters through the District Signal Inspectors.

Long Burning Lamps for Speed Indicators and Route Indicating Signals.

The burners of these lamps are $\frac{5}{8}"$ wide, and wick of similar width is supplied for use in the lamps

/

INSTRUCTIONS FOR USING PARAFFIN VAPOUR LAMPS—"TILLEY" TYPE.—
Pages 285-287.

The instructions under this heading to be deleted and substituted by the following :—

PARAFFIN VAPOUR LAMPS—"TILLEY" TYPE—SPARE PARTS.

The Operating Department Staff can order the following, but care must always be taken to quote the correct number of the respective part when ordering :—

Part No.	Name of Part.	Part No.	Name of Part.
123-90	Globes, Clear (outside lighting).	133-46	*Pumps, without connections.
123-91	Globes, Clear and Frosted (inside lighting).	133-74	*Vaporisers, complete.
133-40	Mantles, Inverted, Large, No. 2.	133-81	Washers, XN Cock Black, No. 160.
133-42	Mantles, Inverted, Small, No. 1, Indoor Lamp (I.L. 37).	133-31	*Cans, Oil, with spout.
133-39	Mantles, Inverted, Floodlight Projector.	119-165	*Angle Funnels, Tin, Gauze strainer.
133-79	Washers, Vaporiser, Black, No. 153.	133-22	*Carriers, Wire, "A," for P.L. 55 and 56 lamps.
133-19	*Caps, Screw, Filler "F."	133-23	*Carriers, Wire, "B," for all other lamps.
133-25	*Connections, Pump, 15".	130-10	Balls, $\frac{3}{8}$ ", No. 147.
133-37	*Lighters, Paraffin.	109-31	Special Brush for cleaning burner

* These items are obtainable only on Repairs Requisitions (Form No. 224), and the old articles must be returned to Swindon.

Spare Parts must be kept by the person in charge of the place where the lamps are installed, i.e., Station, Yard, Depot or Signal Box.

(G A 31—7/56 B.R. 29611)

CLEANING, TRIMMING AND LIGHTING OF TRAIN LAMPS, AND MODE OF WORKING
—Page 310.

The instructions under this heading to be deleted

(G.A.31—7/56 B.R. 29611)

S. G. HEARN,
 Chief Operating Superintendent.

Each member of the Staff receiving a copy of this Circular is required to read carefully and note the contents, and, if supplied with a copy of the General Appendix to the Rule Book to alter or cancel in ink the present instructions on the subject appearing therein, afterwards pasting the amendments in their proper places in the General Appendix to the Rule Book.

Station and Depot Masters are responsible for seeing that copies of the General Appendix to the Rule Book supplied to Signal Boxes, etc., under their supervision, are corrected in accordance with this Circular

B R 30145/31 G.A 31 Op.

(This form must be detached and forwarded to the Head of Department.)

.....Station.

.....1956

RECEIVED copy of Circular G.A.31.Op., dated July, 1956, containing alterations and additions to the General Appendix to the Rule Book.

BRITISH RAILWAYS
(WESTERN OPERATING AREA)

**Alterations and Additions to the
General Appendix to the Rule Book**

To come into operation on receipt.

The following instructions to be inserted on page 70:—

**INSTRUCTIONS FOR RUNNING AND WORKING OF THE LENNOX-LOMAX
EARTH AUGER EQUIPMENT.**

1. The machine must only be used by the staff authorised by the Signal Engineer
2. The machine, which is provided with a special match truck, is capable of self-propulsion at a speed of 10 miles per hour (in forward or reverse gear) and, except when operating, must have the match truck coupled. The machine is fitted with three types of brakes, viz. Hydraulic, Transmission and hand screw wheel. The match truck has a hand brake only.
3. When travelling under its own power the machine or the match truck, as the case may be, must carry a white head lamp and a tail lamp, which must be lighted as necessary. A red flag must be displayed on the rear vehicle by day. The machine must be equipped with red and green hand signal flags, not less than 12 detonators, a hand lamp (lighted when necessary) and a sprag. A portable telephone or "walkie-talkie" apparatus must also be available.
4. When boring operations are required to be carried out an absolute occupation of the line concerned must be arranged. The service on which the machine will be worked to the site of operations and the occupation required must be pre-arranged with the District Operating or District Traffic Superintendent concerned.
5. The machine must be worked to the agreed place near the site of work, or vice versa, by freight train carrying "F" or inferior headcodes and be marshalled next inside the Guard's brake van. The match truck may be leading or trailing. Should it be necessary to run the machine as a special train hauled by a locomotive a brake van must always be provided at the rear, in which a Guard must ride.
6. When the machine is proceeding under its own power to the site where it is required to work the match truck must always be attached and both the screw and chain couplings must be used for the purpose. Such movements must be confined to the shortest possible distance but if it is necessary for the machine to pass completely through one or more sections it must be signalled and dealt with as a Through Ballast Train. When running in a multiple-aspect signalling area automatic signalling must be suspended and the machine dealt with in accordance with T.C.B. Regulation 15. Where an Inner Home Signal is provided the "Is Line Clear?" signal must not be accepted until the Home signal can be lowered.
7. The machine must only be moved under its own power by the Driver who has been passed as competent by the Signal Engineer and he must be accompanied by a man who has been passed as competent by the Motive Power Department in (a) knowledge of the route and (b) protective duties of a Driver. A member of the crew of the machine who has been passed by the Operating Department as competent to carry out the protective duties of a Guard must always be present.
8. The machine must not be relied upon to actuate track circuits and Rule 55 must be observed in all cases by the man acting as Guard. When the machine is detained whilst waiting acceptance by the box in advance it must not draw forward to the signal controlling the entrance to the section ahead or to an Intermediate Block Home signal but must be held opposite the box. No train must be allowed to follow the machine towards the Intermediate Block Signal until "Train out of Section" has been received.

9. A good look out must be kept when approaching level crossings.
10. No movement must be made past stop signals otherwise than with the consent of the Signaller.
11. Protection at the site of boring operations must be in accordance with Rule 217.
12. A portable telephone or "walkie-talkie" sets must be provided to enable contact to be maintained between the site of operations and the signal box in the rear.
13. When the machine is working, trains may pass on an adjoining line without restriction except as provided for in Clause 14.
14. Should boring between running lines be required to be carried out or should there be any possibility that the operations will obstruct the opposite or adjoining line, prior notification must be given to the Operating Department and a responsible member of the Operating Department must be present and the operations must not be commenced without his permission. Before giving such permission he must ascertain from the Signaller in the rear that no train is approaching on the opposite or adjoining line and the Signaller, before agreeing to obstruction of such line, must comply with the provisions of Block Regulation 13. The line affected must, in addition, be protected by a Handsignaller in accordance with Rule 217.
15. When the opposite or adjoining line is clear, the Signaller must be advised and such line must not be further obstructed until the provisions of Clause 14 have again been complied with.
16. At the conclusion of boring operations the machine and match truck will be removed from the section under the power of the machine. If the machine is returned to the signal box in rear, the Driver must bring it to a stand before reaching the detonators protecting the work. The conductor must proceed on foot to obtain the Signaller's Wrong Line Order authorising return to the signal box. The Signaller Department person in charge must give the Signaller an assurance that the section is clear of obstruction. (G.A.32.Op-10/56. OM12832)

S. G. HEARN,

October, 1956.

Chief Operating Superintendent.

Each member of the Staff receiving a copy of this Circular is required to read carefully and note the contents, and, if supplied with a copy of the General Appendix to the Rule Book to alter or cancel in ink the present instructions in the subject appearing therein, afterwards pasting the amendments in their proper places in the General Appendix to the Rule Book.

Station and Depot Masters are responsible for seeing that copies of the General Appendix to the Rule Book supplied to Signal Boxes, etc., under their supervision, are corrected in accordance with this Circular.

B.R. 30145/33 G.A.32.Op.

(This form must be detached and forwarded to the Head of Department.)

..... Station

1956

RECEIVED copy of Circular G.A.32.Op., dated October, 1956, containing alterations and additions to the General Appendix to the Rule Book.

THE RAILWAY EXECUTIVE (WESTERN REGION)

August 11th, 1951.

Dear Sir,

STANDARDIZATION OF PASSIONATE CONGRUENCE.

In connection with future building programmes, further British Standard Coaches, similar to those working in the "Merchant Venturer", are expected from S. 100. At the present they must not be worked to other locations, as they are not suitable for use.

Dimensions of the Coaches are as follows :

- (1). "1" long wheel base, with bogie centres not more or less than 46' 6"
- (2). "2" long wheel base, with bogie centres not more or less than 41' 0"

Passenger carrying Stool, except for the Open First and Open
 this, shall be designated by the letters 'F', 'FV', 'FV' or 'FV' or
 prefixed by the letter 'M', 'H', 'E', 'S' or 'SC'.

The following restrictions are imposed on the use of the Standard British Railway (SBR) by the British Railways Board, and I shall be glad if you will make the necessary arrangements for the use of the SBR by the British Railways Board.

Copy sent:-

Chute

109

- 6

- 5

W. A. Foreman

A. Dwyer

Malang.

2MR Sigs

Arthur Sincere

(1)

P.T.O.

LINES TOTALLY PROHIBITED.

Highworth Branch.
Culm Valley Branch.
Loos Branch.
Liskeard - Loos Branch.
North Liskeard Branch.
Tranmere Branch.
Keyham - H.M. Dockyard.
Burry Port and Gwendreath Valley 3 (Burry Port - Cwmawr).
Shipston-on-Stour.
Pontcysyllte Branch.
Cleobury Mortimer and Eton Priory Light Railway.
Cleobury Branch.
Criggon Branch.
Vron Branch.
Carmarthen and Minera Line (Brymbo-Terminus).
Carmarthen (Blaenavon Bay).
Carmarthen Valley Line and Branches.
Carmarthen Victoria Station.
Carmarthen (MFA Line) Up Line through Station.

LINES TOTALLY PROHIBITED.

N. & S. Wales (N. & S. Wales))
Valley, Liskeard - Cleobury Mortimer)
In South Wales.)
Donau Ford Rd. S3 to Ebbw Vale Jcn.)

Passing
Restrictions.

Will you kindly issue the necessary instructions to all staff
under your control and acknowledge receipt.

Yours truly,

L. Edwards

March 30th, 1949.

$\frac{1}{2} \left(\frac{1}{2} + \frac{1}{2} \right) = \frac{1}{2}$

CLASS OF C.I.T.	W.3. TYPE	ENGINE NOS.	M.P. CLASS.
O-6-CT 26xx Rhy.	41	30-75	4F.T.
	26	76 - 83	3P.T.
	41	155	4F.T.
	41	231, 240-277	3F.T.
	41	203-220*236, 278, 299	4F.T.
	26	303-322*335-399	1P.T.
	41	422-428 *	4F.T.
	41	431-436 *	3F.T.
O-6-CT 57xx	42	36xx, 46xx, 96xx, 37xx, 37xx }	4F.T.
		87xx, 77xx, 87xx, 97xx }	
417 ^{1/2}) 1501)	44	907, 1703-31, 1752-64 }	3 F.T.
		1799, 18xx 2721-99 }	
		-47, 1773-89 }	2 F.T.
	45	1531	0 F.T.
		, 20xx }	2 F.T.
2021)	46	2100-60, 2181-90 }	0 F.T.
		1361-65 }	
	23		1
	57		2
	43		4
	29	(Narrow gauge)	-
Barry	17	784	0 F.T.
N. & B.	"	1715	3 F.T.
S.H.T.	"	1146-1147	1 F.T.
B.P.	"	2162 - 2168	2 F.T.
		2176/92/96/98	1 F.T.
		2193-95, 2197	0 F.T.
O.M.	"	28, 29	2 F.T.
Rhy.	"	90 - 96 *	4 F.T.
A.D.	"	666, 667	3 F.T.
T.V.	"	193 - 195	1 F.T.
L. & M.M.	"	803	2 F.T.
		359	1 F.T.
Cardiff	"	681-684	4 F.T.
W.C.P.	"	5	0 F.T.
O-4-2T 517, 14xx	28	3571/5/7, 14xx 52xx	1 P.T.
O-4-OT 1101	48	1101 - 06	3 F.T.
Cardiff	49	1338	0 F.T.
S.H.T.	"	1142	1 F.T.
		1140/1/3/4/5	0 F.T.
P. & M.	"	1150 - 1153	0 F.T.
O-6-O Diesel	99	15100-06	U

* In process of re-numbering to series shown.

BRITISH RAILWAYS

(WESTERN REGION)

(For the use of the Executive's employees only.)

NEW GROUND FRAME BRABAZON LEVEL CROSSING FILTON WEST JUNCTION.

On Monday, November 15th, 1948, between the hours of 8.0 a.m. and 12.0 noon, or until completion, the Signal Engineer will be engaged in bringing into use a new two-lever Ground Frame to be known as **B.A.C. Ground Frame** and located in the Bristol Aeroplane Company's Gate Control House which is situated on B.A.C. property on the Down side of the Down Main Line between Filton West Junction and Henbury, at approximately 113m. 28ch.

The Ground Frame will be electrically released by Interlocking Lever No. 24 from Filton West Junction Signal Box and will work the level crossing gates which open parallel to the railway lines as indicated in the sketch shown overleaf.

Telephonic communication will be provided between Filton West Junction Signal Box, the Level Crossing Gate Control House and the B.A.C. Flying Control Tower.

District Inspector Old, Bristol, to make all arrangements for safe working in accordance with Rule No. 77, and provide any necessary handsignalmen.

Instructions for Working.

The Level Crossing is provided to allow the passage of Aircraft to and from the runway which is on the Up side of the railway line.

A B.A.C. employee will be in charge at the Level Crossing Gate House and will operate the levers controlling the level crossing gates.

The Flying Controller will advise the Filton West Junction Signaller by telephone as long in advance as possible the approximate time it is anticipated that aircraft will require to pass over the Level Crossing. When the aircraft have been manoeuvred adjacent to the level crossing gates the Flying Controller will telephone to the signaller advising him that the aircraft are ready to cross the railway line. When the signaller is in a position to allow the level crossing to be used he must operate the Interlocking Lever (No. 24), which releases the Ground Frame levers in the Level Crossing Gate Control House and must advise the Flying Controller that this has been done. The Flying Controller will telephone the necessary advice to the B.A.C. employee in charge at the Level Crossing Gate House to open the level crossing gates. When the Level Crossing is again clear for the passage of trains and the gates have been closed the Flying Controller will advise the signaller by telephone and the Interlocking Lever (No. 24) must be restored to its normal position.

Should aircraft come to a stand on the level crossing through failure, the Flying Controller will immediately advise the signaller by telephone in order that the necessary additional protective measures may be taken.

[The above will be included in the next supplement to the appendix to No. 4 Section of the Service Time Tables.]

ACKNOWLEDGE RECEIPT TO HEAD OF DEPARTMENT.

TEMPLE MEADS STATION,
BRISTOL, November, 1948.

R. G. POLE,
Superintendent of the Bristol Division.

Received Notice No. S.2198, re New Ground Frame, Brabazon Level Crossing, Filton West Junction.

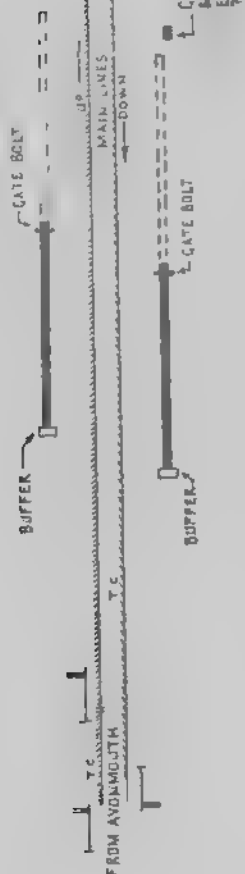
..... Department.

..... Station.

..... Signature.

MR. R. G. POLE,
Divisional Superintendent's Office, Bristol.

FILTON WEST JC B.A.C GRD FRAME.



COPY.

G.W.R

C/-

Chief Goods Master's Office,
Paddington Station.
LONDON, W.2.

Ext 2417.

W/AX.61935.

Information of my telegraph to the General Manager
of the Great Western Railway Company to
advise at the inter-urban railway stations for the purpose of
privately owned and used, power driven on wheels,
either dead or in steam.

It has been decided that the G.W.R. should
be responsible for the provision of the necessary
and power driven on wheels and out of it,
obtained for private use at the inter-urban stations, for
the examination at the inter-urban stations to be made
with the necessary equipment to the G.W.R. locomotives
running on the inter-urban railway, will be examined by the
G.W.R. locomotives on the inter-urban railway.

The information is also applicable to railway
locomotives on the inter-urban railway and other Government
departments or private firms.

This for your information.

In all other respects the G.W.R. will be responsible for
the general management of the inter-urban railway.

For A. Maynard,
signed:- R.J.S.

R.G. Barefoot Esq.,
Bristol.

BRITISH RAILWAYS (Western Operating Area).

Ref. A1/64705.

Dist. Oper. Supt's Office,
Bristol T.H.

14th August, 1950.

Dear Sir,

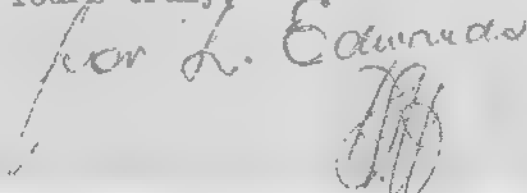
Instructions respecting the Conveyance of Continental
Train Ferry Wagons by Passenger Services.

The Railway Executive have agreed to the conveyance of
Continental Train Ferry Wagons on Passenger Train Services,
subject to the following conditions:-

- (1) Under an international agreement these wagons must be accepted at the Ports, and providing they conform to the coaching stock requirements shown under item (2), and receive a satisfactory examination by the Eastern, North Eastern and Southern Regions as they pass through the Ports, they may run under the same conditions as British Railway Stock marked 'XP'.
- (2) Coaching stock requirements.
 - (1) Fitted with oil lubricated axle boxes or roller bearings.
 - (2) Bearing springs hung in brackets with links and bolts, or on hangers with auxiliary springs, or resting on shoes fitted with retaining bolts.
 - (3) Fitted with automatic vacuum brake or through pipe.
 - (4) Fitted with screw couplings and long buffers.
 - (5) Wheel base to conform to coaching stock conditions applicable to 'XP' wagons (if the wheel base is less than 15ft. the vehicles will automatically be restricted to a maximum speed of 60 m.p.h. under the same ruling as applied to British Railways Stock marked 'XP'.)
 - (6) The vehicles should also conform to the restrictions and prohibitions imposed by the Civil Engineers on account of bridges and load gauge clearances.
- (3) The only prohibitions at present in force in the Bristol District are that these wagons must not work over the Highworth Branch and must not be shunted into the dairy sidings at Frome.
- (4) Special care should be taken in the movement of these wagons over sidings and other lines where gradients exist, and in no circumstances should these wagons be permitted to stand at rest in sidings where gradients occur.

Please note and advise all concerned.

Yours truly,

For L. Edwards


L.M.R. SECTIONAL APPENDIX (CREWE AND SOUTH THEREOF), DATED 1937.

Page 150.—**WAENAYON.**

Delete last paragraph re **Waen Nantyglo Colliery Co's. New Clydach Colliery Siding.**

Page 162 (page 42 of Supplement No. 5).—**Propelling vehicles on running lines outside station limits. Rule 149.**

Delete Brynmawr No. 1 to Waenavon Station.

Page 183.—**Sidings connected with running lines, etc.**

Delete New Clydach Colliery.

Page 188.—**Trains returning from intermediate sidings or stations or stations on Single Lines of railway to the token or staff station in the rear.**

Delete New Clydach Colliery.

CONVEYANCE OF TANK WAGONS.

Clause 1 of the instructions on page 177 of the General Appendix to the Rule Book to be amended to read as follows:

Private Owners' tank wagons having one star painted on each side of each tank, former G.W.R. tank wagons and Messrs. Allsopps (Beer) tank cars, (loaded or empty), also Road Rail and Demountable tanks (loaded or empty), loaded on wagons specially constructed for their conveyance with one star painted on the rail chassis only, may be conveyed on trains scheduled to carry "E" or inferior to "E" headcodes, irrespective of the class of traffic the tanks contain, as indicated on the wagon label.

(E. 76159/5.W.)

"INSTANTER" PATENT COUPLER.

The following to be substituted for the last paragraph of the instruction in regard to "Instanter" couplings shewn on page 159 of the General Appendix to the Rule Book:

Instanter Couplings.

A number of vehicles are fitted with Instanter couplings which can be in either of two positions, viz.:

A—Short position.

B—Long position.

Such vehicles may be attached to trains as shewn below:

(a) Trains Conveying Passengers.

One vehicle only, fitted with Instanter Couplings, may be attached to trains conveying passengers provided it conforms in other respects to coaching stock requirements and subject also to the instructions relative to the conveyance of four-wheeled vehicles on passenger trains. In such circumstances the screw couplings of the adjacent vehicles must be used. This does not modify the authority (where given) to attach at the extreme rear one vehicle not conveying passengers and not fitted with the continuous brake or through pipe.

(b) Coaching Stock Trains Not Conveying Passengers.

Vehicles fitted with Instanter Couplings and conforming to coaching stock requirements may be conveyed in trains composed of coaching stock, other than those conveying passengers, but the screw couplings of the vehicles on each side of the Instanter fitted vehicle must be used. When two or more vehicles fitted with Instanter couplings are marshalled together in such trains, the screw couplings of the vehicles on each side of the Instanter-coupled vehicles must be used and the Instanter coupling, or couplings, in use, must be in the short position.

The instructions respecting the conveyance of four-wheeled vehicles will apply in respect of the running of Instanter-fitted vehicles on empty coaching stock trains.

(c) Freight Trains.

The couplings must be in the long position during shunting operations, and also when used in ordinary freight trains except when the vehicles are conveying cattle when the couplings of the cattle wagons must be in the short position.

Vehicles with Instanter Couplings can be used in fitted freight trains in exactly the same way as screw-coupled vehicles, but when used in the fitted portion of the train the screw coupling of the adjacent vehicle or vehicles should be used if possible. If this is not possible the Instanter Coupling must be in the short position.

Under no circumstances must an Instanter Coupling be used for attaching to vehicles fitted with the Buck-eye Coupling, and a screw coupling must be used.

APPENDIX TO No. 12 SERVICE TIME TABLE.

Loose Runaway Catch Points or Dead End Catch Slidings.

The following entry on page 24 to be deleted:

Station	Up or Down Line	Where situated	If connected with and worked from Signal Box	Gradient One in	Remarks
Co.wall	Up	Ledbury end of Up Platform 356 yards to rear of Up Main Inner Home Signal.	No.	148	*

LIMIT OF SHUNT BOARD.

The Instructions shewn on page 48 to be deleted.

GILBERT MATTHEWS

Operating Superintendent

Paddington, 2nd August, 1950.

G. . R.

Director's Office,
R.F.S. L. G. 17.

14th. November, 1945.

R.F.S.:- A1/11,13.

Dear Sir,

Conveyance of "Warflats" and "Warwells"
on "C" Headlamp Freight Trains.

With reference to my circular letter of January 3rd, 1945 under the heading of "Coupling of Vacuum of full trains of Warwells".

(1). Loaded Vehicles.

Although there is no objection to the whole vacuum brake system coupled up to the engine on special freight trains run with A.P.V.'s loaded on Warflats or Warwells, the speed of the trains must be limited to 20 miles per hour (see R. L. C. booklet of instructions re A.P.V.'s) and in such circumstances the trains must carry "J" headlamps although the train may be composed wholly or partially of vacuum braked vehicles.

Loaded vehicles may not be conveyed on trains booked to carry "C" or "D" headlamps.

(2). Empty Vehicles.

Empty Warflats or Warwells may be conveyed on "J" or "D" headlamp trains and Clause 3 of the instructions referred to in 173 of the General Appendix will in due course be altered to read:-

Empty Great Western and Empty "Foreign" Companies Oil axle-box special wagons of types enumerated in Clauses 1 and 2 must not be conveyed on any freight train carrying superior than "J" headlamps, except the following in temporary use:-

WARFLAT

WARWELL.

Yours truly,

L. K. G. 120
H. 120

PRIVATE AND NOT
FOR PUBLICATION.

GREAT WESTERN RAILWAY.

NOTICE NO A1/-

Divisional Superintendent's Office,
BRISTOL (I.R.) G.W.7.
11c 3 1 12.

WORKING OF STANDARDISED CARS.

Referring to the instructions contained on page 133 of the General Order, No. 1, dated 1st March 1900, Car No. 1, which is supplied with electrically lighted tail lamp and the following instructions will apply:-

"These cars will not carry the G.W. standard head or tail lamps. "A" headlamps are provided by two white lights (horizontally at platform level) and "B" headlamps by one white lamp (at top centre), and these must be exhibited at the leading end when required. The tail lamp will be a G.W. standard tail lamp case fitted with an electric bulb and provided with a short length of cable and two pin connecting plug. The corresponding connecting socket is fitted to the body of the car immediately above the tail lamp bracket and when the tail lamp is placed in position, the Guard must ALWAYS connect the connecting plug to socket and press the pins well home.

"Headlamps, which must be exhibited after sunset, during fog or falling snow or before passing through a tunnel, must be switched on by the Driver.

"The Guard, or man acting as Guard, will be responsible for placing the tail lamp in position, making the electrical connection and switching on the light after sunset, during fog or falling snow, and before passing through a tunnel.

"The electrically fitted lamps are branded on the chimney cap thus:- "Rail Car Electric".

"A spare oil tail lamp is to be carried on each car for use in emergency and when standing in sidings."

ALL CONCERNED TO NOTE.

For P. S. P. G.
S. S. A.

PRIVATE AND FOR
OFFICIAL USE ONLY

GRAND CENTRAL RAILWAY. OFFICE NO. 1/-
Divisional Superintendent's Office,
G.W. (G.C.R.) G.C.R. 7.
G.C.R. 7.

WORKING OF STR. LAMPED CARS.

Referring to the instructions contained on page 133
of the G.W. (G.C.R.) G.C.R. 7. dated March
1907, and the instructions contained on page 133
of the G.W. (G.C.R.) G.C.R. 7. dated March 1907,
the following instructions will apply:-

"These cars will not carry the G.W. standard head
or tail lamps. "A" headlamps are provided by two
white lights (horizontally at platform level) and
"B" headlamps by one white lamp (at top centre),
and these must be exhibited at the leading end when
required. The tail lamp will be a G.W. standard
tail lamp case fitted with an electric bulb and
provided with a short length of cable and two pin
connecting plug. A corresponding connecting socket
is fitted to the body of the car immediately above the
tail lamp bracket and when the tail lamp is placed in
position, the Guard must ALWAYS connect the connecting
plug to socket and press the pins well home.

"Headlamps, which must be exhibited after sunset,
during fog or falling snow or before passing through
a tunnel, must be switched on by the Driver.

"The Guard, or man acting as Guard, will be res-
ponsible for placing the tail lamp in position, making
the electrical connection and switching on the light
after sunset, during fog or falling snow, and before
passing through a tunnel.

"The electrically fitted lamps are branded on the
chimney cap thus:- 'Rail Car Electric'.

"A spare oil tail lamp is to be carried on each car
for use in emergency and when standing in sidings."

ALL CONCERNED TO NOTE.

ASL

Ref:- A1/41,831.

1st. June, 1946.

Dear Sir,

Running of Four-wheeled V. Vans in Express Trains.

Referring to the circular letter of February 17th, 1945, the Chief Mechanical Engineer has been instructed by the General Manager to remove "XP" marking from a number of G.V. four-wheeled vehicles until certain structural modifications have been carried out. The obliteration of the "XP" marking is being carried out immediately at outlying points throughout the Line.

The vehicles concerned have a wheel base of less than 15 feet. (Passenger Cattle Vans with a wheel base of 16 feet), and are as under:-

Horse Boxes.
 Passenger Cattle Vans ("BEETLES C").
 Motor Car Trucks ("DA OS B").
 Goods Vans. ("VANFITS").
 Passenger Fruit Vans. ("PASFRUIT" and "PASFRUIT C").
 Banana Vans (with 12 foot wheel base).
 Goods Fruit Vans (with 10 ft. wheel base & normal carrying capacity of 6 tons).
 Open Carriage Trucks ("SCORPIONS" and "SCORPIONS B").

The modifications referred to above will be carried out as soon as practicable; as and when such are effected the "XP" markings will be restored. For the purpose of distinguishing the vehicles dealt with, the restored "XP" lettering will be enclosed in a square consisting of a white line:-

X. P.

The vehicles can then again be used in accordance with instructions in respect of "XP" vehicles contained in General Appendix Supplement G.A.4 and 16.

The whole of our stock of Horse Boxes, Passenger Cattle Vans, Open Carriage Trucks, and "DA OS B", also the majority of our Passenger Fruit Vans are to be so dealt with. It will, therefore, be necessary forthwith, until the modifications have been effected and "XP" markings have been restored, for these vehicles to be dealt with as laid down for "Non-XP" Vehicles in General Appendix Supplements G.A.4 and 16. So far as the modifications are concerned, priority is being given to Horse Boxes, some of which have already been turned out by Swindon with the new markings.

The following "XP" four-wheeled vehicles, with a wheel base of less than 15 feet, are already equipped with suitable springs, and their dimensions are such that it is not proposed to remove the "XP" markings from these :-

Container Trucks	900
Fish Vans	45
Covered Goods Vans.	1,216
Goods Cattle Wagons.	2,093
Meat Vans.	813
Goods Fruit Vans.	300

The remainder of our four-wheeled stock, all of which have a wheel base of less than 15 feet, are suitable for "XP" running, and will remain so marked are :-

Fish Vans ("BLOATERS").
 Covered Goods Vans ("PYTHONS L")
 Louvre Sided Milk Trucks. ("SIPHONS C")
 Passenger Fruit Vans. ("MULBERRY")
 Passenger Milk Trucks ("MILK", "MILK C" and "MILK L")
 Goods Motor Trucks ("ASHES" & "DASHES A").

The loading of all G.W. four-wheeled "XP" vehicles with a wheel base of less than 15 feet is to be restricted to 3 tons, except when to be conveyed by freight train service, and suitable G.A. instruction is being issued to cover this feature.

The 2-20p.m. (SX), 2-38p.m. (R), parcels Paddington to Plymouth is scheduled to carry "A" and "B" parcels Paddington to Diocet, and "C" parcels Diocet to Plymouth. For information of the public, and to carry "C" parcels from Diocet to Plymouth and special attention given to the service, and to the view to maintaining, as far as practicable, the service at the same times.

The 4-10p.m. parcels train (2-10p.m. Diocet to Plymouth) is scheduled to carry "A" and "B" parcels Diocet to Plymouth and "C" parcels Plymouth to Diocet. This train is to carry "C" parcels from Plymouth to Diocet and special attention given to the service, and to the view to maintaining, as far as practicable, the service at the same times.

It is appreciated that the decision to prohibit the rolling stock mentioned from Class 'A' passenger trains, which has been arrived at on the grounds of safety after exhaustive investigation, will entail considerable difficulty in providing satisfactory services in many cases, particularly in respect of horses and live stock, but the best will have to be done in the circumstances during the transition period.

These instructions apply to G.W. stock only, and Foreign Companies' vehicles carrying "XP" markings will be dealt with as at present.

Please arrange accordingly so far as you are concerned, and acknowledge receipt.

Yours truly,

R. G. Pollock

FOR THE USE OF COMPANY'S EMPLOYEES ONLY

GREAT WESTERN RAILWAY

GENERAL APPENDIX

TO THE

RULE BOOK

To come into operation on August 1st, 1936.

Corrected to GA16 May 6th 1946

Corrected to GA14

Corrected to GA14

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JAMES MILNE,
General Manager.

RULES IN FORCE ON JOINT LINES.

The following list showing which parent Company's Rules apply on the Joint Lines is given below for the guidance of the staff concerned:—

	Joint Line.	Companies' Rules Applicable.
Abersychan and Talywain	G.W.
Brecon and Merthyr and L.N.W.	G.W.
Brynmawr and Western Valleys	G.W.
Chester and Birkenhead	L.M.S.
Clee Hill Branch	G.W.
Clifton Extension	L.M.S.
Easton and Church Hope	G.W.
G.W. and G.C. Joint Committee	Joint
Halesowen	L.M.S.
Hammersmith and City	G.W.*
Nantybweh No. 2 and Rhymney Bridge (inclusive)	L.M.S.
Rhymney Joint and Rhymney Bridge (exclusive)	G.W.
Severn and Wye—		
Portion maintained by L.M.S. Railway		L.M.S.
Portion maintained by G.W. Railway	G.W.
Shrewsbury and Hereford	G.W.
" " Wellington	G.W.
" " Welshpool	G.W.
Tenbury Line	G.W.
Vale of Towy	G.W.
West London	G.W.*
West London Extension	G.W.
Weymouth and Portland	G.W.
Wrexham and Minera—		
Brymbo (exclusive) to Coed Talon		L.M.S.

G.W. staff working over the above-named Joint Lines on which the G.W. Company's Rules do not apply must be in possession of a copy of the instructions governing G.W. staff working over other Companies' systems.

to the G.W. Rule Book.

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SECTION I (a).

ADDITIONS TO THE STANDARD RULES.

to travel in Brake Vans in the course of their normal duties, passenger or goods brake vans if in possession of :—

1. A valid periodical pass ;

2. A valid Operating Superintendent ;

3. An "All Line" gold medal, issued by the Railway Executive.

4. A valid paragraph no member of the Public will be permitted to travel in a Brake Van without the sanction of the Operating Superintendent and provided the Operating Superintendent has arranged for a suitable indemnity to be signed.

5. A valid paragraph no member of the Public will be permitted to travel in a Brake Van without the sanction of the Operating Superintendent and provided the Operating Superintendent has arranged for a suitable indemnity to be signed.

6. A valid paragraph no member of the Public will be permitted to travel in a Brake Van without the sanction of the Operating Superintendent and provided the Operating Superintendent has arranged for a suitable indemnity to be signed.

7. A valid paragraph no member of the Public will be permitted to travel in a Brake Van without the sanction of the Operating Superintendent and provided the Operating Superintendent has arranged for a suitable indemnity to be signed.

8. A valid paragraph no member of the Public will be permitted to travel in a Brake Van without the sanction of the Operating Superintendent and provided the Operating Superintendent has arranged for a suitable indemnity to be signed.

(G.A. 24.—11 49. S 4978 O.M.)

→ G.A.24 See New Rule

17. A valid paragraph no member of the Public will be permitted to travel in a Brake Van without the sanction of the Operating Superintendent and provided the Operating Superintendent has arranged for a suitable indemnity to be signed.

18. A valid paragraph no member of the Public will be permitted to travel in a Brake Van without the sanction of the Operating Superintendent and provided the Operating Superintendent has arranged for a suitable indemnity to be signed.

(G.A. 10. 3/42. O.M. Min. 119)

Reference to the following to be made on page 2:—

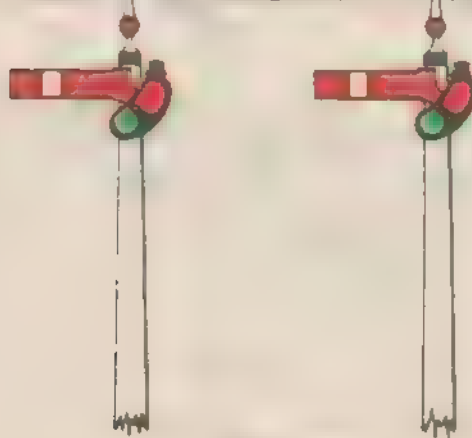
COLOUR LIGHT DISTANT SIGNALS.

Where a colour light Distant signal is placed below a semaphore stop signal, no light will be exhibited in the Distant signal when the semaphore arm above it is in the danger position, but a yellow or green light will be exhibited when the semaphore arm is in the clear position.

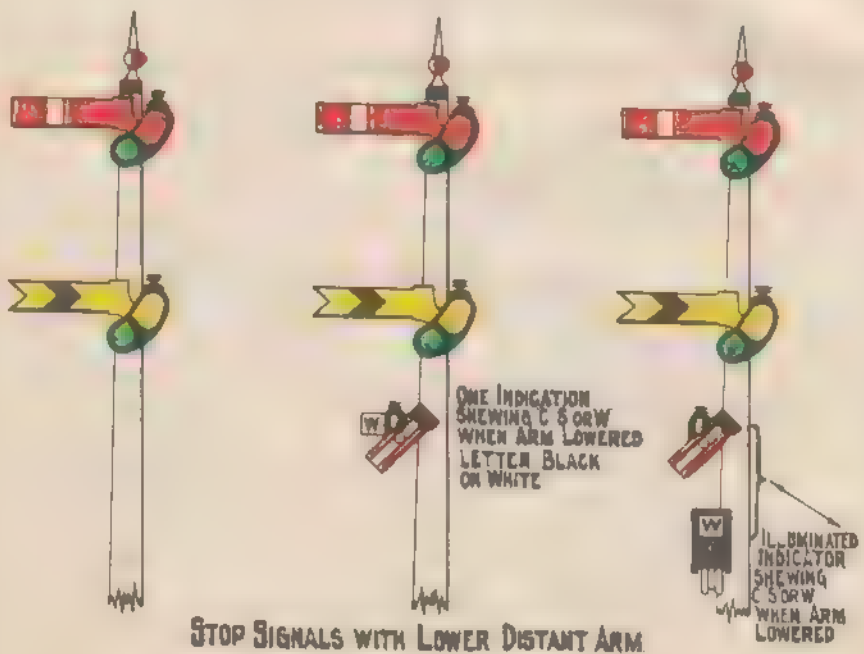
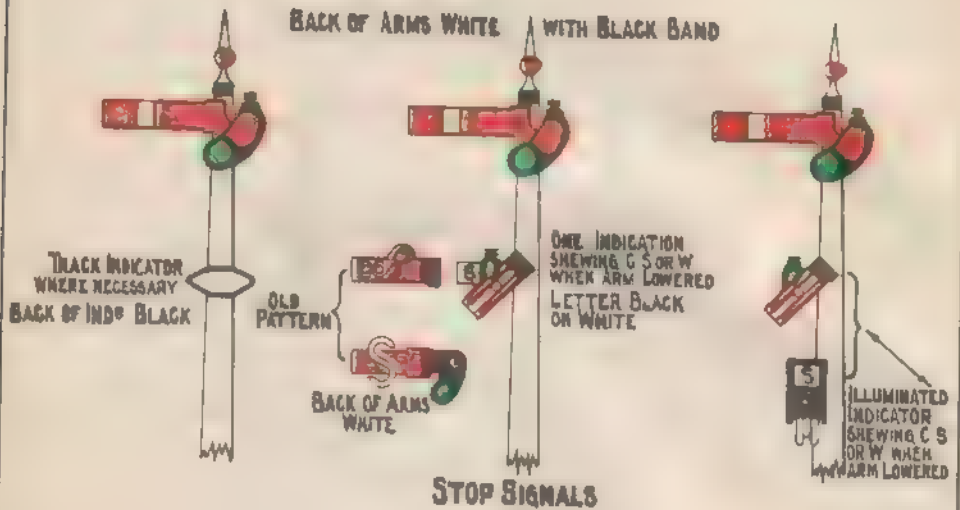
At night no green light will be exhibited by the semaphore signal when placed to the clear position.

(G.A.30 Op.—9/45 L.65733/33)

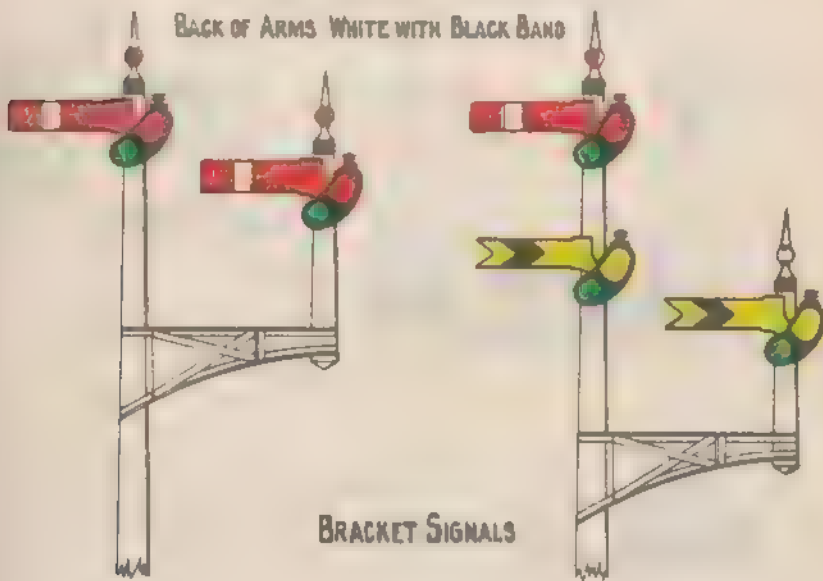
BACK OF ARMS WHITE WITH BLACK BAND



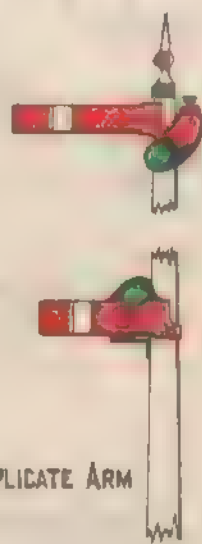
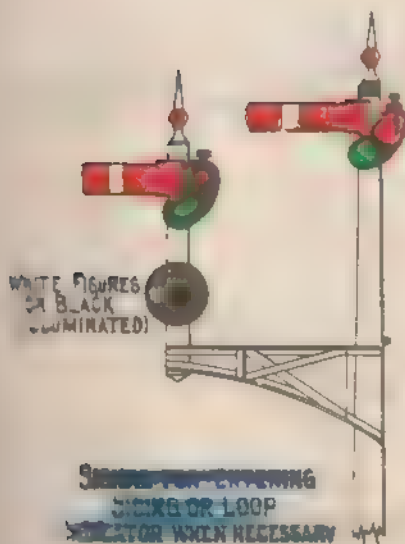
STOP SIGNALS



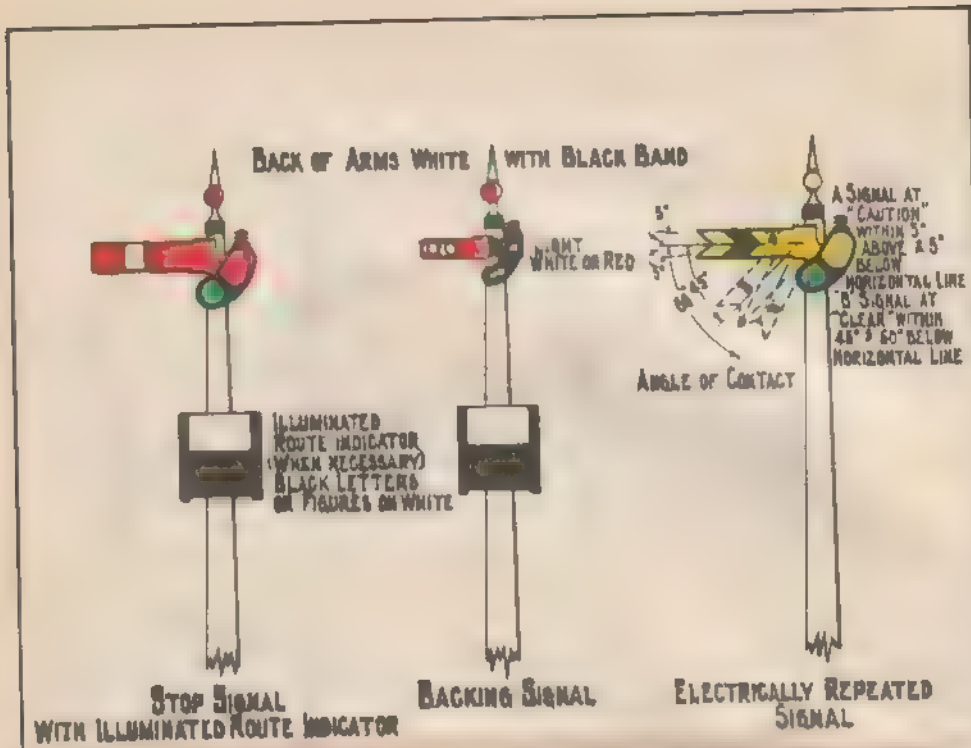
BACK OF ARMS WHITE WITH BLACK BAND



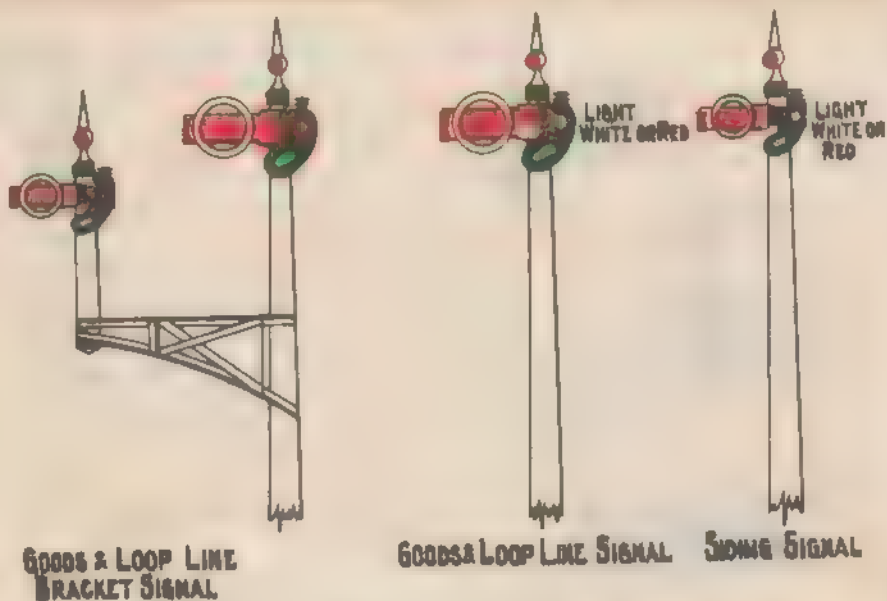
BRACKET SIGNALS



DUPLICATE ARM



Discs, elevated where necessary, will be used to control backing movements instead of backing signals. The revised arrangement will apply in connection with new signalling schemes, signal alterations and renewals. (G.A. 25—1/50 R.E. Stand—L. 56267/89.)



New pattern signals will not carry rings on the arms. The revised arrangement will apply in connection with new signalling schemes, signal alterations and renewals. (G.A. 25—1/50 R.E. Stand—L. 56267/89.)

BACK OF ARMS WHITE WITH BLACK BAND



"DANGER"



"CAUTION"



"CLEAR"



POWER OPERATED UPPER QUADRANT
THREE POSITION SIGNALS.

BACK OF ARMS WHITE WITH BLACK BAND



DISTANT SIGNAL
FIXED AT CAUTION



SLIP SIGNAL



SIGNAL NOT IN USE



SMALL



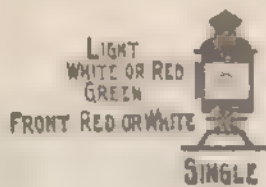
LARGE

BANNER STOP SIGNALS
ILLUMINATED



ELECTRICALLY WORKED
BANNER REPEATING SIGNALS
ILLUMINATED

POINT DISCS



INDEPENDENT DISCS OLD PATTERN LIGHT WHITE OR RED

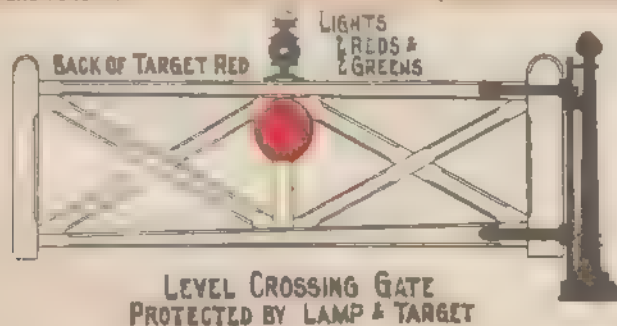


NEW PATTERN LIGHT WHITE OR RED

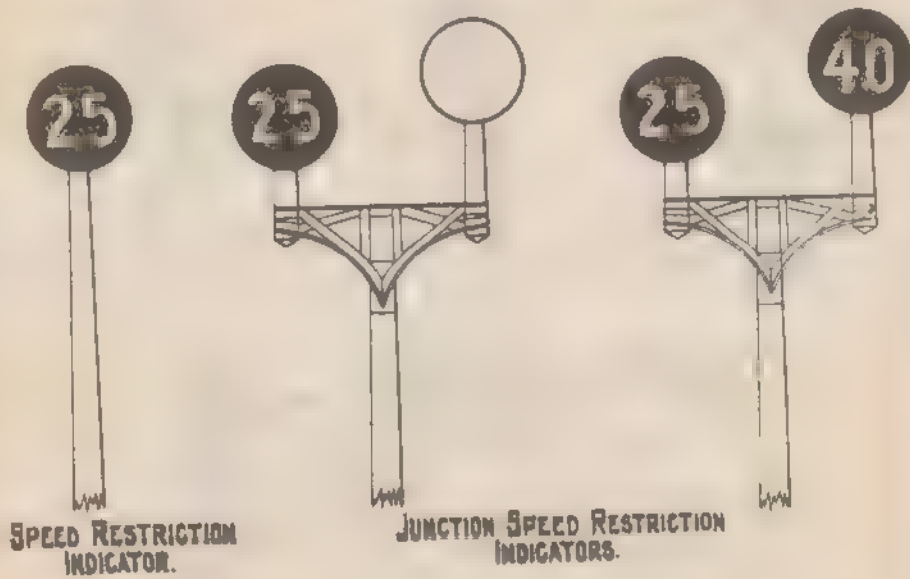
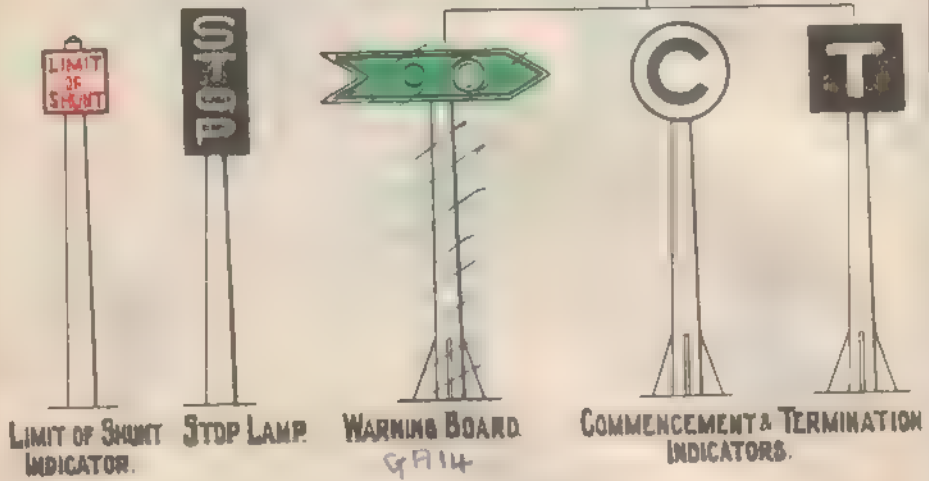


The Illustrations headed "New Pattern" to be amended to show either a red band with red light or yellow band with yellow light and the heading to read "New Pattern Light Yellow or Red." The above revised arrangement will apply in connection with new signalling schemes, signal alterations and renewals.

(G.A.25.—1 50. R.E. Stand :—L.56267 89.)



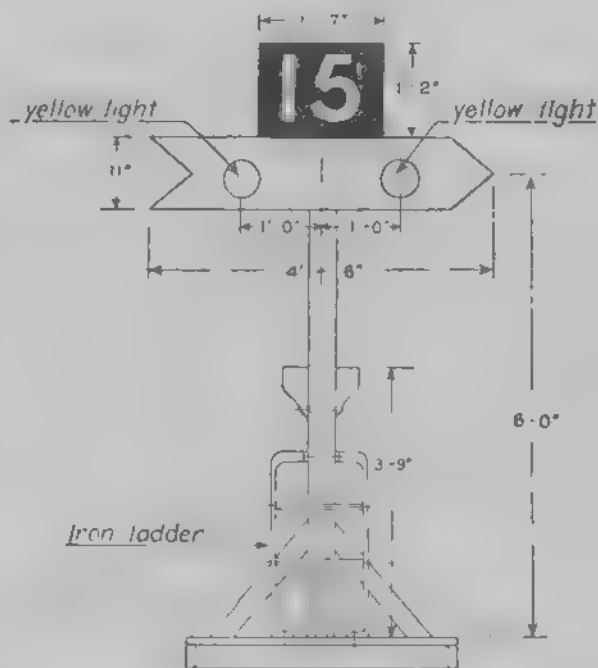
RULE 218



Page 10.

Delete the illustration and description of a Warning Board as shown in G.A.14 and substitute the following :—

**NEW TYPE ARROW WARNING BOARD
WITH ILLUMINATED SPEED INDICATOR.**



The permissible Speed is indicated by lamp placed over the Arrow Warning Board with a stencil in front of an Opal Screen.

The Board to be painted Yellow.

The Post to be painted White.

(G.A.23—7/49. L.K.1/8601/32.)

COLOUR-LIGHT SIGNALS,

DISTANT SIGNAL



DISTANT SIGNAL (1)
AT "CAUTION."

DISTANT SIGNAL (2)
AT "ALL RIGHT."

STOP SIGNAL



STOP SIGNAL (3)
AT "DANGER."

STOP SIGNAL (4)
AT "ALL RIGHT."



STOP SIGNAL (5)
AT "DANGER"

ILLUMINATED INDICATOR SHEWING
MINIATURE ARM
IN "OFF" POSITION,
WITH LETTER "C,"
"S," OR "W," AS
REQUIRED.



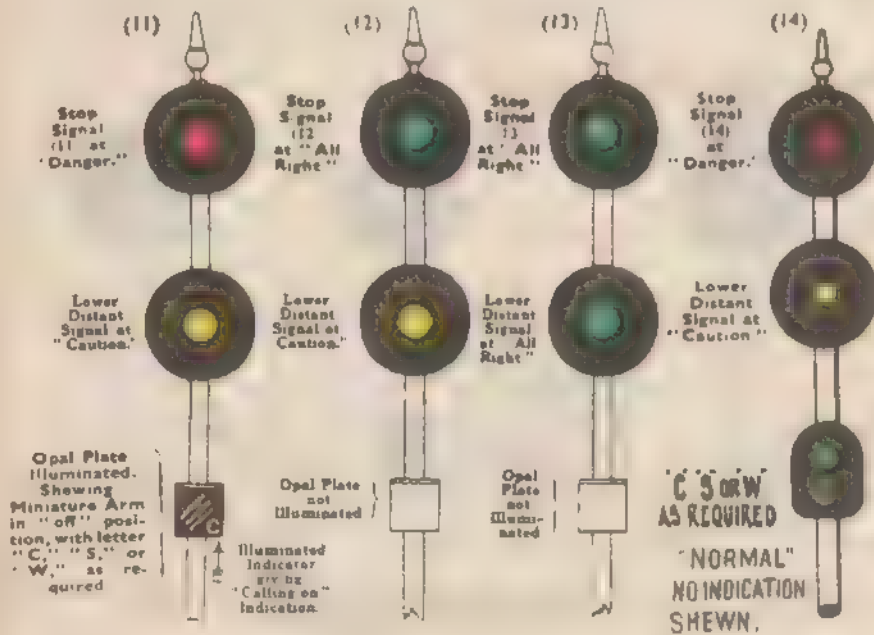
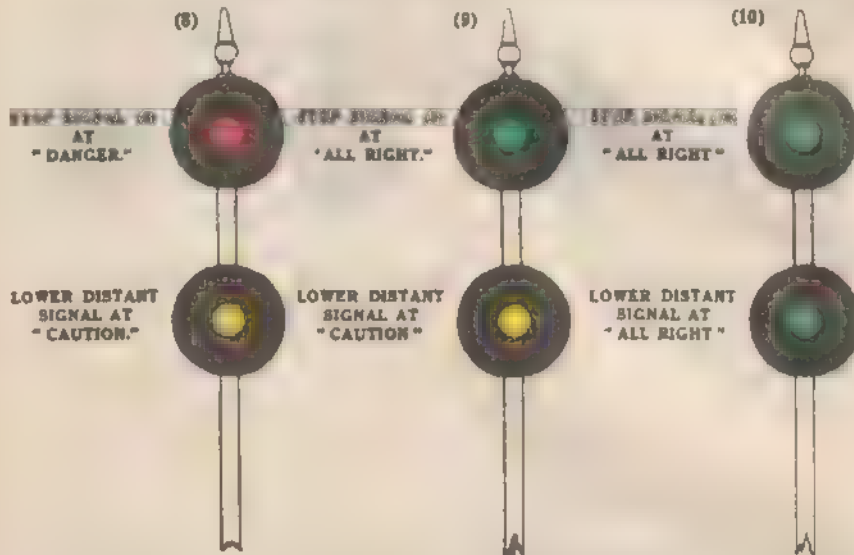
STOP SIGNAL (6) AT "DANGER" AND
ILLUMINATED INDICATOR GIVING
"CALLING ON" INDICATION.



STOP SIGNAL (7) AT
"ALL RIGHT."

FOR RELIEF AND MAIN LINES.

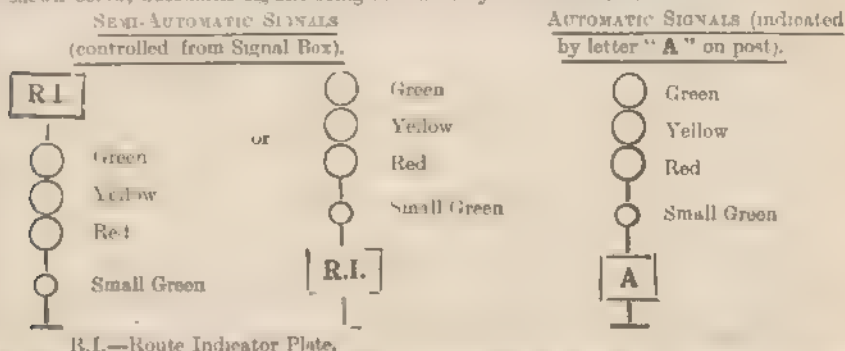
STOP SIGNALS WITH LOWER DISTANT ARMS



ADDITIONS TO THE STANDARD RULES.

SIGNAL LIGHT SIGNALLING INSTALLATION UP AND DOWN CARRIAGE LINES BETWEEN PADDINGTON AND OLD OAK COMMON.

Semi automatic and automatic colour light signals are provided on the Up and Down Engine Carriage lines between Paddington and Old Oak Common East Signal Box. The signals are in form shown below, automatic signals being indicated by a letter "A" :—

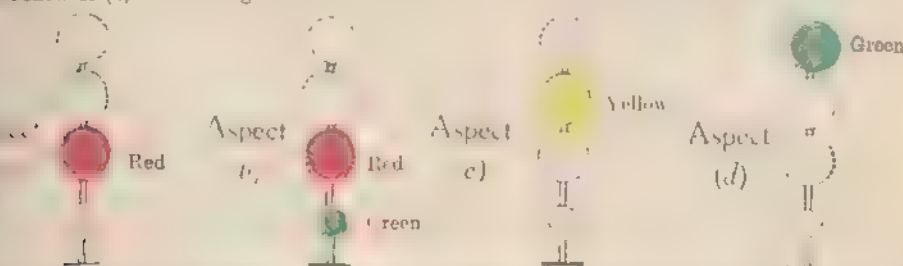


R.I.—Route Indicator Plate.

These signals are provided with a lamp for identification purposes. Semi-automatic signals are controlled from the respective Signal Boxes concerned, in conjunction with track circuiting, and are operated entirely by the passage of trains in the track.

The small Green light is a "Calling-on" signal, and is operated by a short length of track circuit (not 100 feet in length) to the rear of the signal. The "Calling-on" signal shows a "Green" aspect when the signal is cleared, and the provisions of Rule 14 for "Calling-on" must be observed, and the Driver must be prepared to stop at any point in the section ahead where the signal may be shown, or changed to aspect of small green, red, (c) yellow, or (d) green, mentioned below.

The Light Signals consists of a group of three lamps with a smaller lamp underneath and right Aspect exhibited at any one time will be either (a) a Red, (b) a Small Green under a Red, (c) a Yellow or (d) a Green Light as shown below :—



Signals of the Green and Yellow aspects applicable to the Engine and Carriage Lines are as follows :—

Aspect	Meaning
Red	Stop.
Small Green under Red	Section ahead occupied. Proceed cautiously at low speed, prepared to stop short of any obstruction.
Yellow	Line is clear to next signal only. Line is clear two sections ahead.

For movements from the Engine and Carriage lines to the line shown on the Route Indicator, Drivers must be prepared to stop.

For movements to the Engine and Carriage Lines from other lines will be red the green light for such movements will indicate that the road is right for trains to proceed to the Engine and Carriage Line, but not that the line is clear. Drivers must be prepared to stop.

ADDITIONS TO THE STANDARD RULES.

COLOUR LIGHT SIGNALLING—continued.

During fog or falling snow, when visibility is such that enginemmen cannot see far enough ahead to pass the signal, the Driver must stop at the signal, or at the first signal which he can see, and must not proceed until the signal has been cleared. In the case of a signal which is cleared by the Signalman, the Driver must not proceed until he has received the signal.

Should a Driver in clear weather find an illuminated letter "A" on the post showing the line ahead is clear, he must stop short of any obstruction in the section, unless or changed to aspects (b) small green under signal box and report the failure to the Signalman. (or controlled) signal when showing a Red light only, or no light at all, but he can see that the signal in advance may be showing (d) green. He must stop at the next signal (which are distinguished by an or no light at all, but he can see that the signal in advance may be showing (d) green. He must stop at the next

In no circumstances must a Driver pass a signal when showing a Red light only, or no light at all, but he can see that the signal in advance may be showing (d) green. He must stop at the next signal (which are distinguished by an or no light at all, but he can see that the signal in advance may be showing (d) green. He must stop at the next

Rules 35, clause (b iii), 47, 48 and 49.—White Lights in Ground Signals (Disc or Dwarf), Siding Signals and Backing Signals, in place of Red Lights.

Siding Signals, Backing Signals and Ground Signals (Disc or Dwarf) which may be passed in the "On" position are provided with white or yellow lights. When such signals are provided with red lights they must not be passed in the "On" position unless verbal instructions are received from the Signalman, Guard, or Shunter.

Note.—The lights of Calling-on, Warning, Shunt-ahead, Loop and Backing Signals are smaller than the lights of ordinary Running Signals (G A 24—11 49)

GA 24 Rule 35 ~~Distinct Signals. Distinct signals are provided with white, black, or red lights. The lights of the signal must be kept at the "Caution" position when the signal is at the "Caution" position.~~

Keeping Distinct Signals at "Caution" where Permanent Reduction of Speed is ordered. Wherever there is a permanent reduction of speed to 15 miles an hour or less at places situated between a signal and the next signal, or at a place situated between a signal and the next signal, the signal must be kept at "Caution" permanently.

GA 24 ~~When a signal is at the "Caution" position, the signal must be kept at the "Caution" position. The signal must be kept at the "Caution" position when the signal is at the "Caution" position.~~

Distinct Signals. Backing Signals. When a signal is at the "Caution" position, the signal must be kept at the "Caution" position. The signal must be kept at the "Caution" position when the signal is at the "Caution" position.

Rules 47 and 49.—"BACKING" SIGNALS.

"Backing" Signals, where provided, control backing operations over the line in the Wrong direction, and are distinguished thus.—(See diagram—page 6.)

When a Backing Signal is provided, it must be kept at the "Caution" position when the signal is at the "Caution" position. The signal must be kept at the "Caution" position when the signal is at the "Caution" position.

When a Backing Signal is provided, it must be kept at the "Caution" position when the signal is at the "Caution" position. The signal must be kept at the "Caution" position when the signal is at the "Caution" position.

The following to be inserted on page 14:—

Signal Post Signs.

A concerned to please note the following new signal post signs will be exhibited where applicable:—

Letter T in black on white diamond sign.

Letter T in black on white rectangular plate.
Designation of signal in black figures on white rectangular plate.

Fixed on signal to rear of which track circuit is provided and a telephone to signal box is provided in addition.

Telephone to signal box provided (no track circuit).

Provided in colour light signalling areas and for all colour light Intermediate Block Signals.

(G.A.30 Op.—9/54 L.72404/283).

RULE 33 (b).—Page 14. The following to be substituted for the existing entry —

On the Western Region the time signal is signalled daily at 11 0 a.m. and where not received the Station Master must obtain the precise time from the Guard of the first stopping train commencing its journey after 11 0 a.m.
(G.A.24—11 49)

Rule 35 clause (c).

The instruction shown under this heading to be amended to read —

On the Western Region this clause will only apply to the Multiple Aspect Signalling area in the London District
(G.A.30 Op.—9/54)

RULE 50 (c).—On the Western Region a white hand signal is used as follows:—

To acknowledge Guard's white hand-signal—(see addition to Rule 147 in the General Appendix and amplification of Regulations for signalling trains and engines by Permissive Block System over Goods Running Loop Lines and Permissive Lines).

White light held steadily by Signalman

To indicate to Signalman that train or engine is in clear of loop points (see addition to Rule 147 in the General Appendix and amplification of Regulations for signalling trains and engines by Permissive Block System over Goods Running Loop Lines and other Permissive Lines)

White light held steadily by Guard or Fireman

(G.A. 24—11/49)

Rules 59 and 60. The amendments advised in Circular G.A. 10 to Rule 59 clause (a) and first paragraph clause (b) and to Rule 60 first two paragraphs, to be deleted as the Rules have now been amended by Rule Book Supplement No. 13.

(G.A. 12, 4/43, O.C. Min. 2 of 2.)

R.B. Sup. 13.

Rule 59 to be amended as shewn below:—

Clause (a) to read:—

When one or more lanterns are exploded by a train after a stop signal or signal box and a hand signal is exhibited the Driver must, in clear weather, bring his train to a stand and then proceed cautiously towards the place of obstruction or until he receives a signal for his guidance. If, however, the weather is such that he may proceed cautiously towards the place of obstruction or until he receives a signal for his guidance.

Clause (b) the first paragraph to read:—

When a hand signal is exhibited at a stand signal and a yellow hand signal is exhibited the Driver must, in clear weather, bring his train to a stand and then proceed cautiously towards the place of obstruction or until he receives a signal for his guidance. If, however, the weather is such that he may proceed cautiously towards the place of obstruction or until he receives a signal for his guidance.

(G.A. 10, 3/42, Op. Com. Min. 2562).

Rule 60.—The following to be substituted for the first paragraph of this rule:—

When one or more lanterns are exploded by a train after a fixed signal or signal box and a hand signal is exhibited the Driver must, in clear weather, bring his train to a stand and then proceed cautiously towards the place of obstruction or until he receives a signal for his guidance. If, however, the weather is such that he may proceed cautiously towards the place of obstruction or until he receives a signal for his guidance.

If, however, the Driver receives a hand Danger signal, he must bring his train to a stand as quickly as possible and not proceed until he receives a signal for his guidance. If, however, the weather is such that he may proceed cautiously towards the place of obstruction or until he receives a further signal for his guidance.

(G.A. 10, 3/42, Op. Com. Min. 2562).

The following to be inserted on page 15:

Rule 68 (b). A stop signal after being lowered for the passage of a train, may be replaced at Danger before the train has passed it when the Signalman cancels the "Is Line Clear" in accordance with the second paragraph of clause (c) of the "Switching In" instructions in Block Regulation 24.

(G.A. 18, 11/47, O.M. 12354)

Rule 74.—In the case of Long Burning Signal Lamps the lamp which fails should be replaced by a spare lamp

G.A. 24 Sec New Rule

(G.A. 18, 11/47, L.K. 1.)

steadily

steadily
fireman

—11,49.)

ADDITIONS TO THE STANDARD RULES.

~~R 55. Reminder Flaps on Three-position Disc Block Instruments. 1. Movable brass flaps are fitted to the disc block instruments. The normal position of these flaps is turned up.~~ **GA24 Deleted in Error**

Should a signalman have a train or vehicle standing outside his Home Signal (the "Train on line" position) and the "Train at Signal" key is turned down with the flap over the key, which will display the words "Train at Signal," and lock the key in the "Train on line" position.

At places where the "Warning" signal is authorised to be depressed, and it is desired to accept a "Clearing point fouled" position, the "Clearing point fouled" key cannot be depressed.

At places where the "Warning" signal is authorised to be depressed, and it is desired to accept a "Clearing point fouled" position, the "Clearing point fouled" key cannot be depressed.

If it has been placed in the turned down position in accordance with the preceding clause, be raised to enable the "Line clear" key to be operated. When the line is clear to the Home Signal in the case of the flap over the "Train on line" key, and the Home Signal and the clearing point in the case of the flap over the "Line clear" key, the signalman will be responsible for always taking use of these reminders as shown in the instances mentioned.

Reminder Flaps on Tyer's Block Instruments. Where reminder flaps are provided on Tyer's instruments the following instructions must be observed

Instruments Showing Three Indications.

A brass flap is fitted to the button in such a position as to prevent the button being worked and so make it impossible to alter the indication without the flap being removed. The normal position of the flap is away from the button.

The flap must be turned on to the button in either of the following circumstances:

- (1) When a Train or Vehicle is standing outside the Home Signal.
- (2) When the clearing point is fouled.

After the flap has been turned on to the button it must not be taken off until the line is clear to Home Signal, or between the Home Signal and the clearing point, as the case may be.

When the "Warning" signal is authorised to be depressed, and it is desired to accept a "Clearing point fouled" position, the flap may be moved to enable "Train out of section" to be worked, and the "Train out of section" key may be depressed. When the line is clear to the Home Signal, the flap must be turned on to the button.

Instruments Showing Two Indications.

A brass flap bearing the words "Train at Signal" is fitted at the side of the "Train arrived" position.

The flap can be turned over on to the plunger in such a position as to prevent the plunger being worked, and so make it impossible for the "Train arrived" indication to be shown. The normal position of the flap is away from the plunger.

The flap must be turned on to the plunger in either of the following circumstances:—

- 1) When a Train or Vehicle is standing outside the Home Signal.
- 2) When the "Blocking Back" Signal (2-4) has been accepted by the Box in the rear.

When the flap has been turned on to the plunger, it must not be taken off until the line is clear to Home Signal, or between the Home Signal and the clearing point, as the case may be.

Reminder flaps are not intended to cancel the use of lever collars or other similar devices, and are provided as additional safety appliances.

Rule 55 by Trainmen. The signalman must make an appropriate entry in the Train Register when a train or vehicle is signalled out of the section.

When a train or vehicle is signalled out of the section, the signalman must make an appropriate entry in the Train Register, whether track circuiting is provided or not. (See page 132.)

55 b. — Except in the case of a train or vehicle signalled out of the section, whether track circuiting is provided or not, the signalman must make an appropriate entry in the Train Register.

GA24 See New Rule

Interlocking or Disconnection of Facing Points or movable elbows.

The signalman must take care to keep the facing points or movable elbows in the correct position, and the signalman must take care to keep the detector locks working, in addition to the precautions mentioned in the preceding clause, when the signal is affected by the disarrangement in the

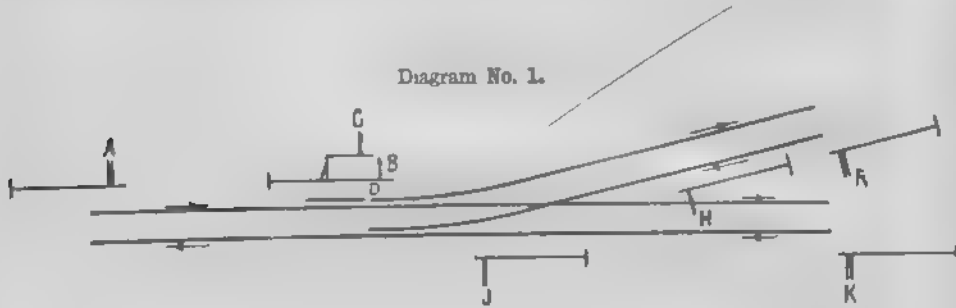
GA24 See New Rule

ADDITIONS TO THE STANDARD RULES.

locking or disconnection of the points, must also be disconnected and kept at danger, and while these Stop Signals are out of use, trains must be held until past them until the repairs are finished and ordinary working can be resumed.

The following diagrams illustrate typical cases where Points are disconnected where mechanical detector exists. When Electrical detector exists the detector must be considered ineffective and the necessary Stop Signals shown disconnected as if the detectors were also disconnected.

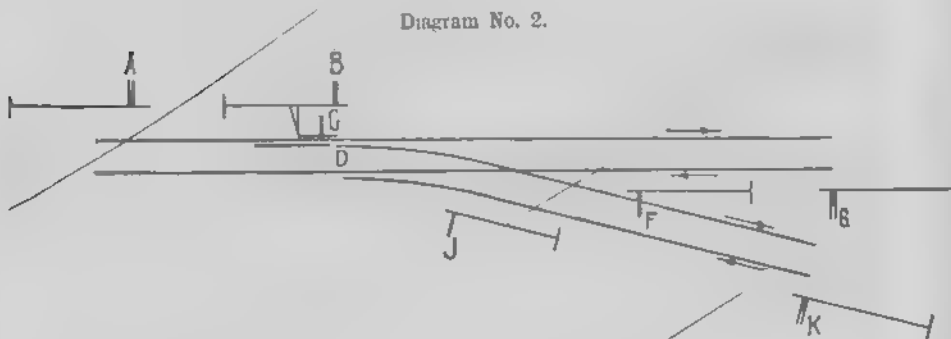
Diagram No. 1.



Signals to be
Disconnected.

- | | | |
|---|---------|-------------------|
| 1. D. Off Detector intact .. .. . | .. .. . | A. |
| 2. D. Off Detector disconnected .. . | .. .. . | A. B. C. H. R. |
| 3. Detector only disconnected .. .. . | .. .. . | A. B. C. |
| 4. Interlocking disarranged and Detector intact .. .. . | .. .. . | A. R. K. |
| 5. Interlocking disarranged and Detector disconnected .. .. . | .. .. . | A. B. C. H. R. K. |

Diagram No. 2.



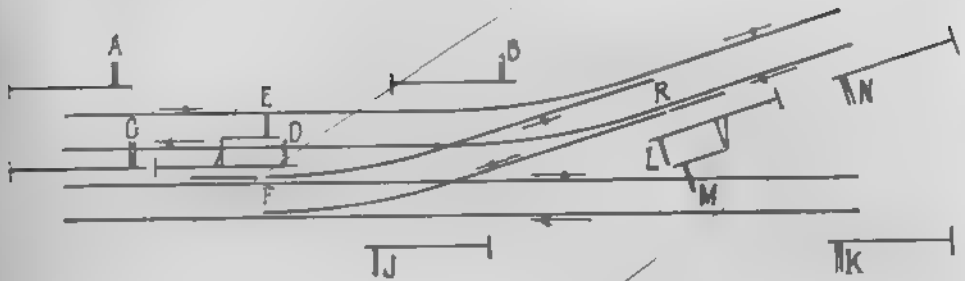
Signals to be
Disconnected.

- | | | |
|---|---------|-------------------|
| 1. D. Off Detector intact .. .. . | .. .. . | A. |
| 2. D. Off Detector disconnected .. . | .. .. . | A. B. C. |
| 3. Detector only disconnected .. .. . | .. .. . | A. B. C. |
| 4. Interlocking disarranged and Detector intact .. .. . | .. .. . | A. G. H. |
| 5. Interlocking disarranged and Detector disconnected .. .. . | .. .. . | A. B. C. F. G. K. |

GA 24 See New Rule

ADDITIONS TO THE STANDARD RULES.

Diagram No. 3.



Signals to be
Disconnected.

- | | |
|---|----------------------------|
| 1. F. Off Detector intact | A. C. N. |
| 2. F. Off Detector disconnected | A. B. C. D. E. F. L. M. N. |
| 3. F. Detector only disconnected | C. D. E. |
| 4. Interlocking disarranged and Detector F. intact | A. C. K. N. |
| 5. Interlocking disarranged and Detector F. disconnected | A. B. C. D. E. K. L. M. N. |
| 6. R. Off Detector intact | C. K. N. |
| 7. R. Off Detector disconnected | C. D. E. J. K. L. M. N. |
| 8. R. Detector only disconnected | C. L. M. N. |
| 9. Interlocking disarranged and Detector R. intact | A. C. K. N. |
| 10. Interlocking disarranged and Detector R. disconnected | A. C. D. E. J. K. L. M. N. |

Diagram No. 4.



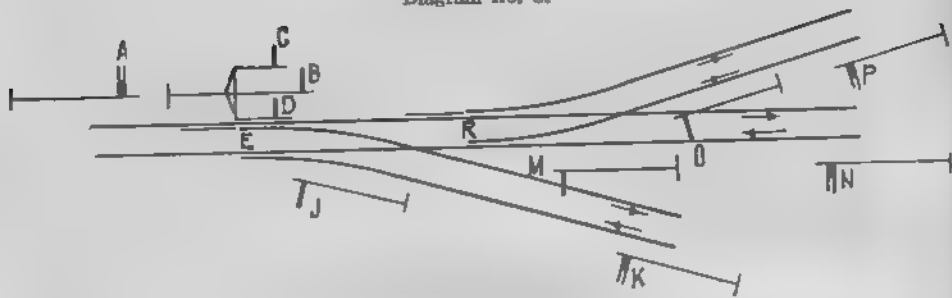
Signals to be
Disconnected.

- | | |
|------------|-------|
| 1. | Nil. |
| 2. | A. C. |
| 3. | A. |
| 4. | N. |
| 5. | A. C. |

See New Rule

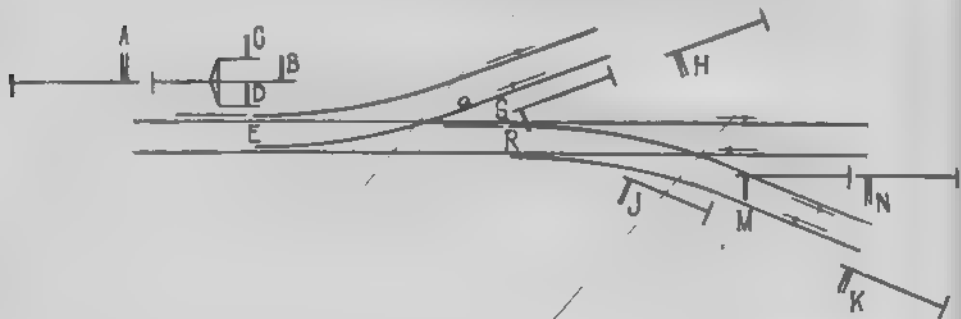
ADDITIONS TO THE STANDARD RULES.

Diagram No. 5.



						Signals to be Disconnected.
1	E. Off Detector intact .. .. .	..	..	..	..	A.
2	E. Off Detector disconnected .. .. .	..	..	..	..	A. B. C. D. M. N. O. P.
3	E. Detector only disconnected .. .. .	..	..	..	..	A. B. C. D.
4	Interlocking disarranged and Detector E. intact .. .. .	..	..	..	..	A. K. N. P.
5	Interlocking disarranged and Detector E. disconnected .. .. .	..	..	..	..	A. B. C. D. M. N. O. P. K.
6	R. Off Detector intact .. .. .	..	..	..	..	A.
7	R. Off Detector disconnected .. .. .	..	..	..	..	A. B. C. O. P.
8	R. Detector only disconnected .. .. .	..	..	..	..	A. B. C.
9	Interlocking disarranged and Detector R. intact .. .. .	..	..	..	..	A. K. N. P.
10	Interlocking disarranged and Detector R. disconnected .. .. .	..	..	..	..	A. B. C. K. N. O. P.

Diagram No. 6.



						Signals to be Disconnected
1	E. Off Detector intact .. .. .	..	..	..	..	A. B. C. D. G. H.
2	E. Off Detector disconnected .. .. .	..	..	..	..	A. B. C. D.
3	E. Detector only disconnected .. .. .	..	..	..	..	A. B. C. D.
4	Interlocking disarranged and Detector E. intact .. .. .	..	..	..	..	A. H. K. N.
5	Interlocking disarranged and Detector E. disconnected .. .. .	..	..	..	..	A. B. C. D. G. H. K. N.
6	R. Off Detector intact .. .. .	..	..	..	..	A.
7	R. Off Detector disconnected .. .. .	..	..	..	..	A. B. D. M. N.
8	R. Detector only disconnected .. .. .	..	..	..	..	A. B. D.
9	Interlocking disarranged and Detector R. intact .. .. .	..	..	..	..	A. H. K. N.
10	Interlocking disarranged and Detector R. disconnected .. .. .	..	..	..	..	A. B. D. H. K. M. N.

GA 24 See New Rule

RULE 77.

Signal Engineer's Occupations.

When the locking is disarranged no points may be moved, signals lowered, or hand signals given to Groundmen stationed at points and signals without the permission of the District Inspector, Reliefman or Signalman in charge of the work.

In cases where the Distant Signal disconnected in connection with Rule 77 is the lower arm on the post of the stop signal of the Signal Box in the rear and is not the standard distance out in accordance with Regulation 4A of the Regulations for Train Signaling on Double and Single Lines, the outermost Distant Signal must also be disconnected.

District Inspector to make all necessary arrangements in accordance with Rules 77 and 78 and the amplification of Rule 77 on page 15 of the General Appendix where the same applies. ~~by A 214~~

(G.A.22—2, 49. LK1/9581/20.)

Rule 83. The signalman is not authorised to request the lineman to release the locking except on authority from the Divisional Superintendent or District Traffic Manager.

Whenever the locking is released the signalman, having obtained the necessary authority and made arrangements with the lineman must enter in the train register "Lock on Lever No. ... released" and the time. This entry must be signed by both men. Should a change of signalman take place prior to the restoration of the lock, the man coming on duty must countersign the entry. When the lock is restored the time must be entered in the train register and the entry signed by the signalman and the lineman.

(G.A.13. 9/43. O.M. 12152.)

Rule 91 (f) —The provisions of the first paragraph of this clause will also apply to Draw-Ahead signals.

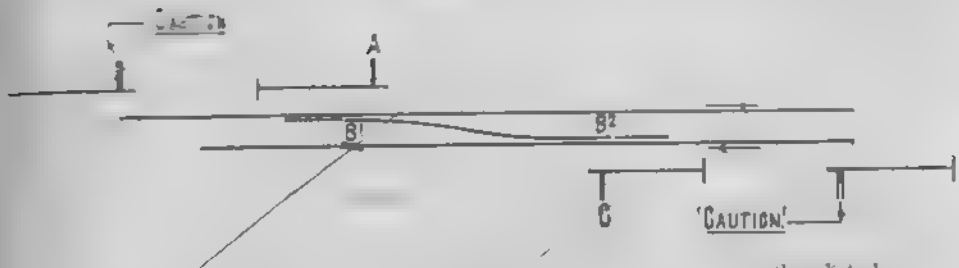
The following to be inserted on page 19 :—

Rule 120.—For the purpose of this Rule it must be understood that fully fitted freight trains which are not required to carry side lights are those classified "C" and signalled by 5 beats on the Block Bell Code, viz., 3 pause 1 pause 1.

(G.A.27.Op. 1/51. LK1/E.)

ATTENTIONS TO THE STANDARD RULES

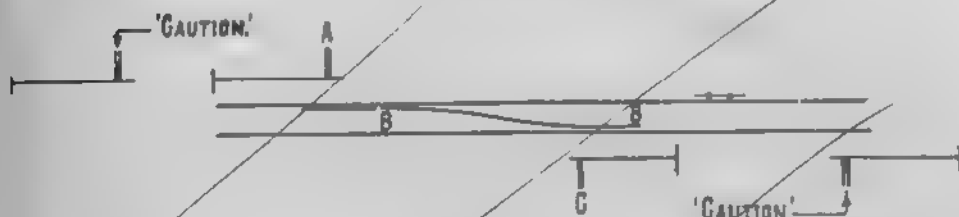
Diagram No. 7.



Signals to be disconnected.

- | | | |
|--|---------|-------|
| 1. B. Off Detectors intact | .. .. . | Nd. |
| 2. B. Off Detector B ¹ disconnected | .. .. . | A. |
| 3. B. Off Detector B ² disconnected | .. .. . | C. |
| 4. Detectors only disconnected | .. .. . | A. C. |
| 5. Interlocking disarranged and Detectors intact | .. .. . | Nd. |
| 6. Interlocking disarranged and Detector B ¹ disconnected | .. .. . | A. C. |
| 7. Interlocking disarranged and Detector B ² disconnected | | A. C. |

Diagram No. 8.



Signals to be disconnected.

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|---|---------|-------|
| 1. B. Off Detector intact | | N |
| 2. B. Off Detector disconnected | | A. C. |
| 3. Detector only disconnected | | A. |
| 4. Interlocking disarranged and Detector intact | .. .. . | Nd. |
| 5. Interlocking disarranged and Detector disconnected | .. .. . | A. C. |

4 H 24 See New Rule

Rule 77 (c) and 81 (c). The instruction in these clauses as to the disconnection of Distant Signals facing points are disconnected is to be understood as applying also when trailing points are set.

Rule 81. A Signal which is electrically repeated in the Signal Box must be regarded as defective if the repeater is out of order and the Signaller is unable to satisfy himself that the Signal arm is working properly or the lamp burning satisfactorily.

Rule 84. Particulars are shown in the respective Appendices to the Service Time Tables, or in local notices, of the places at which the last sentence of this Rule applies.

Rule 110, clause (b) second paragraph.—The prohibition against loose shunting of or against any train containing passengers, includes any person except the Executive's employee in charge of the train.

(G A 24 1940)

Rule 110 clause (c).

Then by the Department of the Army, War Office, 1940.

GA 24 See New Rule

ADDITIONS TO THE STANDARD RULES.

~~Rule 121. Side lamps are not provided on C.W. passenger, empty coaching stock, parcels, perishable, mail and live trains.~~

~~In the case of a Mixed train with a Goods Guard's brake van as the rear brake van, side and tail~~

RULE 121.—page 20.

The existing entry to be deleted. (See New Rule.)

The word "adjoining" in clauses (b) and (c) includes a line running in the same direction where another line used in the opposite direction intervenes. (G.A. 24 -11-43)

Rule 122.—Tail Lamps on Light Engines.

Money First.

Tail lamp to be carried on top lamp stand of tender or bunker when not lighted, but when alight it must be carried on the stand in centre of buffer plank.

Tender or Bunker First.

Tail lamp to be carried on stand at bottom of chimney during the time lamp is not lighted; but when alight it must be carried on the stand in centre of buffer plank.

Rule 129, clause (m). Great Western goods coaches, in addition to carrying the "brake" stick, in this clause must have with them a brake stick.

Rule 133.—The amplification of this rule to be amended to read —

Rule 133.—Tail lamps of passenger, empty stock, perishable parcels trains and "C" Headcode freight trains assisted by the rear uncoupled need not be removed before ascending the incline.

(G.A. 30 Op.—9/54 L.K.1/-)

Rule 131. When engaged to haul a train of less than 200 tons on a vacuum fitted line, the driver must not be coupled to a train of less than 200 tons on a vacuum fitted line, unless the "brake" stick is applied on the Telephone.

Freight engines or light engines which are partly coupled to a train of less than 200 tons on a vacuum fitted line, must not be coupled to a train of less than 200 tons on a vacuum fitted line, unless the "brake" stick is applied on the Telephone, except in cases of great emergency, in which case the driver must be notified by the "brake" stick and both trains worked as ordinary non-vacuum fitted trains.

All such cases of coupling must be reported to the Divisional Superintendent or District Traffic Manager when they occur.

The combined load of the freight train and the empty train must not exceed 200 wagons. Whenever it is found that the combined load of the freight train and the empty train exceeds 200 wagons, the trains must be worked forward singly unless the load can be reduced.

In certain cases the Divisional Superintendent or District Traffic Manager may permit the combined load to exceed 200 wagons, but in no case more than 220 wagons.

Note.—The "wagons" referred to above are ten-ton wagons or equivalent thereto, and the load for each train must be calculated on a ten-ton wagon basis.

Rule 149, exception vii. Propelling Ballast Trains. Trains of less than 200 tons must not be propelled by steam, electric, or other traction, except by the Divisional Superintendent or District Traffic Manager, who may permit a locomotive to be used to propel a train of less than 200 tons on a vacuum fitted line, at a speed not exceeding 10 m.p.h., to the nearest convenient point at which engine can run round train. Maximum speed 10 m.p.h. per hour. Such trains must be worked forward singly unless the load can be reduced. The Divisional Superintendent or District Traffic Manager may permit the combined load to exceed 200 wagons, but in no case more than 220 wagons.

The driver must be notified by the "brake" stick of the combined load of the freight train and the empty train. Whenever it is found that the combined load of the freight train and the empty train exceeds 200 wagons, the trains must be worked forward singly unless the load can be reduced. The Divisional Superintendent or District Traffic Manager may permit the combined load to exceed 200 wagons, but in no case more than 220 wagons.

Rule 153. Particulars of the places where freight trains (not being merely shunting operations) are allowed to be worked on a vacuum fitted line, must be published in the respective Appendices to the Service Time Tables, or other local notices.

Rule 153, clause (b).—Steam and hand travelling cranes with the jib properly secured on a specially constructed match track fitted with roller on which the jib can rest and traverse may be permitted to travel with the jib leading or trailing. Other steam or hand travelling cranes must, when practicable, travel with the jib trailing. Travelling cranes must be moved from place to place, as far as possible, by slow freight trains. (See pages 186 and 295)

Rule 149. exception vii. Propelling Ballast Trains. page 20.

The second paragraph of this instruction to be amended to read —

The restriction in regard to propelling on falling gradients steeper than 1 in 200 may be withdrawn in the case of Engineers'ight inspection trains completely composed of vacuum set coupled throughout also vacuum fitted Hopper ballast wagon trains equipped with fully vacuum fitted or piped brake vans, provided the Engineer has occupation of the line and also that there is a brake van at the end which is leading with someone riding in it who can apply the hand brake and or the vacuum brake

(G A. 30 Op —9 54 L.K.I 11729 365)

The following to be inserted as the fourth paragraph —

Weed killing trains which are not fully vacuum fitted throughout or on which there are less than four fully vacuum fitted vehicles attached to the engine must not be propelled on falling gradients steeper than 1 in 260.

G A 30 Op —9 54 L.K.I 10661,417

... of Vehicles Outside Home Signals on falling gradients.

vehicles must not be placed outside—(a) Outer Home signals or Home signals next in advance of an Outer Home signal or where more than one signal is provided outside the second Home signal in the normal direction of the line is on a falling gradient towards the Signal Box in the rear, except

(1) On any gradient.

(i) An engine, or an engine with one or two brake vans.

(ii) Trains or vehicles, provided the engine is at the lower end.

(2) On gradients not steeper than 1 in 260.

Trains or vehicles, provided the vehicle at the lower end is a brake van in which a guard or shunter is riding.

(3) On gradients steeper than 1 in 260.

Only where authorised by the Operating Superintendent or as shewn in clause (1) above.

Any of the above-mentioned cases the setting back movement must not be made beyond a point which will bring the train or vehicles immediately outside the signal required to effect the movement is required to pass through a connection beyond that point. (G.A.27 Op—1 51 L.K.I. 10428 Gen.E)

Rule 122.—Tail Lamps on Auto Engines.

Tail lamp to be carried on stand at bottom of chimney during the time the lamp is not lighted and in the centre of buffer plank when alight.

(G.A.12 4 43, O.M. 3070)

Rule 123.—Driver and Fireman to be on Engine when in motion.

On the Great Western System and where the passage of engines from one Depot to another will be facilitated by the presence of two engines, the Driver and Fireman must be on the engine together with the following footplate staff in attendance

Two engines Driver and Fireman on the leading engine

Three or more engines, coupled Driver and Fireman on the leading engine as permitted. Driver and Fireman on the intermediate engine

Superintendent's instructions to be observed with regard to the number of engines coupled together. (G.A.20—1 51 L.K.I. 10428 Gen.E)

Rule 138. Loading and Unloading Rails by the "End-on" Method.

When an Engine's mate is train's engaged in loading or unloading rails by the "end-on" method, the Driver must not move the train until the Guard or Fireman has given the necessary hand signal to the Driver to move the train. The Guard or Fireman must take as may be necessary on the side of the train with the rails, and must, in the case of the rails on the verge on which the rails are being loaded or unloaded, be in position to the Permanent Wayman employed by the Inspector of the line. When loading or unloading operations are not being performed the train will be under the control of the Guard.

(G.A.15—12,44 L.K.I. 7486 5A)

Rule 147—page 20.

The following to be substituted for this instruction —

When a train is admitted to a loop goods line or siding, before the last vehicle has passed the Guard or Fireman in the case of a light engine must advise the Signman by the quickest means by telephone or by the exhibition of a hand signal by day or a white light by night fog or falling snow to indicate that the train is in clear. The Guard's hand signal must be given from the brake van and the Fireman's from the footplate. The Guard (or Fireman) must not exhibit the hand signal until it is acknowledged by the Signman, who will, at night or in fog or falling snow, exhibit a white light held steadily. (G.A.24.—1. 49)

Rule 149, exception (vii)—page 20.

The following to be inserted as the third paragraph :—

The restriction may also be withdrawn in the case of vehicles provided for the accommodation of workmen accompanying an engine and snow plough when there is no alternative but for such vehicle returning from the site of obstruction to be propelled over the obstructed line. The vehicle provided for the accommodation of the workmen must be either a vacuum fitted goods brake van or a steam heated passenger brake vehicle.

(G.A.23—7 49 L.K.I./9218 Gen.)

Reference to the following to be made on page 20 .—

151.—When a crane and match wagon working with a breakdown train are detached from the train, the man in charge of the breakdown van train staff will be responsible for satisfying the crane and match wagon are secured and the crane properly scotched.

(G.A.23—7 49. R.E. Stand. L.K.I./9263, 13)

The following to be inserted on page 21:

Rule 205, Clause (b).—On single lines worked in accordance with E.I.T. Regulations cases may arise in which pilot working in accordance with Regulation 205 is in operation for a prolonged period, but the Token stations between which such working operates are required to be closed during the hours of darkness. Where in such cases the same Pilotman is on duty throughout, but a fresh Signaller is on duty the following morning at one or both of the Token stations it will not be practicable for the late-duty Signaller of the previous day to notify the Signaller taking duty of the arrangements in force. In such circumstances the Station Master must sign the form of the late duty Signaller and arrange for this to be handed to the early-duty Signaller the following morning, but where circumstances make this course impracticable a suitable entry must be made across the Train Register by the Signaller going off duty, to the effect that pilot working is still in operation and the pilot working form must be left in the Train Register.

(G.A.18. 11/47. L.K. 1/8780/38.)

Rule 215—Permanent Way Trolleys page 21.

The following to be added as an additional paragraph

When a permanent way trolley is required to be worked over any sidings for which an occupation has not been obtained the Carver or man in charge of the trolley, before allowing it to be put in motion, must come to a clear understanding with the man in charge of the sidings in which such sidings, to the proposed work to be carried out and the movements of the trolley required to be made and the latter must then take the necessary steps to advise all concerned.

(G.A.16. 5/46. LK1/7940/5A)

RULES 189-208.—SINGLE LINE WORKING WHERE MORE THAN ONE RUNNING LINE IS AVAILABLE.

1. Where there are more than two running lines and all the lines in one direction are blocked by accident or are out of use owing to Engineering Department occupation, leaving two or more lines free for traffic, Single Line Working must be put into operation over one of the unobstructed lines for all trains running in the wrong direction. The other line or lines must be used only by trains in the normal direction.

2. The Rules applicable to Single Line Working must be observed in regard to the line over which Single Line Working is put into operation.

3. Where practicable trains travelling over the line or lines used in the normal direction should be turned on to these lines at some point before reaching the Single Line Working and allowed to run to a point beyond the Single Line Working in order that they shall not conflict with trains being worked over the line used in the wrong direction.

4. The Station Master or person in charge who puts the Single Line Working into operation will be responsible for advising the Signaller at Boxes beyond the limits of the Single Line Working when it is necessary to turn trains at the Boxes to and from the parallel line used in the proper direction.

5. Where this method of working is adopted the Pilotman must instruct Drivers of trains passing over the line used in the wrong direction not to change the headlamps at night or during rain or falling snow or when passing through a tunnel.

6. The passage of trains over the line used in the wrong direction must be controlled by —

(a) the block instruments at all Signal Boxes between which Single Line Working is in operation and applicable to the obstructed line nearest to the line being used in the wrong direction, and

(b) the signals at intermediate Signal Boxes and applicable to the obstructed line nearest to the line being used in the wrong direction.

7. In the event of the block apparatus having failed the provisions of Block Regulation 25 must be observed, but it will not be necessary for the Pilotman to accompany every train as shown in Rule 198 (a).

8. Drivers may be instructed in certain cases to bring engines to a stand at a particular signal or point to wait further instructions, and they must be careful to clearly understand and carry out any such instructions.

9. When this method of working involves turning Passenger trains in the normal direction over Goods lines the instructions shewn in clause 5 (b) of the Regulations for signalling trains and engines by Permissive Block system over Goods Running Loop lines and other Permissive Lines will apply.

(G.A.24.—11/49.)

Rules 192 (b) and 202 (a).—When a Wrong Line Order is issued in accordance with Rule 192 (b), the first sentence of Rule 202 (a) will not apply.

(G.A.20 Op —5/50)

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Rule 217. Clause (a)—Provision of Handsignalmen in connection with the use of Lifting Jacks and Jim Crows. Accidents having occurred owing to permanent way men having been unable to remove lifting jacks and jim crows from the line on the approach of trains, special attention is directed to the instructions in this clause as to sending out a handsignalman if from any cause the line is unsafe.

It must be clearly understood by all concerned that whenever there is the slightest possibility of a train approaching too quickly to permit of any instruction being removed from the line a handsignalman must be provided and wherever the sight of approaching traffic is limited at places where the line is being altered by means of a jack a lock or otherwise, the provided, if necessary, to give ample warning to the ganger, ganger or line coverer which train is at high speed. In those cases where in the opinion of the Ganger or man in charge it is not necessary to post a handsignalman, the Ganger or man in charge must nevertheless appoint one man to stand by each appliance and must instruct this man that his duties are to remain by the lifting jack or jim crow wherever it is placed in position near the rail, and continuously to look out for approaching trains and remove the appliance clear of the line in good time.

(G.A. 5 -239 O.M. 11868.)

RULES 189 to 208 and 217.—ARRANGEMENTS DURING PERMANENT WAY OPERATIONS AND SIGNAL ALTERATIONS

Single Line Working and "Absolute Occupation" Arrangements.

The District Inspector or Station Master will appoint the Pointman and Handsignalmen for Single Line working in every case, but Handsignalmen may be drawn from the Engineering Department Staff if necessary.

The line on which the Engineers have to carry out their work must not be occupied by them until written permission is given by the person in charge of the arrangements. Before trains are allowed to resume running on such line the Engineering Inspector or man in charge of the work must give to the person in charge of the traffic arrangements (if present, or if he is not present, to the Signaller at the Signal Box nearest to the spot where the occupation is given up, a written certificate that the line which has been occupied is in proper condition for the passage of trains.

"Absolute Occupations" not involving Single Line Working.

When Single Line working is not put into operation, for example where trains are diverted over other lines, Rule 217 will apply and the Handsignalmen will be provided by the Engineering Department.

The line on which the Engineers have to carry out their work must not be occupied by them until written permission is given by the person in charge of the arrangements or by the Signaller in charge of the Signal Box in the rear, and before trains are allowed to resume running on such line, the Engineering Inspector or man in charge of the work must give a written certificate that the line which has been occupied is in proper condition for the passage of trains.

"Absolute Occupation" by Engineering Department of Single Lines worked by Electric Train Token.

When the Engineering Department have Absolute Occupation of a section of Single Line worked by Electric Train Token the man in charge of the work must unless he has in his possession the token or occupation key for the Section occupied arrange for the obstruction to be protected in accordance with Rule 2.7. The time at which the token is handed to and received from the Ganger or man in charge of the work must be recorded in the Train Register.

A certificate to the effect that the line is safe for the passage of trains must be furnished by the Engineering Inspector or man in charge of the work after the work has been completed in all cases where the Engineering Department have occupation of Single Lines for Relaying, or other work affecting the safety of the line.

"Absolute Occupation" by Engineering Department of Single Lines when Electric Train Token Working is Suspended.

During the period of such Absolute Occupation the instructions in the Electric Train Token Regulations may be regarded as suspended and Drivers of Engineering Department trains may

the setting back movement is subject to the same Rules 189 to 208 and 217 Arrangements during Permanent Way Operations and Signal Alterations.—page 21.

The following to be inserted as the third paragraph of the instructions under heading "Between Trains" Occupations —

"A ballast train must not be set back into a section where intermediate block signals are provided"

(G A 30 Op —9 54 L.K. 11874 420)

and must be ordered to do so by the Engineering Department

proceed on to the Single Line without being in possession of the token, provided they are instructed to do so by the Ganger, man in charge of the work or, in his absence, by the Signaller controlling the entrance to the single line in occupation by the Engineering Department. Drivers must proceed with caution and be prepared to stop short of any obstruction. Guards must keep a good look-out and be specially on the alert.

A certificate to the effect that the line is safe for the passage of trains must be furnished by the Engineering Inspector or man in charge of the work after the work has been completed in all cases where the Engineering Department have occupation of Single Lines for Relaying, or other work affecting the safety of the line.

The setting back of ballast trains in the wrong direction into a block section is prohibited where the gradient is steeper than 1 in 200 falling towards the Box in the rear except where special authority has been previously obtained. Where the inclination is falling gradient towards the Box in the rear the setting back movement is subject to the pertinent instructions shown on page 20 of the General Appendix under heading "Rule 149, exception v. 'Propelling Ballast Trains' being complied with.

When a train working a ballast train is authorised by the Signaller to set back in the wrong direction into a block section exceeding the length at a time when the Engineering Department have "Between Trains" occupation of such section, the setting back movement must commence at a point not less than 1 mile in advance of the most advanced stop signal of the Signal Box next in circuit.

The Engineering Department will provide Hand Signals in accordance with Rule 217 in connection with "Between Trains" occupations.

All ballast trains must stop where ordered to do so by the Engineering Department.

(G.A. 25 -1 50 L.K. 958/20.)

Instructions and Signal Alterations Notice, or in any other Notice that may be issued are only to be granted in clear weather and must be cancelled if owing to fog or falling snow they are likely to interfere with the working of the Trains.

(G.A. 22 -2 49 L.K. 9581./20.)

RULE 218.

Disconnecting Distant Signals in connection with Restrictions of Speed.—Page 21.

The instructions under the above heading to be cancelled.

(G.A. 22 -2 49 R.E. Stand. Op. Com. Mn. 38.)

PERMANENT RESTRICTION OF SPEED INDICATORS.—Page 143.

The following paragraph to be added,

Where a temporary speed restriction is superimposed on the whole or part of a length of track which is subject to a permanent speed restriction it will not be necessary for the Permanent Restriction of Speed Indicator where provided to be obscured, but Drivers must regard the temporary restriction of speed laid down as superseding the Permanent Restriction.

(G.A. 22—2/49. L60164, 89.)

The following to be inserted on page 21 —

Rule 173 (a). The following to be added to the second paragraph of this rule —

In the case of a Driver becoming aware that his train has been accidentally divided and he is unable to see the rear portion and the circumstances are such that it is safe for the front portion to be brought to a stand or he is brought to a stand at a signal at Danger, the Fireman must, before proceeding to meet the Guard, place 3 detonators opposite his engine on any adjoining line in the opposite direction. If the Driver does not consider it safe to stop he must endeavour to attract the attention of the Driver of any train approaching on the opposite line, such as by sounding the engine whistle.

(G A.27 Op — 151 L.K.I.E.)

ADDITIONS TO THE STANDARD RULES.

Rule 218.—Provision of "Arrow" Warning Boards.—"Arrow" warning boards will be provided irrespective of the length of time the restriction is expected to be in force, and will be fixed on the common rate of temporary speed restrictions shown in the Weekly Reduction of Speed and Engineering Arrangements Notice or other special notice. A Handsignaller will be appointed during the first week except on certain specified branch lines, who will act in accordance with clauses (g) and (h) of Rule 217.

Drivers must be prepared at all times to act upon hand signals exhibited by a Handsignaller, whether a "warning" board is provided, in addition, or not.

Rule 218. Provision of Arrow Warning Boards.

Arrow Warning boards will be provided for temporary speed restrictions on all main line and branch lines, and will be fixed on the common rate of temporary speed restrictions shown in the Weekly Reduction of Speed and Engineering Arrangements Notice or other special notice.

Drivers must be prepared at all times to act upon hand signals exhibited by a Handsignaller, whether a "warning" board is provided, in addition, or not.

When a restriction is imposed by telegraph, the Handsignaller will be appointed during the first week except on certain specified branch lines, who will act in accordance with clauses (g) and (h) of Rule 217. The Handsignaller will be appointed during the first week except on certain specified branch lines, who will act in accordance with clauses (g) and (h) of Rule 217. The Handsignaller will be appointed during the first week except on certain specified branch lines, who will act in accordance with clauses (g) and (h) of Rule 217.

Temporary Restrictions of Speed on Branch Lines.

Handsignallers will not be posted on certain specified lines when notices of temporary speed restrictions are shown in the Fortnightly Speed and Engineering Notice and the Handsignaller is not required to be posted on the line as indicated in the notice as required by Rule 218. The word "Handsignaller" will be posted on the line as indicated in the notice as required by Rule 218.

When the advice of a speed restriction is given by telegraph, the Handsignaller will be appointed during the first week except on certain specified branch lines, who will act in accordance with clauses (g) and (h) of Rule 217. The Handsignaller will be appointed during the first week except on certain specified branch lines, who will act in accordance with clauses (g) and (h) of Rule 217.

The Branch lines to which this arrangement applies are selected in the Appendix to the Fortnightly Speed and Engineering Notice.

NOTE.—On Goods and Mineral lines where a permanent speed restriction of 10 m.p.h. is in force, no temporary speed restrictions will be imposed for the purpose of carrying out the work under arrangements made between the Permanent Way and Divisional Superintendent or District Traffic Manager.

The examination of Look out men as required by Rule 234 (a) will be conducted in the case of staff employed in the Signal and Telegraph Department by the Signal and Telegraph Inspector concerned. Look out men employed in Station Works will be examined by the Permanent Way Inspector, but those employed in the Chief Mechanical Engineer's Department in connection with the clearing of water troughs must be examined by Permanent Way Inspectors.

See also page 21 of Rule 217 clause (n) with regard to the provision of look out men where necessary, in connection with the use of lifting jacks.

Rule 234. Clause (e)—Amplified as follows:—

No man must be appointed as Look out man unless he has previously been passed as competent to do so in that capacity by the Permanent Way Inspector or other person authorised by the Engineer or Signal Engineer.

Rule 240. Conveyance by Goods Train of Explosives and Dangerous Goods.—Where consignments of explosives or dangerous goods are forwarded under a Statutory or Statutory Warrant under which the Company is exempted, the limitation in clause (b) of Rule 240 that "not more than five vehicles containing explosives must be on any one train at any one time" will not apply. On receipt of such a Warrant, the Station Master or Goods Agent must immediately wire to Chief Goods Manager, giving particulars of the explosives, etc., tendered for conveyance.

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Rule 234. Provision of "Look-out" Men. -The warning given by a "Look out" man is to be regarded as an intimation to the men he is protecting to stand clear immediately and the "Look out" man must exercise every vigilance to ensure the warning being heard and acted upon by the men.

Rule 234 provides for the posting of more than one look out man as may be necessary. Particular attention is to be given in the case of work of such a character as to necessitate more than ordinary length of warning to the men engaged upon it, or where the look out man might not have a good and distant view of signals, e.g., during the erection of steelwork near the line, painting of structures from ladders. The requirements of safety must be adequately met and consideration be given to whether it is a signalman should be posted under Rule 217.

Arrangements for protection of the Signal and Mechanical Departments will, when necessary, be arranged with Permanent Way Inspectors for the provision of look out men to protect men of other departments when engaged on operations on or near the track. The Chief Mechanical Engineer's Department will provide look out men during cleaning of water troughs, and as required at Swindon Works.

When a look out man is protecting men whose normal hours of duty or meal intervals differ from his own, he must not withdraw his protection while any of the men continue to work. Arrangements are to be made by the Permanent Way Inspector or other person responsible for the provision of look-out men for the necessary alterations in meal times or hours of duty.

Every regularly employed man in the permanent way, length gangs and relaying gangs must be supplied with a copy of the "Forms of Examination of Look out men, Hand-signalmen and Flag-signalmen."

The examination required by Rule 234, as well as being conducted in the case of

Rule 240. Conveyance by Goods Train of Explosives and Dangerous Goods

Clause 8. The distinctive label for explosives will be as shown on page 32 of the General Appendix. Waggons bearing such labels must be treated as containing explosives (see also 113.1.11).

Rule 240.—Conveyance by Goods Train of Explosives and Dangerous Goods.—page 22.

The amplification of Clause (9) to be cancelled —See standard Rule 240 clause (9).

The General Note shown in supplement G.A.8 to be cancelled —See standard Rule 240—last paragraph.

Clause (10). The existing amendment to be deleted and the following substituted —

Rule 240. —Clause (10). The restriction on the number of vehicles containing explosives which may be conveyed by any one train at any one time to a maximum of five does not apply in the case of explosives conveyed on account of —

- (i) The Government (Admiralty, War Office, Air Ministry and Ministry of Supply).
- (ii) A Trader for transit to a Government establishment.
- (iii) A Trader for transit to a Trader on account of the Government.

The marshalling arrangements laid down in the Rule must, however, be observed.

Rule 240—Conveyance by Goods Train of Explosives and Dangerous Goods—page 22.

Clause 9 to be inserted as amplified below:

Guard's
Responsibility

(9) Whenever vehicles containing explosives, inflammable liquids, or other dangerous goods have to be forwarded by train, the special attention of the Guard must be called to the vehicles by a duly authorised person, and the Guard will be responsible for the proper observance of these instructions while the goods are being conveyed on the train and until they are delivered into the safe custody of the station staff.

In addition, the Guard must inform the Driver and Fireman of the train engine and of any assisting engine the number of vehicles containing such traffic and their position on the train.

Clause 10, to be amended as follows:

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Max m
Number

1. Not more than five vehicles containing explosives must be conveyed by any one train at any one time. Vehicles containing explosives must be run taken as near the middle of the train as possible.
2. Trains conveying not more than five vehicles containing explosives must have at least two vehicles either empty or containing no explosive traffic marshalled between the engine and the vehicle containing explosives, except that for short distances between depots or private sidings and marshalling yards these two vehicles need not be provided.

Note. The foregoing paragraph does not apply in the case of Explosives conveyed on account of:

1. The Government (Admiralty, War Office, Air Ministry and Ministry of Supply).
2. A trader for transit to a Government Establishment.
3. A trader for transit to a trader on account of the Government,
4. at where possible odd wagons of explosives should be marshalled near the middle of provided no undue delay is caused.
5. Wagons containing highly inflammable liquids or compressed liquefied gases must not, at any time, be run short distances between depots or private sidings and marshalling yards, except near engine but at least one vehicle, either empty or containing non-explosive traffic, must be marshalled between the engine and the first tank wagon.

to the following to be made on page 22 :-

~~Rule 234. Clause (d) - The regular staff in length and relaying gangs must be examined by Way Inspectors within six months of the date of their attachment to such gangs.~~
~~Board will be put in their place.~~
(G.A. 23 7, 49. R.E. Stand. - C.E. K.I. 57,293)

~~Rule 234. Clause (d) - The regular staff in length and relaying gangs must be examined by Way Inspectors within six months of the date of their attachment to such gangs.~~
~~the members must carry eight~~

Deleted 9A24 (G.A. 8.-5/41. LK1/6233/10.)

ADDITIONS TO THE STANDARD RULES.

to be inserted on page 22

Rule 234. Clause (d) New entrants to the regular staff in length and relaying gangs must be examined by Way Inspectors within six months of the date of their attachment to such gangs.

(G.A. 1 3/37 C.E.O J2 34395)

SECTION I. (b).

EXTRACTS FROM REGULATIONS FOR TRAIN SIGNALLING ON DOUBLE AND SINGLE LINES

for the guidance of Enginemen, Guards and others concerned.

SECTION I. (b.)

EXTRACTS FROM REGULATIONS FOR TRAIN SIGNALLING ON DOUBLE AND SINGLE LINES

for the guidance of Enginemen, Guards and others concerned.

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51

(Regulations not included herein do not directly affect Drivers, Firemen or Guards.)

NOTES.—ANY ADDITIONS TO, OR ALTERATIONS IN, THESE REGULATIONS DIFFERING FROM THE AGREED RAILWAY CLEARING HOUSE STANDARD ARE SHOWN IN THIS SPECIAL TYPE.

At this point it is not a bad idea to extract from the above Relations a proposition which,

EXTRACTS FROM REGULATIONS FOR TRAIN SIGNALLING ON DOUBLE LINES

Absolute Block System The object of absolute block signaling is to prevent more than one train being in a block section between two signal boxes on the same line at the same time.

Regulation 5. Section clear but Station or Junction blocked. (Warning arrangement 11.)
or conveying to Drivers that they are required to proceed at a limited speed. Section 6, and Rule 41 of this Regulation is dealt with in Rule 41.

Regulation 5—Section Clear but Station or Junction Blocked (Warning arrangement),
Clause (d) to be amended to read :—

IN THE PRESENCE OF FOG OR FALLING SNOW A TRAIN MUST NOT BE DRAWN TOWARDS THE SIGNAL CONTROLLING THE ENTRANCE TO THE SECTION AHEAD TO AWAIT ACCEPTANCE OF THE SIGNAL BOX IN ADVANCE EXCEPT WHEN A SIGNALSMAN IS ON DUTY AT THE SIGNAL UNLESS THE SIGNALSMAN IS ABLE TO SATISFY HIMSELF THAT THE SIGNAL IS CLEAR AND THE LINE TO THE REAR OF SUCH SIGNAL IS TRACK CIRCUITED OR A SIGNALSMAN STANDING AT THE SIGNAL WILL BE WITHIN HIS VIEW. (G.A.19-10/48. L.K.1/8617/31.)

COST MAY BE LOWERED.

[illegible]

• If after this Driver or Engineer has conferred with the Signaller, the train is not to be
• signalled ahead in blocks, the signal may be lowered and the coupling of the signal allowed to
• operate at the train ready position in accordance with clause 1 of F 141, and if he is not
• satisfied the Signaller is unable to satisfy himself that the train is at a stand at the signal
• it is to be lowered at the driver's request in accordance with clause (c) of the General Instructions
• to the signaller in Part 1 of the Rules. See page 71

Regulation 6. Engine assisting in rear of Train.—AN ENGINE MUST NEVER ASSIST A TRAIN ENCL. AT THE REAR EXCEPT WHERE AUTHORIZED BY THE SUPERINTENDENT OF THE LINE AND THE MECHANICAL ENGINEER.

Regulation 9. Trolley going into or through Tunnels When it is necessary for a trolley to enter any of the tunnels specially designated in the appendix to the service regulations, within the application of this Regulation, it must be signaled on the block instruments in accordance with the prescribed "Is Line Clear?" signal.

Should the trolley after passing into or through the tunnel, be removed from the track before it is set to by the trolley or man. If the trolley is forward and a trolley is approaching from behind, it is clear if the one and the signman must then send the trolley out of the tunnel.

When the train was to be saved, the Conductor or man in charge must return to the "parallel" position. When the "S" signalman that the track is clear of the line, the Signalman must then report.

THEY'VE OWN SIGNALS.

16. TRAILIES PROPELLED BY SUCH MEANS AS ENABLE A FAST SPEED TO BE ATTAINED MUST BE RUN IN ACCORDANCE WITH THIS REGULATION WHETHER THERE BE A TUNNEL IN THE SECTION OR NOT. TRAILIES MUST ALWAYS BE RUN THROUGH THE WHOLE BLOCK SECTION, EXCEPT AT A SIGNAL BOX, AND MUST ALWAYS BE RUN THROUGH THE WHOLE BLOCK SECTION.

EXTRACTS FROM BLOCK REGULATIONS—DOUBLE LINES

REGULATION 12—"OBSTRUCTION DANGER" SIGNAL—page 26.

The following to be inserted:

Regulation 12. Obstruction Danger Signal. (b) If after the "Obstruction Danger" signal has been given, and when a train has started an engine may be allowed to enter the section for this purpose at either end in accordance with Regulation 14A, clause (c).

(G.A.18. 11/47. R.C.H. Op. Supts. Min. 78.)

Regulation 13.—Blocking Back—page 26.

Delete clause (b) under this heading and substitute the following :—

(b) Unless special permission is given by the Operating Superintendent no train or vehicles, other than an engine or engine with one or two brake vans, must be placed outside a Home signal where the line is on a falling gradient steeper than 1 in 260 towards the signal box in rear unless there is an engine at the lower end. WHERE SUCH PERMISSION IS GIVEN THERE MUST BE A BRAKE VAN AT THE LOWER END WITH A MAN IN IT.

(G.A.27.Op.—1/51. L.K.1/10428/Gen.E.)

REGULATION 17.—STOP AND EXAMINE TRAIN.—page 27.

The following to be inserted as clause (d):

IF AT THE SIGNAL BOX AT WHICH THE "STOP AND EXAMINE TRAIN" SIGNAL IS RECEIVED IT IS NOT POSSIBLE TO DETACH THE VEHICLE IN RESPECT OF WHICH THE EMERGENCY SIGNAL WAS SENT, OR TO RECTIFY THE DEFECT, OR OTHERWISE DEAL WITH THE EMERGENCY, AND IT IS CONSIDERED THAT THE TRAIN CAN SAFELY BE ALLOWED TO PROCEED TO A SIGNAL BOX IN ADVANCE WHERE THE MATTER CAN BE DEALT WITH AS A CONSEQUENCE THEREOF THE TRAIN MUST BE SIGNALLED FORWARD BY THE APPROXIMATE POSITION OF THE "STOP AND EXAMINE TRAIN" SIGNAL BEING SENT IMMEDIATELY FOLLOWING THE ACKNOWLEDGMENT OF THE "TRAIN ENTERING SECTION" SIGNAL.

(B.R. 7. 11/47. O.M. 12182.)

The following to be inserted as clause (g):

Should the rear-most portion of a divided train arrive within the home signal at the signal box in advance, complete with full ramp and accompanied by the Guard, the Signaller may, after ascertaining from the Guard that the rear portion is intact and so informing the Signaller at the signal box in rear, give the "Train Out of Section" signal in accordance with Regulation 17, but the first train requiring to travel over an adjoining line in the opposite direction must be stopped and the Driver informed of the circumstances and instructed to proceed cautiously.

The Signaller receiving the "Train Out of Section" signal must, in such circumstances, stop the first train requiring to proceed over the section which the divided train has traversed and on any adjoining parallel line, and inform the Driver of the circumstances and instruct him to proceed cautiously.

(G.A. 18. 11/47. Op. Com. Dec. 757.)

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The following to be inserted as clauses (c) and (d):

(c) Should a Signatory receive information which in his opinion necessitates the examination of the line he has drawn, he shall advise the Signatory at the opposite end of the section or sections affected, and no transport shall be allowed to proceed through the section or sections until the line has been examined and is found to be in accordance with the provisions of the Convention; however, he shall be allowed to enter the section or sections affected in the same or in opposite direction, as allowed for in clauses (a) and (b).

[illegible]

at a certain time the "Stop and Examine Train" will be the purpose of ascertaining if the locomotives are clear, and if not, also, in accordance with the instructions laid down in clauses (a) and (b), a signal will be sent to the locomotive and the train will stop. If the locomotive is clear, the "Stop and Examine Train" will be the same procedure as the same as the train when the locomotive is clear.

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EXTRACTS FROM BLOCK REGULATIONS—DOUBLE LINES—Continued.

(b) If it is necessary for a train to follow the last portion of a train, which has become divided such that it is not possible to enter the section, the Signalman at each end must be satisfied that the section is clear. The Signalman in rear must then advise the Signalman in advance of the train which is about to enter the section and give the Trainmen the Section signal. When this signal has been acknowledged he must inform the Driver of what has occurred, and instruct him to pass at DANGER THE SIGNAL CONTROLLING THE ENTRANCE INTO THE SECTION AHEAD IN ACCORDANCE WITH RULE 38, CLAUSE (b) and to proceed cautiously. The same course must be adopted in the event of the second portion of the divided train following the last portion through the section ahead.

(c) Should a train that is approaching a signal box from the wrong line, find the signal at danger, the Signalman must advise the Driver of what has occurred, and instruct him to pass at DANGER THE SIGNAL CONTROLLING THE ENTRANCE INTO THE SECTION AHEAD IN ACCORDANCE WITH RULE 38, CLAUSE (b), and to proceed cautiously.

Should the train be allowed to proceed in the opposite direction, except for the Examination of Line in accordance with Regulation 14A.

Should the train be allowed to proceed in the opposite direction, except for the Examination of Line in accordance with Regulation 14A.

AS A PASSENGER TRAIN, AND CARRY THE SLIP TAIL SIGNALS TO DESTINATION.

Regulation 22. Train or Vehicles running away on Wrong Line. Should the runaway train or vehicle stop in the section, or assistance be required from the box in rear, the provisions of Regulation 14A must be observed. If the train or vehicle is allowed to proceed, the provisions of Regulation 4, Section 2, must be observed. If the train or vehicle is allowed to proceed, the provisions of Regulation 4, Section 2, must be observed. If the train or vehicle is allowed to proceed, the provisions of Regulation 4, Section 2, must be observed. If the train or vehicle is allowed to proceed, the provisions of Regulation 4, Section 2, must be observed.

Regulation 23. Train or Vehicles running away on Right Line. Should the runaway train or vehicle stop in the section, or assistance be required from the box in rear, the provisions of Regulation 14A must be observed. If the train or vehicle is allowed to proceed, the provisions of Regulation 4, Section 2, must be observed. If the train or vehicle is allowed to proceed, the provisions of Regulation 4, Section 2, must be observed. If the train or vehicle is allowed to proceed, the provisions of Regulation 4, Section 2, must be observed.

Regulation 24. Opening and Closing of Signal Boxes where Switches are provided. General Switching Rules. The Signalman must be satisfied that the train is clear of the section before he can switch the signal. The Signalman must be satisfied that the train is clear of the section before he can switch the signal. The Signalman must be satisfied that the train is clear of the section before he can switch the signal. The Signalman must be satisfied that the train is clear of the section before he can switch the signal.

In the event of a failure of the block instruments, the following instructions must be observed:—
(a) (1) A train must not in any circumstances be allowed to pass a signal box into that section of the line where the failure exists without having been previously brought to a stand and the Driver and Rear Guard, also the Driver of an engine assisting in rear, if any, advised of the failure. The Driver or Drivers must in all cases be instructed to proceed on the signal side of the section.

When in immediate block signals controlled from the signal box in rear are provided, should any failure of the block instruments, bells or gongs occur after a train has been allowed to proceed

See Page 29

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2. Following to be inserted between the first and second paragraphs of clause (a) (i) —

When a Driver has been stopped at a Signal Box and advised by the Signalman of the failure of the instruments or bells or of a track circuit controlling the block instruments, the Driver must draw the train forward and bring it again to a stand with the brake van near to the Signal Box to enable the Signalman to verbally inform the Guard in rear and Driver of an engine assisting in the rear, if any, and the Driver. After the train has thus been brought to a stand for the Signalman to communicate with the Guard and Driver of an engine assisting in rear, the Driver must not start again until he receives a verbal signal from the Signalman.

(G.A. 5.—2.39. O.M.11869.)

EXTRACTS FROM BLOCK REGULATIONS—DOUBLE LINES—Continued.

REGULATION 25. FAILURE OF INSTRUMENTS, BELLS OR GONGS. —page 28.

The following to be substituted for the existing regulation:

25. Failure of Instruments and or Bells. In the event of any failure of the block instruments and or bells, the following instructions must be observed:

A train must not in any circumstance be allowed to pass a signal box into that section of the line where a failure of any instrument or bell has previously been reported until the Driver and the Driver and Rear Guard, and the Driver of an engine or locomotive, have received the necessary instructions. The Driver, or Drivers, must not proceed into the section until they have received the signal controlling the entrance into the section and proceed in accordance with Rule 25 (a) and (b) and to proceed cautiously through the section.

When a Driver has been stopped at a Signal Box and advised by the Signaller of the failure of instruments and or bells or of a track circuit controlling the block instruments the Driver must stop his train towards a danger signal or a stop signal with the brake van near to the Signal Box and enable the Signaller to verify the position of the Guard and Driver of the train. After the train has been stopped the Driver must not start the train until he receives a green hand signal from the Signaller.

When instruments or block signals controlled from the Signal Box in rear are provided, should any failure of the instruments or of a track circuit controlling the block signals be reported, the Driver must stop his train towards a danger signal or a stop signal with the brake van near to the Signal Box and enable the Signaller to verify the position of the Guard and Driver of the train. After the train has been stopped the Driver must not start the train until he receives a green hand signal from the Signaller.

(v) When the speaking instrument is not available, the Driver must advise the Signaller at the next signal box that the instrument is not available. The Driver must not proceed until he has received the necessary instructions. The Driver must not proceed until he has received the necessary instructions.

Time Interval Working.

(v) When the speaking instrument is not available, the Driver must advise the Signaller at the next signal box that the instrument is not available. The Driver must not proceed until he has received the necessary instructions. The Driver must not proceed until he has received the necessary instructions.

A train must not be allowed to follow another train until the time usually taken by the preceding train to clear the section has elapsed. When the time interval has elapsed, the Driver must not proceed until he has received the necessary instructions.

(v) If the event of a partial failure of any means of communication between the boxes is available for a short time, the Driver must not proceed until he has received the necessary instructions. The Driver must not proceed until he has received the necessary instructions.

In cases where trains on one line have to be worked on the time interval system in consequence of the failure of the speaking instrument, the Driver must not proceed until he has received the necessary instructions.

through the section.

In cases where trans c. one line have to be worked on the line after at system in consequence of the failure of a component within on the extraction, the car must be stopped and the Driver of each train instructed to proceed on the system in respect of a single track, not however, be stopped and cautioned when the line is clear and the not worked track is clear with the Absolute Block Regulations.

(b) Steps must be taken immediately to have the apparatus put into working order and when the failure has been remedied and the apparatus is again in working order, OR SHOULD SPEAKING COMMUNICATION ONLY BE RESTORED, The Driver, the next allowed stop, proceed through the stop over the line, or lines, on which the failure exists must be entered and signed for on a not on FORM 810 indicating that the train carries its notice while the train is working on THE FIVE MINUTE INTERVAL SYSTEM through the line and be must also be noted at that point in signal box and hand this note to the Signaller. The Signaller will not allow the train to enter the "Train out of Section" signal area in accordance with Regulation 20 and the ordinary method of working, or signaling, by speaking instruments will be resumed.

When the train is worked by two engines in front or is assisted by an engine or engines in rear, the engine or engines in front are called **locomotives** and the engine or engines in rear are called **helpers**.

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WHEN IT IS KNOWN THAT THE ELECTRICAL COMMUNICATION AT ALL CROSS-
ING IS NOT IN WORKING ORDER, THE DISTANT SIGNALS (IF SUCH ARE PROVIDED)
PROTECTING THE CROSSING MUST BE KEPT AT CAUTION UNTIL LINE SIGNAL HAS
BEEN RECEIVED THAT THE COMMUNICATION IS AGAIN IN WORKING ORDER.

IF DISTANT SIGNALS ARE NOT PROVIDED GRIAT CARE MUST BE TAKEN TO ASCERTAIN, AS FAR AS POSSIBLE, THAT NO LAYIN IS APPROACHING THE CROSSING IN EITHER DIRECTION BEFORE THE GATES ARE OPENED TO THE PUBLIC.

(4) When transmitting a signal, a ship must, if possible, be kept at a distance from the port of the Home Signal as proposed above, so as to be at a standing water level out of the Home Signal's influence. It is essential to be 6 Decretes away from the port of the Home Signal, and the Home Signal as far as possible, for in such a position the ship can be seen by the Home Signal.

If a train requires to stop outside a Home Signal for the purpose of attaching or detaching a car or through any other cause, the Signalman must, at least 100 yards in the advance of a Hand Signal, provide with the necessary stop signals and hand signals, so that no train shall be sent out, in excess of the limit from the rear of the train to afford protection. Until the Hand Signalman has been provided, a train must not be stopped outside the Home Signal to attach or detach cars.

(C.A. 12-11-17, Q.M. 12041-1-18942)

EXTRACTS FROM REGULATIONS FOR TRAIN SIGNALLING ON SINGLE LINES BY THE ELECTRIC TOKEN BLOCK SYSTEM.

NOTES.—ANY ADDITIONS TO OR ALTERATIONS IN THESE REGULATIONS DIFFERING FROM THE AGREED RAILWAY CLEARING HOUSE STANDARD ARE SHOWN IN THIS SPECIAL TYPE.

Wording which is not an actual extract from the above Regulations is printed in italics.

When the term "Token Station" is used in these Regulations it applies to a signal box or token station which is open.

Where the term "Signalman" is used it applies to the person in charge of the Token working acting as Signalman.

Electric Token Block System.—The object of the Electric Token Block System is to prevent more than one train occupying a section between two Token Stations at the same time, and, when no train is in the section, to enable the Signalman to clear the section for traffic in both directions. It is assumed that every train is fitted with a token, and that every train obtains the token from the Token Station at the same signal box at the start of the section.

(b) The signal box of the Electric Token Block System does not in any way interfere with the use of fixed or portable signals, whenever and wherever such signal apparatus may be requisite to protect obstructions on the line.

Drivers not to leave Token Station without Token and Proper Signals being exhibited.—In effect as provided in Regulations 14, "Section obstructed by accident or disabled train", and 20, "Failure of Token apparatus", a Driver will render himself liable to dismissal if he leaves a Token Station without the Token for that section of the line over which he is about to run, unless the Token has been shown to him as provided in the following clause.

(b) When a train has more than one engine in front, or is assisted by an engine or engines in rear, or when two or more engines are coupled together, the Token must be shown to each Driver, and delivered to, and carried by, the Driver of the rearmost engine.

(c) After receiving the Token, the Driver must not proceed until all the necessary fixed or other signals have been exhibited. He must keep the Token under his own charge, except as hereinafter provided in Regulations 14, "Section obstructed by accident or disabled train", 14a, "Failure of Token apparatus", 14b, "Working of trains towards and from point of obstruction", and 25, "Failure of Token apparatus", until he reaches the end of the section, when he must give it up to the Signalman or the duly authorised person, except as provided in Regulation 14a, "Failure of Token apparatus of train left on Single Line."

(d) The Driver must be careful not to take the Token beyond the Token Station at which it should be left.

(e) The person in charge of the token working will render himself liable to severe punishment should he contribute to any irregularity in the Token working.

(f) Each Token has engraved or marked on it the name of the Token Station at each end of the section to which it applies, and the Tokens of adjoining sections differ essentially in pattern or colour.

Custody and Transference of Token.—Except when some other person is specially appointed to the duty, the Signalman is the sole person authorised to receive a Token from, and, or if necessary, the Driver or Fireman, or to pass the Token in or take it from, except in the circumstances here provided. When necessary, due to a case of non-stopping trains, two competent men may be employed, one to receive and the other to deliver the Token. The Driver while the Token is in his charge, must see that it is placed in a safe position on the engine.

In no circumstances, except as provided in Regulations 14, "Section obstructed by accident or disabled train", 14b, "Failure of Token apparatus of train left on Single Line", 14c, "Working of trains towards and from point of obstruction", and 25, "Failure of Token apparatus", must a Token be transferred from one train to another without being passed through the Token Machine if

Working of Fixed Signals.—In the circumstances referred to in Regulations 14, "Section obstructed by accident or disabled train", 14a, "Failure of Token apparatus", 14b, "Working of trains towards and from point of obstruction", and 25, "Failure of Token apparatus":—
(a) the signal controlling the advance of traffic in the section ahead is not to be lowered, but such signal may be passed at Danger upon the Driver being instructed verbally to do so by the Signalman.

(b) When a shunting movement is being made in a loop line in the opposite direction, it is not to be lowered, unless there are points which are set to prevent the shunting movement fouling the single line, in the shunting movement has come to a stand and the Driver has been instructed that no further movement towards the starting signal must be made.

Similarly when a train is at a crossing place has been lowered for an approaching train a shunting movement must not be made towards the starting signal applicable to the opposite loop until the train approaching in the single line has passed clear on to the opposite line, unless there are points which are set to prevent the shunting movement fouling the single line.

Regulation 5. Section clear but Station or Junction blocked. (Warning Arrangement.)—The method of conveying the Drivers that they are required to proceed into the section in accordance with the provisions of this Regulation is dealt with in Rule 41.

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STANDING AT THE SIGNAL WILL BE WITHDRAWN FROM THE TRACKS. BE RECEIVED AT
AT STATION WHERE SPECIAL SIGNAL IS GIVEN FOR TRAINS. BE RECEIVED AT
IN THE WARNING ARRANGEMENT WHILE THE SIGNAL IS IN THE
SIGNAL THE SIGNAL MUST BE KEPT AT DANGER IN SUCH CIRCUMSTANCES UNTIL THE
TRAIN HAS BEEN STOPPED AT IT, AFTER WHICH IT IS THE DUTY OF THE SIGNALMAN WHERE PROVIDED
MAY BE LOWERED.

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In order to clear the line for the next train
If Should the Guard of the fastest train require his train to return to the Park Station in rear
of going through to the Token Station in advance he must bring the signal post down of
S. 6 before the train enters the section. When the train has arrived back complete and
the S. 6 is again clear, the Signalman at rest give the Token to the train and give the
"proceeding" signal to the Token Station in advance.

[illegible]

(i) **Trolleys going into or through Tunnels.**

Trolleys going into or through Tunnels.

When a signal is set for a trolley to go into or through any of the tunnels, the following information is appended to the Service Time Tables as a "Warning" to apply when of this Road, the trolley or man in charge of the trolley must be in possession of the token. SEE THE TOKEN OR OCCUPATION SYSTEM. IN OCCUPATION KEY SYSTEM IS IN OPERATION.

OPERATION.

Should the trolley be removed from the rails before reaching the next take-off station, the Conductor must take the trolley to the Signboardman, notify him and inform him that the trolley is clear of the line. The Signboardman must place the token in the trolley and send the "Instrument off" signal. If, however, time could be saved, the Conductor may place a streamer with the token in the trolley, notify the Signboardman that the trolley is clear of the line, the Signboardman must restore the token to the instrument and send the "cancelling" signal.

WHERE AN OCCUPATION KEY IS HELD THE TROLLEY WILL NOT BE SIGNALLED ON THE
TAKEN INSTRUCTIONS BUT THE SIGNALMAN MUST BE KEPT ADVISED. THE CHANGING OF MAN IN CHARGE
MUST, AFTER THE TROLLEY HAS BEEN REMOVED FROM THE PATH, BE ASKED TO RETURN THE
SECTION, RESTORE THE OCCUPATION KEY TO THE NEAREST OCCUPATION KEY INSTRUMENT,
INFORMING THE SIGNALMAN THAT THE SECTION IS CLEAR.

ORDINARY TROLLIES PASSING THROUGH ALL OTHER TUNNELS MUST BE DEALT WITH AS FOR ORDINARY SECTIONS AND PROTECTED BY HANDSIGNALMEN IN ACCORDANCE WITH RULE 215, UNLESS THE TOKEN OR OCCUPATION KEY IS HELD. IN THE CASE OF MOTOR TROLRIES THE GANGER OR MAN IN CHARGE MUST BE IN POSSESSION OF THE TOKEN OR OCCUPATION KEY

(ii) Trolleys going through Section.

When it is necessary for a trolley to proceed through a section from one token station to the next, the ganger or man in charge must be in possession of a token or occupation key, and the instructions contained in clause (i) will apply

In the event of it being found that the distance between the two points is less than 100 ft, the distance between the two points is to be measured from the point on the line where the distance is to be measured to the point on the line where the distance is to be measured.

(e) When the track is clear to the home signal but is occupied by a train or otherwise obstructed within the home signal, and assistance is required from the rear, the Signaller in rear must be informed of the circumstances. In the case of a train requiring assistance, provided it is arrived with full help, treated, the Train Controller Section must be informed if this has not already been done, or where the Obstruction Danger signal has been sent, the Obstruction Removed signal must be given. The assisting train or engine or locomotive must be accepted, and if the disabled train or obstruction is within the authority of the rear, the assisting train must be kept on the main line, except under special arrangements. The driver of the assisting train or engine must be clearly informed of the position of matters at the signal box ahead.

(f) Should it be necessary for the assisting train or engine to continue in rear of the disabled train through any T-intersection or section in advance of the section obstructed, the Signaller in rear, when forwarding the "In Advance" signal for the disabled train or if this signal has already been sent, must inform the Signaller in advance that the approaching train is being assisted in rear by a train or engine.

(G.A.3.—12/37. O.M.11795).

EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS—Contd. and

in the section have come to a stand, and, if there is no Token Station or other point where speaking communication exists from which this information can be obtained, the Driver must send his fireman on foot for the purpose.

If the gradients of the line over which the train has run are such that it can be concluded that the train or rear portion is at a stand at the time when the engine requires to return, the Driver may return for his train or rear portion thereof, without having ascertained that the rear portion has been cleared by a main signal at a risk, and if the Driver is at a Traction Station he must observe the green light signal, the danger signal and the green main signal in that order, there to the home signal.

(ii.) The Driver must (except as described in paragraph (i.)) take the front portion forward to the nearest place where it can be disposed of.

(vi.) The Guard must secure the train or rear portion thereof left behind and place on the rear three (3) to a (5) yard square rails or lumber and head of the portion left behind. He must then protect his train in rear in accordance with Rule 179.

(iv) If it is necessary to allow an engine to enter the section at the Token Station in rear of the section, then for the purpose of removing the obstruction to the more convenient end of the section, the water tank may be admitted in accordance with Regulation 12, "Not obstructed by accident or disabled train."

c. The driver will not return for the portion of the train that has been left behind unless it is passed by a Token Station without the permission of the Signalman.

pass any Token Station without the permission of the signaller.

3. If a Driver in section possesses a T. which is held at a point where the track is blocked from the section, a signaller is necessary from that point to remove the track from the T. However, it is necessary to release the track at a point ahead of the next Track Stop with the Driver in possession of the T. and to instruct the Driver to stop at the next Track Stop. The Driver is given a T. which is valid for use up to the point where the section is blocked. It is necessary to do so in accordance with Regulation 14, "Section obstructed by accident or disabled train."

in accordance with Regulation 14, "Section constructed by accident or distressed vehicle."
 (c) A white light must be placed on the leading vehicle of the train, indicating that it is
 is propelled to the Token Station in advance or drawn back to the Token Station in rear.

[illegible]

from the section, I observed a sharp increase in the number of birds, this section at the 1st station. Station 11, the water level was 11.5 feet below the tide with local distance 14. Section observed at 11.5 feet below the tide, "doubled train."

to the rear of the train, not being aware of the situation, the train conductor assisted engine No. 1 to back up. Regularly, the engine back up, then, and the disabled engine must not be moved until the relieving engine has arrived.

[illegible]

The provisions of Regulation 14, Section 1, do not require that a driver when the assistance is required to the disabled engine must agree to enter the section to render assistance to the disabled engine.

Regulation 14C Working of Trains to and from Point of Obstruction (a) Should an accident or obstruction occur of such a nature as to block the line and a train is likely to be stopped for any considerable time, special arrangements should be made by a runner if necessary be made for working trains to and from the point of obstruction.

(i) If a Token is out of the instrument it must be retained to work trains between the point of obstruction and the last station or mile before the obstruction. The other side the traffic must be conducted by Pilotman.

EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS—Continued.

(11.) If the obstruction is caused by a derailed or disabled train and the engine cannot proceed the Guard must put the Driver in charge at the point of obstruction, and give him a written order instructing him not to move his engine until authorised by the Pilotman. The Driver must then hand the Token to the Fireman, and the Guard must instruct the Fireman to which Token Station he must take it so that it can be used to work trains between that station and the point of obstruction; the Fireman must protect the train in accordance with Rule 179 as he proceeds with the Token to that station. The Guard must protect the train in accordance with Rule 179 on the other side of the obstruction, after which he must proceed to the Token Station on that side and arrange with the person in charge there for a Pilotman to be appointed.

(iii.) If the engine, or engine and front portion, of a disabled train can proceed to the Token Station in advance, this must be arranged, and on the way the Driver must stop to allow the Fireman to place three detonators, 10 yards apart, on the rail in rear of the engine, or engine and front portion at three quarters of a mile from the obstruction; on arrival at the Token Station in advance the person in charge of the Token working must be advised of the circumstances, and the Token delivered to him to enable it to be used for the purpose of working up to the obstruction on that side. The Guard must protect the train or rear portion in accordance with Rule 179, on the other side, after which he must proceed to the Token Station on that side and arrange with the person in charge there for a Pilotman to be appointed.

(iv.) During the whole time the line is obstructed the obstruction must be protected on each side in accordance with Rule 179, but, except as provided in paragraphs (ii.) and (iii.), the use of a baton or a quarter of a mile and under at 1 ft. 6 in. from the obstruction at a slow speed with. The Guard and Fireman will be responsible for this being done until two men provided with hand signals and detonators are appointed specially to perform the duty.

(v.) After sunset, during fog or falling snow, or if the disabled train is in a tunnel, a red light must be exhibited at both ends of such train.

(vi.) The person arranging working by Pilotman must fill up, sign and address the necessary forms, such as 41 for signaller, which must be kept with him. The form 41 is to be filled up, "Failure of Token apparatus," clause (c), except that where it is possible to provide a person in charge at the point of obstruction a form must be addressed to him and conveyed by the Pilotman when travelling with the first train thereto; and it will not be necessary to supply the Station Master and Signaller at the opposite end of the Token section with forms.

The Pilotman must wear the red smilet as shown in Regulation 25, "Failure of Token apparatus" - a red smilet and red flag must be held and a red flag must accompany each train to and from the point of obstruction.

(vii.) When the line is again clear no train must be allowed to pass the point where the obstruction existed without the Token. The Pilotman must accompany the first train through the obstruction to the point that has been used for working in the past. After the Token has been given up to the Signalman and all the forms issued for working by Pilotman have been collected by the Pilotman, ordinary working may be resumed.

In no case of obstruction away from a Token Station must the Token be restored to the instrument at either end of the section until the section is clear.

d. Should be taken out of the instrument and the time be only lost to that of the failure of a bridge or embankment or other cause and it is not when necessary to refer to the original plan marked with a line to be put in operation by the engineer. It is not a rule the next Town Street men should be taken out of the way when they are not remains in operation: a responsible man should be placed in charge at the point of obstruction.

The line on each side of the obstruction must be protected in accordance with Rule 217 by men specially appointed to perform the duty. After sunset during the day, a red light must be exhibited in a tunnel a red light must be exhibited on both sides of such obstruction.

When the line is again clear, a Pilotman must proceed to either end of the section and after the firms for working is, Pilotman have been with train at that end, they may bring any he first train far end the section which has been obstructed, and collect the remaining cars on arrival at the opposite end of the section ordinary working may be resumed.

(c) All forms which have been issued for working by Pilotman must be cancelled by writing the word "Cancelled" across them, afterwards being sent to the Divisional Superintendent or District Traffic Manager.

(d) If there is a level crossing in the section not situated at a Token Station, the Signaller, by telephonic communication with the crossing is available, must inform the Crossing Keeper of the circumstances.

The Signaller must instruct the Driver of each train proceeding in the direction of the crossing during the time the Token instruments are out of use to approach the level crossing in each direction cautiously, sound the engine whistle and be prepared to stop short of any obstruction at such crossing. If a Locomotive Pilotman is appointed the Signaller must request the Pilotman to convey this instruction to the Driver of each train concerned.

(e) Clauses (c), (d), (g) and (h) of Regulation 25, "Failure of Token apparatus," must also be observed, except as otherwise provided for in this Regulation (14c).

EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS—Continued.

Regulation 16. Fouling Single Line for Shunting Purposes. *b*, Except as shown in paragraphs (a), (b), (c), (d), (e), (f), (g), (h), (i), (j), (k), (l), (m), (n), (o), (p), (q), (r), (s), (t), (u), (v), (w), (x), (y), (z), (aa), (ab), (ac), (ad), (ae), (af), (ag), (ah), (ai), (aj), (ak), (al), (am), (an), (ao), (ap), (aq), (ar), (as), (at), (au), (av), (aw), (ax), (ay), (az), (ba), (bb), (bc), (bd), (be), (bf), (bg), (bh), (bi), (bj), (bk), (bl), (bm), (bn), (bo), (bp), (bq), (br), (bs), (bt), (bu), (bv), (bw), (bx), (by), (bz), (ca), (cb), (cc), (cd), (ce), (cf), (cg), (ch), (ci), (cj), (ck), (cl), (cm), (cn), (co), (cp), (cq), (cr), (cs), (ct), (cu), (cv), (cw), (cx), (cy), (cz), (da), (db), (dc), (dd), (de), (df), (dg), (dh), (di), (dj), (dk), (dl), (dm), (dn), (do), (dp), (dq), (dr), (ds), (dt), (du), (dv), (dw), (dx), (dy), (dz), (ea), (eb), (ec), (ed), (ee), (ef), (eg), (eh), (ei), (ej), (ek), (el), (em), (en), (eo), (ep), (eq), (er), (es), (et), (eu), (ev), (ew), (ex), (ey), (ez), (fa), (fb), (fc), (fd), (fe), (ff), (fg), (fh), (fi), (fj), (fk), (fl), (fm), (fn), (fo), (fp), (fq), (fr), (fs), (ft), (fu), (fv), (fw), (fx), (fy), (fz), (ga), (gb), (gc), (gd), (ge), (gf), (gg), (gh), (gi), (gj), (gk), (gl), (gm), (gn), (go), (gp), (gq), (gr), (gs), (gt), (gu), (gv), (gw), (gx), (gy), (gz), (ha), (hb), (hc), (hd), (he), (hf), (hg), (hh), (hi), (hj), (hk), (hl), (hm), (hn), (ho), (hp), (hq), (hr), (hs), (ht), (hu), (hv), (hw), (hx), (hy), (hz), (ia), (ib), (ic), (id), (ie), (if), (ig), (ih), (ii), (ij), (ik), (il), (im), (in), (io), (ip), (iq), (ir), (is), (it), (iu), (iv), (iw), (ix), (iy), (iz), (ja), (jb), (jc), (jd), (je), (jf), (jg), (jh), (ji), (jj), (jk), (jl), (jm), (jn), (jo), (jp), (jq), (jr), (js), (jt), (ju), (jv), (jw), (jx), (jy), (jz), (ka), (kb), (kc), (kd), (ke), (kf), (kg), (kh), (ki), (kj), (kk), (kl), (km), (kn), (ko), (kp), (kq), (kr), (ks), (kt), (ku), (kv), (kw), (kx), (ky), (kz), (la), (lb), (lc), (ld), (le), (lf), (lg), (lh), (li), (lj), (lk), (ll), (lm), (ln), (lo), (lp), (lq), (lr), (ls), (lt), (lu), (lv), (lw), (lx), (ly), (lz), (ma), (mb), (mc), (md), (me), (mf), (mg), (mh), (mi), (mj), (mk), (ml), (mm), (mn), (mo), (mp), (mq), (mr), (ms), (mt), (mu), (mv), (mw), (mx), (my), (mz), (na), (nb), (nc), (nd), (ne), (nf), (ng), (nh), (ni), (nj), (nk), (nl), (nm), (nn), (no), (np), (nq), (nr), (ns), (nt), (nu), (nv), (nw), (nx), (ny), (nz), (oa), (ob), (oc), (od), (oe), (of), (og), (oh), (oi), (oj), (ok), (ol), (om), (on), (oo), (op), (oq), (or), (os), (ot), (ou), (ov), (ow), (ox), (oy), (oz), (pa), (pb), (pc), (pd), (pe), (pf), (pg), (ph), (pi), (pj), (pk), (pl), (pm), (pn), (po), (pp), (pq), (pr), (ps), (pt), (pu), (pv), (pw), (px), (py), (pz), (qa), (qb), (qc), (qd), (qe), (qf), (qg), (qh), (qi), (qj), (qk), (ql), (qm), (qn), (qo), (qp), (qq), (qr), (qs), (qt), (qu), (qv), (qw), (qx), (qy), (qz), (ra), (rb), (rc), (rd), (re), (rf), (rg), (rh), (ri), (rj), (rk), (rl), (rm), (rn), (ro), (rp), (rq), (rr), (rs), (rt), (ru), (rv), (rw), (rx), (ry), (rz), (sa), (sb), (sc), (sd), (se), (sf), (sg), (sh), (si), (sj), (sk), (sl), (sm), (sn), (so), (sp), (sq), (sr), (ss), (st), (su), (sv), (sw), (sx), (sy), (sz), (ta), (tb), (tc), (td), (te), (tf), (tg), (th), (ti), (tj), (tk), (tl), (tm), (tn), (to), (tp), (tq), (tr), (ts), (tu), (tv), (tw), (tx), (ty), (tz), (ua), (ub), (uc), (ud), (ue), (uf), (ug), (uh), (ui), (uj), (uk), (ul), (um), (un), (uo), (up), (uq), (ur), (us), (ut), (uu), (uv), (uw), (ux), (uy), (uz), (va), (vb), (vc), (vd), (ve), (vf), (vg), (vh), (vi), (vj), (vk), (vl), (vm), (vn), (vo), (vp), (vq), (vr), (vs), (vt), (vu), (vv), (vw), (vx), (vy), (vz), (wa), (wb), (wc), (wd), (we), (wf), (wg), (wh), (wi), (wj), (wk), (wl), (wm), (wn), (wo), (wp), (wq), (wr), (ws), (wt), (wu), (wv), (ww), (wx), (wy), (wz), (xa), (xb), (xc), (xd), (xe), (xf), (xg), (xh), (xi), (xj), (xk), (xl), (xm), (xn), (xo), (xp), (xq), (xr), (xs), (xt), (xu), (xv), (xw), (xx), (xy), (xz), (ya), (yb), (yc), (yd), (ye), (yf), (yg), 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(Er), (Es), (Et), (Eu), (Ev), (Ew), (Ex), (Ey), (Ez), (Fa), (Fb), (Fc), (Fd), (Fe), (Ff), (Fg), (Fh), (Fi), (Fj), (Fk), (Fl), (Fm), (Fn), (Fo), (Fp), (Fq), (Fr), (Fs), (Ft), (Fu), (Fv), (Fw), (Fx), (Fy), (Fz), (Ga), (Gb), (Gc), (Gd), (Ge), (Gf), (Gg), (Gh), (Gi), (Gj), (Gk), (Gl), (Gm), (Gn), (Go), (Gp), (Gq), (Gr), (Gs), (Gt), (Gu), (Gv), (Gw), (Gx), (Gy), (Gz), (Ha), (Hb), (Hc), (Hd), (He), (Hf), (Hg), (Hh), (Hi), (Hj), (Hk), (Hl), (Hm), (Hn), (Ho), (Hp), (Hq), (Hr), (Hs), (Ht), (Hu), (Hv), (Hw), (Hx), (Hy), (Hz), (Ia), (Ib), (Ic), (Id), (Ie), (If), (Ig), (Ih), (Ii), (Ij), (Ik), (Il), (Im), (In), (Io), (Ip), (Iq), (Ir), (Is), (It), (Iu), (Iv), (Iw), (Ix), (Iy), (Iz), (Ja), (Jb), (Jc), (Jd), (Je), (Jf), (Jg), (Jh), (Ji), (Jj), (Jk), (Jl), (Jm), (Jn), (Jo), (Jp), (Jq), (Jr), (Js), (Jt), (Ju), (Jv), (Jw), (Jx), (Jy), (Jz), (Ka), (Kb), (Kc), (Kd), (Ke), (Kf), (Kg), (Kh), (Ki), (Kj), (Kk), (Kl), (Km), (Kn), (Ko), (Kp), (Kq), (Kr), (Ks), (Kt), (Ku), (Kv), (Kw), (Kx), (Ky), (Kz), (La), 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(Rl), (Rm), (Rn), (Ro), (Rp), (Rq), (Rr), (Rs), (Rt), (Ru), (Rv), (Rw), (Rx), (Ry), (Rz), (Sa), (Sb), (Sc), (Sd), (Se), (Sf), (Sg), (Sh), (Si), (Sj), (Sk), (Sl), (Sm), (Sn), (So), (Sp), (Sq), (Sr), (Ss), (St), (Su), (Sv), (Sw), (Sx), (Sy), (Sz), (Ta), (Tb), (Tc), (Td), (Te), (Tf), (Tg), (Th), (Ti), (Tj), (Tk), (Tl), (Tm), (Tn), (To), (Tp), (Tq), (Tr), (Ts), (Tu), (Tv), (Tw), (Tx), (Ty), (Tz), (Ua), (Ub), (Uc), (Ud), (Ue), (Uf), (Ug), (Uh), (Ui), (Uj), (Uk), (Ul), (Um), (Un), (Uo), (Up), (Uq), (Ur), (Us), (Ut), (Uu), (Uv), (Uw), (Ux), (

as the Driver is in possession of the Token for the section so named.

(c) Where sharing is not provided in the Token instrument, a Driver, who is in possession of the Sharing Token and when authorized by the Signaller to do so, may proceed via the route that the Signaller indicates and if the section to which the sharing Token applies, is far ahead of the train for sharing purposes at the token station, without being in possession of a Token for the section.

4. Where the above T-Units as described in paragraph 10 are not provided, the Signalman may, if necessary, provide a person who has been given for a train to approach from the right, the right of the signal, that the train is to be moved outside the home signal, but before doing so, he must see that the 'Backward' Signal and until this signal has been acknowledged, the line outside the home signal must not be occupied.

The signal line is, in no case, for signaling purposes, to be occupied outside the home signal and cleared of this occupation at the same time, provided that it was approaching in the section, and that the Blocking Release signal has been given to, and acknowledged by, the Signaller at the opposite end of the section.

at the opposite end of the section.

iii. If a train is to flag in the section away from the Toker Station at which shunting operations have to be performed, it is necessary to occupy the single line in the line direction, this may be done without displaying the "Blocking Back" Signal to the Toker Station at the opposite end of the section. It is indicated by the "Train out of Section" signal has been received from the Toker Station, must if the single line is still occupied outside the line section give the "Blocking Back" signal to the Toker Station at the opposite end of the section, which signal must be acknowledged.

which signal must be acknowledged

It is necessary for the Signal Line to be lit if the Signal is flashing. This is with the exception of passing on the occasion of a stop, but may be a signal in the case of the signal being flashing, or if the signal is flashing, the signal must be acknowledged. It has been found that a signal may be flashing, with the signal being the "Blocking Back" Signal to the Token Station at the "Stop" end of the section, but immediately the key is returned to the key box the signalman must, after the signal is signalled, light the Home Signal with the "Blocking Back" Signal to the Token Station at the opposite end of the section, which signal must be acknowledged.

ACKNOWLEDGE
I have special permission to travel by the Divisional Superintendent's train at Thiruvananthapuram station or you may mail I place outside a home signal where the train is a foreign train and I travel as the foreign train in the and then only if there is a lookman at the rail and a man with it.

40. A Denver resident, in my opinion, had the single line for shunting purposes in case he has received the authority of the Signalman to do so.

Regulation 17. Stop and Examine Train. (a) Should the Signalman who receives the "Stop and Examine Train" signal be unable to get out from the Trainmen, after examination of the train, and the signal was sent to him, to prevent this traveling in the opposite direction, inform the locomotive engineer or conductor of the train to proceed cautiously to the next signal block. The locomotive engineer or conductor of the train who receives the signal to stop or to halt the latter may, if necessary, caution the Driver of the next following train.

b. Stop all the "Stop and Go" Trains." should have been sent in account of a door being open on the passenger train the Southern was doing the said train at a base the Southern in rear of it, and first train, even if the train or engine used it be damaged, until the engine has been detached from the train and is not used but may be stopped, the Driver informed of the ground stakes, and instructed to proceed to the next Token Station, keeping a good look out.

Regulation 20. Train Divided. If the divided train is assisted by an engine in rear, it is run as one after the grade is cleared, the level or between short sections, where the stopping of the engine or the train is required with the second part on the S. M. main, provided permission has been obtained from the Ticket Station in advance for the train to proceed, in station exhibit the Driver's signal to stop the first portion, but must exhibit to the Driver a green hand signal waved slowly from side to side.

from side to side, and if it permits, it will be obtained from the Ticket Station in advance for the train to proceed, or after the train is in motion, the Engineer will not allow it to be obtained by any means in the yard and the station is in the yard, it is not allowed. The Signalman must not step the danger signal to stop the first portion of the train, but it is necessary that the signal must not be exhibited. The first portion, when stopped, must be satisfied that it is not necessary to exhibit a danger signal to the second portion, or otherwise dealt with as may be necessary to prevent the second portion coming into collision with it.

b. In the circumstances described in (a) the Signalman must, as soon as the first portion of the train has passed, or otherwise been dealt with, place or maintain his Signals at Danger, and the proper barriers for leaving the second portion, placing detectors on the rail and exhibiting a red hand signal to attract the attention of the Trainmen.

β When the rear portion of a disabled train requires to be removed from a section the provisions of Regulation 14, *Section obstructed by Accident or Disabled Train*, must be observed.

Regulation 14.—Fouling Single Line for Shunting Purposes—page 36.

Delete clause (f) and substitute the following :—

(f) Unless special permission is given by the Operating Superintendent no train or vehicles, other than an engine or engine with one or two brake vans, must be placed outside a Home signal where the line is on a falling gradient steeper than 1 in 260 towards the Token Station in rear unless there is an engine at the lower end. WHERE SUCH PERMISSION IS GIVEN THERE MUST BE A BRAKE VAN AT THE LOWER END WITH A MAN IN IT.

(G.A.27.Op.—1/51. L.K.I, 10428 Gen.E.)

EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS—Continued.

(c) It is necessary for a train to proceed into a section through which the front portion only of a disabled train is visible, such train must not be allowed to enter the section until the Signaller has been satisfied that the section is clear. The Signaller must then advise the Signaller at the end of the train which is ready to enter the section and after permission to withdraw a Token has been given, give the "Train entering Section" signal. When this signal has been acknowledged he must inform the Driver of what has occurred, instruct him to **PASS AT DANGER THE SIGNAL CONTROLLING THE ENTRANCE TO THE SECTION AHEAD IN ACCORDANCE WITH RULE 38 (b)**, and to proceed cautiously. The same course must be adopted in the event of the second portion of the disabled train following the first portion through the section ahead.

Regulation 21. Shunt Train for following Train to pass. (a) No train must shunt for another train to pass except at a Token Station or at a siding where a special Token arrangement is provided and special instructions are issued authorising trains to be shunted into such siding.

Regulation 22. Train or Vehicles running away. (a) Should the runaway train or vehicles stop in the section and assistance be required, the provisions of Regulation 4, "Section obstructed by Accident or Disabled train," must be observed. If the train or vehicles are removed from the section other than under Regulation 14, "Section obstructed by Accident or Disabled train," the Signaller must come to a clear understanding as to what has been done, and when the next train requires to pass over the line affected it must be signalled in the usual way, but the train must be stopped and the Signaller when handing the Token to the Driver must inform him of what has occurred and instruct him to proceed cautiously.

Regulation 23. Token Damaged or Lost. (a) Should a Token be damaged after it has been withdrawn and before it is taken forward into the section in advance, the driver for whom it has been withdrawn must not be detained unless it is necessary to do so to avoid any inconvenience working a Pilotman, but it must be set away with the damaged Token and the driver must be informed. When a Token is damaged and cannot be passed through the instrument, it may be used by a Pilotman to take a train to the other end of the section for the purpose of establishing ordinary working. He must keep the Token in his possession in accordance with Regulation 24, "Failure of token apparatus," clause (e).

(c) Should a Token be lost, working by Pilotman must be continued in accordance with Regulation 24, "Failure of Token apparatus," until every possible precaution has been taken to ensure for the missing Token, and when it has been established that the lost Token cannot be found the driver must be set forward the instrument to proceed, but when the ordinary working can be restored, if the event of the Token being afterwards found, it must be kept by the Station Master until the driver at the other end of the section returns, and if the Token is not found before the Pilotman is set forward the instrument and working by train is in operation, the driver must be informed that the Pilotman who will then withdraw the Token, after which the section must be restored to the instrument and ordinary working may be resumed.

Regulation 25. Failure of Token Apparatus. (a) In the event of the failure of the Token apparatus between two Token Stations, steps must at once be taken to have the defect put right by the Electrician. If these services are not obtained immediately, the Station Master or other responsible person in charge at each end of the section must communicate with the other end in the most expeditious manner, and who shall arrange for working by Pilotman, and have a clear understanding as to the arrangements to be put into operation.

If a Token is not available it will generally be found more expeditious for the Station Master or other responsible person at the opposite end of the section to the that with a train at request to enter the section to use the arrangements, as the Pilotman will then only have to go through the section in one direction to issue the forms.

(b) A competent person must be appointed as Pilotman, who must wear, round his left arm above the elbow, a red armband with the word "Pilotman" shewn thereon in white letters, thus:—



If this armband is not immediately available the Pilotman must wear a red flag in the position indicated until the proper armband is obtained.

The person arranged, working by Pilotman must fill up sign, and address the necessary forms (see page 41 for specimen form) to:—

(i) The Signaller at each end of the Token Section.

The person who will act as Pilotman.

(ii) The Station Master at each end of the section, except where the signal box at which the Pilotman commences or finishes is not at a station and the ordinary working at the station will not be interfered with.

EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS. *Continued.*

These forms must be handed to the Pilotman, who must also sign all the forms issued and deliver the necessary form to the Signaller in charge of the Token Station at which working by Pilotman commences and when satisfied that the Signaller understands that no train is to be allowed to enter the section until he returns, proceed to the other end of the section.

The Pilotman when proceeding to the other end of the section to deliver the forms must do so as quickly as possible, either by rail or road, except as provided in clause (f) of this Regulation, using the best means at his disposal for the purpose, but must not, unless a Token is in his possession, use an engine or any railway vehicle other than a trolley. See Regulation 9 (iv.), *Working of Trolleys.*

IF THE FAILURE OCCURS ON A SECTION OF LINE WHERE OCCUPATION KEY BOXES ARE IN USE AN ASSURANCE MUST BE GIVEN BY THE SIGNALMAN TO THE PILOTMAN THAT THE OCCUPATION KEY OR KEYS ARE IN THE KEY INSTRUMENT.

IN THE EVENT OF THE TELEPHONE COMMUNICATION ALSO HAVING FAILED, AND THE PILOTMAN IS STATIONED FROM A PLACE WHERE NO CONTROL INSTRUMENT EXISTS, AS HE WILL BE UNABLE TO ASCERTAIN THAT THE CONTROL SLIDERS ARE IN THE NORMAL POSITION, IT WILL BE NECESSARY FOR HIM WHEN WALKING THROUGH THE SECTION WITH THE PILOT WORKING FORMS, IN ACCORDANCE WITH CLAUSE (f) OF THIS REGULATION, TO MAKE CERTAIN BY PERSONAL OBSERVATION THAT THE OCCUPATION KEYS ARE PROPERLY IN THE KEY BOXES.

AT TOKEN STATIONS WHERE CONTROL INSTRUMENTS ARE NOT PROVIDED, THE PILOTMAN MUST, WHEN DISTRIBUTING THE PILOT WORKING FORMS, SATISFY HIMSELF BY PERSONAL OBSERVATION THAT THE OCCUPATION KEYS ARE PROPERLY IN THE KEY INSTRUMENTS.

DUPLICATE KEYS OF THE PUTS ARE HELD BY THE SIGNALMEN TO ENABLE THIS TO BE DONE.

ON THE ARRIVAL OF THE PILOTMAN AT THE OTHER END OF THE SECTION HE MUST DELIVER THE NECESSARY FORMS TO THE STATION MASTER AND SIGNALMAN, AND TAKE POSSESSION OF A TOKEN SHOW IT TO THE SIGNALMAN. EACH PERSON WHEN RECEIVING THE FORM MUST SIGN THE PILOTMAN'S FORM.

WHEN A STATION MASTER OR SIGNALMAN AS PILOTMAN HE MUST RETAIN ONLY THE PILOTMAN'S FORM, AND UNLESS HIS STATION COMES WITHIN THE EXCEPTION MENTIONED IN SECTION (iii.) OF THIS CLAUSE (c), HE MUST ADDRESS AND GIVE A FORM TO THE PERSON HE LEAVES IN CHARGE OF HIS STATION.

IF THE STATION MASTER OR SIGNALMAN HAS NO FORMS IN THE SECTION THE PILOTMAN MUST ADVISE THE PERSONS IN CHARGE OF SUCH PLACES THAT WORKING BY PILOTMAN IS IN OPERATION.

IF STATION MASTERS AND PERSONS IN CHARGE ISSUING AND RECEIVING FORMS FOR WORKING BY PILOTMAN WILL BE RESPONSIBLE FOR THE IMPROPER SIGNING AND THEREFORE MUST BE KEPT ACCQUAINTED WITH THE CIRCUMSTANCES IMMEDIATELY, AND INSTRUCTED IN THEIR NECESSARY DUTIES.

Twelve forms for working by Pilotman must be kept in a convenient place at each Token Station so as to be available at any time.

(e) If a Token is out of the instrument at either end of the section, or if one can be withdrawn at the end of the section, when a Pilotman is appointed, provided the Token is delivered with the occupation key or keys of the Signaller at the other end of the section, the Pilotman must first take possession of the instrument and then if there is a train available and permission has been obtained from the Signaller to proceed into the section, he may use such train for the purpose of delivering the forms to the other Station at the other end of the section. On arrival at the other end of the section he must, when delivering the forms to the Signaller there, show him the Token, and also show it to the Signaller. If the Token is not in possession, over the section during the time working by Pilotman is in operation, the Pilotman must keep the Token in his possession until it is required to be taken away by the Lineman or until the Token apparatus is again repaired and ready for use.

IF SIGNALMEN, STATION MASTERS OR PERSONS AS WELL AS THE TOKEN APPARATUS HAVE FAILED AND THE MEN AT THE STATIONS ARE UNABLE TO COMMUNICATE WITH EACH OTHER THE STATION MASTER OR OTHER PERSONS RESPONSIBLE FOR THE SECTION MUST ARRANGE FOR WORKING BY PILOTMAN AND THE PILOTMAN MUST PROTECT THE SECTION, ORDER THAT THEY MAY MEET AND ON LEAVING THE SECTION MUST RETURN THE NECESSARY FORMS TO THE STATION AT THE OTHER END OF THE SECTION FROM WHICH THEY STARTED. THE PILOTMAN, WHEN RETURNING TO THE END OF THE SECTION FROM WHICH HE STARTED, MUST COLLECT THE FORMS WHICH HAD BEEN DELIVERED AT THAT END OF THE SECTION AND RETURN THEM AND THE OTHER FORMS IN POSSESSION OF THE PERSON WHO FILED THEM UP AT THE OTHER END OF THE SECTION BY WRITING THE WORD "CANCELLED" ACROSS THEM. THE OTHER PILOTMAN MUST HAND ONE OF HIS FORMS TO THE STATION MASTER OR PERSON IN CHARGE AND ONE TO THE SIGNALMAN AND THEN ACT AS THE PILOTMAN.

When possible the Post Office telephones should be used to arrange working by Pilotman instead of appointing two men as laid down in the preceding paragraph.

(g) After all the forms have been signed as laid down in clause (c) trains may be allowed to enter the section by the permission and under the control of the Pilotman in accordance with the following provisions.

REGULATION 25.—FAILURE OF TOKEN APPARATUS. page 37.

Clause (g) (i)—The first paragraph to be amended to read:

(i) No train must be allowed to pass on to or foul the section under the Pilotman's control without the Pilotman being present. The Pilotman must inform the Driver and/or Guard of the train, and if the train is an engine assisting a train, of the nature, and when practicable the approximate time. He must also instruct the Driver or Guard TO PASS AT DANGER THE SIGNAL CONTROLLING THE ENTRANCE INTO THE SECTION AHEAD IN ACCORDANCE WITH RULE 15, CLAUSE (b) AND TO proceed cautiously through the section. When in charge of a section for the use of the Pilotman is applied to a train, it must be attached to the front unless it is necessary for a train to be used for ASSISTING purposes where the use of ASSISTANT engines is authorized. If the Pilotman travels on a train with two or more engines he must ride in the foremost engine. If the Pilotman travels on an electric train, rail motor, auto train, OR STREET-RAILWAY RAIL CAR, he must ride with the Driver.

Clause (g) (ii)—Amended to read:

THE SIGNAL CONTROLLING THE ENTRANCE TO THE SECTION AHEAD MUST BE MAINTAINED AT DANGER IN ACCORDANCE WITH RULE 8 CLAUSE (b), AND the Pilotman must obtain the permission of the Signaller before allowing a train to enter the section.

(G.A.18. 11 47. O.M. 12248.)

Regulation 25. Failure of Token Apparatus—page 37.—continued

The following to be inserted as clause (k):

(k) WHEN A TOKEN FAILURE OCCURS IN A SECTION IN WHICH AN INTERMEDIATE TOKEN INSTRUMENT IS PROVIDED AT A SIDING OR LOOP, THE PILOTMAN WHO WILL BE APPOINTED IN ACCORDANCE WITH THIS REGULATION, WILL, IF NECESSARY, APPLY TO THE LINEMAN FOR A TOKEN FOR ABL WORK TO BE CARRIED OUT AT THE SIDING OR LOOP. IN SUCH CIRCUMSTANCES THE PILOTMAN MUST, WHILE A TRAIN IS WORKING AT THE SIDING OR LOOP, REMAIN WITH SUCH TRAIN UNLESS IT IS NECESSARY FOR HIM TO PROCEED TO THE TOKEN STATION IN ADVANCE OR RETURN TO THE TOKEN STATION IN REAR TO ENSURE OTHER TRAINS NOT PASSED OVER THE SINGLE LINE, IN WHICH CASE HE MUST SATISFY HIMSELF THAT THE TRAIN IS CLEAR OF THE SINGLE LINE, AND THAT THE POINTS ARE SECURED TO PREVENT THE SINGLE LINE BEING FOULED. HE MUST KEEP THE TOKEN IN HIS POSSESSION UNTIL THE TRAIN AT THE INTERMEDIATE SIDING OR LOOP HAS BEEN WITHDRAWN OR THE FAILURE RECTIFIED. THE TIME THE TOKEN IS WITHDRAWN AND RETURNED MUST BE ENTERED IN THE TRAIN REGISTER BOOK, AND THE ENTRIES INITIALED BY THE SIGNALMEN AND PILOTMAN.

SHOULD HOWEVER A TOKEN FAILURE OCCUR WHEN A TRAIN IS AT SUCH SIDING OR LOOP THE PILOTMAN HAVING ACCOMPANIED A TRAIN TO THE POINT WHERE THE INTERMEDIATE TOKEN INSTRUMENT IS SITUATED MAY INSTRUCT THE DRIVER OF THE TRAIN ON WHICH HE HAS TRAVELLED TO PROCEED TO THE TOKEN STATION IN ADVANCE, WHILE HE REMAINS AT THE SIDING TO DEAL WITH THE TRAIN AT THE SIDING OR LOOP. IF THE PILOTMAN IS NOT IN POSSESSION OF THE TOKEN, THE SERVICES OF THE LINEMAN MUST BE OBTAINED TO RELEASE THE TOKEN FROM THE INTERMEDIATE TOKEN INSTRUMENT AND ON COMPLETION OF THE WORK AT THE SIDING THE PILOTMAN MUST RETURN THE TOKEN TO THE LINEMAN WHO WILL RESTORE IT TO THE INTERMEDIATE TOKEN INSTRUMENT UNLESS THE FAILURE HAS IN THE MEANTIME BEEN RECTIFIED IN WHICH CASE THE TOKEN WILL BE TAKEN BY THE PILOTMAN TO THE TOKEN STATION TO WHICH HE IS PROCEEDING IN ORDER THAT WORKING BY PILOTMAN MAY BE CANCELLED IN ACCORDANCE WITH THE REGULATIONS.

The existing clauses (k) and (l) to be re-lettered (l) and (m) respectively.

(G.A.18. 11 47. O.M. 12249.)

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EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS—Continued.

THE SIDING POINTS, AND THE LINEMAN IS AUTHORIZED TO EXTRACT A TOKEN FROM THE INSTRUMENT AND GIVE IT TO THE PIOTMAN FOR THE PURPOSE NAMED. THE PIOTMAN MUST HIMSELF OPEN AND CLOSE THE SIDING POINTS, AND KEEP THE TOKEN CLOSELY IN HIS POSSESSION UNTIL THE TRAFFIC ARRANGEMENTS AT THE SIDING POINTS PERMIT HIM TO RESTORE THE TOKEN TO THE SAME INSTRUMENT FROM WHICH IT WAS WITHDRAWN, WHICH MUST BE DONE IN THE PRESENCE OF THE LINEMAN AND SPINAMAN. THE PIOTMAN MUST NOT APPLY TO THE LINEMAN FOR A TOKEN EXCEPT IT IS NECESSARY FOR A TRAIN TO CALL AT A SIDING DURING THE TIME PIOT WORKING WILL BE IN FORCE. THE TOKEN WHEN USED FOR SUCH PURPOSES MUST NOT BE ALLOWED TO BE IN POSSESSION OF THE ENGINEMAN, AND MUST BE KEPT SECURELY A KEY TO OPEN AND CLOSE THE SIDING POINTS, AND MUST IN ANY CASE BE RESTORED TO THE INSTRUMENT FROM WHICH IT WAS TAKEN BEFORE THE PIOT WORKING IS RESUMED. THE TIME THE TOKEN IS WITHDRAWN AND RESTORED MUST BE ENTERED IN THE BOOKS KEPT FOR THIS PURPOSE, AND THE ENTRIES INITIALED BY THE SPINAMAN AND PIOTMAN.

WHEN OCCUPATION OF THE TOKEN APPARATUS IS GIVEN THE SIGNAL DEPARTMENT FOR REPAIRS OR OCLEANING PURPOSES, THE SPINAMAN SHALL TAKE WITH HIM A TOKEN DURING THE TIME THE APPARATUS IS IN THE HANDS OF THE SIGNAL DEPARTMENT MEN.

"L" When the Token apparatus is found to be in such a condition that it is not provided with sufficient power to operate the Token apparatus, the Signalman, if telephonic communication with the Signal Department is not possible, shall ascertain whether the indicators, bells or gauges are still in working order.

If there is no telephonic communication with the Signal Department or the telephone has failed, the Signalman must prepare the PIOTMAN to extract the Driver of each train proceeding in the direction of the crossing with the Token apparatus, and to stop the train at the crossing. The time taken must be extended sufficiently to allow for the call time likely to be occupied in carrying out this instruction.

WHEN IT IS KNOWN THAT TELEPHONIC COMMUNICATION AT A CROSSING IS NOT IN WORKING ORDER, THE DISTANT SIGNALS MUST BE KEPT CLOSED UNTIL THE CROSSING MUST BE KEPT AT DANGER UNTIL INFORMATION HAS BEEN RECEIVED THAT THE COMMUNICATION IS AGAIN IN WORKING ORDER.

IF DISTANT SIGNALS ARE NOT PROVIDED, GREAT CARE MUST BE TAKEN TO ASCERTAIN, AS FAR AS POSSIBLE, THAT NO TRAIN IS APPROACHING THE CROSSING BEFORE THE GATES ARE OPENED TO THE PUBLIC.

"M" When trains are being worked in accordance with these regulations, all trains must be brought to a halt at the protection of the home signal as previously provided, to enable a train starting without a permission to pass the home signal, the Signalman must, if necessary, inform the Driver to draw forward and allow the train to bring the rear portion within the signal in accordance with Rule 38 (b) (iii).

If a train requires to stand outside a home signal for the purpose of attaching or detaching traffic or the ordinary operation of the signal must require the assistance of a Haul Signalman, provided with the necessary authority and signals, who must be sent out a sufficient distance from the rear of the train to afford protection. Until this Haul Signalman has been provided, a train must not be stopped outside the home signal to attach or detach traffic.

Regulation 34. Sidings Controlled by means of Token. Points giving communication between sidings and the single line, or between sidings, shall not be opened without a Token for that section of the line where the siding is situated, and the Token shall be returned until the points have been placed in the proper position for trains to pass on the single line, and securely locked to prevent vehicles passing from the sidings on to the single line.

When arriving at a siding the points of which are controlled by Token, the Driver must hand the Token to the Guard or Trainman, if the siding is to be opened, the points to be worked. When the necessary shunting has been completed and the points have been placed in the proper position for trains to pass upon the single line, the Guard or Trainman of the siding must return the Token to the Driver, and the latter must not proceed on his journey until he has obtained possession of it.

Regulation 35. Speed of Trains. When exchanging Tokens by hand, Drivers must be careful not to exceed a speed of 10 miles an hour.

WHEN EXCHANGING TOKENS BY MEANS OF EXCHANGING APPARATUS (SEE PAGE 42), THE SPEED MUST NOT EXCEED 15 MILES PER HOUR. SPECIAL INSTRUCTIONS ARE ISSUED IN REGARD TO THE SPEED WHERE AUTOMATIC PICKING UP AND SETTING DOWN APPARATUS IS PROVIDED.

EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS—Continued.

PILOTMAN'S FORM.

(Form referred to in Regulations 14C, 23 and 25 of the Regulations for Train Signalling on Single Lines by Electric Token Block System.)

GREAT WESTERN RAILWAY.

(2988A)

ELECTRIC TOKEN BLOCK SYSTEM.

WORKING OF SINGLE LINES BY PILOTMAN—

- * { (a) DURING OBSTRUCTION.
(b) DURING FAILURE OF TOKEN APPARATUS, OR WHEN TOKEN IS DAMAGED OR LOST.

This form must be filled up and used whenever it is temporarily necessary to work the traffic by Pilotman.

....., Stat on.

..... 19

- (a) The single line between..... and being obstructed, the traffic between and the point of obstruction will be worked by Pilotman. will act as Pilotman, and must accompany every train to and from..... station and the point of obstruction.

The signalling of trains by bell or gong must be suspended.

- (b) The Token apparatus for the section between..... and..... having failed or the Token for this section having been damaged or lost the traffic between these places will be worked by Pilotman.

..... will act as Pilotman and no train must be allowed to pass into or foul the section unless he is present and gives permission.

The signalling of trains by bell or gong must be maintained or the provisions of Block Regulation 25, clause (g/ii) or (g/iii), as the case may be, observed.

This order is to remain in force until withdrawn by the Pilotman.

(Signed).....

To Time.

Noted by at

Noted by at

Noted by at

Noted by at

Noted by at

Noted by at

*(a) Noted by at point of obstruction.

Noted by Pilotman.

* Strike out portions (a) or (b), not applicable.

FORM OF PILOTMAN'S TICKET.

(Ticket referred to in Regulation 25 of the Regulations for Train Signalling on Single Lines by Electric Token Block System).

ELECTRIC TOKEN BLOCK SYSTEM.

PILOTMAN'S TICKET.

To be used when it is necessary to work the traffic of a single line by Pilotman during a failure of the Token apparatus, or in the case of a Token being damaged or lost.

To the Driver of, _____ train from _____
to, _____

You are authorised to proceed from, _____ to _____
Pilotman following.

Signature of Pilotman, _____

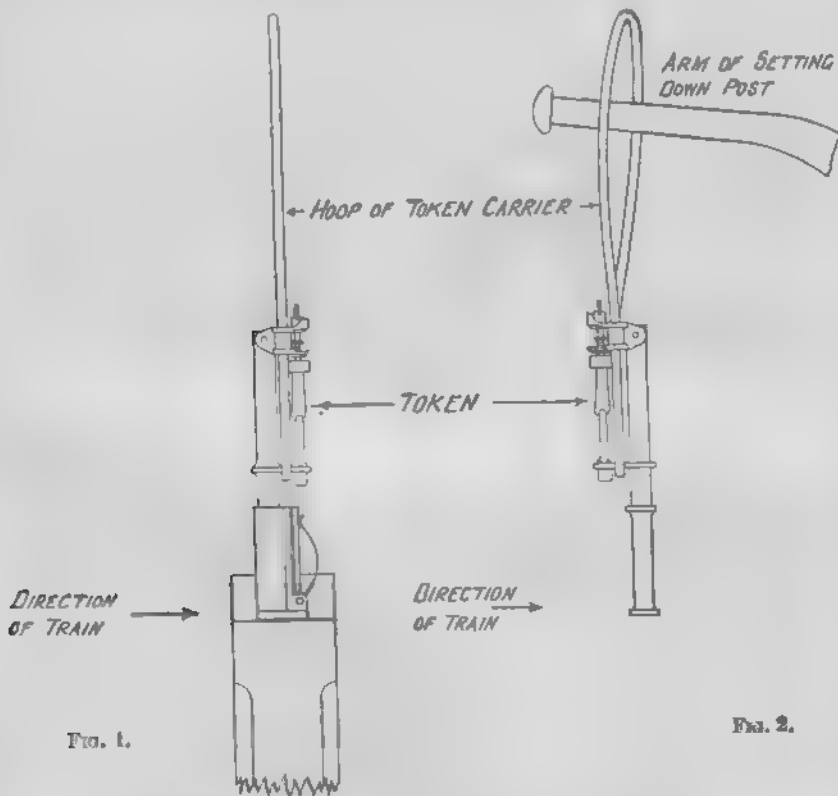
Date _____

This ticket must be given up by the Driver to the person in charge of the Token working at the place to which he is authorised to proceed, immediately on arrival.

NOTE — Pilotman's Tickets are not used on the Great Western Railway.

ELECTRIC TRAIN TOKEN EXCHANGING APPARATUS.

1. In this Apparatus the Token is fixed in a carrier to which is attached a hoop.
 2. The post on which the Signaller fixes the Token and from which the Fireman takes it before he enters the Token Section, is called the "Picking up" post.
- The post on which the Fireman hangs the Token after passing through the Section, and from which the Signaller afterwards fetches it, is called the "Setting down" post.



1) To place token in instrument.

1) To place token in instrument.

When the French has seen that the movement the day after tomorrow will draw thousands of new volunteers, the *Paris* Evening Post says, "it will take a Russian to see that the Russian officer corps is not better equipped than the French in that department, for in the course of the last few years it has been

After obtaining the authority of the appropriate Signaller by means of the telephone provided to withdraw a token from the column of the magazine to the centre position of the instrument, press the token forward as if using an ordinary key in a lock (the key end of the token must engage on the centre pin of the instrument), then turn the token right to left as far as possible. Wait until the needles in both the indicators are deflected (this takes place when both Signallers hold down on their respective token ringing keys), and afterwards continue to turn the token from right to left until the token is free when it can be withdrawn from the instrument. Advise Signaller by means of the telephone provided that the token has been obtained from the instrument. (G A 10 3/42 L 49736 39).

After obtaining the authority of the appropriate Signaller by means of the telephone provided to withdraw a token from the column of the magazine to the centre position of the instrument, press the token forward as if using an ordinary key in a lock (the key end of the token must engage on the centre pin of the instrument), then turn the token right to left as far as possible. Wait until the needles in both the indicators are deflected (this takes place when both Signallers hold down on their respective token ringing keys), and afterwards continue to turn the token from right to left until the token is free when it can be withdrawn from the instrument. Advise Signaller by means of the telephone provided that the token has been obtained from the instrument. (G A 10 3/42 L 49736 39).

ELECTRIC TRAIN TOKEN EXCHANGING APPARATUS—Continued.

3. To draw a Token, the Fireman must hold it at full length, with the hoop facing fairly and squarely, to the front.

4. When at the "Setting down" post the hoop must be placed over the projecting arm and the Fireman should be careful not to release his hold of the Token until it has passed to the arm.

The Token should on no account be thrown over the arm.

5. To pick up a Token, the Fireman should pass his hand and forearm, fairly through the hoop and the Token carrier will then exactly draw it from the spring box of the "Picking up" post.

6. The Signaller must be careful in placing a Token carrier in the spring box of the "Picking up" post to see that the hoop faces fairly and squarely, the direction of the approaching train, and that the spring box is in proper working order.

7. The speed at which the above operations are to be performed is in no case to exceed 15 miles per hour.

8. When new "Picking up" and "Setting down" posts are brought into use, or positions are altered, notices are issued by the District Superintendent or District Traffic Manager, giving the positions. The positions of existing posts are shown in the Section Appendix.

The Appendixes to the Section Appendixes give particulars of the places where "Picking up" and "Setting down" posts are provided.

9. All Train Token exchanging apparatus, including "Setting down" and "Picking up" posts and platforms for same where necessary, are provided and maintained by the Signaller in charge of the Local Signal Inspector should be advised of any repairs which may be required to same.

With the object of preventing Tokens being damaged by falling lights where "Picking up" and "Setting down" posts are provided, the following instructions must be observed:—

Instructions to Signaller. The Token carrier must be placed at the "Picking up" post that the Token is farthest away from the Fireman. Tokens supplied to the Fireman are exchanged by hand (Fig. 1).

Instructions to Enginemen. The Token carrier must be placed on the "Setting down" post with the Token nearest the Fireman (Fig. 2).

EXCHANGING TRAIN STAFFS, TOKENS OR TABLETS BY HAND.

When exchanging Train Staffs, Tablets, Tokens or Tablets by hand, Drivers must be careful not to exceed a speed of 10 miles per hour.

When exchanging staffs between the person holding the staff which is being handed over must hold it over the key, ensuring that the person taking the staff does not pick it up by the end.

As the Driver of a ~~Steam locomotive~~ Electric locomotive is to exchange the Token, which the person in charge must be brought to a stand at a signal box or at a fixed point and the Signaller must go to the car and exchange the Token. Electric car must not be used. GA27

INSTRUCTIONS FOR WORKING AUXILIARY ELECTRIC TRAIN TOKEN INSTRUMENTS AT PLACES WHERE THE TOKEN IS WITHDRAWN BY THE FIREMAN.

The Fireman must first obtain permission from the Signaller by means of the telephone to withdraw the Token. When this permission has been given, the Fireman must lift the Token from the platform to the centre opening of the instrument, press the Token down, thus forcing it over the key, and then the key end of the Token must engage on the centre point of the instrument. When the Token is right to it as far as possible. The Fireman must then wait until the Signaller has given the signal to turn the Token from right to left, after which the Token is free to be withdrawn from the instrument. He must then inform the Signaller on the telephone that the Token has been withdrawn.

EXTRACTS FROM REGULATIONS FOR TRAIN SIGNALLING ON SINGLE LINES BY THE TRAIN STAFF OR TRAIN STAFF AND TICKET BLOCK SYSTEM.

ANY ADDITIONS, OR ALTERATIONS IN THE REGULATIONS DIFFERING FROM THE AGREED RAILWAY CLEARING HOUSE STANDARD ARE SHOWN IN THIS SPECIAL TYPE.

Wording which is not an actual extract from the above Regulations is printed in italics.

Train Staff or Train Staff and Ticket Block System. The limit of the Train Staff or Ticket Block System of signalling is to prevent more than one train being in the blocks of line between two signal boxes at the same time.

The signaller at the Train Staff or Ticket Block System does not in any case with the use of fixed land signals, or detonators, when ever and wherever such signals or detonators may be requisite to protect obstructions on the line.

Warning of Fixed Signals at Crossing Places.

When a signal is given to a train approaching a crossing on a single line, the signal must not be lowered, unless there are points which are set to pass the train over the crossing, until the shunting movement has come to a standstill. The signaller must ensure that no further movement towards the starting signal must be made.

EXTRACTS FROM BLOCK REGULATIONS—SINGLE LINES—Continued.

When a home signal at a crossing place has been lowered for an approaching train a shunting movement must not be made towards the starting signal applicable to the opposite loop until the train approaching on the single line has passed clear on to the loop line, unless there are points which are set to prevent the shunting movement fouling the single line.

Regulation 5. Section Clear but Station or Junction Blocked. (Warning Arrangement.) The method of conveying to Drivers that they are required to proceed into the Section in accordance with the provisions of this Regulation is dealt with in Rule 41.

Regulation 5.—Section Clear but Station or Junction Blocked (Warning arrangement),
 Clause d) to be amended to read —

(f) AT TERMINAL STATIONS WHERE SPECIAL AUTHORITY IS GIVEN ON THE FOOTNOTES FOR TRAINS TO BE ACCEPTED UNDER THE "WARNING" ARRANGEMENT, WHILE VEHICLES ARE STANDING AGAINST THE BUFFER STOPS, THE HOME SIGNAL MUST BE KEPT AT DANGER IN SUCH CIRCUMSTANCES UNTIL THE INCOMING TRAIN HAS BEEN STOPPED, OR NEARLY STOPPED, AT IT, AFTER WHICH IT (OR THE "CALLING ON" ARM WHERE PROVIDED) MAY BE LOWERED.

Regulation 6. Engine Assisting in Rear of Train. AN ENGINE MUST NEVER ASSIST A PASSENGER TRAIN AT THE REAR EXCEPT WHERE AUTHORISED BY THE SUPERINTENDENT OF THE LINE AND THE CHIEF MECHANICAL ENGINEER.

Officers' Special Train G.A.26

Regulation 8. Ballast Train, Freight Train, or Inspection Train requiring to Stop in Section.

When a ballast train, freight train, or inspection train requiring to stop in the section, a man with hand signals and detonators has been sent out to protect such shunting.

(d) Should the Guard of the ballast train require his train to return to the signal box in rear, he must obtain the verbal permission of the Signalman before the train enters the section.

Regulation 9. Trolley Going Into or Through Tunnel.—(a) When it is necessary for a trolley to enter or pass through a tunnel, the trolley must be signalled in the approach to the working time table as coming within the application of this Regulation, the Ganger or man in charge of the trolley must be given the train staff or ticket to the signal box in rear and must carry the trolley with the block instruments. TROLLEYS PASSING THROUGH ALL OTHER TUNNELS MUST BE PROTECTED SOLELY BY THE PERMANENT WAY MEN'S OWN SIGNALS.

When the trolley has to remain stationary in the section in which the tunnel is situated although it has been signalled on the block instruments, the train staff must be given to the Ganger or man in charge of the trolley, who must be told when receiving the staff to which end of the section it is to be taken and at what time it is to be there, in order to clear the line for the next train.

(c) Should the trolley, after passing into or through the tunnel, be removed from the rails before reaching the next signal box, the Ganger or man in charge must take the train staff or ticket to the Signalman in advance, and inform him that the trolley is clear of the line; the Signalman must then send the "Train out of Section" signal. If, however, time would be saved, the Ganger or man in charge must return with the train staff or ticket to the signal box in rear and inform the Signalman that the trolley is clear of the line; the signalman must then send the "Cancelling" signal.

TROLLEYS PROPELLED BY SUCH MEANS AS ENABLE A FAST SPEED TO BE ATTAINED MUST BE SIGNALLED IN ACCORDANCE WITH THIS REGULATION, WHETHER THERE BE A TUNNEL IN THE SECTION OR NOT. TROLLEYS MUST NOT BE PUT ON THE RAILS EXCEPT AT SIGNAL BOXES, AND MUST ALWAYS BE RUN THROUGH THE WHOLE BLOCK SECTION, AND CARRY THE TRAIN STAFF OR A TICKET.

Regulation 10. Train Out of Section.—(b) When the last vehicle of a train does not pass the signal box before it has been shunted into a siding, or when a train has been brought to a stand within a home signal, and it is necessary to give the "Train out of Section" signal before the train passes the signal box, the Signalman must, before giving such signal, ascertain from the Guard or Shunter in charge of the train that the whole of the train, with tail lamp attached, has arrived, and the Guard or Shunter, will be held responsible for giving this information to the Signalman, the Fireman being similarly responsible in the case of a light engine.

Regulation 12A. Animals on Line. (a) Until it has been ascertained that the line is clear, all trains allowed to proceed must be brought to a stand and the Driver verbally informed of the circumstances and instructed to proceed cautiously.

(b) Should there be a tunnel in the obstructed section, the Signalman at either end of the section must verbally explain the circumstances to the Driver and Guard of each train entering the section and instruct them that although the train may proceed into the section it must not enter the tunnel until they have ascertained the tunnel is clear.

Regulation 13.—Blocking Back—page 44.

Delete clause (b) under this heading and substitute the following:—

(b) Unless special permission is given by the Operating Superintendent no train or vehicles, other than an engine or engine with one or two brake vans, must be placed outside a Home signal where the line is on a falling gradient steeper than 1 in 160 towards the signal box in rear unless there is an engine at the lower end. WHERE SUCH PERMISSION IS GIVEN THERE MUST BE A BRAKE VAN AT THE LOWER END WITH A MAN IN IT.
 (G.A.27 Op.—1/51. L.K.1/10428, Gen.E)

Regulation 5.—Section Clear but Station or Junction Blocked (Warning arrangement), Clause (d), to be amended to read

DURING FOG OR FALLING SNOW A TRAIN MUST NOT BE DRAWN TOWARDS THE SIGNAL CONTROLLING THE ENTRANCE TO THE SECTION AHEAD TO AWAIT ACCEPTANCE FROM THE SIGNAL BOX IN ADVANCE EXCEPT WHEN A SIGNALSMAN, S.O.S. LUTY AT THE SIGNAL UNLESS THE SIGNALSMAN IS ABLE TO SATISFY HIMSELF THAT THE SIGNAL IS AT DANGER, AND THE LINE TO THE REAR OF SUCH SIGNAL IS TRACK-CIRCLED OR A TRAIN STANDING AT THE SIGNAL WILL BE WITHIN HIS VIEW (G A 9 0 48 L K I 8617 31)

THE SIGNALSMAN MUST BE ABLE TO SEE THE SIGNAL AT IT AFTER A SHORT TIME

EXTRACTS FROM BLOCK REGULATIONS—SINGLE LINES—*Continued.***Regulation 14. Section Obstructed by Accident or by Disabled Train.**

(a) Should a Signalman receive information from the Fireman of a disabled train or from the Signalman at the box in advance that an ASSISTING TRAIN OR ENGINE is required to enter the section to assist the disabled train, or should it be necessary for the breakdown van train to enter a section obstructed by accident or otherwise the ASSISTING TRAIN OR ENGINE or the breakdown van train as the case may be, may after having been brought to a stand and the Driver AND GUARD informed of the circumstances, be allowed to enter the section under the following arrangements provided the Driver is in possession of the train staff except as shown in Regulation 18 clause (i) paragraph (ii), of the Regulations for Working on Single Lines by Train Staff and Ticket.

(i) The Signalman must instruct the Driver to pass at Danger the signal controlling the entrance into the section ahead as provided for in Rule 18 clause (b).

(ii) It will not be necessary for the Signalman in rear of the obstruction to detain the assisting train OR ENGINE until the arrival of the Guard or Fireman of the disabled train at his box if information has been received from the Signalman in advance that the Guard or Fireman is coming back. On receipt of this information the Signalman in rear may allow the assisting train OR ENGINE to enter the obstructed section after the Driver has been informed that the Guard or Fireman of the disabled train is coming back and has been instructed to keep a look out for such Guard or Fireman. The Guard or Fireman of the disabled train when

Regulation 14A. Examination of Line. (a) When it is necessary in accordance with Regulation 14 "Stop and Examine" to ascertain if the line is clear and speaking or if it is necessary to examine the two signal boxes which will admit of a proper understanding being arrived at between the Signalman, an engine may be allowed to enter the section provided that a Train out of Section signal has been sent or received, as the case may be, for the provision of the line. A constable or station explainer to the Driver, who must be in possession of the train staff, must be sent to the post at Danger the signal controlling the entrance into the section and to be provided with a whistle, or a whistle and provided cautiously through the section, prepared to stop short of any obstruction. Where practicable, the engine must be accompanied by a Station Master or other competent person. After sunset, during fog or falling snow, or where a tunnel intervenes, the engine must always be so accompanied.

Regulation 16. Fouling Single Lines for Shunting Purposes. (a) Except in the case of a disabled train working in the section and which has to return to the signal box in rear, see Regulation 14, "Blocking Back" or "Blocking Back" Signal, the signal box at the end of the section must not be used for a train to approach in the single line with no other signal outside the home signal, and must not be used for a train to approach in the single line with no other signal outside the home signal after permission has been given for a train to approach.

(b) Except as shown in paragraph (a), no train must be allowed to foul the single line outside the home signal, and as the Driver is in possession of the train staff, the section must not be occupied.

(i) The Signalman may, if necessary, and provided permission has not been given for a train to approach from the opposite end of the section, allow the single line to be fouled outside the home signal, but before doing so he must send the "Blocking Back" signal in accordance with Regulation 14 and until this signal has been acknowledged the line outside the home signal must not be occupied.

The single line may, if necessary for shunting purposes, be occupied outside the home signal at both ends of the same section at the same time, provided no train is approaching in the single line and that the "Blocking Back" signal has been given to, and acknowledged by, the Signalman at the opposite end of the section.

(ii) If a train is travelling in the section away from the signal box at which shunting operations have to be performed, and it is necessary to occupy the single line outside the home signal, this may be done without first giving the "Blocking Back" Signal to the signal box at the opposite end of the section, but immediately the "Train out of Section" signal has been received for that train, the Signalman must, if the single line is still occupied outside the home signal, give the "Blocking Back" signal to the signal box at the opposite end of the section, which signal must be acknowledged.

Regulation 17. Stop and Examine Train.

Should the Signalman who receives a "Stop and Examine" signal be unable to ascertain from the Fireman after examination why the signal was sent, he must, if the next train is travelling in the opposite direction, inform the Driver of that train of the circumstances, and instruct him to proceed cautiously to the next signal box. The Signalman must also communicate with the Signalman who sent the signal, and if necessary, caution the Driver of the next following train.

(ii) It will not be necessary for the Signaller in rear of the obstruction to detain the assisting train OR ENGINE until the arrival of the Guard or Fireman of the disabled train at his box if information has been received from the Signaller in advance that the Guard or Fireman is coming back. On receipt of this information the Signaller in rear may allow the assisting train OR ENGINE to enter the obstructed section after the Driver has been informed that the Guard or Fireman of the disabled train is coming back and has been instructed to keep a look-out for such Guard or Fireman. The Guard or Fireman of the disabled train, when picked up, will ride on the assisting engine or engine of the assisting train and pilot it to the rear of the disabled train.

(iii) If there is a tunnel in the obstructed section the Driver of the assisting train OR ENGINE or breakdown van train unless he is accompanied by the Fireman of the disabled train must not enter the tunnel until he has met the Guard or Fireman of the disabled train or it has been ascertained that the tunnel is clear. The Driver of the assisting train OR ENGINE, or breakdown van train, must be instructed accordingly by the Signaller.

When the line is clear again and permission has been obtained for another train to proceed, such train must be stopped and the Driver instructed to proceed cautiously.

(G.A.3.—12/37. O.M. 11795)

[illegible]

The following to be inserted as the second paragraph of clause (a) (1)

(G.A. 5 239 OM1 809)

IF THE INSTRUMENTS ARE NOT AVAILABLE TIME INTERVAL WORKING MUST BE
 FUNCTION. IN ADDITION THE DRIVER OF THE FIRST TRAIN THAT IS BEING CAUTIONED
 MUST BE HANDLED A COPY OF FORM 1581, PROPERLY FILLED UP AND SIGNED AND BE
 INSTRUCTED TO STOP AT THE SIGNAL BOX TO WHICH IT IS ADDRESSED AND HAND
 THE FORM TO THE SIGNALMAN THERE.

(v) In the event of a partial fail to release some means of communication between the hoists is available for signalling it expects the Driver of the first train traveling in an adjoining line must be advised of the circumstances, and instructed to proceed cautiously through the section.

[illegible]

(d) When the block is run into and/or belled (HAW) and there is a level crossing in the Section provided with indicators and/or bells, but WHICH is not a block post, the Signaller, if he has a colour light, must, with the exception of a late arrival, inform the person in charge of the crossing that the block indicators and/or bells, as the case may be, are not in working order.

If there is no telephone communication with the crossing, or the telephone was failed, the Signalman must instruct the Driver of every train proceeding in the direction of the crossing during the failure to approach the crossing with caution, sounding the engine whistle and be prepared to stop short of any obstruction at each crossing. The time interval must also be extended sufficiently to allow for the additional time likely to be occupied in carrying out this instruction.

WHEN IT IS KNOWN THAT THE ELECTRICAL COMMUNICATION AT A LEVEL CROSSING IS NOT IN WORKING ORDER THE DISTANT SIGNALS (IF SUCH ARE PROVIDED) PROTECTING THE CROSSING MUST BE KEPT AT CAUTION UNTIL INFORMATION HAS BEEN RECEIVED THAT THE COMMUNICATION IS AGAIN IN WORKING ORDER.

IF DISTANT SIGNALS ARE NOT PROVIDED, GREAT CARE MUST BE TAKEN TO ASCERTAIN, AS FAR AS POSSIBLE THAT NO TRAIN IS APPROACHING THE CROSSING IN EITHER DIRECTION BEFORE THE GATES ARE OPENED TO THE PUBLIC.

(e) When trains are being worked on the *time interval system* clause (a) paragraph (v), all trains must be brought within the protection of the Home Signal as promptly as possible, and to obviate a train standing with its rear portion outside the Home Signal, the Signaller must, if necessary, authorise the Driver to draw forward a sufficient distance to bring the rear portion within the home signal AS PROVIDED FOR IN RULE 58 (b) (ii).

If a train requires to stand outside a Home Signal for the purpose of attaching or detaching traffic, or for any other cause, the Signalman must, if practicable, unless he has ascertained from the Driver that the presence of the train staff obtain the assistance of a Handsignman provided with the necessary hand signals, who must be sent out a sufficient distance from the rear of the train to afford the necessary protection. Until this Handsignman has been provided, a train must not be detained outside the Home Signal for the purpose of attaching or detaching traffic.

REGULATIONS FOR WORKING ON SINGLE LINES BY TRAIN STAFF AND TICKET.

1. (a) A train staff, or train ticket indicating that the staff will follow (see page 54 for specimens), must be carried with every train, and no train must be permitted to leave any staff station with a train staff ticket unless the staff for that portion of the line over which it is to travel is then at the station.

Train staff or ticket to be carried.

No train may leave any staff station at the station.

(b) Except as provided in Regulations 17 and 20, a driver must not leave a train without having discussed it with the staff station with which the staff or ticket for the section over which he is about to run, or if he leaves with a ticket, with the driver of the train to which the proper staff

Penalty for Driver leaving without staff or ticket.

is attached. A person in charge of the staff working shall render himself liable to severe punishment should he contribute to any irregularity in the staff working.

Irregularity in staff working.

2. Every staff has engraved or marked on it the name of the staff station at each end of the section to which only it applies. The staffs, ticket boxes and tickets for the different sections are of different colours, and the staffs are different in shape.

Description of staffs, boxes and tickets.

EXTRA TS FROM TRAIN STAFF AND TICKET REGULATIONS—Continued.

Place for staff
when at station.

3 The staff, when at the station, must not be kept in the ticket box, but in the place provided for it.

Ticket to be
kept in the
ticket box
is the key.

4 The tickets must be kept in the proper ticket box fastened by an inside lock, the key to open the box being the staff, or a key attached to the staff, for the same section as the box, so that if the ticket box is kept locked, for which the person in charge of the staff working will be held strictly responsible, access to the tickets cannot be obtained unless the proper staff for the section is then at the station.

(b) Only one ticket must be taken from the ticket box at a time except as provided for in Regulation 14. The box must be locked after a ticket is taken out (or two tickets in the case of an assisted train) and not again opened until it is necessary to obtain another ticket for a following train.

Staff to be
be locked up.

5 All spare tickets must be kept in the lock and key, and in the care of the person in charge of the station.

Custody of staff
ticket.

6 The person in charge of the staff working for the time being is the sole person authorised to receive and deliver the staff or ticket.

For use of
single line.

7. (a) Except as provided in Regulation 10, no permission must not be given for a train to approach on the single line when there is any obstruction outside the home signal, nor must the single line be fouled outside the home signals after permission has been given for a train to approach.

(b) A Driver must not, under any circumstances, foul the single line for shunting purposes unless he has received the authority of the Signaller to do so.

Train not to
shunt for
an other except
at staff station.
may be

8. (a) No train must shunt for another train to pass except at a train staff station.

(b) When a shunting movement is made on a loop line at a crossing place the home signal applicable to a train approaching on the single line in the opposite direction must not be lowered, unless there are points which are set to prevent the shunting movement fouling the single line, until the shunting movement has come to a stand and the Driver has been instructed that no further movement towards the starting signal must be made.

When a home signal at a crossing place has been lowered for an approaching train, the signal must not be lowered towards the starting signal applicable to the opposite loop until the train approaching on the single line has passed clear on to the loop line, unless there are points which are set to prevent the shunting movement fouling the single line.

Working of
signals.

9. The normal indications of fixed signals at staff stations and signal boxes must be as under:

Stop signals—DANGER Distant Signals—CAUTION

and before any signal is lowered care must be taken to see that these and other relevant regulations have been complied with, and (except as otherwise provided in the Regulations for Train Signalling on Single Lines by the Train Staff or Train Staff and Ticket Book system) it is assumed that the line on which the train is about to run is clear.

(b) At all other places, except where instructions are issued to the contrary, the fixed signals, where provided, must be maintained in the Clear position, unless required to be placed at Danger for the protection of trains having to stop in the section, or for the protection of any other obstruction on the running line.

(c) When trains which have to cross each other are approaching a staff station in opposite directions the signals in both directions must be kept at Danger and when the train which has to be first admitted into the station has been brought to a stand, the home signal applicable to such train may be lowered to allow it to draw forward to the station or to the starting signal, and after it has again come to a stand and the Signaller has seen that the line on which the other train will arrive is clear, the necessary signals for that train may also be lowered.

(d) At staff stations the signal controlling the entrance of trains into the section ahead must not be lowered until the staff is available, or a ticket has been obtained, for the train to proceed into that section, or Pilot-working is in operation and the Pilotman is present.

Points to be
locked or
securely held.

10. All points not interlocked must be pillocked or securely held by hand for the safe passage of trains in the facing direction.

EXTRACTS FROM TRAIN STAFF AND TICKET REGULATIONS—Continued.

10

of the line where the siding is situated, and the staff cannot be removed until the points have been placed and locked in the proper position for trains to pass upon the running line, and to prevent vehicles passing from the sidings on to the running line.

(b) When shunting has to be performed at a siding, the points of which are controlled by the train staff, the Driver must hand the staff to the Guard or man in charge of the siding to enable the points to be unlocked. When shunting has been completed, and the points have been placed and locked in the proper position for trains to pass upon the running line, the Guard or man in charge of the siding must return the staff to the Driver, and the latter must not proceed on his journey until he has obtained possession of it.

11. (a) No train must be permitted to leave a staff station until the Driver has obtained the proper staff or ticket for that section of the line over which he is to travel, and he must not take the staff or ticket from any other than the person in charge of the staff working for the time being. After receiving the staff or ticket he must not start until the proper signals have been exhibited. On arriving at the station to which the staff or ticket applies, the staff or ticket must be immediately handed up to the person in charge of the staff working. All tickets given up must be at once cancelled by the word "Cancelled" being written across them, and the tickets must afterwards be dealt with in accordance with the instructions of the Divisional Superintendent or District Traffic Manager.

(b) The Driver must be careful not to take the staff or ticket beyond the staff station at which it should be left.

Driver not to take staff or ticket beyond proper staff station.

Staff or ticket to be given up.

Driver not to take staff or ticket beyond proper staff station.

When staff is to be given to Driver.

When ticket is to be given to Driver.

12. (a) When a train is ready to start from a station, and no second train is to follow before the staff will be required for a train in the opposite direction, the person in charge of the staff working must give the staff to the Driver or Fireman, and the Driver must see that it is placed in a safe position on the engine.

(b) If another train is to follow in the same direction before the staff can be handed out, the staff must be handed to the person in charge of the staff working by the Driver or Fireman of the first train; the staff for the section must be shown to the person in charge of the ticket. This same procedure must be followed with any other train except the last, the staff itself being given to the Driver or Fireman of the last train. The person in charge of the staff working after handing the ticket to the Driver or Fireman must satisfy himself that the train has proceeded with such ticket and that the line is clear in accordance with the Regulations before he allows another train to follow. After the staff has been sent away no other train must, except as provided in Regulations 17 and 20, clause (a), leave the station to follow in the same direction until the staff for that section has been returned.

13. The person in charge of the staff working must consider it his first duty to deal with the train staff or ticket on arrival of the train, and at once to pass on the staff or ticket to the train in the opposite direction, if one line train has arrived complete, or to the last train attached before handing the staff or ticket to the Driver or Fireman of the train about to travel in the opposite direction.

Staff or ticket to be given to Driver or Fireman of train in opposite direction, or to last train attached before handing to Driver or Fireman of train about to travel in opposite direction.

Assisted trains.

14. When a train is worked by more than one engine, or two or more engines are coupled together and such train or engines have to carry the staff, each engine except the rearmost must carry a ticket and the rearmost engine the staff. In cases where the train is to be followed by another train the rearmost as well as the leading engine, or engines, of the first train must carry a ticket. In such cases when a Driver or Fireman is handed a ticket the Driver must be shown the staff. When an assisting engine is allowed, by the authority of the Superintendent of the Line, to return to station from which it started, without running through the entire section, it must always carry the staff.

15. (a) When a ballast train has to work on the line, the staff only must be given to the Driver in charge of it. If a ballast train is required to run over a section of the line from one staff station to the other without stopping to work on the way, it may travel with staff or ticket as required. Under no circumstances must a ballast train stop to do work on the line unless the Driver is in possession of the staff.

(b) Should the Guard of a ballast train require his train to return to the staff station in rear instead of going through to the staff station in advance he must obtain permission of the Signaller before the train enters the section.

(c) The Driver of a ballast train that has to do work on the line, must be told, when handing the staff, to which end of the section it is to be taken, and at what time it is to be there, in order to clear the line for the next train.

Ballast train to work on line.

EXTRAITS FROM TRAIN STAFF AND TICKET REGULATIONS—Continued.

(d) When a ballast train working in the section has to return to the staff station in rear, no shunting outside the home signal at that end must be allowed until a man with hand signals and detonators has been sent out to protect such shunting.

Wrong staff.

16. Should the person in charge of the staff working receive a wrong staff he must return it by the most expeditious means; but a train or trolley must not be used for the purpose unless the person returning with the wrong staff is also in possession of the proper staff.

Staff lost or damaged

17. (a) Should a staff be lost, or damaged so that it will not open the ticket box, the Station Master or other responsible person in charge of the staff working at each end of the section must communicate with each other by the most expeditious means, and make arrangements to conduct the traffic over the section to which the staff belongs by Pilotman until the staff has been found or repaired, or a relief train staff and ticket boxes have been supplied; block working, where in operation, being maintained or the provisions of Block Regulation 25, "*Failure of Instruments or Bells*," clause (a) observed. The Lineman must be sent for immediately if necessary.

Working by Pilotman.

(b) The Station Master or other responsible person at each end of the section must agree as to who shall arrange for Pilot-working, and have a clear understanding as to the arrangements to be put into operation.

If the staff is not available, it will be generally found more expeditious for the Station Master or other responsible person at the opposite end of the section to that at which a train is next required to enter the section to undertake the arrangements, as the Pilotman with Pilot-working forms will then only have to go through the section in one direction to get the necessary forms signed.

If the staff has been damaged it should be retained to institute working by Pilotman, who must take possession of it, and until required by the Lineman or Driver, a worked staff and boxes the damaged staff must be shown by the pilotman to the Driver or Drivers of each train passing over the single line.

Should, however, a train be waiting to proceed it need not be detained for Pilot-working to be established, unless it is necessary to do so to avoid delay, but must be sent away with the damaged staff.

Pilotman's armet.

(c) A competent person must be appointed as Pilotman who must wear, round his left arm above the elbow, a red armet with the word "Pilotman" shown thereon in white letters, thus:—



If this armet is not immediately available the Pilotman must wear a red flag in the position indicated until the proper armet is obtained.

Issue of forms.

(d) The person arranging Pilot-working must fill up, sign, and address Pilot-working forms—(see page 55 for specimen) to:

- (i) The Signaller at each end of the staff section.
- (ii) The Signaller at any signal box that is intermediate in the staff section provided such signal box is open or likely to be open during the time Pilot-working is in operation.
- (iii) The person who will act as Pilotman.
- (iv) The Station Master at each end of the staff section, except where the signal box at which Pilot-working commences or finishes is not at a station and the ordinary working at the station will not be interfered with.

These forms must be handed to the Pilotman who must also sign all the forms issued and deliver the necessary form to the Signaller in charge of the signal box at which the Pilot-working commences and then proceed to the other end of the section.

The Pilotman when proceeding to the other end of the section to deliver the forms must do so as quickly as possible, either by rail or road, using the best means at his disposal for the purpose, but must not, unless the train staff is in his possession, use an engine or any railway vehicle other than a trolley.

The Pilotman must leave the necessary form with the Signaller at any intermediate signal box and must advise the Station Master at any intermediate station that Pilot-working is in operation.

EXTRACTS FROM TRAIN STAFF AND TICKET REGULATIONS—Continued.

On arrival of the Pilotman at the other end of the staff section he must deliver necessary forms to the Station Master and Signalmen, and if in possession of the staff show it to the Signalmen.

Each person when receiving the form must sign the Pilotman's form.

When a Station Master himself acts as Pilotman he must retain only the Pilot-form, and unless his station comes within the exception mentioned in section (iv) clause (d), he must address and give a form to the person he leaves in charge of the station.

Should any intermediate signal boxes or stations be opened after Pilot-working commenced, the Pilotman must, as soon as practicable, advise the persons in charge of such places that Pilot-working is in operation. He must also hand forms by the person who arranged the Pilot-working and himself, to the Signalmen at each end, who must sign the form held by the Pilotman.

Should the speaking instruments have failed and the train staff be lost or the Station Master or person in charge at each end of the section must be absent for Pilot-working, and the Pilotman appointed at each end must proceed along the railway in order that they may meet, and on doing so they must go together to the nearer or more convenient end of the staff section. The Pilotman who returns to the station from which he started must collect the forms which had been delivered to him at the other end of the section, and return them and the train staff to the person who filled them up, and the latter must then cancel them by writing the word "Cancelled" across them. The other Pilotman must hand one of his forms to the Station Master or person in charge and one to the Signalmen, and then act as Pilotman.

Failure of speaking instruments.

When practicable the Post Office telephones should be used to arrange Pilot-working instead of appointing two men as laid down in the preceding paragraph.

Station Masters and persons in charge issuing and receiving Pilot-working forms will be responsible for the Inspectors, Signalmen, and others concerned at stations being made acquainted with the circumstances immediately, and acted in their necessary duties.

Twelve Pilot-working forms must be kept in a convenient place at each staff end, so as to be available at any time.

After a form has been signed in accordance with clause 1 of this Regulation, it may be used to pass a train over the section by the permission and under the control of the Pilotman. When block working or block signalling is in operation, the form may be used to pass a train over a section by the permission of the Signalmen, and it shall be used to pass a train over a section by the permission of the Signalmen.

Pilotman obtains form from Station Master or Signalmen at each end of section.

The Pilotman must inform the Driver or Drivers and the Guard of each train of the circumstances, and when practicable must accompany every train. When a train is to be followed by one or more trains in the same direction before the first train has been started from the other end, the Pilotman must personally accompany each train to proceed and must ride upon the engine of the last train. Regulations applying to a train carrying the train staff will apply to a train accompanied by the Pilotman and the Regulations applying to a train carrying a train staff will apply to a train accompanied by the Pilotman's authority. A train accompanied by the Pilotman must not be accompanied by him.

(h) Signalmen must not allow any train to pass into any section that is being worked by Pilotman unless he is present.

(i) Should it be necessary to change the Pilotman or should the Signalmen be changed during Pilot-working the provisions of Rule 20 must be observed.

Change of Pilotman or Signalmen.

When ordinary working can be resumed, the Pilotman must withdraw the train staff from one end of the section, then take the staff, if available, to the other end of the section, and after delivering it to the Signalmen there and with the Pilot-working forms at that end of the section, the train staff will be again issued in accordance with the Regulations. All forms which have been issued must be cancelled and the cancelled forms must be sent to the Divisional Superintendent or District Manager.

Resumption of ordinary working.

After the Pilot-working forms have been issued ordinary working must not be resumed until such forms have been withdrawn and cancelled, although the missing form has been subsequently found or the damaged staff repaired.

EXTRACTS FROM TRAIN STAFF AND TICKET REGULATIONS—Continued.

When missing
staff is found.

(k) If the missing staff be found it must be handed to the Station Master or person in charge of the staff working at either end of the section to which it applies, who must, if the staff be undamaged, make arrangements for ordinary working to be resumed as provided for in the preceding clause. Before the regular staff is brought into use the relief staff and ticket boxes, if supplied, must be withdrawn.

Working of
sidings
connections.

(l) In the event of failure of the apparatus in a section in which there are siding connections controlled by staff or ticket and it is necessary for trains passing through the section to stop at the sidings for traffic purposes the Pilotman must request the Lineman to accompany him with the train to the sidings and arrange for the points to be released to enable the shunting to be performed; the Pilotman must remain with the Lineman at the points until they have been restored to their normal position.

Train carrying
staff disabled.

15. (a) In the event of a train which carries the staff becoming disabled between two staff stations, the Fireman must take the staff to the staff station at which assistance can be obtained, inform the person in charge there of the circumstances and show him the staff. Except as shown in the following paragraph, the Fireman must personally hand the staff to the Driver of the assisting train, and conduct the assisting train to the disabled train.

Should it be found on arrival of the Fireman at the staff station that assistance can be obtained more readily at the opposite end of the section the staff must be transferred to that end of the section by the most expeditious means. If time would be saved by the staff being so transferred by a competent person other than the Fireman, the Fireman may give up the staff for this purpose and then return to his train. The person in charge must arrange for the staff to be conveyed to the opposite end of the section and advise the Signaller in that that the staff is being so transferred.

Train carrying
ticket disabled.

(b) (i) Should the train that fails be carrying a ticket instead of the staff assistance must, except as provided in paragraph (ii), be obtained from the station at which the ticket has been left. If the box ahead be the nearer, the Fireman may, instead of going in the direction of the station where the staff is, go to the box ahead so that the Signaller there may advise the Signaller in rear that assistance is required, and if assistance is obtained from the rear in such circumstances the Guard must ride on the assisting train and point out to the Driver the position of the disabled train. The Fireman must take the ticket with him when he goes for assistance.

(ii) Should a train carrying the staff or a ticket be following the disabled train and have left the staff station in rear, assistance must only be given by such following train, or the engine thereof, and the staff must not in these circumstances be transferred to the staff station in advance as described in paragraph (ii). If the engine of the following train is detached for the purpose of rendering assistance, the Driver of such engine must not, if he is in possession of a ticket, after the disabled train has been removed from the section return to his own train without the written authority of the Guard or Signaller, as the case may be, as prescribed in Rule 183 or 184.

(iii) If assistance can be more readily obtained at the staff station towards which the train was proceeding, immediate steps must, except as shown in paragraph (ii), be taken to have the staff transferred from the rear staff station to that end of the section, and the Fireman must accompany the assisting train to the disabled train.

Disabled train
not to be moved.

(c) The Driver of the disabled train must not allow it to be moved until either the Fireman has returned with the staff, or, when arrangements have been made for the staff to be transferred to the opposite end of the section as provided in the second paragraph of clause (a), until the assisting train has arrived with the staff.

Station Master
responsible for
arrangements.

(d) The Station Master at the station where assistance is provided will be responsible for carrying out all necessary arrangements during the continuance of the obstruction.

Protection of
disabled train.

(e) The disabled train must be protected in accordance with the Rules applicable to trains stopped by a defective signal, obstruction or other exceptional cause. The Fireman doing this on his way for assistance, and the Guard in the opposite direction. In the case of a light engine the Fireman must protect the engine as directed when going for assistance, but before doing so must protect the engine in the opposite direction by putting down 3 detonators on the line 10 yards apart not less than 300 yards from the engine.

INSTRUCTIONS FROM TRAIN STAFF AND TICKET REGULATIONS—Continued.

(c) The assisting train or engine must run at reduced speed and great caution Caution to be observed
must be observed by all concerned.

19. (a) When a train has to be left, or divided and the rear portion left, on the single line, owing to accident or inability of the engine to take the whole forward, or from any other cause, the following instructions must be observed:— Intentional division of train on single line.

(i.) The man who divides the train must inform the Driver how many vehicles, if any, are being taken forward, and after sunset or during fog or falling snow, or if the division is made in a tunnel, he must place ON THE RAIL 3 detonators 10 yards apart not less than 100 yards ahead of the portion left behind.

(ii.) The Driver must not, if he is in possession of a ticket, return for the train or rear portion without the written authority of the Guard as prescribed in Rule 183, clause (f). If the Driver is in possession of the staff he may return to the train or rear portion without obtaining instructions from the Guard authorising him to do so. The Driver must, before returning to the rear portion of the train, satisfy himself that the front portion is complete.

(iii.) The Guard must protect his train in the rear in accordance with the Rules applicable to trains stopped by accident, failure, obstruction or other exceptional cause.

(b) When a train or portion of a train has been accidentally left on the single line, the following instructions must be observed:— Accidental division of train on single line.

(i.) If the train has become accidentally divided between two signal boxes and the front portion has not arrived at the home signal for the box in advance, the front portion may be set back to the rear portion in accordance with Rule 183, clause (4).

(ii.) The Driver must (except in the case described in preceding paragraph) take the front portion forward to the nearest place where it can be disposed of.

(iii.) The Guard must SECURE THE TRAIN OR REAR PORTION THEREOF AND place 3 detonators on the line 10 yards apart not less than 100 yards ahead of the portion left behind. He must then protect his train in the rear in accordance with the Rules, whether the Driver is in possession of the staff or a ticket.

(iv.) Except as otherwise provided for in the following paragraph, the Driver must not, whether he is in possession of the staff or a ticket, return for the train or rear portion thereof, until it has been ascertained that the whole of the vehicles of the train have come to a stand, and, if there is no signal box near from which this information can be obtained, the Driver must send his Fireman on foot for the purpose. If the Driver is in possession of a ticket he must not return for the train or rear portion until he has received written authority from the Guard as prescribed in Rule 183, clause (f).

If the gradients of the line over which the train has run are such that it can be concluded that the train or rear portion is at a stand at the time when the engine requires to return, the Driver may, if in possession of the staff, return for his train or rear portion thereof, without having ascertained that the rear portion has been secured, but must proceed cautiously, and if approaching a signal box he must bring his engine to a stand at the distant signal and the Fireman must walk in front from there to the home signal.

(v.) If the divided train has carried the staff and it is necessary to allow an engine to enter the section at the signal box in rear of the divided train for the purpose of removing the obstruction to the more convenient end of the section, the staff must be conveyed from the signal box in advance to the other end of the section by the most expeditious means, but for this purpose an engine or any railway vehicle other than a trolley must not be used. If necessary the train staff may be transferred in accordance with Regulation 18, clause (a).

The Driver when returning for the portion of the train that has been left behind must not pass any signal box without the permission of the Signal- Driver returning for rear portion of train.

(d) The Driver must retain possession of the Staff or Ticket as the case may be, until the train is removed from the section, unless it is necessary for another engine to be used for the rear portion. If however, it is necessary to dispose of the front portion at a signal box of the next Station, the Driver must give up the Staff or Ticket and the engine must be used for the rear portion until he has again received the Staff or Ticket, as the case may be. Driver to retain possession of Staff or Ticket

EXTRACTS FROM TRAIN STAFF AND TICKET REGULATIONS—*Continued.*

ASSISTANCE
FROM REAR.

(e) A white light must be placed on the leading vehicle of the rear portion before that portion is propelled to the signal box in advance or drawn back to the signal box in rear.

Line obstructed.

20. (a) Should an accident occur of such a nature as to block the line, and the traffic is likely to be stopped for any considerable time, special arrangements must be made for working the trains to and from the staff station on each side of the obstruction.

The staff must be retained to work trains between the point of obstruction and the staff station on one side, no tickets being used; on the other side Pilot-working must be arranged in accordance with Regulation 17 and the Pilotman must accompany each train to and from the point of obstruction.

The person arranging Pilot-working must also issue a Pilot-working form for the man in charge at the point of obstruction and the Pilotman must deliver the form.

(b) Block working, where in operation, must be suspended and two competent men, provided with hand signals and detonators, must be appointed to protect the obstruction, one on each side.

When the
obstruction is cleared.

(c) When the line is again clear no train must be allowed to pass the point where the obstruction existed without the staff. The Pilotman must accompany the first train carrying the staff to the staff station, and, after the Pilotman has withdrawn his arrangements for Pilot-working, ordinary working may be resumed.

Staff Stations
and the
Appendices to
Service Time
Tables.

21. THE APPENDICES TO THE SERVICE TIME TABLES WILL CONTAIN A LIST OF THE STAFF STATIONS, AND OF THE SIDING PLANTS CONTROLLED BY THE TRAIN STAFF.

FORM OF TRAIN STAFF TICKET.

No.	
GREAT WESTERN RAILWAY.	
TRAIN STAFF TICKET.	
.....Line or Branch.	
To the Driver of.....train from	
to	
You are authorised, after seeing the train staff for the section, to proceed from .....to....., and the train staff will follow	
Signature of person in charge	
Station	
Date	
(Back of Ticket.)	
This ticket must be given up by the Driver to the person in charge of the staff working at the place to which he is authorised to proceed, immediately on arrival.	

EXTRACTS FROM TRAIN STAFF AND TICKET REGULATIONS—Continued
PILOTMAN'S FORM.

[Form referred to in Regulations 17 and 20 of the Regulations for working on Single Lines by Train Staff and Ticket System, and in Regulations 11 and 13 of the Regulations for working on Single Lines by one engine in steam or two or more engines coupled together.]

GREAT WESTERN RAILWAY. (2989 B.)

TRAIN STAFF AND TICKET SYSTEM, OR ONE ENGINE IN STEAM, &c.

WORKING OF SINGLE LINES BY PILOTMAN—

- * { (a) WHEN STAFF IS LOST OR DAMAGED
 (b) DURING OBSTRUCTION

This Form must be filled up and used whenever it is temporarily necessary to work the traffic by Pilotman.

..... Station

..... 19 ..

(a) The train staff for the section between
 and having been lost or damaged the traffic between these places will be worked by Pilotman. will act as Pilotman and no train must be allowed to pass into the section unless he is present and personally orders the train to start. In the case of lines worked by one engine in steam, etc., he must accompany every train over the section.

* { Block working, where in operation, must be maintained or the provisions of Block Regulation 25, clause (a), observed.

(b) The single line between and
 being obstructed, the traffic between and the point of obstruction will be worked by Pilotman. will act as Pilotman, and must accompany every train to and from station and the point of obstruction.

Block working, where in operation, must be suspended.

This order is to remain in force until withdrawn by the Pilotman.

(Signed).

To Time.

Noted by at

Noted by at

Noted by at

Noted by at

Noted by at

* (b) Noted by at point of obstruction.

Noted by Pilotman.

* Strike out portions (a) or (b) not applicable.

FORM OF PILOTMAN'S TICKET WHERE IN USE.

TRAIN STAFF AND TICKET SYSTEM.

PILOTMAN'S TICKET.

To be used when it is necessary to work the traffic of a single line by Pilotman when the Train Staff is lost, or so damaged that it will not open the ticket to .

To the Driver of train from.....
 to.....

You are authorised to proceed from... to ..
 Pilotman following

Signature of Pilotman.

Date.....

This Ticket must be given up by the Driver to the person in charge of the staff working at the place to which he is authorised to proceed, immediately on arrival.

NOTE.—Pilotman's Tickets are not used on the Great Western Railway.

REGULATIONS FOR WORKING ON SINGLE LINES BY ONE ENGINE IN STEAM OR TWO OR MORE ENGINES COUPLED TOGETHER.

(Where train signalling by the block system is in operation on the section the Regulations for Train Signalling on Single Lines by the Train Staff or Train Staff and Ticket Block System will apply.)

Only one engine in steam or two or more coupled together, to be on line at a time.

1. Only one engine in steam, or two or more coupled together, which are then to be treated as one engine or train, must be allowed to be on the line at a time.

Train staff.

2. A train staff is provided, which has engraved or marked on it the name of the staff station at each end of the section to which only it applies, and an engine must not be permitted to enter upon the Single Line unless the Driver is in possession of this staff. When, however, a train is worked by two or more engines the staff must be shown to the Driver of each engine and handed to and carried by the Driver of the rearmost engine.

Staff to be given to Driver before commencing journey and given up at end of journey.

3. The staff must be shown or given to the Driver, as directed in Regulation 2, by the person in charge of the staff working immediately before commencing a journey. At the end of the journey the Driver must give up the staff to the person in charge of the staff working at that place, and be careful not to take the staff away and the station at which it should be left.

Penalty for Driver leaving without staff

4. Except as provided in Regulations 2, 11 and 13, a Driver will render himself liable to dismissal if he leave a staff station without the proper staff.

Place for staff

5. The staff, when at the station, must be kept in the place provided for it; when on the engine it must be carried in a safe position.

Custody of staff

6. The person in charge of the staff working for the time being is the sole person authorised to receive and deliver the staff.

Driver not to start until proper signals exhibited

7. A Driver after receiving the staff, must not start until the proper signals have been exhibited.

Train to arrive completely before another train enters section.

8. The person in charge of the staff working must satisfy himself when a train arrives that it is completely with the lamp attached, before he allows another train to enter the section.

Points to be padlocked or securely held, Signal points controlled by train staff

9. All points not interlocked must be padlocked or securely held by hand for the safe passage of trains in the facing direction.

When shunting has to be performed at a siding the points of which are controlled by the train staff the Driver must hand the staff to the Guard or man in charge of the siding to enable the points to be unlocked. When shunting has been completed, and the points have been placed and locked in the proper position for trains to pass upon the running line, the Guard or man in charge of the siding must return the staff to the Driver, and the latter must not proceed on his journey until he has obtained possession of it.

Wrong staff.

10. Should the person in charge of the staff working receive a wrong staff he must return it by the most expeditious means; but a train or trolley must not be used for the purpose unless the person returning with the wrong staff is also in possession of the proper staff.

Staff lost.

11. Should a staff be lost, the Station Master or other person responsible in charge of the staff working at each end of the section must communicate with each other by the most expeditious means and take arrangements to prevent the traffic over the section to which the staff belongs by Pilotman in accordance with the instructions contained in Regulation 14 of the Regulations for working on Single Lines by Train Staff and Ticket so far as they apply. (For specimen Pilot working form see page 55)

The Pilotman must accompany every train over the section.

Disabled train

12. In the event of a train becoming disabled and assistance being required, the Fireman must confer with the Guard as to the best means of obtaining assistance, then take the staff to the station whence assistance can be obtained, inform the person in charge thereof of the circumstances and show him the staff. The latter, on receipt of such information, may allow an assisting train to enter upon the single line. The Fireman must personally hand the staff to the Driver of the assisting train, and conduct the assisting train to the disabled train.

The second sentence of clause 5 (b) to be amplified to read:

In addition all facing points not equipped with a facing point lock (with facing point lock car or track circuit lock) must be clipped and padlocked, the keys being kept by the Officer or other person in charge of the operations.

(G.A.16. 5/46.O.M.12278)

Clause 8 (b) — As soon as a train has passed on to the loop line and inside the catch or siding points the Guard (or Fireman in the case of a light engine) must advise the Signaller by the quickest means either by telephone or by the exhibition of a hand signal by day, or a white light by night or during fog or falling snow to indicate that the train is in clear. The Guard's hand-signal must be given from the brake-van and the Fireman's from the footplate. The Guard (or Fireman) must continue to exhibit the hand signal until it is acknowledged by the Signaller who will, at night or during fog or falling snow, exhibit a white light held steadily.

(G.A.24.—11 49.)

(c) During fog or falling snow, every train must be stopped dead before entering the section, whether occupied or not. In clear weather, if the loop line is unoccupied, a train may run direct into the loop line at a speed not exceeding 10 miles per hour.

G.A.3.—12/37. L.K.1/4513/13.)

The following to be added as Clause 9 (d) :—

Trains which are admitted to a permissive line immediately following a Diesel car must be brought to a stand at the signal box in accordance with clause 6 of the Permissive Block Regulation and the Driver verbally told that a Diesel car is in the section ahead. The Driver of a train which is so admitted must proceed with caution and must take care not to buffer up to the Diesel car.

(G.A.3.—12/37. O.M.11767.)

REGULATIONS FOR WORKING OF SINGLE LINES—Continued.

1. A train must be protected by 3 detonators, 10 yards apart, being Protection.
more than 100 yards from the train, by the Fireman on his way assistance.
- The Driver of the disabled train must not allow it to be moved until the Fireman Disabled train
has returned and the assisting train has arrived with the staff. not to be moved
- The assisting train must run at reduced speed and great caution must be observed Caution to be
by all concerned. observed.
1. Guard of the disabled train will be responsible for the safe and proper working of the line until it is again clear.
13. (a) Should an accident occur of such a nature as to block the line, and Line
traffic is likely to be stopped for any considerable time, if necessary special arrange- should
ments must be made for working trains to and from the obstruction, where this is possible, the staff being retained to work trains between the point of obstruction and the staff station on one side, and on the other side Pilot working being arranged as shown in Regulation 11.
- The person arranging Pilot-working must also issue a Pilot-working form for the man in charge at the point of obstruction and the Pilotman must deliver this form.
- (b) A competent person provided with hand signals and detonators, must be appointed to protect the obstruction, on one or both sides as required.
- (c) When the line is again clear, no train must be allowed to pass the point where the obstruction existed without the staff. The Pilotman must accompany the first train carrying the staff to the staff station and, after the Pilotman has withdrawn, his arrangements for Pilot-working, ordinary working may be resumed.

EXTRACTS FROM THE REGULATIONS FOR SIGNALLING TRAINS AND ENGINES BY PERMISSIVE BLOCK SYSTEM OVER GOODS RUNNING LOOP LINES AND OTHER PERMISSIVE LINES.

1. The loop lines will be worked in accordance with the Standard Diesel Loop Block Diagrams with the following modifications and additions:—
4. (a) More than one freight, mineral, or empty coach train or engine may be allowed to be in the block section at the same time.
5. (a) The goods loop line must not be used as a running line for passenger trains except where the usual running line is not available, either by reason of a defect of the track or a signal failure. A train Engineer by previous arrangement with the Divisional Superintendent or District Traffic Manager.
- (b) Whenever the goods running loop is used for passenger trains as set out above, a permissive block working must be maintained and passenger trains must only be accepted on the loop line at a signal and the regulations applying to the main line. In addition, all facing points must be equipped and pulled, the keys being kept by the officer in charge of the operations.
- (c) A passenger train must never enter a goods loop unless the loop arm has been lowered, and the driver has received verbal instructions from the signalman to do so.
8. (a) As soon as a train has passed on to the loop line and inside the catch or stopping points the Guard must exhibit a hand signal by day and a green light by night to the signalman to indicate that the train has passed in clear.

9. (a) Unless the section is clear every train before entering must be stopped at the signal controlling the entrance to the loop and the signalman must then lower that signal to allow the train to draw into the loop. In case where the loop line is unoccupied a train may run direct into the loop and at a speed not exceeding 10 miles per hour.

(b) If the gradient of the loop falls more than 1 in 100 in the same direction as the train is running, every train must be stopped dead before entering the loop, whether there is another train in the loop or not.

(c) During fog or falling snow, every train must be stopped dead before entering the loop, whether occupied or not.

(d) Trains which are admitted to a permissive line immediately following a Diesel car must be brought to a stand at the Signal Box and the Driver verbally told that a Diesel car is in the section ahead. The Driver of a train which is so admitted must proceed with caution and must take care not to buffer up to the Diesel car. (G.A. 7 41, O.M. 12000)

10. In all cases, irrespective of the nature of the signal, it is the responsibility of the signalman to stop a train at the signal to enter the loop only as an indication that the points are in a proper position, and must expect that the road will be clear through the loop, and the train will be held responsible for stopping their trains short of any obstruction which may be in front of them.

11. After sunset or during fog or falling snow, in addition to the usual tail lamp, while trains are in the loop must be exhibited on the side further from the Main Line and a white light on the side nearer the Main Line. Loop runs alongside a Parallel Line and forms a third parallel line trains on the must not exhibit any side lights. (G.A. 7 41, O.M. 12000)

12. If trains passing over the loop line must never exceed 10 miles an hour, and during fog or falling snow must not exceed 4 miles an hour.

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TRAFFIC FROM REGULATIONS FOR SIGNALLING—GOODS LOOP LINES *(continued)*

14. If it is desirable to back a train into the loop, this may be done provided there is no train travelling in the section in the right direction towards it, or that any train which has been admitted has come to a stand.

15. If a train is already at a stand in the section, and it is necessary afterwards to shunt a train from the main line or siding to the loop line, the train on the loop line may be set back to admit the train from the main line, after the Guard of the rear train has gone back at least 300 yards or to the box in the rear or the entrance of the loop if within that distance, to protect it.

17. Trains stopped by accident, failure, obstruction or other exceptional cause must be protected in accordance with the provisions of Rules 178 to 188. If a second train arrive before the obstruction has been removed, the Guard of the second train must protect his train as directed in those Rules, and the Guard of the first train, having assured himself that the Guard of the second train has gone back with the necessary hand danger signal and detonators for the protection of the second train, may then rejoin his own train. If other trains arrive, the same arrangements must be carried out, the Guard of the last train providing the protection for the whole.

During fog or falling snow when trains are stopped at a Home Signal, the Guard of the first and each succeeding train must instantly go back with detonators and hand signals to protect it, and must proceed 100 yards, or to the box in the rear or the entrance of the loop if within 100 yards, and put down one detonator on the loop line, and at once return to his train. Should a train be assisted by an engine in the rear, the duty of so protecting the train will devolve upon the Guard in the same way as if no engine were assisting in the rear. In the case of a light engine the Fireman must, if the engine is stopped at a Home signal or in the rear of a train or another light engine, place one detonator on the rail as above directed. When there are two or more light engines coupled together, the Fireman of the rearmost engine must perform this duty. When a detonator, which has been put down as described above, is exploded by an engine, the Driver must be prepared to stop clear of the train or engine in front of him. Should the train or engine to the rear of which the detonator was placed, have moved forward, he must proceed cautiously so as to be able to stop at any moment. When a train is brought to a stand, the Driver must not start until the Guard has had time to return to his brake van.

In clear weather, if not required for shunting operations, the Guard instead of going back must be responsible for carrying out these instructions in the same manner as the train Guards.

The provisions of Rule 53 will not apply on lines operated under these Regulations.

If it is necessary to allow a train to remain on a loop line, the Guard must be present at the rear of the train, and before leaving, place one detonator in the rear of the train or at the entrance of the loop if within that distance, in the rear of the train and, during the absence of the Guard, the Driver must be prepared to stop at any moment. The Inspector, or person in charge of the line, must be aware of the circumstances and regulate the working accordingly.

20. If a train passes over a public crossing the crossing must never be fouled by a train, and the crossing must be kept clear of any obstruction. The Signalman or Guard in the crossing box will be responsible for keeping the crossing clear.

21. Points on a loop line, and points at the entrance of a goods loop line, such points must be worked by a key which must be kept in the nearest signal box.

22. When the signal box in the rear is switched out of circuit, the loop may be used as a refuge siding from the signal box in advance, and when the box in advance is switched out the loop may be used as a refuge siding from the signal box in the rear.

If at the usual switching-out time there should happen to be a train standing in the loop, the signal-box (at either end) may be switched out, provided the train is not required to leave the loop at that end.

EXTRACTS FROM REGULATIONS FOR WORKING ON GOODS LINES WHERE THE ABSOLUTE BLOCK SYSTEM IS NOT IN OPERATION OR WHERE NO SPECIAL REGULATIONS ARE IN FORCE.

(NOT APPLICABLE TO SINGLE LINES OF RAILWAY).

Working of Signals.

1. Except where instructions are issued to the contrary, the normal indications of fixed signals must be:—Stop signals—**Danger**. Distant signals—**Caution**. and before any signal is lowered care must be taken to see that these Regulations have been complied with.

Interval between Trains.

2. UNLESS A SIGNALMAN IS SATISFIED THAT THE LINE IS CLEAR TO THE NEXT SIGNAL BOX, OR UNLESS INSTRUCTIONS ARE ISSUED TO THE CONTRARY, NO TRAIN MUST FOLLOW ANOTHER TRAIN ON THE SAME LINE WITHIN 5 MINUTES. IF ANY TRAIN SHOULD ARRIVE 5 MINUTES AND LESS THAN 10 MINUTES, AFTER THE PASSING OF THE PREVIOUS TRAIN, THE SIGNALMAN MAY, AFTER HAVING BROUGHT THE TRAIN TO A STAND, AND VERBALLY WARNED THE ENGINE DRIVER OF THE TIME OF THE PASSING OF THE PRECEDING TRAIN, ALLOW IT TO PROCEED, THE HOME SIGNAL BEING LOWERED AND A GREEN HAND SIGNAL SHOWN TO THE ENGINE DRIVER HELD STEADILY.

IN ANY CASE, UNLESS A SIGNALMAN IS SATISFIED THAT THE LINE IS CLEAR TO THE NEXT SIGNAL BOX, A FOLLOWING TRAIN MUST NOT BE ALLOWED TO ENTER THE SECTION UNTIL IT HAS BEEN BROUGHT TO A STAND AND THE DRIVER VERBALLY CAUTIONED. DRIVERS WILL BE HELD RESPONSIBLE FOR STOPPING THEIR TRAINS SHORT OF ANY OBSTRUCTION.

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control

continued

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The following note to be inserted on page 58 following clause 22 :—

Note.—The above regulations are to be regarded as also applying to loops and goods lines controlled by one signal box insofar as they are applicable.

(G.A.12. 4/43, O.M. 12091.)

EXTRACTS FROM REGULATIONS FOR SIGNALLING—GOODS LINES—Continued

signal is lowered for a train at a place where owing to the position of the signal box a green hand signal cannot be exhibited to the Driver, the Driver must proceed at such a speed as will enable him to stop short of any obstruction.

When a train is approaching the rear of another train, the Driver must proceed cautiously and at such a distance as will enable him to avoid colliding with the train ahead, and he must not pass a signal which has been lowered for the train ahead until the signal has been replaced to Danger and lowered for his train.

When time following.

3. During fog or falling snow trains must travel at reduced speed and great caution must be observed.

Fog or falling snow

4. (b) Should a train pass without a tail lamp, or if a Signaller observe or a vehicle on fire, a hot axle box or other mischief, or should a train become divided or vehicles be running away, he must, if necessary, place and maintain his signals at Danger, and take any other measures that may be necessary and most expedient under the circumstances. He must also, if necessary, stop any following train or any train going in the opposite direction, and instruct the Driver as required so as to avoid danger in the event of the line on which he is running being obstructed. The Driver of the train so warned must caution the Driver of any train proceeding on the other line, and on arrival at the signal box in advance advise the Signaller of the circumstances. The Signaller must also, if necessary, communicate with the Station Master by the most expeditious means.

Train incomplete
Signal
Signal

5. No engine or vehicle must be shunted or moved from one running line to the other, or from the running line into a siding or from a siding on to the running line, until the signals, where provided, protecting the movement, have been exhibited in one or both directions as may be required. The running line must not be obstructed by shunting operations when the distant signal has been lowered for a train until such train has passed or has come to a stand.

Shunting operations to be protected.

6. Trains stopped by accident, failure, obstruction or other exceptional cause must be protected in accordance with the provisions of Rules 178 to 188. If a second train arrive before the obstruction has been removed, the Guard of the second train must protect his train as directed in those Rules, and the Guard of the first train, having assured himself that the Guard of the second train has gone back with the necessary hand Danger signal and detonators for the protection of the second train, may then rejoin his own train. If other trains arrive, the same arrangements must be carried out, the Guard of the last train providing the protection for the whole.

Protection of train stopped by accident, failure, obstruction, or other except

7. During fog or falling snow, when trains are stopped at a home signal the Guard of the first and each succeeding train must at once go back 100 yards, or to the Box in the rear or the entrance to the line if within 100 yards, and place one detonator on the rail to protect his train and then return to his brake. Should a train be assisted by an engine in the rear, the duty of so protecting the train will devolve upon the Guard in the same way as if no engine were assisting in the rear.

Guard to protect train during fog or falling snow.

In the case of a light engine the Fireman must, if the engine is stopped at a home signal or in the rear of a train or another light engine, place detonators on the rail as above directed. When there are two or more light engines coupled together the Fireman of the rearmost engine must perform this duty.

When a detonator, which has been put down, as described above, is exploded by an engine, the Driver must be prepared to stop clear of the train or engine in front of him. Should the train or engine to the rear of which the detonator was placed, have moved forward, he must proceed cautiously so as to be able to stop at any moment.

When a train is brought to a stand, the Driver must not start until the Guard has had time to return to his van.

8. Trains conveying passengers must not be allowed to run on lines used for goods and mineral traffic only, except when it is necessary to divert them in case of accident or other emergency, when printed or written instructions must be issued to the Signaller, and special arrangements made for the working of the trains. Such a train must not be allowed to leave the signal box in rear until it has been ascertained that the line is clear to the signal box in advance, and no train must be allowed to follow a train conveying passengers until it has been ascertained that the latter train has passed the signal box in advance. All facing points not equipped with facing point lock and bar must be secured by clip or scotch.

Working of passenger trains

In order to obviate delay to trains, the speaking instruments may be used for the purpose of transmitting instructions for the working of passenger trains over these Goods lines, provided all such instructions before transmission are written and signed by the Station Master or person in charge of the special working arrangements, the messages to be repeated by the receiver to sender to ensure accuracy.

REGULATIONS FOR WORKING ON SINGLE LINE BY PILOT GUARD.

Pilot Guard's
badge
Pilot Guard to
start all trains.

1. The Pilot Guard will be distinguished by a special badge; and a train must not under any circumstances be allowed to run on the line unless it is either accompanied or personally started by the Pilot Guard.

Pilot Guard,
when practicable,
to accompany
every train.

2. The Pilot Guard must, when practicable, accompany every train, but when it is necessary to start two or more trains from one end of the section under his control before a train has to be started from the other end he must furnish the Guard in charge of a train not accompanied by himself with one of the printed Pilot Guard's tickets (see specimen on page 61) where provided, properly filled up and signed. He must also personally start such train, and himself accompany the last train. The ticket issued will apply only to the journey to the other end of the section where it must be given up immediately to the person in charge of the station. All tickets so given up must be cancelled at once by the word "Cancelled" being written across them, and the tickets must afterwards be dealt with in accordance with the instructions of the Divisional Superintendent or District Traffic Manager. The Driver must not start his train without seeing the Pilot Guard, and, when the Pilot Guard does not accompany the train, until he has received from the Guard of his train the Pilot Guard's ticket (where provided) authorising him to proceed. **PILOT GUARD'S TICKETS ARE NOT USED ON THE GREAT WESTERN RAILWAY.** A Driver working an engine unaccompanied by a Guard must observe the same Regulations as herein laid down for a Guard with a train.

Driver working
without Guard.

Starting of
trains.

3. Before starting any train, the Pilot Guard must ascertain from the Guard of the train that all is right, and that he is ready to proceed.

Signalman not
to allow train to
proceed unless
authorised by
Pilot Guard.

4. No train must be allowed to enter upon any single line section without the permission of the Signalman who must not allow it to proceed until he is perfectly satisfied that the Pilot Guard is accompanying it or has given authority for it to start.

Points to be
padlocked or
securely held.

5. All points that become facing points to trains, if not interlocked, must either be padlocked or securely held by hand for the safe passage of such trains.

Disabled train.

6. (a) In the event of a train accompanied by the Pilot Guard becoming disabled, the Pilot Guard must make the best arrangements possible for obtaining assistance with the least delay. If it be necessary for the Pilot Guard to leave the engine on the line he must, before leaving, give the Driver a written order not to move his engine until he returns.

(b) Should a train unaccompanied by a Pilot Guard become disabled, the Guard of the train must take the necessary steps for the protection of his train, and communicate with the Pilot Guard as soon as possible.

Portion of
train left on
single line.

7. (a) When a train or a portion of a train is left upon the single line from accident, or inability of the engine to take the whole forward, or from any other cause, the Driver must not, if he be in possession of a Pilot Guard's ticket, or if unaccompanied by the Pilot Guard, return for it, except upon written instructions from the Guard of the train, as prescribed in Rule 193, clause (f). If the Pilot Guard be with the train and accompany the engine with the first portion, the Driver may return to the rear portion of his train without obtaining instructions from the Guard of the train authorising him to do so, but the Pilot Guard must accompany the engine when it returns for the rear portion of the train.

(b) When the train has to be divided the man who divides it must inform the Driver how many vehicles, if any, are being taken forward, and after sunset or during fog or falling snow, or if the division is made in a tunnel, he must place 3 detonators on the line 10 yards apart not less than 100 yards ahead of the portion left behind.

In the case of the train being accidentally divided the Guard must place 3 detonators on the line 10 yards apart not less than 100 yards ahead of the portion left behind.

(c) The Guard must protect his train in the rear in accordance with the Rules applicable to trains stopped by accident, failure, obstruction or other exceptional cause whether the train is accompanied by the Pilot Guard or not.

(d) A white light must be placed on the leading vehicle of the rear portion before that portion is propelled to the signal box in advance or drawn back to the signal box in rear.

FORM OF PILOT GUARD'S TICKET.

No.

.....RAILWAY.

.....Line or Branch.

PILOT GUARD'S TICKET.

To the Guard and Driver of.....train,

You are authorised to proceed from.....to.....Pilot Guard
following.

Signature of Pilot Guard.....

.....Station.

Date.....

This ticket must be given up by the Driver to the person in charge of the
station to which he is authorised to proceed, immediately on arrival.

NOTE.—A Pilot Guard's Ticket is not used on the Great Western Railway.

SECTION I. (c.)

Matters relating to the Working and Maintenance of Piers 187-2,

SECTION I. (c.)

Matters relating to the Working and Maintenance of Points and Signals.

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INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEMS OF MAINTENANCE ON SINGLE LINES WORKED BY ELECTRIC TOKEN.—Page 65.

The following to be added to Clause 1 :—

Where authority is given by the Divisional Superintendent or District Traffic Manager, a motor trolley or inspection car may be stalled in a traffic signal at a time when the Ganger is not in possession of the occupation key, provided the consent of the Signaller is obtained, and the trolley is protected by a competent man with red flag and detonator.

(G.A. 1. 3/37. R. & R. Min. 915.)

INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEMS OF MAINTENANCE ON SINGLE LINES WORKED BY ELECTRIC TOKEN.—page 65.

Clause 3. The following to be added at the end of the second paragraph:

Where the key instruments are situated in a Signal Box it is the duty of the Signaller to replace in the instrument the key for one section before withdrawing the key for the section in which the Ganger next requires to enter, but this does not relieve the Ganger of the responsibility of satisfying himself that he is in possession of the correct key for the section next to be occupied.

(G.A. 7.—3/40. LK1/5482/8.)

INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEM OF MAINTENANCE ON SINGLE LINES.—Page 65.

Section 4, Paragraph (d) to be amended to read

(d) Whether occupation is required for trolleying or inspection of the line and whether it is a Motor Trolley, Motor Trolley with Trailer, or a Motor Trolley with Trailer and Inspection Car travelling throughout the Section.

(G.A. 19—10 48. L.K. 4 26.)

Section 4.—The following to be inserted as Clause (e) :—

(e) The points to which the Motor Trolley (with or without Trailer) or Inspection Car will be run and removed from the line.

(G.A. 19—10 48. L.K. 4 26.)

1. On trolleys have means of which (and 217), S carry out op of the works, key instrument The insp thoroughly e Each Ga A trailer will The mot to place, as r The mot The port motor trolley provided at p

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(d) W

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Section 6.—

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the motor the same

INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEMS OF MAINTENANCE ON SINGLE LINES WORKED BY ELECTRIC TOKEN.

N.B. The word "Ganger" in these instructions embraces also the Sub Ganger, or other man in charge authorised to arrange occupation of the line in the absence of the Ganger.

1. On certain sections of single line worked by Electric Token, motor inspection cars and motor trolleys have been provided for use in conjunction with occupation key instruments and telephones by means of which occupation of the line may be obtained without sending out Hand Signals. Rules 215 and 217. Should occupation of the line be required for trolley working, or should it be necessary to carry out operations which would interfere with the running of trains, or in case of any slip or failure of the works, or if from any cause the line is unsafe, the withdrawal of the occupation key from the key instrument will afford the necessary protection.

The inspection car is provided for the use of the Ganger, but in order that structures, etc., may be thoroughly examined he must walk over the entire length once a week.

Each Gang will be equipped with one motor-driven trolley, capable of carrying men, tools, etc. A trailer will also be available for conveyance of materials and tools.

The motor trolley will be utilised to run the gang to the site of the work and back, and from place to place, as necessary.

The motor trolley may also be used to convey Fogg signalmen to and from their posts.

The portable turntable provided must always be carried on the trolley and by this means the motor trolley can be moved from, or replaced upon, the rails expeditiously. "Run-offs" will be provided at places where occupation key instruments exist.

2. Neither the inspection car, motor trolley nor trailer must be placed on the line, nor must the line be obstructed in any way, unless an occupation key or electric token for the section of line concerned has been withdrawn and the Ganger is in possession of it.

Note.—In the case of failure of the electric token apparatus the inspection car or motor trolley may be used for the purpose of conveying the Plotman, carrying the Plot Papers through the section, or from the point where the token is located, in accordance with the Electric Token Block Regulation 24, clause 2, when, owing to failure of the apparatus, an electric token or occupation key cannot be withdrawn. The Signalman at each end of the section will be responsible for ensuring that a correct and proper understanding before the car or trolley is placed on the rails, and must record particulars in their Train Registers.

If, while an obstruction occurs on the line becomes unsafe during any time when the Ganger may be unable to obtain an occupation key or electric token, the protection required by Rule 2.7 must be provided and the circumstances explained to the Signalmen by telephone.

3. Each length of line is equipped with a group of instruments and the key to work with these instruments is marked upon it the length to which it applies. The key may be used in any key instrument in the group to which it belongs, but a key belonging to one group must not be used in any other group, so that it affords the Ganger the necessary protection, except when used in the group to which it applies.

When a Ganger has occasion to occupy a portion of the line belonging to one token section, and then a portion of the line belonging to another token section, he must be careful to replace the key appertaining to the one token section before asking for the key of the other token section.

4. When a Ganger wishes to obtain occupation he must either apply verbally to the Signalman or telephone from the instrument in which the occupation key has been left, stating:—

(a) Place or number of instrument from which message is being sent.

(b) Length of time for which occupation is required.

(c) Points between which it is intended to do the work.

(d) ~~When occupation is required for performing an inspection of the line, the points to which the motor trolley inspection car or motor trolley and trailer will be run and removed from the line.~~

5. The Signalmen at each end of the section must record full particulars respecting all applications for occupation in their Train Registers, and as an additional reminder, rollers must be placed on the levers of the signal controlling the entrance to the section in which the occupation is given. Record must also be made of the key instrument in which the key is replaced, and the time this is done.

Section 6.—Amended to read:—

On receiving the information set out in Clause 4, the Signalman must communicate with the Signalman at the other end of the Section, advising him whether it is a Motor Trolley, a Motor Trolley with Trailer, or a Motor Trolley with Trailer and Inspection Car which is required to occupy the Section. He must indicate whether the Motor Trolley (with or without Trailer) will be removed from the line in the section and whether the Inspection Car is travelling through it, and must then agree whether the permission may be granted or not. If it is decided that occupation cannot be given, the Ganger must be so informed. If the Signalmen agree to grant occupation, the Ganger must be told the exact times between which the occupation can be given. In deciding these times it must be arranged for the occupation to cease five minutes before a train is due to leave a station at either end of the section. The Ganger must in every case repeat the telephone message to show that he understands it.

(G.A.19—10 48 L.K.4 26.)

INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEMS OF MAINTENANCE ON SINGLE LINES—Continued.

7. The method of operating the Occupation Key Instruments is as follows:—

GRANTING OCCUPATION.

(a) Token Section in which "Control" instrument and one Occupation Key is provided.

Immediately the Occupation has been agreed the Signaller at the other end of the section must press the bell key of the Control instrument. This will make the Control side of the Occupation Control instrument, which must then be withdrawn fully to No. 3 position. After the control slide has been withdrawn fully a small side marker "Occupation Key" for releasing the Occupation Key, is freed and must be drawn fully out. A press button and indicator are fixed immediately above this slide and the press button must be pushed in. This will cause the indicator needle to be deflected, and allow the Ganger to signal that the key is at a Signal Box to withdraw the Key, after which the needle will resume the normal vertical position and the press button must then be released.

(b) Token Section in which "Control" Instrument and two or more Occupation Keys are provided.

The method is the same as in (a) but it will be necessary for the Signaller who is giving the occupation to ask the Ganger to press the bell key of the Control instrument at the other end of the section. If the Occupation Key is at a Signal Box, to press the plunger to enable the applicable Group Slide to be withdrawn.

It is possible for two or more Occupation Keys in the Token Section to be out at the same time, but they must all be restored before Token working can be resumed.

(c) Token Section where "Control" Instruments are not provided.

Immediately the Occupation has been agreed the Signaller at both ends of the Token Section must press the bell keys of their respective Token Instruments.

If the section of Occupation Key is provided in the Token Section, only one can be out at a time.

(d) Withdrawal of Occupation Key.

Immediately the Ganger is informed that he is to have occupation, the occupation key must be turned so that the index shows No. 2 in which position the Key can be released by the Signaller.

After turning the Key to No. 2 position the Ganger or Signaller must wait until the word "Free" appears on a small indicator above the Key, which must then be turned so that the index shows No. 3. The Key must then be turned to the Key Instrument. When the Key has been withdrawn the Signaller who has given the occupation must be informed on the telephone.

The Ganger must replace the Key in the box as soon as the work of the occupation is finished.

(e) Giving up Occupation.

The Ganger, when he has obtained occupation of the Line, must so arrange his work as to be able to track the Occupation Key to be reported free. He may put the key into any position in the Group of bell keys, as long as it is in the instrument in that Group, and allow it to the Plunger working key as in any Key Instrument. When putting back the Key, he must wait until the Indicator shows No. 1. Having thus restored the Occupation Key, the Ganger must call up the Signaller on the Telephone and notify him that he has restored the Key (giving the number of the Key Instrument) and that the Line is safe for the passage of trains over the Section. Where "Control" instruments are provided, the Ganger, after restoring the Key, must press the plunger provided in connection with the Key Instrument until notified by the Signaller that the "Control" apparatus has been properly reset. The Signaller must then replace to No. 2 position the Slide applied to the Control instrument in which the Key has been restored, when, if the Occupation Key has been properly restored to the No. 2 position, the Needle will be deflected and the Occupation Slide can be replaced in the No. 1 (normal) position. When this has been done the Control Slide must be placed to No. 1 (normal) position, restoring the Electric Train Token working. The Signaller will then notify the Signaller at the other end that the Occupation Key has been replaced.

When the key is being tested in the Key instrument the Signaller must immediately withdraw a Token in accordance with Regulation 27 (Testing instruments) of the Electric Train Token Regulations, and the Ganger must remain at the key instrument until the test has been made and the Signaller has informed him that everything is again in order. The Ganger will then replace the receiver and shut and lock the box.

Where two or more occupation keys are provided in a token section, the Signaller must make the test after the last key has been replaced.

Delete clause 8 and substitute the following:—

8. In the event of any obstruction arising during the time the Ganger is in possession of an occupation key, or if from some other cause the line is unsafe for the passage of trains, the Ganger must not replace the occupation key at the time arranged for the occupation to cease, but must proceed to the nearest telephone and call up the Signaller by pressing the call key on the telephone; if no call key on the telephone is provided he must take the receiver off the hook and attract the attention of the Signaller by shouting into the telephone. The Ganger must then advise the Signaller that the line is unsafe for the passage of trains, and that the key has not been restored. If, however, the obstruction is by a runaway train or vehicles the Ganger must take the key for the section which is obstructed to the nearest token station, and before restoring the occupation key to the key box or handing it to the Signaller must explain the position to the Signaller. If the line is not open at the time, the Ganger must take the first means of advising the Traffic Department man in charge or nearest available Signaller and this must be done before the next train is due to leave the signal box on either side.

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Should he not receive any reply after a reasonable time the Ganger must proceed on foot to the nearest telephone or signal box and establish communication.

(G.A. 12 + 43 L.R. 2 9153 S)

When an Occupation Key Box other than the Control Instrument, is situated in a Signal Box, the Signman when replacing the Occupation Key which has been handed to him by the Ganger, must carry out the instructions applicable to the Ganger, and immediately call up the Signman at the Box at which the Control Instrument is situated, on the telephone, and notify him that he has restored the key.

INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEMS OF MAINTENANCE ON SINGLE LINES—Continued.

When it is necessary for an inspection car or a Motor Trolley with Trailer or Motor Trolley with Trailer and inspection car to enter or proceed through a section from one token station to the next, the driver must be in possession of the token or occupation key. If it is necessary for the driver to be in possession of the token or occupation key, the driver must be in possession of the token or occupation key.

(b) When it is necessary for an Inspection Car or trolley to go into or through any of the tunnels specially enumerated in the appendices to the Service Time Tables, the ganger must be in possession of the Token or Occupation Key and Electric Train Token Regulation 9, clause (i), must be carried out.

Where a token is held, should the trolley be removed from the rails before reaching the next token station, the ganger must take the token to the Signalman at the station and inform him that the trolley is clear of the section. The Signalman must then place the token in the "Clear of Section" signal. If, however, time would be saved, the Ganger must return with the token to the token station in rear and inform the Signalman that the trolley is clear of the line; the Signalman must then restore the token to the instrument and send the "Cancelling" signal.

Where an occupation key is held the trolley will not be signalled on the token instruments, but the signal must be worked. The ganger must, after the trolley has been signalled, inform the Signalman that the section is clear. The Signalman must then restore the occupation key to the instrument and inform the Signalman that the section is clear.

(c) When the signal boxes are used the Ganger may pass the signal at danger and the inspection car or trolley will pass over the same road in both directions at crossing stations. The Signalman must set the road for one direction before going off duty.

The ganger must see that the points are properly set for the passage of the inspection car or motor trolley.

9. Working single line by Pilotman. Should it be necessary to work a section by Pilotman, and the latter is not in possession of the Electric Token for the section, must be given by the Signalman that the Occupation Key or key box at the key instrument station must be secured by the Signalman observing the instrument for the section. The key instrument has been pulled out, or where the Control Instruments are fixed at the other end of the section, by the Signalman as shown on the telephone that the lines are properly worked by Occupation Control Instrument.

In the event of the telephone communication being cut off, and the Pilotman is not in possession of the Occupation Key, it will be necessary for the Signalman to be in possession of the Occupation Key. The Signalman must be in possession of the Occupation Key, and the Pilotman must be in possession of the Occupation Key. The Signalman must be in possession of the Occupation Key, and the Pilotman must be in possession of the Occupation Key.

At Token stations where Control Instruments are not provided, the Pilotman must, when distributing the line working forms, satisfy himself by personal observation that the Occupation Key is properly in the Key Instruments.

Duplicate keys of the huts are held by the Signalmen to enable this to be done.

10. If an inspection car or motor trolley be required to pass over a point or level crossing, the driver must be in possession of the token instrument for the section. The driver must be in possession of the token instrument for the section. The driver must be in possession of the token instrument for the section. The driver must be in possession of the token instrument for the section.

11. When an occupation is required in the morning before a Token station is open, for example, for examination of the line, arrangements can be made for the occupation key to be withdrawn by the Signalman overnight and left in a locked box at the Token Station. The Signalman must be in possession of the occupation key to the locked box from which it was taken, or if the token stations have in the morning period, it may be restored in any instrument on the ground in accordance with the instructions.

12. Each Guard who regularly works over the line must carry a key of the instrument boxes to enable him or the Driver, as the case may be, to obtain access to the telephone.

In the event of an engine failing and assistance being required, the telephone circuit may be used for the purpose of ascertaining from which place assistance is most likely to be obtained, and he must then act in accordance with Clause (a) of Electric Train Token Regulation 14.

If a train becomes accidentally divided the Guard in charge of the rear portion is authorised to use the telephone circuit for the purpose of communicating with the Token station, so as to expedite the arrangements for removing the obstruction from the line.

13. If it be necessary to foul the single line outside the home signal for shunting purposes, while the Ganger is in possession of the occupation key, this may be done without first sending the Blocking Back signal to the Token station at the opposite end of the section after the Driver, when the engine is leading, or the Guard or Shunter in charge when the vehicles are propelled, has been warned that a trolley may be approaching, but immediately the key is returned to the key instrument the Signalman must, if the single line is still fouled outside the home signal, give the "Blocking Back" signal to the token station at the opposite end of the section, which must be acknowledged. When the key is returned to the home signal he must send the "Obstruction removed" signal, which must be acknowledged.

When it is necessary for an inspection Car, a Motor Trolley, Motor Trolley with Trailer and Inspection Car to enter or proceed through section to the next, the ganger must be in possession of the token or certificate. The inspection car or trolley must be signalled in accordance with the signal being worked. If the section key is held in an inspection car or trolley, it must not be signalled on the token instruments, but the signals must be worked.

GA 19-10 48 LK 4 26)

INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEMS OF MAINTENANCE ON SINGLE LINES—Continued.

If the Ganger requires occupation while the single line outside the home signal is fouled for shunting purposes, this may be started, after the Ganger has been warned that shunting outside the home signal is going on, and if having to work at that point the Ganger must be told to approach with care and be prepared to stop short of any obstruction. Immediately the Occupation Key is returned to the Key Instrument the Signaller must of the signal state that the line is clear outside the home signal and so advise the Signaller at the other end of the section. When the line is clear to the home signal he must send the "Obstruction removed" signal, which must be acknowledged.

14. Should the inspection car and motor trolley, with or without trailer, be required to occupy the Section at one and the same time, the inspection car must be at the rear and every care must be taken to ensure that a safe distance is maintained between the motor driven vehicles, so as to avoid any risk of a collision taking place. The Ganger must ride on the inspection car and must hold the Occupation Key. He will be held responsible for seeing that both car and trolley together with the trailer, are removed from the line before he replaces the key in the key instrument, and he must give an assent to the Signaller when the inspection car and trolley are ready to proceed through the section, and tender to be removed from the line. Should it be necessary for the motor trolley and trailer to be removed from the line, the inspection car must stay behind and must not be allowed to pass the motor trolley until the inspection car has been removed from the line. After the inspection car has been removed from the rails or has passed out of the section.

Under the terms of this instruction an Inspection Car and two motor trolleys, with or without trailer, may also be permitted to enter the section at the same time, and from the same end of the section.

15. A motor trolley and trailer must not be uncoupled except at a Token Station or for the purpose of removing either or both from the line in the section.

16. Neither the inspection car nor motor trolley must be allowed to exceed a speed of 20 m.p.h. Both the car and trolley must be kept in gear when running down long or steep gradients, and when running out of gear they must be kept under complete control. When traps exist the inspection car and trolley must pass over carefully and be lifted clear of the points if necessary.

When the trolley or car is standing at a run-off, it must be secured by passing a chain through the wheels and pulling it, or with wheels of the other pattern, the vehicle must be secured by a chain passed round a part of the frame to an anchor ring or staple.

Special precautions must be taken in frosty weather as a sudden application of the brakes will cause the wheels to pick up. When snow is on the rails or on the ground may be caused if the trolley or car wheels mount the snow.

17. When the motor trolley is required to haul a loaded trailer over a steep gradient the Ganger must arrange for a man to travel on the trailer to apply the brake if necessary. In certain circumstances a trailer be propelled by a motor trolley. Ordinary trolleys must not be attached to motor trolleys.

18. The Inspection Car or motor trolley must approach a station with care and at a speed not exceeding 10 m.p.h. on passing the distant signal and be prepared to stop in case of shunting going on between the distant and home signals or other station duties being performed.

19. Every motor driven vehicle must exhibit a red lamp in front and at the rear and these must be lighted during darkness, fog or falling snow, and when going into or through a tunnel. If the trailer is attached to the motor trolley the tail lamp must be fixed to the rear of the trailer. Each motor driven vehicle must carry a staff lamp, a whistle, a fire extinguisher, and be provided with a clock and a time-keeping device, which must be shown frequently when passing into or through a tunnel.

20. The inspection car or trolley must exhibit a red lamp when passing over any public level crossing, and a flashing light or whistle when passing the main danger of the vehicle and the key on horn or other warning device sounded frequently.

21. Men riding on a motor trolley must exercise every care to ensure that they and any material on the motor trolley and/or trailer are kept in the signal box. The vehicles must not be over-loaded and the weight must be evenly distributed on the bogies and no weight is to be placed on the sides or ends of the vehicles, only to prevent a tilting effect on the wheels. Men must not place their legs between the trolley and the trailer in tow.

22. These instructions supersede those in local notices in so far as they apply. The Permanent Way Inspector must ensure that every man concerned in the operation of Motor Trolleys is thoroughly conversant with them.

23. When a trolley is used for carrying Signal Department Staff for the purpose of testing occupation key apparatus the trolley must always be removed from the line before the occupation key is placed in the occupation key instruments, and the trolley must not be put on the line again until the key has been obtained in the authorised manner.

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Section 6.

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Section 14.—The following to be added to the second paragraph :—

The Signaller must be notified as required by Section 4 (d) and will act in accordance with
Section 6. (G A 19—10, 48. L.K.4/26.)

INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEM OF MAINTENANCE ON SINGLE LINES WORKED BY WOODEN TRAIN STAFF ONE ENGINE IN STEAM, OR TWO OR MORE COUPLED TOGETHER).

N.B. The word "Ganger" in these instructions embraces also the Sub-Ganger, or other man in charge authorised to arrange occupation of the line in the absence of the Ganger.

1. On certain sections of single line worked by wooden train staff (one engine in steam or two or more coupled together) inspection cars and motor trolleys have been provided, and have been fixed at certain points by which means the Ganger may communicate with the Signaller in the Branch to obtain occupation of the line under these instructions without providing Hand-signal in accordance with Rules 215 and 217.

The inspection car is provided for the use of the Ganger but in order that structures, etc., may be thoroughly examined he must walk over the entire length once a week.

Each gang will be equipped with a motor driven trolley, capable of carrying men and tools, etc. A trailer will also be available for conveyance of materials and tools.

The motor trolley will be utilised to run the gang to the site of the work and back, and from place to place, as necessary.

The portable turntable provided must always be carried on the trolley, and by this means the motor trolley can be removed from, or replaced upon the rails expeditiously. Run-offs will be provided at places where telephones exist.

2. Neither the inspection car, motor trolley, nor trailer must be placed on the line, nor must the line be obstructed in any way, unless permission has first been obtained.

3. When it is necessary to run the inspection car and/or trolley, or trolley and trailer, along the line, or to carry out operations which would render the running of trains unsafe, the Ganger must go to the nearest station or telephone, and call the attention of the person in charge of the station or Signal Box in the direction from which the next train is due, and having ascertained from him that he is in possession of the train staff, and that the section is unoccupied, make his requirements perfectly clear by stating:—

- (a) Point from which message is being sent.
- (b) Length of time for which occupation is required.
- (c) Points between which it is intended to work.
- (d) When occupation is required for trolley or inspection of the line, the points to which the motor trolley, inspection car or trailer will be run and removed from the line.

4. If the person in charge of the staff working can permit the work to be done he must communicate with the person in charge of the station or signal box at the opposite end of the section, and if the occupation can be agreed to the Ganger must be informed the exact times between which the occupation may take place and each person agreeing the occupation must place three detonators, ten yards apart, and exhibit a red flag by day or a red light at night, and during fog or falling snow, on the running line, and in addition, a cover or lamp must be placed on the lever of the signal controlling the entrance to the section. It is to be understood that occupation may not be granted unless the section is clear and the train staff is in the possession of the person in charge of the staff working at one end or the other.

After permission has been given to the Ganger for occupation of the line, the detonators and red flag or light must not, in any circumstances, be removed, nor may a train be allowed to leave either station or signal box in the direction of the place where occupation of the line is authorised, until the person in charge of the station or signal box from which the Ganger obtained permission to occupy the line has received a definite message from the Ganger that the line is clear, and this message has been transmitted to the person in charge at the opposite end of the section.

If authority cannot be given for the occupation the Signaller or person in charge must make it perfectly clear to the Ganger, and the latter must acknowledge the message he receives, and repeat it so that there shall be a proper understanding between the person in charge of the staff working and the Ganger.

5. On completion of the occupation the Ganger must immediately advise the person in charge of the station or signal box from which he obtained permission for the occupation was granted, and the latter must repeat the message to the Ganger, and show that he has received this information to the person in charge at the opposite end of the section. Upon receiving this information the detonators and red flag or light must be removed from the line, and the lever of the signal controlling the entrance to the section.

6. In the case of failure of the telephone instruments rendering it impossible for the Ganger to use them to report the completion of the occupation, the Ganger must proceed on the inspection car or motor trolley to the station from which the train is due to start, and report the completion of the occupation to the Signaller or person in charge that the line is clear.

7. Should any permanent way work have been undertaken which it is found cannot be completed within the time for which occupation was granted, the Ganger must immediately advise the person in charge of the station or signal box from which he obtained permission for the occupation was granted, and the latter must repeat the message to the Ganger, and show that he has received this information to the person in charge at the opposite end of the section. Upon receiving this information the detonators and red flag or light must be removed from the line, and the lever of the signal controlling the entrance to the section.

INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEM OF MAINTENANCE ON SINGLE LINES WORKED BY WOODEN TRAIN STAFF -Continued.

8. Should the obstruction occur on a trolley wire or within the overhead equipment, the section master, at the time, the trolley car must be at the rear and every man must take care to ensure that a suitable distance is maintained between the motor car and the obstruction, as at any risk of the car coming to a stop, the trolley car must be at the rear of the obstruction. The trolley car must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction.

9. Should it be necessary for the motor trolley and trailer to be removed from the line in the section and for the trolley car to proceed through the section, the section master must be at the rear of the trolley car and the trolley car must be at the rear of the obstruction. The trolley car must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction.

10. These instructions for the trolley car must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction. The trolley car must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction.

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13. If it is necessary to fail the section for the purpose of shutting operations with the trolley car, the section master must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction. The trolley car must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction.

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17. The trolley car must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction. The trolley car must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction.

18. When a motor trolley is required to haul a loaded trailer over a steep gradient the trolley car must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction. The trolley car must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction.

19. Every motor trolley must show a red lamp in front as a head signal, and also at the rear as a tail signal, when it is going through darkness, fog or falling snow, or when going up or down a steep gradient. The trolley car must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction.

20. The trolley car must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction. The trolley car must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction.

21. When running on a motor trolley must exercise every care to ensure that they and any material on the trolley are clear of the trolley, and the trolley car must be at the rear of the obstruction, and the trolley car must be at the rear of the obstruction.

22. The instructions supersede those in the instructions so far as they apply. The Permanent Way Inspector must ensure that every man concerned in the operation of motor trolleys is thoroughly conversant with them.

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BRITISH RAILWAYS

(WESTERN OPERATING AREA)

Alterations and Additions to the General Appendix to the Rule Book

To come into operation on receipt.

The following instructions to be inserted on page 70:—

INSTRUCTIONS FOR RUNNING AND WORKING OF THE LENNOX-LOMAX EARTH AUGER EQUIPMENT.

1. The machine must only be used by the staff authorised by the Signal Engineer

2. The machine, which is provided with a special match truck, is capable of self-propulsion at a speed of 10 miles per hour (in forward or reverse gear) and except when operating, must have the match truck coupled. The machine is fitted with three types of brakes, viz. Hydraulic, Transmission and hand screw wheel. The match truck has a hand brake only.

3. When travelling under its own power the machine or the match truck, as the case may be, must carry a white headlamp and a tail lamp, which must be lighted as necessary. A red flag must be displayed on the rear vehicle by day. The machine must be equipped with red and green handsignal flags, not less than 12 detonators and a hand lamp (lighted when necessary) and a sprag. A portable telephone or "walkie-talkie" apparatus must also be available.

4. When boring operations are required to be carried out an absolute occupation of the line concerned must be arranged. The service on which the machine will be worked to the site of operations and the occupation required must be pre-arranged with the District Operating or District Traffic Superintendent concerned.

5. The machine must be worked to the agreed place near the site of work or vice versa, by freight train carrying "F" or inferior headcodes and be marshalled next to the Guard's brake van. The match truck may be leading or trailing. Should it be necessary to run the machine as a special train hauled by a locomotive a brake van must always be provided at the rear, in which a Guard must ride.

6. When the machine is proceeding under its own power to the site where it is required to work the match truck must always be attached and both the screw and chain couplings must be used for the purpose. Such movements must be confined to the shortest possible distance but if it is necessary for the machine to pass completely through one or more sections it must be signalled and dealt with as a Through Balast Train. When running in a multiple aspect signalling area automatic signalling must be suspended and the machine dealt with in accordance with FCB Regulation 15. Where an Inner Home Signal is provided the "Is Line Clear?" signal must not be accepted until the Home signal can be lowered.

7. The machine must only be moved under its own power by the Driver who has been passed as competent by the Signal Engineer and he must be accompanied by a man who has been passed as competent by the Motive Power Department in (a) knowledge of the route and (b) protective duties of a Driver. A member of the crew of the machine who has been passed by the Operating Department as competent to carry out the protective duties of a Guard must always be present.

8. The machine must not be relied upon to actuate track circuits and Rule 55 must be observed in all cases by the man acting as Guard. When the machine is signalled whilst waiting acceptance by the box in advance it must not draw forward the signal controlling the entrance to the section ahead or to an Intermediate Block some signal but must be held opposite the box. No train must be allowed to follow the machine towards the Intermediate Block Signal until "Train out of Section" has been received.

9. At the conclusion of boring operations the machine and match truck will be moved from the section under the power of the machine. If the machine is required to be signalled in rear, the Driver must bring it to a stand before reaching the boundaries protecting the work. The conductor must proceed on foot to obtain the Signalman's Wrong Line Order authorising return to the signal box. The Signalman's person in charge must give the Signalman an assurance that the road is clear of obstruction.

(G.A.32 Op—10/56. OM 2632)

Reference to the following to be made on page 71 :—

TELEPHONES AT STOP SIGNALS.

When a train has been brought to a stand at a signal where telephone communication with a signal box is provided the following code of instructions must, after telephonic communication has been established, be observed by the Trainman and Signalman.

The Trainman must be careful to ascertain the name of the signal box with which he has established telephonic communication, and that it is the box from which he requires to obtain instructions.

1. Trainman to Signalman.

To intimate that train has been brought to a stand owing to the signal being at Danger.

To communicate as follows :—

train waiting at †..... .. Signal on

Line.

*—Full description to be given.

†—Name or number of signal and name of Line on which train is standing to be given.

2. Signalman to Trainman.

If train to be held at signal

To communicate as follows :—

Stop till signal clears. (If after waiting 5 minutes, or other prescribed period, signal does not clear, Trainman must again communicate with the Signalman.)

If signal is defective or cannot be lowered and train must not proceed

Wait at telephone for further instructions. (Trainman must communicate with Signalman at intervals of not more than 5 minutes, or other prescribed period, unless otherwise instructed.)

If signal is defective or cannot be lowered but train may proceed

Applicable at signal controlled from a Signal Box.

Pass * Signal at Danger and proceed cautiously to (name point to which train can proceed).

Applicable at Automatic Signal or Semi-Automatic Signal working automatically.

Pass *.... .. Signal at Danger and proceed cautiously.

*—Name or number of signal to be given.

(used)

If signal is defective or cannot be lowered but train may proceed

Applicable at signal controlled from a Signal Box.
Pass * Signal at Danger and proceed cautiously to (name point to which train can proceed)

Applicable at Automatic Signal or Semi-Automatic Signal working automatically.

Pass * Signal at Danger and proceed cautiously.

* —Name or number of signal to be given

3. The Trainman and Signaller must not terminate a conversation until they are sure that a clear understanding has been reached

4. If a Trainman cannot establish communication with the Signaller, the Driver must, unless instructions to the contrary are exhibited in the top of the box, send his Trainman to the signal box from which the signal is controlled in order to receive the Signaller's instructions. (This clause is not applicable at Automatic or Semi-Automatic Stop signals.)

5. At intermediate Block Signals the Trainman must communicate with the Signaller at intervals of 3 minutes. See instructions headed "Intermediate Block Signals."

(G A 25.—1 50 R E Stand L.K.I./9986/Gen.)

INTERMEDIATE BLOCK SIGNALS.

Object.—The object of intermediate block signals is to allow a train to leave the signal box in rear before the preceding train has passed the signal box in advance but to prevent more than one train being in the section between the signal box in the rear and an intermediate block Home Signal or between an intermediate block Home Signal and the signal box in advance.

Distant Signals working in connection with the Intermediate Block Home Signals are provided the usual distance in rear of the Intermediate Block Home Signal.

General Instructions.—(a) After a train has been brought to a stand at an intermediate block home signal, controlling entrance to the section ahead, the provisions of Rule 41, clause 1, will apply when the signal is lowered.

(b) Where intermediate block signals controlled from the Signal Box in rear are provided and a train is stopped by accident, failure, obstruction or other exceptional cause at a signal controlling the entrance to the intermediate block section and the intermediate block home signal or between the intermediate block home signal and the signal box in rear, the provisions of Rules 183 and 184 must be carried out. The telephone provided at the intermediate block home signal must, if possible, be used by the Trainmen when by this means the Signalman can be notified of the cause of the failure.

Where intermediate block signals controlled from the signal box in rear are provided, should it be necessary for a train to return in the wrong direction to such a signal box, after the train has not have passed the intermediate block home signal, the provisions of Rules 183 and 184 must be complied with and a "Wrong Line" order form "C" or "D," as the case may be, issued to the Driver before the movement is made.

(c) Should a train be brought to a stand at an intermediate block home signal controlled from the Signal Box in rear and that signal remain at Danger, the Driver must, if the signal box in rear is immediately accessible, instruct the Trainman to use the telephone provided at the signal box in rear to inform the Signalman of such detention and act upon instructions received from him.

The Signalman, in receipt of the advice, will inform the Trainman why the signal cannot be lowered, and if it is due to a failure of the track circuit or signal apparatus, the Signalman must, if possible, instruct the Trainman that the signal may be lowered by the Driver, and the train may proceed into the forward section, and until the Trainman has received the signal to proceed, must remain at the telephone and the train must not proceed. If the movement, the Trainman must, at intervals of three minutes, remind the Signalman that the train is still waiting at the intermediate block home signal.

(d) Where intermediate block signals controlled from the signal box in rear are provided, should any failure of these signals or of the track circuits or telephones in connection with these signals occur, or should the light of an intermediate block signal be out when it should be showing, the signal must be lowered to proceed towards the intermediate block home signal, and the Signalman must be informed. If the signal has been lowered by the box in advance, the Driver must be instructed by the Signalman to proceed with the train to the next block home signal at Danger, and to be prepared to stop short of any obstruction. At the same time the form shown below must be properly filled up and forwarded to the Driver at the circumstances explained to him, and the Guard or Guards and the Driver of any following engine.

Order to proceed against intermediate block signals.

.....Signal Box.

.....day of....., 19

To the Driver of..... train from..... to.....

The..... signals are out of order and you must proceed with caution to the Home

Signal at

Signed,

(e) Should, however, a train have proceeded towards the intermediate block home signal before the failure occurred and the signal remains at Danger, the Driver must, if the signal box in rear is immediately accessible, instruct the Trainman to use the telephone provided at the signal box in rear to inform the Signalman of such detention and act upon instructions received from him. The Signalman must wait three minutes or ten minutes where a tunnel exists between that signal and the box in advance, give one long whistle and proceed cautiously at a speed not exceeding 10 miles an hour as far as the line is clear or to the next stop signal in advance, and to be prepared to stop short of any obstruction. The Driver must inform the Signalman at the next box in advance of the circumstances.

(f) In the event of single line working by Pilotman being in operation, the block telegraph must be used to inform the Signalman that the signal is out of order and the train must be brought to a stand at Danger and passed in the wrong direction.

INTERMEDIATE BLOCK SIGNALS—Continued.

7. All delays at an intermediate block home signal must be noted by Drivers and Guards on their journals.

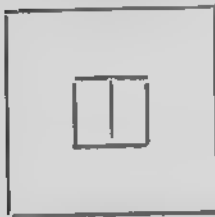
If any defect hindering, or likely to hinder, the proper working of the intermediate block signals is noticed by any employee on the line, steps should at once be taken to communicate with the Signaller on either side, so that the defect may be remedied without delay.

No employee other than a properly authorized employee of the Signal Engineer's Department is permitted to make any wire connection with the rails or do any work whatever in connection with the special signalling appliances.

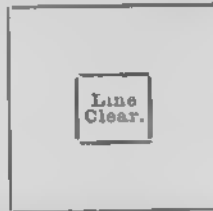
INSTRUCTIONS TO SIGNALMEN AND GATEMEN WHOSE POSTS ARE NOT BLOCK POSTS, BUT WHO ARE SUPPLIED WITH INDICATORS, AND ALSO IN SOME CASES BELLS, TO INFORM THEM WHEN A TRAIN IS APPROACHING.

1. On double lines an indicator must be provided having two discs—one for the up line and the other for the down line. The indicator will show as the position of the disc when the line is free and also when a train is signalled.

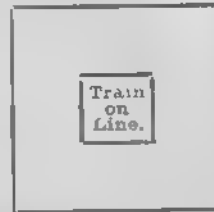
When the Line is free both indicators must show half white and half red.



When permission has been given for a Train to leave next Signal Box the Indicator shows white.



When a Train has left next Signal Box the Indicator shows red.



2. On single lines worked by the block, only one disc is provided, which shows the same three indications as above, but it does not show for which direction the train is up or down.

3. On Single Lines worked by the Electric Train System an indicator is provided having three positions, as follows:—

When the Line is free.



When permission has been given for an Up Train to leave next Signal Box.



When permission has been given for a Down Train to leave next Signal Box.



A bell or telephone in some cases provided in addition to the disc, and all the bell signals passing between the signal boxes on either side sound on this bell also.

Trains or other heavy carriages, heavy vehicles or heavily loaded vehicles or droves of cattle, must not be allowed to pass over the level crossing when the indicator shows that permission has been given for a train to approach.

LESS SPECIAL INSTRUCTIONS ARE ISSUED TO THE CONTRARY, FOOT PASSENGERS, HORSEMEN AND THE LIGHTER KINDS OF VEHICLES TRAFFIC MAY BE ALLOWED TO PASS OVER THE CROSSING WHEN THE INDICATOR IS SHOWING TRAIN ON LINE UNTIL SUCH TIME AS IT MAY BE NECESSARY TO CLOSE THE CROSSING FOR THE PASSAGE OF THE TRAINS.

If the bell or indicator should get out of working order, the Station Master must at once be advised.

When it is known that the indicator is out of order at a level crossing, and in working order the distant signals, if such are provided, protecting the crossing must be kept at attention until information has been received that communication has been restored.

If distant signals are not provided, great care must be taken to ascertain, as far as possible, that no train is approaching the crossing, and that the gates are open to the public.

In the case of Fog or Falling Snow, unless a signalman or stationer at the distant signal the gates must not be used for road traffic after a train has left the next signal box in the rear until it has passed the crossing.

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INTERMEDIATE BLOCK SIGNALS—Pages 71 and 72.

The following to be inserted as Clause (h) paragraph on page 72 :—

n) Trolley Working.

Where Intermediate Block Signals are provided, before a trolley is placed upon or run over a track circuit the permission of the Signaller to whose box the track circuit is connected must first be obtained in such cases a telephone is provided which may be used by the Ganger to communicate with the Signaller.

Where the overlap track circuit of the Intermediate Block Home Signal does not extend to the Home Signal of the signal box in advance and a trolley is required to be placed on the line in advance of the overlap track circuit of the Intermediate Block Home Signal, the Handsignaller, when going back to protect such trolley must, if he should reach the Intermediate Block Home Signal telephone to the Signaller and, after acquainting him with the position, continue to go back protecting the trolley until reaching the $\frac{1}{2}$ mile point as required by the Rule. He must then act in accordance with Rule 215, Clause (d).

When the trolley proceeds the Handsignaller, upon following the trolley, advises to the telephone at the Intermediate Block Home Signal, advising the Signaller that the trolley has proceeded. The Signaller must be similarly advised if the trolley is removed from the line before the Handsignaller has passed in advance of the Intermediate Block Home Signal.

The Signaller, upon being advised by the Handsignaller, must keep the Intermediate Block Signal in the 'On' position until such time as he receives information that the trolley has either gone forward to a point $\frac{1}{2}$ mile in advance of the Intermediate Block Home Signal or has been removed from the line.

(G.A.31—7/56 O.M.12789)

TRAINS NOT COMPLETELY WITHIN FIXED SIGNALS.

(a) In cases where—

- (i) an outlet signal is provided to control movements from a siding to a running line, and
- (ii) a shunting signal is provided to control set-back movements

Drivers must regard such signals as controlling the movement, although the engine may be standing on the wrong side of the signal, and must not move their engines until they are satisfied the signal is off. Where, however, the Driver cannot see the signal and the movement is accompanied by a Shunter, the latter must advise the Driver when the signal is lowered.

If for any reason the signal cannot be lowered, the Shunter or person in charge will be responsible for ascertaining from the Signaller that it is in order for the movement to be made and for satisfying himself that the points are in the proper position. In the case of a light engine unaccompanied by a Shunter, this duty must be carried out by the Fireman.

(b) On platform lines or other running lines when an engine is ahead of the signal controlling the starting of trains owing to the length of the train, a Driver must regard that signal as controlling his movement. When the Driver cannot see such signal, or back indication where provided, or the signal cannot be lowered owing to the engine occupying a track circuit or other apparatus which prevents the signal being lowered, the Driver must not proceed until he receives a green hand signal from the Signaller or verbal intimation to do so from the person acting under the instructions of the Signaller. The Driver must not be authorised to proceed until it has been ascertained that any points concerned have been correctly set.

In cases, however, where Absolute Block Working is not in operation and the signal has been lowered to enable the train to draw forward for station duties the signal must be placed at Danger in accordance with Rule 68a (i) and (ii) in such circumstances the signal must not be regarded as controlling the further movement of the train. If it is necessary for the Signaller to allow a conflicting movement to take place ahead of the standing train he must not do so until the Driver of such train has been advised of what is about to be done. After the conflicting movement has been completed and when the train is ready to continue its journey, the Driver must not proceed until he has received a verbal intimation to do so from the Signaller or the person acting under the Signaller's instructions, in addition to the Guard's "right away" signal.

(c) When the fixed signals referred to in clauses (a) and (b) lead to more than one running line, the Driver should satisfy himself by observation on which line he is travelling over, but the person in charge of the movement mentioned in clause (a) or the Signaller or person acting under his instructions as mentioned in clause (b) must, whenever practicable, also inform the Driver over which line he is about to travel.

(G.A.30 Op.—9/54 476/E)

WORKING OF FIXED SIGNALS AT DIVERGING POINTS—page 73.

The words "or a Class 'C' or a Class 'D' train" to be substituted for the words "or a fully fitted or a partially fitted freight train" in the second and third lines

(G.A.26 Op.—5 50. R.E. Stand)

The following to be inserted as the last item on page 73 —

WORKING OF FIXED SIGNALS AT DIVERGING POINTS.

At those places where Automatic Train Control is not in operation, in the event of it being necessary for a passenger train, or other train composed of coaching stock, ~~on a fully fitted or a partially fitted freight train~~ to be run from one line to another through a junction over which it is necessary that speed should be reduced and such route is not the regular booked route for the train to travel, the Junction or Stop Signal must not be lowered until the train is close to such signal and the Signaller must, where practicable, satisfy himself that the speed of the train has been suitably reduced.

Where there is a stop signal worked from the same Signal Box on a diverging line ahead of the junction, such signal should not be lowered until the Junction or Stop Signal is lowered and then only in accordance with the Block Regulation

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PUBLIC LEVEL CROSSINGS LOCKED BY PADLOCK.

Public Crossing gates supplied with padlocks must always be properly locked when trains are signalled from the box or either side of the crossing.

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ELECTRIC REPEATERS AND LAMP INDICATORS.

These instruments receive in the signal box a signal which they are to repeat the position of signal arms, slots, facing point switches and bolt locks, and also show whether the lamps are "In" or "Out."

Signal Repeater Normal position "On". This repeater shows whether the arm is "On," "Off," or "Wrong." It is not a lamp, but a position. The instrument is not to be altered by the working of the arm itself.

When the Arm Indicator is showing "Wrong," the necessary steps must be taken at once to have the arm put in proper working order, and to carry out Rule 81.

Slot Repeater Normal position "On". This repeater shows whether the slots are "On," "Off," or "Wrong." It is not a lamp, but a position. The instrument is not to be altered by the working of the slot itself.

Lamp Indicator. When the lamp is not burning, or the light is insufficient, the Indicator in the box will show "Lamp Out."

After a lamp has been lighted a very short time the indicator should show "Lamp In," when the switch, where provided at the bottom of the instrument, must be turned from "Bell Battery Off" to "Bell On."

By the reversal of the switch where provided, the bell is brought into action, and in the event of the lamp going out, the indication "Lamp Out" shown in the indicator instrument, and the bell immediately commences to ring.

As soon as the Signalman has been warned, as above, that the lamp is not burning properly, he must turn the switch where provided to "Bell Off," in which position it must remain until the lamp has been attended to, and the indication "Lamp In" is again showing.

When the Lamp Indicator Switch where provided stands out to the "Lamp Out" or "Lamp Battery On" position, the bell will not ring, and when the lamp is again lighted, the switch must be completely turned to the "Bell Battery On" position.

When a Lamp Indicator is used, it must be closely watched every time the keys concerned are operated, and any difficulty or irregularity in their working immediately reported.

Switching off Signal and Slot Repeaters and Lamp Indicator Batteries. The following rules must be strictly complied with:—

When a signal box is closed, the switches where provided must be returned to "Signal Battery Off" and "Lamp Battery Off" respectively, as well as to "Slot Battery Off" where slot repeaters are in use. The switch for the signal arm indicator must be kept to "Signal Battery On" and "Slot Battery On" except as above provided for, but the switch for the lamp indicator must be kept to "Lamp Battery On" when it is necessary to attend to the lamp, as shown when the lamp is "In" or "Out."

At signal boxes where are operating dark side switches where provided, they must be kept to "Signal Battery On" as well as "Lamp Battery On" and "Slot Battery On," if a lamp indicator is in use, in regard to the signal arm lamp and slot where slots exist.

Electrically repeated Long Burning Lamps. In all cases where electrically repeated long burning lamps are in use, the repeater switches must be controlled by a master switch in the "Bell Battery On" position, in order that immediate attention may be called to the failure of a lamp whenever it may occur.

Facing Point and Bolt Lock, &c., Repeaters. At boxes where electric repeaters are provided in connection with facing point switches or bolt locks, and in other types of repeaters are provided in connection with electrically operated point switches, the necessary precautions must be taken to ensure that they get out of order or misaligned, and that the signalman is aware of the fact. If they are not working correctly, the necessary precautions must be immediately taken for the safety of working of traffic until matters have been put right.

Rule 81. A signal which is electrically repeated in the signal box must be regarded as defective if the repeater is out of order and the Signalman is unable to satisfy himself that the arm is working properly or the lamp burning satisfactorily.

SIGNALMEN TO REPORT IRREGULARITIES.

Any infringement or disregard of instructions, or any occurrence coming under the notice of Signalmen affecting the safe and proper working of the railway must be reported to the Signaller or other in accordance with Rule 1(b) of the Rule Book and noted in the Train Register Book.

PROPER WORKING OF SIGNALS.

The attention of Signalmen is directed to the importance of strictly carrying out the provisions of the Rule Book, Regulation 1A, clause (d) of the Regulations for Signalling Instructions, and the instructions in the Rule Book in regard to the working of signals, and the instructions on this page in regard to "Electric Repeaters and Lamp Indicators."

ADJUSTMENT OF SIGNAL WIRES.

The breaking of signal wires frequently arises from want of proper adjustment when the temperature suddenly becomes lower, in the special attention of signallers is directed to Rule 63 of the Rule Book with regard to the adjustment of Signal Wires and the proper working of Signals.

A Signal may be working properly in the daytime, but at night, should the temperature become much lower, a great strain is put upon the Signal Wire if it is not let out, and Signal men are instructed to see that the wires are adjusted whenever necessary owing to a sudden change of temperature.

DEFECTIVE SIGNALS AND POINTS.

~~Where it is found that any Signal or Point will not answer to the lever in the Locking Frame, or when revers is a Fault, the point should immediately be set to work and at once be taken to endeavour to trace the cause of the defect.~~

It may be found in the Home Signal Wip requiring adjust-

In other uses of Stones or Piers not answering to the lever, not only may it be from the wire or rod getting stuck out of a stone getting in the wheel, or the chain or wire getting off the wheel, and the rise of the water before the water becoming wedged in the runners over which the constrained water got stuck, the danger of your water being raised. Such cases as these can be detected generally by the sudden stopping of the lever in the Frame and whenever any one of the kind occurs, steps must at once be taken to have the wire or rod traced to the Signal Points commencing from the connection with the lever and through the Signal Box.

If a Signaller is unable to leave his Box for the purpose of training he must obtain assistance from the Station or Yard Signaller. If assistance is not at once obtained, the teleoperator or the signaller, the Lineman must be immediately sent for. When sending for the Lineman, full information as to the nature of the defect should be stated.

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TESTING ELECTRICALLY LOCKED SIGNALS.

To ensure the correct working of electrical locks on signals, Signaller working such signals must test the electrical locks by pulling the levers in order to make sure that the lock is effective.

So that the test may be made regularly, the Signalman who is on duty at 12 noon will be held responsible for trying the indicator in the manner described, and these tests must be recorded in the Train Register.

Any failure must be reported at once to the Lineman and also to the Station Master.

CLEANING OF SIGNAL BOXES.

The attention of Signatories is drawn to the necessity for keeping their Boxes clean and tidy, and removing all cancelled notices.

Station Masters and District Inspectors will be held responsible for seeing that this is done.

PRIVACY OF SIGNAL BOXES.

Ladies and other men of the Signal Department must not take their meals in signal boxes nor nor in the telegraph office, except for the purpose of the work in the hands to do. If I was part of signal boxes may be used for shelter or taking meals in the absence of any other suitable place.

UNDER PORTION OF SIGNAL BOXES.

The inner portion of Signal-boxes must not be used as storehouses for Coal, Straw, Firewood, Ribs, etc., but must be kept clear, in order that nothing may interfere with the working of the lever gear and signal wires, and also that the Signalman may have easy access to the machinery and have the means of repairing any defects without difficulty.

The keys of the under portions of Signal Boxes must be kept in the custody of the Signalmen in order to prevent the entrance of unauthorised persons thereto.

Reference to the following to be made on page 74:—

**FAILURE OF ELECTRICAL APPARATUS IN SIGNAL BOXES WHERE
OVERLOAD PROTECTION SWITCHES ARE PROVIDED ON THE
OPERATING FLOOR.**

Should a failure occur, ascertain that all switches are in the "ON" position. If any switch has dropped to the "OFF" position, reset to "ON".

Send for lineman if switch will not remain in "ON" position.

(G.A.29.Op.—5/52 SE.—G.40907/Clg./Fm.)

Reference to the following to be made on page 74:—

LIMITED CLEARANCE—WARNING TO STAFF.

The attention of all concerned is directed towards the need for exercising care when working at places where there is a restricted clearance between the running line or siding and adjacent structures, or between running lines and or sidings. This applies to Footplate Staff and Guards as well as to staff working on the ground.

At certain places the limited clearance may be indicated by a Red and White chequered board bearing the words "Warning—Limited Clearance."

(G.A.30 Op.—9/54 L.K 1/10483/364)

DEFECTIVE SIGNALS AND POINTS.—Page 74.

Delete the instructions under the above heading and substitute the following —

Defective Signals and Points.

If it is found that any signals or points do not respond to the working of the controlling lever or that it is not possible to correctly set up a route, the Signaller must first replace the levers and operate them again, provided it is safe for him to do so, if this is not successful it is probable that the cause is due to some hold up in the mechanism, such as a stone in the points or other obstruction in the wire or rodding. The Signaller should then endeavour to locate the fault and, if possible, remove the obstruction.

If the Signaller is unable to leave his box for the purpose of tracing the fault he should obtain assistance from a member of the station or yard staff.

Should the Signaller be unable to trace or remove the obstruction he must send for the Lineman.

No attempt should be made by the Signaller to interfere in any way with electrical signalling apparatus.

(G.A.30 Op.—9/54 476/E)

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EMERGENCY OPERATION OF ELECTRIC POINT MACHINES.—Page 75.

The following to be inserted as the fourth and fifth paragraphs —

The crank handle must not be restored to the circuit controller if the Signalman has given permission for a train to pass over the points, until such train has cleared the points.

When the failure has been rectified, and the points set in a position corresponding to the lever in the frame, i.e. normal or reverse the crank handle must be placed in the circuit controller and a test made to ensure that the points are working correctly

(G.A.30 Op.—9/54 O.M.12594)

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APPENDIX TO No. 14 SERVICE TIME TABLE

Page 150.—Siding off loop line between South Fork and East Box.
Delete entry.

(K2/336 -3/56)

APPENDIX TO No. 16 SECTION OF THE SERVICE TIME TABLES

Page 21.—Ground Frames and Intermediate Sidings.
Buttington Brack Yard

Amend to read:

Name of Station or Siding	Situated between	By whom attended	How locked	Remarks
Buttington	Buttington Jct. and Buttington Gates.	Guard, Shunter or person-in- charge.	Keys released electrically from Buttington Jct. Signal Box.	See pages 72 and 73

Pages 73 and 74

BUTTINGTON

Ground Frame.

The instructions to be amended as under:

The connections from the Down Main Line to and from the Cattle Pen Siding, Down Siding and Up Siding are worked by means of a three lever Ground Frame. Each lever is released by a separate key, locked in the appropriate key release instrument and released electrically from Buttington Junction Signal Box. A telephone is provided at the Ground Frame connected to the Signal Box.

When it is necessary to work the Ground Frame the Guard, Shunter or person in charge must request the Signaller by telephone to release the appropriate key. The Signaller, if permission can be given, will operate the appropriate interlocking lever which will enable the key to be withdrawn. The key must then be inserted in the Annett's lock for the point lever concerned which will then be released for the shunting to be performed.

As soon as the shunting has been completed and the Guard, Shunter, or person in charge has satisfied himself no loose vehicles have been left on the running line, the lever must be restored to normal, the key withdrawn from the Ground Frame lock and replaced in the key instrument and the Signaller advised.

The instructions shown on page 77 of the General Appendix (GA 19) headed "Instructions for working Ground Frames operated by interlocking lever at Signal Box and key release instrument at Ground Frame" apply.

(K2/335—2/56)

INSTRUCTIONS FOR SIGNALLING TRAINS DURING FOG OR FALLING SNOW
DATED OCTOBER 1951

Distant Signals at which Fogsignalmen are not provided.

Page 17 Reading and Cogoad via Westbury

Add: Curry Rivel Junction—Up Line (Colour Light).

(K2/335—2/56—NW13053)

INSTRUCTIONS FOR SIGNALLING TRAINS DURING FOG OR FALLING
SNOW, DATED OCTOBER 1951

Distant Signals at which Fogsignalmen are not provided

Page 18

The following to be deleted from the instructions under this heading in the section "Pontypridd and Merthyr Brandy Bridge".

Signal Box				Distant Signals NOT Fogged	
				Up Line	Down Line
Brandy Bridge .. ..	..	..	..	*Main.	

(K2/335—2.56—W9929)

***• INSTRUCTIONS FOR SIGNALLING TRAINS DURING FOG OR FALLING SNOW, DATED OCTOBER 1951**

Particulars of permanent speed restriction indicators shewing whether Fogsignalmen provided

Page 68—The following to be included:

Location of Speed Indicator	Mileage, m. ch.	For Up or Down trains	Restricted Speed m.p.h.	Whether Fogsignalmen provided	
				Independent of Distant Signal	At Distant Signal to include Speed Indicator
Penpergwm ..	25 25½	Down.	50	No.	Yes
Penpergwm ..	26 22½	Up.	50	No.	Yes.

K2/332—2/56—W.10450

***• GENERAL APPENDIX TO THE RULE BOOK**

Emergency Operation of Electric Point Machines—Pages 75, 76 and 77

The seventh paragraph of these instructions, shown on page 75, to be amended as follows:

"In every case where there is a failure of power-operated points whether operated by hand crank or not, both facing and trailing ends must be clipped and padlocked in the required position. A handsignalman must be appointed."

(K2/332—2/56—O.M.12814)

***• GENERAL APPENDIX TO THE RULE BOOK**

Automatic Train Control System in use on the Western Operating Area—Pages 80, 81 and 82

The following to be added at the end of the paragraph shown on page 82 under the heading "Automatic Train Control Ramps under Repairs"

"Similar action must be taken if a ramp is removed for other purposes."

(K2/332—2/56—O.M.12815)

***• GENERAL APPENDIX TO THE RULE BOOK**

Instructions to Guards and Ticket Collectors on Corridor Trains—Page 115

The following to be added as a fifth paragraph of Clause 7:

Lavatory door locks. In the latest B.R. Standard carriage the locking device is incorporated in a separate indicator unit in which case it is necessary to withdraw the four screws securing the indicator frame, remove frame and pull enamelled indicator plate and operating spindle away from the unit. This will expose a square hole and by inserting a carriage key the inside catch can be released.

(K2/332—2/56—LK2/13532/275)

GENERAL APPENDIX TO THE RULE BOOK

Extracts from the Regulations for Signalling Trains and Engines by Permissive Block System over Goods Running Loop Lines and over other Permissive Lines—pages 57 and 58.

The following to be added as paragraph (e) to Clause 9:

(e) "In multiple-aspect signalled areas where a delayed miniature yellow signal is provided at the entrance to Goods Lines or Loops paragraphs (a), (b) and (c) of this clause will not apply, but the speed of trains entering a Goods Line or Loop must not exceed 10 m.p.h.

(LK2/336—3/56—OM.12816)

INSTRUCTIONS TO BE OBSERVED FOR WORKING TIME RELEASE INSTRUMENTS, Etc., IN CONNECTION WITH APPROACH LOCKING OF SIGNALS AND DISCS.

Approach locking of signals and discs controlling movements over electrically operated points has been provided at certain places. Time Release instruments have been installed to enable the approach locking to be released when required.

The Time Release instruments are set as follows:—

For running movements governed by signals—2 minutes.

For movements governed by discs and backing signals— $\frac{1}{2}$ minute.

(Any variation of the time setting of the Release instruments as set out above will be notified by special instruction in the signal box concerned.) (G.A.19—10/48. O.M.12453.)

Should it be necessary:—

1. To divert an approaching train from one line to another after the signals or discs have been lowered, or

2. To replace a signal to Danger after it has been lowered and before the approaching train has passed the signal, and the track is out of normal position, the following instructions must be observed:—

(a) The signal lever concerned must be put back to the normal check lock position in order to restore the signal to Danger.

(b) The handle of the Time Release instrument must then be given a twisting movement to the left until the pointer has withdrawn away from the normal stop (up to 10 minutes or less, as the case may be), to zero (0) position.

(c) When the pointer has returned to zero position the signal lever may be returned to normal position. Provided the train has not passed the signal, the signal lever may be returned to the normal position. If the handle of the Time Release instrument has not returned to the normal position, otherwise the signal levers will be locked.

In the case of independent electrically operated signals the Signaller should wait for the track circuit to be cleared of the train or backing signal, and then allow the signal to be cleared.

If this is not done it will be necessary to operate the Time Release instrument with the previous instruction.

The provisions of the Time Release instruments does not cancel the instructions contained in Double Line Block Signalling Regulation 4, Clause (c).

The instructions in Rule 70, as far as they apply to the signals or discs affected, must also be carried out.

Entries must be made in the Train Register of each occasion when the Time Release instrument is used.

EMERGENCY OPERATION OF ELECTRIC POINT MACHINES.

Where hand cranks are provided for emergency purposes they are kept in the Signal Box. The key must be kept in a glass fronted case. The glass must be kept in the Signal Box.

In the event of the failure of points to respond to the movement of the signal, the Signaller may be placed in its normal position either normal or reverse as the case may be. The Signaller must be placed in its normal position by the Signaller, and the Lineman must be placed in its normal position by the Signaller. In the majority of cases the electric power is at first to be broken down at the point machine. There are different methods of accomplishing this, varying with the point machines.

When the hand crank has been inserted in the machine it may be rotated to move the points as required, under the direction of the Signaller.

In the event of the operator leaving the points before ratification of the movement, the hand crank and return to the Signaller who will be responsible for its safe use and replacement in its proper position.

In every case where points are operated by hand crank they must be supplied and maintained in the required position and a hand signalman must be appointed.

Entries must be made in the Train Register Book of the time the hand crank is extracted every being signed by the Signaller, and the hand crank is replaced an entry must be made in the Train Register Book to be signed by the Signaller and Lineman.

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O.M.12594)

EMERGENCY OPERATION OF ELECTRIC POINT MACHINES—Continued.

When points have to be moved during failure and a hand crank is not provided, the motor can be disconnected from the points by removing the pins and uncoupling the driving rods. When so disconnected, the points must be clipped and padlocked in the required position.

When points have been worked on the ground whether by hand crank or other means, no attempt must be made to resume normal working from the Signal Box until permission is given by the Signal Lineman.

In all cases of failure at electrically operated points the signals concerned must be maintained at Danger unless necessary for the reasons provided and this in working order, when the applicable signals may be lowered. The key of the padlock securing the points must be in the custody of the Signaller before the Stop signals are lowered.

The provisions of Rules 77, 81 and the instructions on page 15 of the General Appendix to the Rule Book must be observed while the points are out of order.

The key of the small glass-fronted case must be kept by the Lineman.

See following instructions for details respecting the method of using the particular type of hand crank provided:—

General Railway Signal Company's Type Machine.

1. In no circumstances must the hand crank be inserted in the machine while the motor is running.
2. To use the hand crank the large cover over the hole in the point machine must be lifted and the crank inserted, moved slightly sideways and then pushed into the machine as far as it will go. The handle must then be rotated to move the points into either the normal or reverse positions as required.

In moving facing points either way the rotation of the hand crank must not be stopped when it is seen that the points have been moved, but the rotation must be continued to its fullest extent to ensure the points being properly held and bolted.

3. The insertion of the hand crank in the point machine cuts off the electric power from the points so that while the hand crank is inserted into the operating mechanism, it can only be operated electrically.

4. When the points are again in order the hand crank must be returned to the Signal Box.

Siemens's type machine.

The metal piece of the point machine cover must be removed by withdrawing the projection on the hand crank for the purpose. The end of the crank must then be fitted over the circular spindle having a slot in the top. The crank may then be rotated to move the points into either the normal or reverse positions as required. In moving facing points either way the rotation of the hand crank must not be stopped when it is seen that the points have been moved, but the rotation must be continued to its fullest extent to ensure the points being properly bolted.

2. No circuit breaker is provided in connection with the hand crank, that is to say, the electric power cannot be cut off from the points before the hand crank is used. The point lever must not, therefore, be moved without the consent of the man in possession of the hand crank.

3. When the points are in working order again the hand crank must be returned to the Signal Box.

Westinghouse Brake and Saxby Signal Company's type Machine, Style "C."

1. The hand crank is inserted in an electric circuit controller fixed in the locked box in the Signal Box.

2. To withdraw the hand crank it must be turned slightly which will enable it to be withdrawn from the circuit controller. The electric power will then be cut off from the points.

3. One end of the crank is fitted to hold the square end of plug in the point machine casing which must be unscrewed and removed. The hand crank can then be inserted in the plug hole of the point machine and placed on the square end of the spindle of the machine, when the crank may be rotated to move the points into either the normal or reverse positions as required. In moving facing points either way the rotation of the hand crank must not be stopped when it is seen that the points have been moved, but the rotation must be continued to its fullest extent to ensure the points being properly bolted.

4. The hand crank must be returned to the circuit controller after use to complete the power circuit.

5. In some cases where a number of point machines are installed, separate and differently shaped hand cranks inserted in circuit breakers are provided. The cranks are so made that they only fit the point machines to which they apply. The key for each hand crank is to be locked up in a separate glass-fronted box.

INSTRUCTIONS FOR WORKING GROUND FRAMES OPERATED BY INTER-LOCKING LEVER AT SIGNAL BOX AND KEY RELEASE INSTRUMENT AT GROUND FRAME.

When the train arrives at the ground frame, the guard or person in charge must call up the signalman on the telephone for permission to use the ground frame. If he is in a position to grant the permission, the signalman, provided the applicable signal is at 'Danger,' must pull over the ground frame interlocking lever and inform the guard or person in charge on the telephone that the ground frame key can be released.

The key in the key release instrument at ground frame must then be turned "anti-clockwise" from No. 1 to No. 2 position and when the indicator shows "Free" further turned to No. 3 and then withdrawn. The key when inserted in Annett's lock releases the ground frame.

After the work is completed the key must be withdrawn from the Annett's lock and re-inserted in the key release instrument and turned "clockwise" to No. 1 position and the signalman informed on the telephone. Where a plunger is provided this must be pressed.

Signalman must press plunger provided in connection with the electric lock on the interlocking lever and place the lever to normal. He must then inform the guard or person in charge of the ground frame on the telephone that apparatus is restored. Until this message is received the guard must not rejoin or allow his train to proceed. Neither must he do so until he has given an assurance to the signalman that no vehicle has been left foul of the main line.

Undue force must not be used in manipulating the release instrument key.

INSTRUCTIONS FOR WORKING GROUND FRAMES OPERATED BY KEY CONTROL INSTRUMENT AT SIGNAL BOX AND KEY RELEASE INSTRUMENT AT GROUND FRAME.

An electrically locked key control instrument is provided at the signal box for effecting locking with the applicable signal, and an electric key release instrument at the ground frame.

When the train arrives at the ground frame, the guard or person in charge must telephone to the signalman for permission to use the ground frame. If he is in a position to grant the permission, the signalman, provided the applicable signal is at 'Danger,' must turn handle of the key control instrument from No. 1 to No. 2 position and then to No. 4 position and inform the guard or person in charge on telephone that ground frame key can be released.

The key in the key release instrument at ground frame must then be turned "anti-clockwise" from No. 1 to No. 2 position and when indicator shows "Free" further turned to No. 3 position and key withdrawn. The key when inserted in Annett's lock must be turned to release the ground frame.

After the work is completed the key must be withdrawn from the Annett's lock and re-inserted in key release instrument which must then be turned "clockwise" to No. 1 position and signalman informed on the telephone.

Signalman must turn handle of key control instrument from No. 4 to No. 3 position and request guard or person in charge to press plunger at ground frame. The signalman must turn handle of key control instrument from No. 3 to No. 1 position. The guard or person in charge of ground frame should then be informed on telephone that apparatus is restored and until this message is received the guard must not rejoin nor allow his train to proceed. Neither must he do so until he has given an assurance to the signalman that no vehicle has been left foul of the main line.

Undue force must not be used in manipulating the key release or the key control instruments.

(G.A. 19-10 48. S.E.—G.38,480 D.s H.)

EMERGENCY OPERATION OF ELECTRIC POINT MACHINES—Continued.

Westinghouse Brake and Saxby Signal Company's Type Machine, Style M.3.

1. One end of a metal strap or hasp is hinged to the centre of the top of the point machine case and the other end of the strap is secured to the side of the case by means of a padlock. This strap when secured to the side of the case covers a plug in the top of the case. *In no circumstances must the strap be unfastened while the motor is running.*

2. To work the points by hand in the case of failure the key of the padlock and the hand crank must be taken from the Signal Box by the hand signalman. He must unlock the strap or hasp on the point machine and lift it back on to the top of the case. This will release a spring plug which will cut off the electric power from the points. The hand crank must then be placed over the plug in the top of the case and rotated to move the points as required.

If the points are fitted with a facing point lock the hand crank must be turned until they are properly bolted.

3. When the points are again in order the strap or hasp must be replaced in its normal position and padlocked. The key and the hand crank must be taken back to the Signal Box.

USE OF SIGNALMEN'S LEVER COLLARS.

A lever collar must be placed on the down signal lever, or any other lever affected in the box, when a train or engine is standing on the running lines or the signal in question, a protecting an obstruction. Lever collars when not in use must be placed in the box provided, and not left on the levers except when used as a reminder of a train, engine or vehicle standing on the running lines.

BADGES FOR PLACING OVER LEVERS IN LOCKING FRAMES.

Reversible metal badges with the words 'Disconnected' and 'Workman' respectively are provided in Signal Boxes for attaching to any lever when disconnected from its normal operation to the Signaller and thus prevent possible injury to himself by his pulling a lever in the wrong way, or to indicate to the Signaller that the workman is engaged in the lever or is about to operate that lever. Before moving the lever the Signaller must inform the workman when he is about to move the lever, that he is about to do so, but when they are connected by a speaking tube the Signaller must, before moving the lever, give two or three short blows on the bell to inform the workman that the lever is about to be reversed. The Signal Department must be responsible for placing the badges on the levers.

SIGNAL DEPARTMENT WORKMEN SIGNING ON DUTY.

When the time-keeping arrangements for Lancashire and Yorkshire Division of the Signal Department provide for the booking of all signaller's duties in the Signal Box, the signaller must sign on duty at the time he is to go on duty, on form 506A or 506B, the time being 15 minutes before the time at which they come on duty. The Signaller will initial the entries and make a note of any absences and of the reason for absence, if known.

Where convenient the Lancashire and Yorkshire Division of the Signal Department may sign on duty on form 506A or 506B at the Telegraph Office at their home station.

Signal Department men returning to their Depots after the usual closing hour must book off duty at the home station, the Traffic Department Officer on duty at the time initialing the necessary entries on form 506A or 506B.

In all such cases forms 506A or 506B should be passed to the responsible Signal and Telegraph Inspector each Friday night.

MAINTENANCE OF GROUND FRAMES LOCKED BY ELECTRIC TOKENS.

To facilitate the carrying out of cleaning, oiling and repairs by the Signal Department at ground frames locked by the electric token, the following arrangements must be observed:—

(a) For examination, oiling or small repairs, which can be done between trains, the Signal Inspector must first arrange with the person in charge of the station at which the work can be carried out. The person in charge will then arrange for a responsible person to meet the Signal Lineman at the nearest token station and accompany him with the token, in the interval between two trains, to the ground frame, remaining with the Lineman while the work is being carried out and, on completion, returning to the station with the token.

(b) Where the ground frame is too far from the station, or the intervals between trains are too short for the purpose of this arrangement being carried out, arrangements may be made for a train or engine to stop for a short time at the ground frame.

MAINTENANCE OF GROUND FRAMES LOCKED BY ELECTRIC TOKENS. *Continued.*

For heavy repairs or renewals necessitating a full day's stay at the ground frame as cannot be effected between trains are arranged to be taken out of service at the points in the yard line and those points are to be taken out of service before the train is started and properly secured in their normal position with the proper keys and padlocks. The person in charge of the work must have the token for the section in his custody while the points are being worked, and who must retain the padlock keys in his possession during the whole of the time the work is in progress.

On completion of the work, arrangements must be made for a train or light engine to call at the siding with the token for the necessary reset the points back into normal and to report to this end by the Traffic Department man.

(1) When it is necessary for the Signal Department to examine or carry out all repairs to ground frames it is essential that a special box containing the keys and padlocks for duplication keys is in operation. The Signal Department will arrange for the provision of the necessary duplicate. Another set to enable keys to be carried out. The original key shall be replaced in the key-box on completion of the work.

REPAIRS, &c., TO SIGNALS AND POINTS AT INTERMEDIATE SIDINGS.

When it is necessary for Signal Department staff to go to repair the points or signals at intermediate siding at which no Signalman is stationed, permission must be given to the person in charge of the siding station where the keys, if they are kept, and signals are kept, and the repairs are of a serious nature. It must be made known to the nearest staff station by the person in charge and given with them at the siding station. The work must be done so that the points are set safe and the points and signals locked in their proper position.

If the person in charge of the siding is a Signalman, the person in charge of the siding must be notified of the work, so that the points and signals can be taken in accordance with Rules 77 and 78 of the Rule Book.

CARRYING OUT WORK ENTAILING DISCONNECTION OF POINTS AT SMALL STATIONS OR OUTLYING SIGNAL BOXES.

When it is necessary to disconnect points at small stations or outlying Signal Boxes in connection with work to be done by the Engineering Department, the person in charge of the Divisional Superintendent or District Traffic Manager will discuss the arrangements with the Signal Department for the points to be disconnected. The previous Saturday afternoon the padlocks and keys belonging to the points to be disconnected are to be taken to the person in charge of the work. It must remain until the points are reconnected by the Signal Department on the following Monday.

ENGINEERING OCCUPATIONS ON SUNDAYS ON LINES CLOSED FOR TRAFFIC PURPOSES.

When on the open line the Divisional Superintendent or District Traffic Manager, on any day would be required to go to the line for a special purpose, such as to work with the ballast train to open up points or signals where necessary, instead of working signal boxes especially for the purpose, both on double lines and on single lines not provided for in the following paragraph.

ENGINEERING OCCUPATIONS ON SUNDAYS ON ELECTRIC TOKEN LINES WHEN THE LINE IS CLOSED.

When it is necessary to run a last train at a time when the line would otherwise be closed a token may be withdrawn by the Signalman for the section or sections concerned before leaving duty. The train, so withdrawn, must be taken to the Train Register or other specially appointed place.

The ballast train when traveling through two or more sections must in all cases be accompanied by a Relief Signman who must be a qualified signman with the working of the Token Station and he must change the token at each Token Station and work the necessary points and signals.

The certificate as to the status of the line which is required to be furnished by the Permanent Way Department, should be left with the token at the last Token Station of the particular section in which the work has been done. In such cases where a special box is provided in which to place the token, the certificate should be placed with the token in the box.

WORKING OF CRANES IN CONNECTION WITH MISHAPS OR
ENGINEERING OPERATIONS. PROTECTION OF TRAINS ON ADJOIN-
ING LINES—page 78

Clause I amended to read

No train must be allowed to pass the site where the crane is working without the permission of the Operating Department District Inspector, who must not give his permission until (a) the person in charge of the crane has ensured that it is clear of the line on which the train will run and no further movement of the crane will be made, and (b) the hook and lifting beam (where used) is secured to prevent movement.

The following to be added at the end of Clause V, paragraph 1—

If, however, the Handsman when going out to protect an obstruction should arrive at an Intermediate Block Home Signal before he has reached the distance of 1 mile, he must make use of the telephonic procedure, and request the Signaller to maintain the Intermediate Block Home signal at Danger until the Handsman has informed him that the obstruction has been removed and the line is clear and safe for the passage of trains. Under these circumstances the Handsman must remain at the Intermediate Block Home signal, place on the rail 3 detonators, 17 yards apart, and exhibit a hand danger signal, until the Operating Department District Inspector authorises the train to proceed. Should the telephone at the Intermediate Block Home signal be failed, the Handsman must proceed for the prescribed distance in accordance with Rule 217.

(G A 29 Op—5 52 LK 9665 Gen E.)

WORKING OF CRANES IN CONNECTION WITH MISHAPS OR ENGINEERING OPERATIONS—PROTECTION OF TRAINS ON ADJOINING LINES.

Where a crane is being used in connection with mishaps or engineering operations and it is necessary for trains to travel over any line which may be fouled by the movement of the crane the following precautions must be taken :—

- (i) A District Inspector (or other responsible member of the Operating Department staff) must be in attendance and no line must be fouled by the operation of the crane until his permission has been given. He must keep in touch with the Signaller or Signalmen concerned so as to obtain accurate information as to the running of trains. Where necessary a portable telephone in communication with the signal box or boxes concerned must be provided.
- (ii) No train must be allowed to pass the site where the crane is working without the permission of the Operating Department District Inspector, who must not give his permission until (a) the person in charge of the crane has ensured that it is clear of the line on which the train will run and no further movement of the crane will be made, and (b) the hook is secured to prevent movement and the boom (where used) is removed from the hook.
- (iii) After a train has passed the site of the work the crane may re-commence operations as soon as the Operating Department District Inspector has ascertained that there is a suitable interval for work to proceed and after the protective arrangements shewn in clause (iv) have been carried out.
- (iv) When the site at which the crane is working is not within the protection of the Fixed signals of the lines on which trains require to run Handsignalmen must be appointed in accordance with Rule 217. When the Handsignalmen have taken up their positions a train may be allowed to enter the section, but the Handsignaller at the site of the work must continue to exhibit a danger signal until the Operating Department District Inspector authorises the train to proceed.

If the site at which the crane is working is within the protection of the Home signal of the line on which trains require to run, such line must not be fouled within the authorised clearing point by the crane until the "Blocking Back Inside Home Signal" signal has been sent to the signal box in rear and acknowledged. Where block instruments are not provided the "Blocking Back" (2/4) signal must be sent by bell or telephone and the signaller at the box in the rear must place a lever control on the lever of the signal controlling the entrance to the section and must not acknowledge the "Blocking Back" signal until this has been done.

In the case of an Intermediate Block Home signal controlled from the signal box in the rear, if the site where the crane is working is within the clearing point of such signal the Operating Department District Inspector must request the Signaller at the box in rear to place a lever control on the lever controlling the Intermediate Block Home signal and also on the lever of the signal controlling the entrance to the Intermediate Block section until the conditions in clause (ii) are carried out. Where the site at which the crane is working is ahead of the clearing point of the Intermediate Block Home signal Handsignalmen must be appointed in accordance with Rule 217.

At places where automatic signalling is in operation a Handsignaller must be stationed at the automatic Stop signal in rear of the site of the crane working and wherever possible this signal must be placed and maintained at Danger in which case a distant Handsignaller will not be required. If it is not possible for the signal to be kept at Danger a distant Handsignaller must be appointed in accordance with Rule 217. On the arrival of a train at such signal the Handsignaller must advise the Operating Department District Inspector who, after ensuring that the line is clear in accordance with paragraph (ii), may instruct the Handsignaller to authorise the train to proceed.

(G.A.24—11/49. R.E. Stand :—L.K.1/9665 Gen.)

ENGINEERING OCCUPATIONS ON SUNDAYS—Continued.

When Sunday Engineering occupations are in force a token may be withdrawn at each Taken Station, and on the Signalman must place the token in a locked box provided for the purpose. For leaving duty, making a suitable entry in the Train Register. In addition to the Signalman's key, the Engineer or other appointed Engineering Department man, will be provided with a key of the box to enable him to obtain the token on Sunday morning, and will be held responsible for seeing that it is kept locked up in the box provided, on completion of the work, so that the Signalman will be able to obtain it when morning duty. The Signalman before replacing the token in the instrument must make an entry in the Train Register.

EXAMINATION OF LOCKING GEAR.

Signalmen must check the interlocking of the Levers and test it as frequently as possible. If at any time they find that a Lever will not be moved, when in their opinion it should be locked, they must immediately report the occurrence to their superior officer, who will transmit the Report to the Signal Engineer and to the Divisional Superintendent or District Traffic Manager.

This does not relieve the Signal Department from the responsibility of regularly testing the Locking and keeping it in proper working order.

LAYING IN NEW SWITCHES.

1. When new switches are to be put into Main Passenger Lines the work must be done at the nearest convenient point to that appointed for connecting up to the Signal Box, and the Engineering Department must see that the switches are properly secured for the future to be fitted and rodged up, so that on the day appointed for cutting them in the work may be done without the need of special arrangements.

2. The Permanent Way Inspector and ganger must see that the switches when being fixed or trailing which have been cut in, but not connected up to the signal box and brought into use, are secured in the following manner:—

(a) The switch to be fixed by a screw clip, pulled out, and kept in place by a stop, bolted down to the sleeper by two fang-bolts.

(b) The switch to be cut in must be fixed by the front end being properly secured by a complete set of fang-bolts, with the use of an oak block wedge between the switch and the stock rail, the wedge being bolted down to the sleeper by two fang-bolts.

3. The Traffic Department will supply the rail clips and padlocks, and the keys must be held by the Divisional Superintendent or District Traffic Manager, who will supply a complete set of keys to the ganger to lock the switches and supervise any work which it may be necessary to carry out between the time the switches are cut in and when they are finally rodged up and brought into use.

PROTECTION OF SIDINGS, GOODS SHEDS AND OTHER BUILDINGS TEMPORARILY IN THE OCCUPATION OF THE ENGINEERING OR LOCOMOTIVE DEPARTMENTS.

Whenever the whole or any portion of a siding, goods shed or other building into which vehicles are moved is in the exclusive occupation of the Engineering Department, the siding must, if possible, be closed for traffic in the following manner:—

(a) **Controlling of Points Worked by Hand.**—The points must be either spiked or fixed over with cap and pin blocks or a suitable piece of timber placed across the rails and secured by fang-bolts. If timber is used a red hand signal, showing in the direction from which vehicles might approach, must be provided.

(b) **Controlling of Points Worked from a Signal Box.** The Engineering Department must advise the Traffic Department by notice giving particulars of the time that the siding will be required, and the Signalman must be instructed by the Inspector for an hour before the start of the work not to permit any vehicle to pass the points until after the time specified. The points should be secured by clip and padlocks or spiked over with cap and pin blocks, and a piece of timber placed across the rails, securely fastened and secured by fang-bolts towards the direction from which vehicles might approach must be provided. Where practicable a lever collar or workman's badge must also be placed on the levers affected.

It must not be possible to close the siding in the instructions mentioned in the previous paragraph. In addition a Handsgnallman must be appointed to stand on the siding or at a sufficient distance from the men at work in the siding or on the siding to give effective warning of the approach of a vehicle.

AUTOMATIC TRAIN CONTROL SYSTEM IN USE ON THE GREAT WESTERN RAILWAY.

General Description.

Objects achieved by the system.

(1) The primary object of this system is to give audible warning on the engine when the train is approaching a distant signal, or passing a lower distant signal fixed below a "Stop" signal, and the distant signal being in the "On" (proceed with caution) position, also, in the event of this warning being disregarded, to apply the brakes automatically, so as to ensure the train being pulled up before it reaches the home signal.

Another and distinctive audible indication is also given on the engine when the distant signal is "Off" (Proceed). The value of this latter indication is that it facilitates the running of the train when the semaphore signals cannot be seen during fogs and snowstorms.

A. Line signals

(2) The audible signals given are the sounding of a siren indicating "Proceed with caution," and the ringing of a bell, indicating "Proceed."

Location at which the audible signals are operated

(3) The point at which the audible signals are given is usually about 140 yards before the distant signal is reached. When, however, the distant signal is a lower arm or a "Stop" signal, the audible signals are given just as the "Stop" signal is passed.

Apparatus on Permanent Way

(4) The apparatus fixed on the permanent way for operating the audible signals on the line is a steel \perp bar mounted on a baulk of timber. The ramp at its highest point is $3\frac{1}{2}$ inches above rail level.

Apparatus in Signal Box.

(5) A telegraph wire connects the ramp with a switch in the Signal Box through a contact attached to the distant signal arm.

This switch is attached to the lever controlling the distant signal, so that when the lever is operated to place the distant signal to the "Off" (Proceed) position, an electrically controlled switch is opened to the ramp, provided the signal has correctly responded to the movement of the lever.

When the lever is replaced to restore the signal to the "On" (Proceed with caution) position, the battery is disconnected from the ramp.

The ramp is, therefore, electrified when the distant signal is "Off."

When the distant signal is "On," the ramp is electrically "Dead," as is also the case in the event of the battery failing, or the arm not responding correctly to the lever, or the telegraph wire breaking.

Apparatus on engine

(6) The apparatus on the engine comprises a contact shoe with switch, and an electrically controlled brake valve and siren, and a handle in the engine cab.

The contact shoe is fixed in the centre line of the engine and projects to within $2\frac{1}{2}$ inches above rail level, in which position it is held by gravity assisted by a powerful spring. It is capable of being raised vertically, and being in line with the ramp it is lifted one inch whenever a ramp is passed over. This lift of one inch is utilised for effectively opening a switch attached to the contact shoe. The switch is connected with the electrically-controlled brake valve and siren in such a way that whenever it is opened the siren is set in motion and the brake valve is closed, admitting air through the siren to the train pipe. This happens when an engine passes over an electrified ramp. The Driver, by acknowledging the warning given by the siren can stop the siren sounding and stop the application of the brakes. This he does by raising a handle provided for the purpose.

When the ramp is electrified by the distant signal being placed in the "Off" (Proceed) position the brake valve is not raised by the engine passing over the ramp, but the bell on the Engine rings instead. The contact shoe is lifted as before, but the electric current from the ramp is prevented from reaching the contact shoe, or rendered inoperative, the switch attached to the contact shoe; so that, although the switch is opened it does not release the valve admitting air through the siren to the train pipe.

When an engine is at a stand and remains thus for more than half an hour, the automatic battery switch operates and cuts the battery off from the cab apparatus, thus economising battery power. This battery switch is operated by the vacuum maintained in the engine reservoir. When the vacuum is restored by the Engineman the automatic switch pulls up and closes the battery circuit and energises the cab apparatus.

Failures.

(7) In the event of a failure to pick up the electric current when a ramp is passed over, the effect on the engine apparatus is the same as though the ramp was not electrified, that is, the valve admitting air through the siren to the train pipe is opened, and the automatic brake is applied on the train, thus ensuring that any failure of the electrical apparatus shall produce the warning indications irrespective of the position of the signals.

Single lines.

(8) On engines fitted for working over single lines the apparatus is so arranged that the indications in the cab are only given when passing over the ramps applicable to the direction in which the train is travelling.

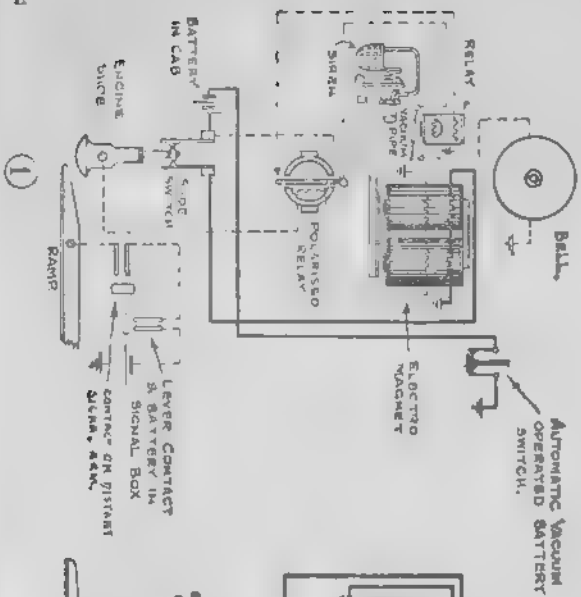
(9) The special attention of Enginemen is directed to Rule 82 of the Rule Book.

(10) If from any cause the position of the distant arm should not accord with the audible signal received in the cab of the engine, it must in all cases be treated as a "Caution" Signal, unless a hand signalman is present and exhibits a green hand signal.

AUTOMATIC TRAIN CONTROL.

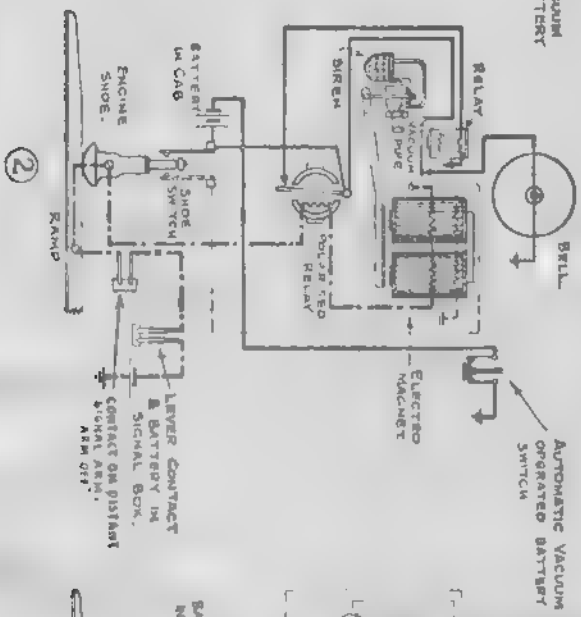
NORMAL.

CLOSED CIRCUIT HOLDING UP ARMATURE OF ELECTRO MAGNET SHOWN THUS. ———



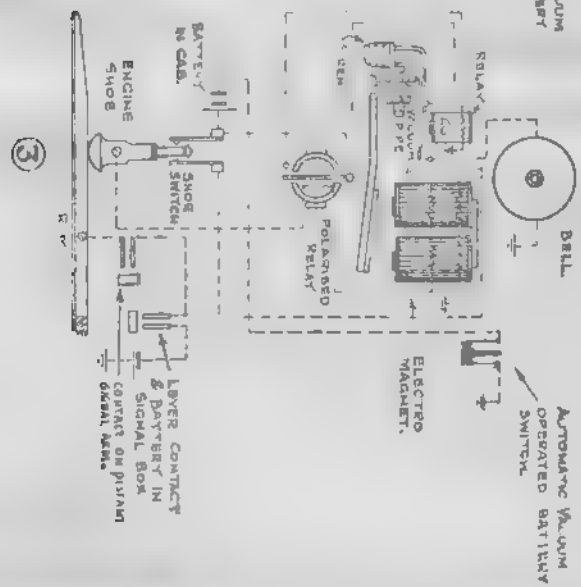
PROCEED (Distant Signal "Off.")

CLOSED CIRCUIT BROKEN BY SHOE SWITCH. ARMATURE OF ELECTRO MAGNET HELD UP BY CURRENT FROM SIGNAL BOX SHOWN THUS. ———. CURRENT FROM SIGNAL BOX ALSO ENERGISING POLARISED RELAY AND CLOSING BELL CIRCUIT SHOWN THUS. ———



PROCEED WITH CAUTION (Distant Signal "On.")

CLOSED CIRCUIT BROKEN BY SHOE SWITCH. NO CURRENT FROM SIGNAL BOX ARMATURE OF ELECTRO MAGNET DROPPED VALVE OF SIREN OPENED. THUS. ———



AUTOMATIC TRAIN CONTROL SYSTEM IN USE ON THE G.W.R. *Continued.*

(11) Printed forms (No. 4,074) are provided for the purpose of enabling Drivers to record any irregularity in the working of the automatic train control apparatus, and these should be properly filled up as occasion requires and handed in when booking off duty at the end of the trip.

(12) The existing distant signals will continue in use (except on the Fairford Single Line Branch and the Great Ouse Valley Branch) and therefore at present only supplement, and not take the place of, the semaphore distant signals on double lines.

The automatic train control system is in operation on the undermentioned sections of the line (double line sections except where otherwise indicated).

AUTOMATIC TRAIN CONTROL SYSTEM IN USE ON THE WESTERN OPERATING AREA.

The second paragraph of Clause 12 to be amended to read:—

The automatic train control system is in operation on the undermentioned sections of the line (double line sections except where otherwise indicated).

Paddington and Didcot (Four lines.)
West Ealing and Greenford.
Henley Branch.
Reading and Penzance via Westbury.
Didcot and Newbury.
Thingley Junction and Bradford Junction.
Bathampton and Westbury.
Castle Cary and Weymouth.
Newton Abbot and Paignton.
Didcot and Taunton via Bath.
Swindon and Severn Tunnel Junction via Gloucester.
Wootton Bassett and Bristol and Severn Tunnel Junction via Badminton.
Severn Tunnel Junction and Fishguard.
Skewen and Llandilo Junction. (Swansea District Line.)
Old Oak Common and Saltney Dee Junction via Birmingham.
Didcot and Aynho Junction.
Oxford and Wolverhampton via Worcester.
Worcester and Newport.
Fairford Branch. (Single Line.)
Handsworth Junction and Stourbridge Junction.
Tyseley and Gloucester.
Shrewsbury and Hereford.

(G.A.30 Op.—9/54 LKI/11041/232)

Automatic Train Control Ramps under Repairs.

While an automatic train control ramp is under repair and out of use, a Hand-signalman must be posted at the distant signal to repeat the distant as is ordinarily done during fog or falling snow.

COUPLINGS STRIKING AUTOMATIC TRAIN CONTROL RAMPS.

Cases have occurred of automatic train control ramps being struck by the ball on lever of screw connection of other running gear, when the screw connection is not in use and the "D" shackle is hanging on the carrying hook.

Chains are provided on the headstocks of certain other Companies' covered goods wagons, refrigerator vans, etc., to hold the screw connection levers above the regulation clearance of tanks from rail level, and care must be taken when coupling on vehicles to see that the levers are properly secured by the chains.

TRACK CIRCUITS.

General Description.

A track circuit indicates in the Signal Box the presence of a train on any particular section of line so equipped. This is accomplished by passing an electric current through the rails so that the connection from the battery at one end of the section to the relay or magnet at the other is established directly through the actual running rails. The rails through which the current passes are bonded together by wire, and, at the extremities of the track circuit, insulated joints are provided by

Action to be taken if bell indication received when the Distant Signal is at 'Caution.'

Should a Driver receive the bell indication "Proceed" at a ramp, but find that the Distant signal controlling the ramp is at "Caution," he must stop at the Signal Box to which the Distant Signal applies and inform the Signaller who must advise him of the position of the Signal Box in rear. In order that subsequent trains passing through the section may be stopped and the Drivers advised that the A.T.C. ramp is defective. The Signaller first advised of the failure must promptly call the Lineman and the District Operating Superintendent or District Traffic Superintendent must be notified immediately.

The Distant Signal concerned must be maintained at "Caution" until the Signaller to whom the failure was first reported is advised by the Lineman that the ramp is in order when he must notify the Signaller at the Signal Box in rear that normal working may be resumed.

An entry must be made by each Signaller in his Train Register Book shewing the time the defective A.T.C. ramp is reported. The Signaller at the Signal Box to which the Distant Signal applies must upon being advised that the failure has been rectified notify the Signaller at the Signal Box in the rear. Each Signaller must then enter the time in the Train Register Book, and the entry in the Train Register Book at the Signal Box to which the Distant Signal applies must be countersigned by the Lineman.

(G.A.30 Op.—9/54 O.M./12652).

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TRACK CIRCUITS.—Pages 82-83.

The following to be added at the end of these instructions:—

Sand—use of by Enginemen.

Enginemen must, as far as practicable, avoid the use of sand when standing on or passing over track circuits, lock bars, points or crossings. If, however, it is essential that sand be used in these circumstances the minimum quantity necessary should be used.

(G.A.30 Op.—9/54.)

The "General Instructions" at the end of these regulations amended to read:—

General Instructions.

1. Signalmen must watch the action of track circuit indicators each time a train or engine passes on to or off that portion of the line to which each indicator applies, and if it fails to give a correct indication, immediately advise the Lineman.

If an indicator fails to show when a train or vehicle is on the track circuit the lineman must be advised by the most expeditious means, and arrangements made for the vehicles to be examined as soon as possible and, except as shown below, they must be taken out of traffic and held until such examination has been made.

Should the vehicle or vehicles be loaded and undue delay would be caused to the contents, or in the case of an empty vehicle specially required for traffic, a wire must be sent to the destination or exchange station requesting that an examination be made there.

In every case of failure of a vehicle to operate a track circuit a detailed report must be sent to the District Operating Superintendent or District Traffic Superintendent from the Station at which the failure occurred.

An entry must be made in the Train Register Book of all irregularities in the working of track circuits.

2. In case of emergency during the time a track circuit is out of order, a lock may be released by the Lineman after an understanding with the Signaller, to allow a signal to be restored to normal, or points moved to either normal or reverse, on the written authority of the Station Master, Assistant Station Master or District Inspector.

An entry must be made in the Train Register Book and signed by the Signaller and Lineman. In no circumstances must a lock be released to allow a signal to be lowered or a block instrument be released to allow "LINE CLEAR" to be given when the track circuit controlling either is out of order.

4. Referring to Rule 83, if, in consequence of a derailment, permanent way operations, signalling alterations or other exceptional causes, one or more track circuits are occupied for an extended period, certain movements cannot be signalled even though they would be clear of the obstruction, the Lineman at the request, IN WRITING, of the Station Master, Assistant Station Master or District Inspector may, after satisfying himself that the obstruction is clear of the movement to be made, give the necessary release. The Signaller must make use of the lever collars or other similar devices where provided and points both facing and trailing which are locked by the track circuit concerned must be secured by clip or scotch until the movement is completed.

A separate release must be given for each Movement.

After each movement has been completed the Lineman must restore the full track circuit controls. The time and details of the release and the restoration of the controls must be entered in the Train Register Book on each occasion and signed by the Signaller and Lineman and also the Station Master, Assistant Station Master or District Inspector as the case may be.

5. Enginemen must not throw cinders on track circuited portions of the lines.

6. An Engineer's trolley, motor trolley or velocipede must not be relied upon to operate track circuits and protection must be provided in accordance with the Rules or Instructions applied.

7. Before a trolley is placed upon, or run over, any portion of a line that is track circuited, permission of the Signaller in whose box the track circuit is indicated must be obtained.

(G.A.30 Op.—9/54 LK1.)

METHOD OF WORKING TO BE ADOPTED DURING FAILURE OF TRACK CIRCUITS OR RELAYING OF TRACK CIRCUITED LINES.—Page 83.

The following to be added to the last paragraph:—

In addition the Locomotive Foreman or Inspector must be immediately advised of any failure which is attributed to the presence of sand on the rail.

(G.A.1. 3/37. LK.1/4332.)

METHOD OF WORKING TO BE ADOPTED DURING FAILURE OF TRACK CIRCUITS OR RELAYING OF TRACK-CIRCUITED LINES. —page 83.

The following additional instruction to be added as the third paragraph:

In the case of failure of a track circuit controlling the signal at a level crossing worked points in advance of a Home Signal where the track circuit is used to operate the Home Signal, in consequence of which the Home Signal is not worked at danger, the signal at the District Signal must be maintained at "Caution".

(G.A. 18. 11/47. LK. 1/8706/Gen.)

Facing Points controlled by Track Circuits.—Emergency Release.

When track circuits are provided in lieu of Facing Point Lock Bars, in certain cases emergency releases will be provided and the following instructions must be carried out in the event of a failure of the track circuit:—

1. When the track circuit fails and the facing point lock lever cannot be restored to normal, the Emergency Release provided in the signal box must be used and the following instructions must be carried out by the Signaller in the order shown —
 - (a) Satisfy himself by personal observation, or by assurance from a responsible person, that no track circuit controlling the Facing Point Lock lever is fouled by any vehicle and that it is in order for the facing points to be reversed or restored to normal as the case may be.
 - (b) Break the glass on the front of the box containing the release plunger.
 - (c) Operate plunger and restore F.P.L. lever to normal, thus releasing the points.
 - (d) The facing points can then be operated normally and when the F.P.L. lever is reversed the appropriate signal, if not locked by the track circuit which has failed, can be lowered, provided the detection is intact.
 - (e) An entry must be made in the Train Register Book with the time whenever the glass is broken. The entry must be signed by the Signaller, who must promptly report the circumstances to the Station Master and Lineman.
 - (f) While the track circuit lock is out of use it will be necessary to press the emergency plunger on each occasion when the F.P.L. lever is to be restored to normal.
 - (g) While the track circuit remains out of use a Handsignaller or competent man must be appointed as required by Rule 77(e).
 - (h) When the track circuit is out of use the Signaller must, before restoring the F.P.L. lever to normal, satisfy himself by personal observation, or by assurance from the Handsignaller or competent man, that any train or engine signalled to pass over the facing points has passed clear of same.
2. The Station Master must specially report to the District Operating Superintendent or District Traffic Superintendent every occasion on which the glass front has been broken and must see that it is renewed immediately normal working is resumed, and that the paper label fixed to the new glass bears the Lineman's signature and the date replaced. An entry must be made in the Train Register Book showing the time the track circuit is again in order and normal working resumed, and this must be signed by the Lineman.
3. The Emergency Release Plunger is for use in connection with the failure of the track circuit only.

TRACK CIRCUITS—Continued.

means of some non-conducting material, in order to confine the current to that part of the section. The normal position of the indicator in the signal box is "Track Clear," and when the Track Circuited section is occupied the indication shows "Track Occupied." The best stop signal in rear being then displayed at danger. In some cases an illuminated diagram is provided and where necessary, the occupation of the track circuit to the rear of a Home signal or within the clearing point, prevents a following train being accepted on the block instruments.

Where Provided.

Usually to the rear of home starting, or advanced starting signal, except a long distance from the signal box, or where there is not a good view from the box. Platform lines which cannot be easily seen from the signal box have also been track-circuited.

At a number of important junctions and stations, home signals a quarter of a mile from inner home signals have been provided to enable Signalmen to accept trains on each converging line in order to meet the "Line Clear" signal. These are installed to the rear of such signals to advise Signalmen of arrival of trains and avoid necessity of Trainmen going to box.

Where track circuiting is provided white enamelled notice plates, as shown in the margin hereof, are fixed on signal posts or adjacent thereto as a reminder to the Trainmen that it is necessary for them to report to the signal box to carry out Rule 75, except that the Engine whistle or Klaxon horn, where no whistle is provided, must be sounded in accordance with the Rule.



"VEHICLE ON LINE" SWITCHES IN CONNECTION WITH TRACK CIRCUITS.

Where platform lines are track circuited, "Vehicle on Line" switches must be provided. The switch must be placed to the "Vehicle on Line" position whenever the platform is occupied by a vehicle or vehicles with an engine, and must be returned to the "Line Clear" position when the platform is clear. The door of the switch box must be kept closed.

When the switch is operated it will check the stop signal in the rear, and also operate the indicator in the signal box in the same manner as the presence of a train or vehicle on the track circuited portion of line would do.

Whenever a vehicle or vehicles are left or placed on the track circuited portion of line, and there is no engine attached, the Signaller or person operating the signal must personally operate the proper switch in order to protect such vehicles, and the switch must be returned to its normal position when the vehicle or vehicles have been removed and the line is free for reconstruction.

Track circuits must not be used open to protect vehicles to which no engine is attached, and the person in charge of the station concerned must see that these instructions are properly carried out.

METHOD OF WORKING TO BE ADOPTED DURING FAILURE OF TRACK CIRCUITS OR RELAYING OF TRACK CIRCUITED LINES.

Addition to Standard Rule 81 (b) to be observed during Failure of Track Circuits, Track Circuit Indicators, or Relaying of Track Circuited Lines.

When a track circuit to the rear of a home signal or indicator applying to a section is out of order, and a Hands-on-man has not been appointed, a train must not be accepted from the box in rear until the line is clear of the starting signal, or the starting clearing point, and the starting signal. The Signalman in rear must be advised of the failure and he must carry out the provisions of Rule 81 (b) and 77 (e).

When a track circuit to the rear of a starting or advanced starting signal or indicator applying thereto is out of order, and a Hands-on-man has not been appointed, a train must not be allowed to proceed towards that signal, except for starting purposes, until the block indicator has been placed at "Line Clear," and the signal has been lowered for the train to proceed into the forward section.

Where a Hands-on-man is not employed, a lap of the signal must not be given without the track circuit being proved, during the time that such a track circuit or platform switch is out of order, a man must be deputed by the person in charge to advise verbally the Signaller of any vehicles detached on the track circuited line concerned, also when it is clear.

The Station Master or person in charge must advise the Divisional Superintendent or District Traffic Manager immediately any failure occurs, and forward full particulars of the circumstances and repairs made.

TRACK CIRCUITED BLOCK SECTIONS.

Where a track circuit extends through a short block section from the most advanced starting signal box to the outermost home signal of the box in advance, the signal box in rear must be "Line Clear" and the block instruments are free of any locking with the track circuit, and the signal in rear must be set to "Line Clear" where no starting signal is provided governing the entry of the train.

The signal in rear is controlled by the "Line Clear" indication of block instrument and by the "Line Clear" signal where no starting signal is provided governing the entry of the train.

SIGNAL DEPARTMENT MEN ASSISTING IN SNOW STORMS.

When a heavy snow storm comes on and looks like continuing for some time, and men who are engaged at out stations are compelled to cease work, owing to the storm, they should report themselves to the Permanent Way Inspector or Ganger, and ask if their services are required. If such are accepted they must obey any instructions given to them.

Men employed at Depot stations must hold themselves in readiness to render assistance, and the Inspector in charge of the Depot should ask the Permanent Way Inspector or Ganger if any assistance is required, and instruct his men accordingly.

If the Permanent Way Inspector at Reading requires any further assistance than can be furnished him by the Signal and Telegraph Inspectors, he may apply to the Signal Engineer at the Reading Signal Works, who will arrange for assistance to be given.

Linemen will be exempt from assisting other men if their services are required in connection with the overhead wires, and at least one of them should devote their attention to seeing that the locking bars, facing points, etc., are in working order.

If assistance is required from the Signal Department after working hours, the Permanent Way Inspector will apply to the Signal and Telegraph Inspectors at the various Depots and the Signal Engineer at Reading for the necessary men.

The Permanent Way Inspectors have a supply of brooms, shovels, and salt, but men of the Signal Department should take with them whatever brooms and shovels are available.

The time occupied in clearing snow must be appropriated to the Engineering Department.

SNOW-PLOUGHS.

1. Snow ploughs, which must be kept ready for immediate use, are stationed in charge of the Locomotive Department at the following Depots:—

Alfreton.	Chester.	Noath.	Reading.	Tondu.
Banbury.	Cross Newydd (2).	Newport (Pbbw Jet.).	Shrewsbury.	Tyseley.
Barnston 2.	Derham.	Newton Abbot.	Stroud.	Westbury.
Bristol.	Exeter.	Oxford.	Stroud (Jct).	Widley (Lampton).
Cardiff (Canton).	Gloucester.	Plymouth (Laira).	Swindon.	Worcester.
Cardiff (Cathays).	Hereford.	Pontypool Road.	Taunton.	Yeovil.
Carmarthen.	Manchester 2.	Dewsbury.	G.A. 23.	

2. Whenever there is a heavy fall of snow, the Station Master, must order the snow ploughs to be sent to the nearest of the above-mentioned Depots.

A Snow-Plough, attached to a locomotive of suitable type, will be sent to the place where its services are required. If the snow is very deep, the plough may be sent with a locomotive, and the engine may be used to clear the snow. The ploughs are sent to the place where they are required with one or more men, and the men are to be provided for the men to ride in if this can be done without causing undue delay.

3. Important information as to the state of the Line by snow must also be given by the Station Master to the District Superintendent, the Permanent Way Inspector, and the Permanent Way Inspector, who are to be kept informed of the state of the Line by snow, and to furnish every assistance that may be at their command.

4. When a Plough cannot be obtained promptly, and it appears imminent that the Line will be closed, the Station Master must order the Plough to be sent to the nearest of the above-mentioned Depots, and to keep the rails as clear as practicable, and to prevent an accumulation of snow. This precaution should be taken on a Line on which there are few Transversal and/or Branch Lines, so that the Line is not cut off by night or on Sunday.

5. The Station Master must report to the District Superintendent or District Traffic Manager, and to the Superintendent of the Local Passenger, the circumstances connected with the case, stating the locality, the nature of the drift, with the length and depth of the drift that had to be cut through.

DAMAGE TO ELECTRIC CABLES BY RATS AND MICE. THROWING AWAY SCRAPS OF FOOD.

Damage has been caused to electric cables, etc., by rats and mice, which are enticed to certain places owing to scraps of food being thrown out by men working in the vicinity. All men are requested to avoid doing this, in order to prevent such damage.

DETONATORS. PERIODS KEPT IN STOCK. PAINTING OF DETONATORS.

The attention of the staff is directed to the importance of carrying out the instructions in regard to the use and storage of detonators. P.L.S. 58.

A sufficient supply of detonators must be kept ready for use at each station, depot and signal box.

Except where instructions are issued to the contrary, detonators must be returned to the Stores Department at the expiration of the period from the date stamped upon them for reissue, but on Stations receiving a list of detonators from the Stores Department must use such detonators before returning them to the Stores Department. At places where authority is given to use detonators up to five years old, they must be returned to the Stores Department at the expiration of that period. Detonators must not, under any circumstances, be used after they are five years old.

Year ending—June 30th.

1942/3	.. Blue	1946/7	.. Red
1943/4	.. White	1947/8	.. Green
1944/5	.. Black	1948/9	.. Grey
1945/6	.. Brown	1949/50	.. Yellow

(G.A.12. 4/43. L.K.1/6940/5)

Reference to the CLEARING POINT.

In order to facilitate places on the system.

- (a) Steam lar
- (b) Armoured
- (c) Brass con
- (d) "Y" conn
- from one engine.
- (e) One pair

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3. The flexible hose

4. The Driver is himself that the two-wa position to direct steam or Engineering Depart with the Driver as to th can be closed down to a pressure built up in the

5. Salt should be melting snow turning u

6. After the points Signal Box from which

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Reference to the following to be made on page 84.

CLEARING POINTS OF SNOW USE OF STEAM LANCES.

In order to facilitate the clearance of snow from points, special equipment has been supplied to certain places on the system. The equipment comprises the following:

- (a) Steam lances to be attached to the steam cock on an engine by means of the flexible hosepipe
- (b) Armoured flexible hose—issued in 20ft. lengths.
- (c) Brass connection—used for coupling two 20 ft. lengths of hose
- (d) "Y" connection—provided at certain specified places to enable two steam lances to be operated from one engine. This enables two lengths of hose to be coupled to the steam cock
- (e) One pair of leather gloves for each steam lance set.

The equipment is intended for use at any place in the vicinity of the Signal Box to which it is allocated, where the operation of points is impeded by snow. When not in use the equipment will be stored in the Signal Box with the Locomotive Superintendent or District Traffic Manager, the gloves to be kept in one of the Signal Box Lockers.

The following instructions must be observed by all concerned:—

1. When the equipment is required the Station Master or Signman must apply to the Station Master who supervises the nearest Signal Box where the apparatus is stored, or in his absence to the Signman on duty in such Box, and must also advise the Locomotive Department Foreman. The latter to be informed whether an engine is available in the vicinity of the place concerned.

2. If an engine is not available at the place concerned, the Station Master must advise the Locomotive Foreman concerned, the approximate time at which the steam lance or lances will be ready for use.

3. The flexible hosepipe must be taken under the rail to avoid the possibility of the hose being damaged.

4. The Driver is responsible for coupling up the apparatus to the steam cock, and he must satisfy himself that the two-way cock on the steambox, where fitted on certain classes of engines, is in the correct position to direct steam to the lance cock. The steam jet must be directed on to the switch by any Traffic, or Locomotive Department staff available, who will be responsible for coming to a proper understanding with the Driver as to the amount of steam to be admitted to the hose. The steam cock or any alternative can be closed down to any extent to reduce pressure of steam in the hose and there should be very little steam pressure built up in the rubber hose if throttled down as necessary.

5. Salt should be applied immediately after the use of the lances to prevent the water formed by the melting snow turning to ice. Supplies of salt for this purpose are kept by the Permanent Way staff.

6. After the points have been cleared of snow, the equipment must be returned immediately to the Signal Box from which it was obtained so that it may be available if required at some other point.

7. The Station Master who supervises the Signal Box where the equipment is stored must make inspection of such equipment at monthly intervals in order to satisfy himself that the whole of the equipment is available and that there is no sign of deterioration.

(G.A.16. 5/46. L.K.1/6172,5)

DETONATORS. PERIODS KEPT IN STOCK. PAINTING OF DETONATORS — page 84.

The following information regarding the painting of detonators to be inserted on page 85:—

Year ending—June 30th.			
1950 51 ...	Blue.	1954 55	Red
1951 52 ...	White.	1955 56	Green
1952 53 ...	Black.	1956 57	Chestnut
1953 54 ...	Brown.	1957 58	Black

(G A 26 O—)

DETONATOR "PLACER" MACHINES—page 85.

Delete the third and fourth paragraphs and substitute the following:—

The special detonators provided for use with the machines are obtainable from the Stores Department. They must not be used for any other purpose.

When placing detonators in the machines care must be taken to see that the metal strips are where necessary, bent in such a manner as to secure the detonator in the machine. The clearance between the bottom of the detonators and the top of the rail, when the detonators are in position for exploding, must not exceed one-eighth inch and Signalmen must adjust the detonator concerned if the clearance is more than this.

The detonators fixed in the machines must be replaced on the first Monday in each month and a record made in the Train Register Book when the change is made and the Station Master will be responsible for seeing this is done. Those taken from the machines must be returned to the Stores Department.

Station Masters must frequently examine each detonator placer under their control, in order to satisfy themselves that the detonators are in good condition and should it be found on making such examination that one or both of the detonators are damaged in the slightest degree, fresh detonators must be at once substituted, and the matter reported to the District Operating Superintendent or District Traffic Superintendent.

The Ganger or other authorised person, when walking his length, must immediately advise the Signaller of any detonators which have been exploded, damaged or displaced in any detonator placer on his length, and replace a detonator where necessary. For this purpose the Ganger must have a few detonators of these types in his possession.

Each Signaller must on commencing duty each day test the operating lever strap and, as far as is practicable, see that the apparatus places the detonators properly on the line. The apparatus must also be tested when the Ganger or other authorised person passes the signal box whilst he is examining the line.

If at any time the detonators are exploded, the Signaller must take steps to have fresh detonators immediately placed in the apparatus and will be held responsible for so doing. The Signaller must record in the Train Register Book the time when and the train by which they are exploded and the time when they are replaced. He must also report the circumstances to the Station Master, a report afterwards being sent to the District Operating Superintendent or District Traffic Superintendent.

A supply of not less than 12 but not more than 24 detonators must always be kept on hand. Any defect in the apparatus must be immediately reported to the Signaller in person.
(G A 29 Op — 5 52 — LK 1 .0733 3r E)

No. 15.

2.55 p.m. Paddington to Pembroke Dock.

Van Second X, Second X, Compo X, Bk. Compo X, Paddington to Pembroke Dock.

Compo X, Second X, Van Second X, Paddington to Milford Haven.

Kitchen Buffet, Compo Dining Saloon, Compo X, Van Second X, Paddington to Swansea.

No. 16.

3.30 p.m. Paddington to Plymouth.

Van Second X, Second X, Second X, First (7) X, Second X, Van Second X, Refreshment Car, Bk. Compo X, Paddington to Plymouth.

Bk. Compo X, Compo X, Second X, Van Second X, Paddington to Paignton.

No. 17 (M).

b3.33 p.m. Paddington to Bristol (T.M.).

9½25 p.m. Bristol (T.M.) to Malago Vale.

b Bk. Van 98 or 185 (RR), Old Oak Common to Neyland, proceeding from Bristol (T.M.) 1.5 a.m. Monday.

Note.—This vehicle will pass by this service only when the 2.2 p.m. (Fish Empties) Old Oak Common to Neyland the previous day has been cancelled.

(Set No. 400 MO)

No. 18.

4.15 p.m. Paddington to Plymouth.

Van Second X, Second X, Second X, Second X, Compo X, Compo X, Second X, Second X, Second X, Van Second X, Paddington to Plymouth.

No. 19.

5.30 p.m. Paddington to Exeter.

Van Second X, Second X, Second X, Second X, Compo X, Compo X, Second X, Second X, Second X, Bk. Compo X, Paddington to Exeter.

No. 21.

5.55 p.m. Paddington to Swansea.

Bk. Compo X, Second X, Compo X, Kitchen Buffet, Compo Dining Saloon, Second X, Van Second X, Paddington to Swansea.

Second X, Second X, Compo X, Second X, Van Second X, Paddington to Cardiff.

No. 22.

6. 5 p.m. Paddington to Trowbridge.

Bk. Compo X, Second X, Paddington to Trowbridge unbalanced.

Van Second X, First X, Second X, Van Second X, Paddington to Trowbridge, balanced 7.15 a.m. Trowbridge.

Van, Paddington to Trowbridge, unbalanced.

(Forms No. 402 next day.)

No. 23.

6.35 p.m. Paddington to Cheltenham.

Van Second X, Second X, Second X, Second X, First (7) X, Refreshment Car, Second X, Van Second X, Paddington to Cheltenham.

Second X, Compo X, Van Second X, Paddington to Swindon, balanced 9.0 a.m. Cheltenham.

No. 24.

7.15 p.m. Paddington to Bristol (T.M.).

10½0 p.m. Bristol (T.M.) to Malago Vale.

Van Second X, Second X, aCompo X, aCompo X, Second X, Van Second X, Paddington to Bristol (T.M.) balanced to Paddington. MO to 4½ a.m. Bristol (T.M.) to Swindon (Week-day Set No. 468): —

Van Second X, Compo X, Second X, Van Second X, Paddington to Bristol (T.M.) (Set No. 576 MO)

Standard vehicles

DETONATORS—Continued.

In order to ensure that detonators shall not be kept in service for more than five years, arrangements are made for detonators to be withdrawn as may be necessary, and stations with a stock of detonators are re-issued to those stations using more within the effective life of five years.

Detonators will be painted externally a different colour for the period July 1st to June 30th, each year.

Year ending December 31st, 1931 Red			
" " " " 1932 Green			
" " " " 1933 Grey			
January to June 30th, 1934 Yellow			
Year ending June 30th.			
1934/5 Blue.	1938/9 Blue.		
1935/6 White.	1939/40 Green.		
1936/7 Black.	1940/1 Grey.		
1937/8 Brown.	1941/2 Yellow.		

Each detonator will be clearly stamped on the end of the place as to the date of manufacture, the number of the single chamber type.

The month and year (in figures) of manufacture will also be shown on the label on the side of each packet of detonators.

It must be clearly understood by all concerned that the total existing stock of green detonators is exhausted, and that the stock of blue detonators are to be used. Stations with a stock of blue detonators must be used, and the stock of blue detonators must be used. The stock of blue detonators must be used, and the stock of blue detonators must be used.

The stock of blue detonators must be used, and the stock of blue detonators must be used. The stock of blue detonators must be used, and the stock of blue detonators must be used.

When a packet of detonators is opened, the label on the side of the packet must be returned at once to the Stores Department.

Should any detonator fail to explode at any time, the driver or the passenger must report the fact to the District Superintendent or the District Traffic Manager, and the defective detonator forwarded to him for examination.

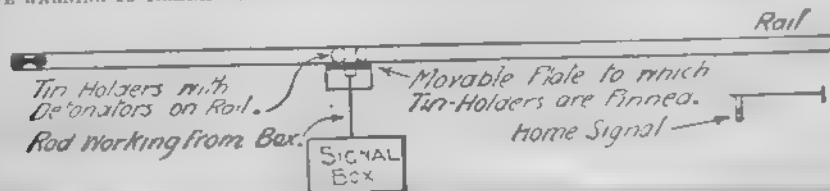
Station Yard Masters, who are responsible for the Staff, are also responsible for seeing that these instructions are properly carried out by the Staff.

District Inspectors must also visit the Staff, and see that the Staff are properly arranged, and that the Staff are properly arranged.

DETONATOR "PLACER" MACHINES.

Where detonator "Placer" machines have been provided, the Staff must use them by means of a lever in the locking frame to place two detonators on the rail, one on each side of the signal. At places where these machines are fixed, the Staff must use them to place two other detonators on the line, one on each side of the signal, in order to comply with Clause 4 of the Double Line Block Telegraph Regulations.

These machines must also be made use of for the purpose of warning the Driver of any train which is found to be approaching at a speed greater than the permitted speed, or if that signal is at Danger, or in any other case, when it may be necessary to give warning to trainmen.



In order that the machines may always be ready in case of emergency, the signaller working them must examine them to satisfy himself that they are in proper working order and that the detonators

are properly placed on the rail, and the machines must be kept in the Signal Box, and the Staff must be responsible for seeing that the machines are properly arranged, and that the Staff are properly arranged.

See 4.2.24

DETONATOR "PLACER" MACHINES—Continued.

Whenever the detonators are exploded, the Signaller on duty at the time must report the facts to the Station Master, who must communicate them to the Divisional Superintendent or District Traffic Manager. The Signaller must also record the circumstance in the train register book.

Arrangements have been made for special fixings to be supplied for these machines, which will prevent the second detonator being blown out of the clip by the explosion of the first and when a fresh supply of tin caps is required the requisition should clearly state that they are for use in the detonator "Placer" machines and must be supplied with the special fixings.

The Signal Lineman must be immediately advised should any defect be found in the apparatus.

"THREE-SHOT" DETONATOR MACHINES WORKED IN CONJUNCTION WITH TRAILING POINTS.

These machines are provided at certain junctions and work in conjunction with the points. In some cases machines are provided on each converging line at a junction, one set of detonators being normally "on" and the other "off".

In order that the machines may always be ready in case of emergency the Signaller working them must frequently examine them to see that the detonators are intact and work properly on to the rail.

Three fresh detonators will be placed in the machine every month by the Signal Lineman when going his rounds. The latter should inform the Signaller that he has changed the detonators in order that a note of the fact may be made in the Train Register Book by the Signaller. The old detonators taken out of the machine in this way must be sent by the Station Master to the Stores Department in the first luggage van of the next train leaving the station.

Station Masters and District Inspectors must examine the machines at frequent intervals and satisfy themselves that they are in proper working order.

The Signal Lineman must from time to time be advised should any defect be found in the apparatus.

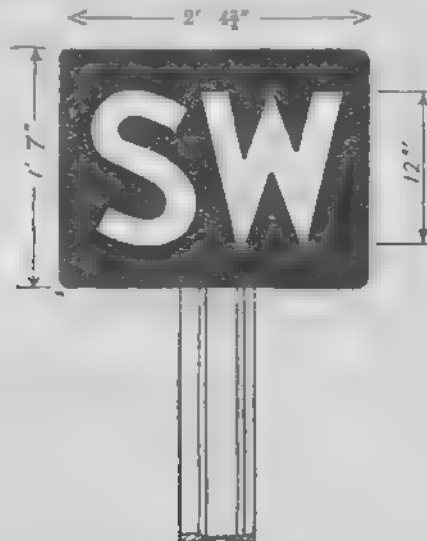
Whenever the detonators are exploded, the Signaller on duty at the time must report the facts to the Station Master and District Inspector, who will communicate them to the Divisional Superintendent or District Traffic Manager.

The Signaller must also record the circumstances in the Train Register Book and see that the machine is immediately recharged with detonators.

When the points are started from one position to another, Signaller should be careful not to turn the points until they are clear of the vehicles having passed beyond the machine, and Drivers must be careful to place any vehicles they may be stopping clear of the machine to avoid damage to it.

WHISTLE BOARD.

Whistle boards, as illustrated in the diagram below, are provided:—



The letters "SW" signify "Sound Whistle."

Drivers must sound the engine whistle when passing these boards.

Instructions for Signalling Trains during FOGS and FALLING SNOW and in Frosty Weather.

See General Manager's Circular.

"THREE SHOT" DETONATOR MACHINE WORKED IN CONJUNCTION WITH TRAILING POINTS.—Page 86.

The third paragraph of the above instructions to be deleted and the following substituted —

The Station Master must see that three fresh detonators are placed in the machine every month and a note to the effect that the detonators have been changed, must be made in the Train Register Book. Where the change is not made by the Signaller the person appointed to change the detonators must countersign the entry. The old detonators taken out of the machine in this way must be returned by the Station Master to the Stores Department on the 1st June and 1st December each year with the name of the sending station.

(G A.30 Op.—9/54 LK1/10730 363E)

SECTION II.

GENERAL INSTRUCTIONS AFFECTING THE WORKING OF TRAINS.

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SECTION II. (a).

INSTRUCTIONS CONCERNING PASSENGER TRAINS.

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The following to be added:—

Water Troughs, Clearance of Ice.

1. The Length Gangster will be responsible for keeping ice broken and cleared from the troughs.
2. The Length Gangster must use his discretion when the ice thickens as to when the troughs should be closed but must not allow the ice to become more than $\frac{1}{2}$ in thick before arranging for the troughs to be closed. If the temperature is so low that the water freezes as quickly as it is cleared the troughs must be closed.
3. If ice collects in the four foot to a depth of 1 in due to spillage the troughs must be closed.
4. When the Length Gangster finds it necessary to close the troughs as set out in Clause 2, he must advise the nearest Signman who will notify the District Controller. In turn the District Controller will immediately notify the following:—

Chief Mechanical and Electrical Engineers Outdoor Machinery District Mechanical Foreman or Local Mechanical Chargehand and responsible Water Fitter whichever is most convenient.

Motive Power Depots.

District Motive Power Superintendent

Headquarter's Control.

Chief Mechanical and Electrical Engineers Works Manager

District Engineer.

The Length Gangster must also advise his Permanent Way Inspector

5. When a thaw sets in after prolonged frost the Chief Mechanical and Electrical Engineer's Outdoor Machinery District Mechanical Foreman or Local Mechanical Chargehand or responsible Water Fitter, whichever is most convenient after consulting with the Gangster will say when normal taking of water can be resumed and will advise the nearest Signman and arrange for the water to be run off from the following:— In this case, advise the District Controller and he will notify the Departments listed in Clause 4 that normal working of the trough can be resumed.

6. The District Operating Superintendent to notify promptly the Chief Operating Superintendent who will issue the necessary instructions in regard to the taking of water while the water trough is out of use.

PASSENGER TRAIN INSTRUCTIONS.

WATER TROUGHS, G.W.R. AND JOINT LINES.

Water Troughs are situated as under: -

[illegible]

SLIP CARRIAGE WORKING.

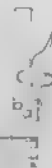
Description of Apparatus and Instructions for Working.

1. The operation of slipping is performed by the use of one lever (with three positions), as shown below :—

(1)

(2)

(3)



— MAIN TRAIN —
(RUNNING POSITION)

SLIP & BRAKE ON

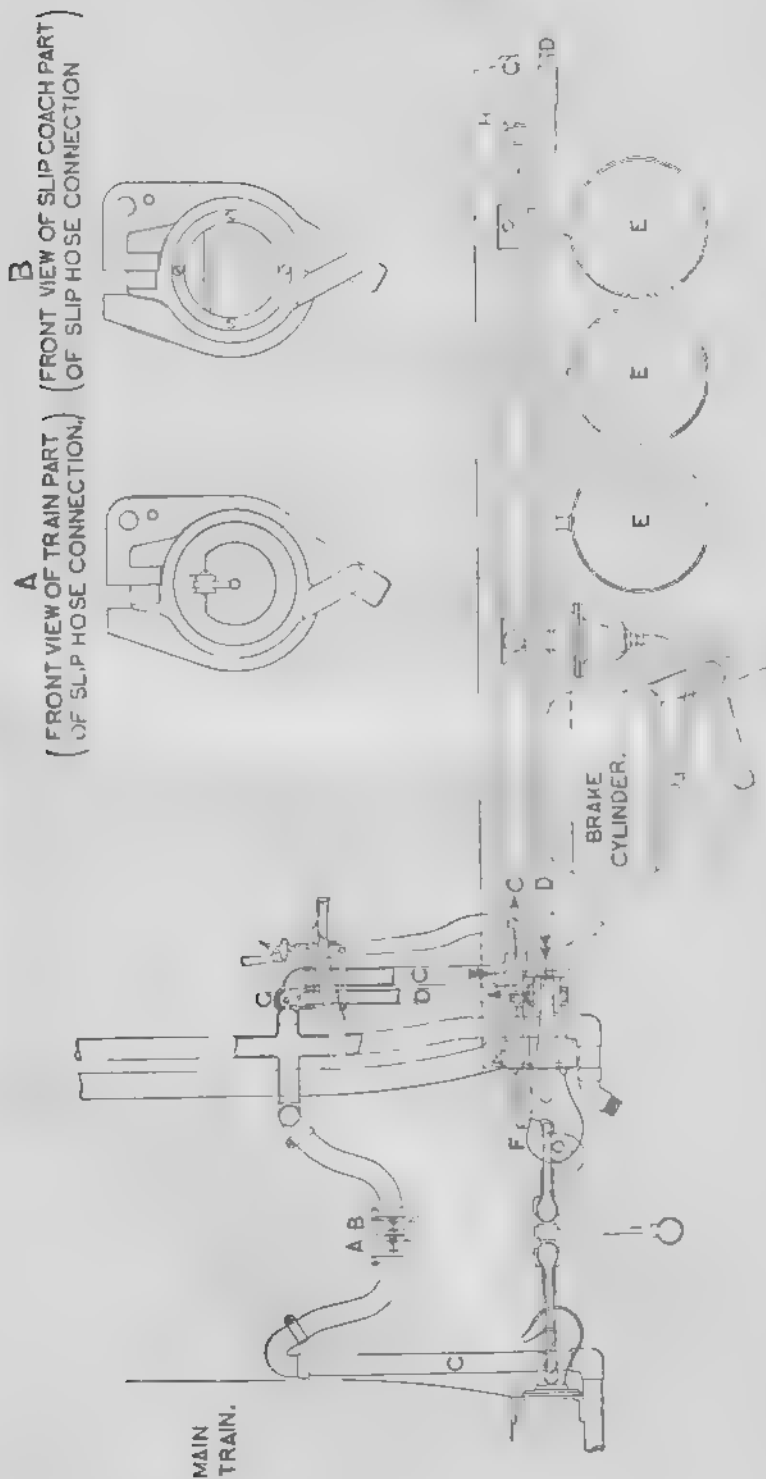
- RELEASE -

[illegible]

--SLIP COACH WORKING-- DIAGRAM N°1--

99

PASSENGER TRAIN INSTRUCTIONS. SLIP CARriage WORKING—Continued.



- G. SLIP VALVE. (SEE DIAGRAM N°2.)
- H. VALVE CONNECTING TRAIN PIPE WITH RESERVOIR PIPE.
THIS VALVE IS SO CONSTRUCTED THAT AIR MAY BE DRAWN FROM.
BUT CANNOT RETURN TO, THE RESERVOIRS.
- C. TRAIN PIPE.
- D. RESERVOIR PIPE.
- E. RESERVOIR.
- F. SLIP HOOK.

SLIP CARRIAGE WORKING.—Page 91.

The following additional paragraphs to be added to Clause 4.

- (d) The rear vehicle of the main train to which the slip coach is to be attached and from which it is to be slipped must always be a Western Region or B.R. Standard stock type vehicle. Other Region's vehicles are prohibited from working as the rear vehicle of the Main Train and if it is necessary for such vehicle to be conveyed on the train it must be marshalled inside the rear Western Region or B.R. standard vehicle of the Main Train or arrangements must be made for the train to stop at the Slipping Station.
- (e) When a B.R. Standard coach is the last vehicle on the Main Train, a special gangway door, special steam pipe safety chain bracket, and coupling safety clip for use on the buckeye draw hook, must be used for the attachment of the slip portion.

(G.A.30 Op.—9/54 LK1/8247/2/Gen.)

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PASSENGER TRAIN INSTRUCTIONS.

SLIP CARRIAGE WORKING—*Continued.*

touching the catch handle, and the catch not raised again until it is required to replace the lever in the running position. The number of times the brake can be released will depend upon the number of vehicles attached to the slip coach. Should there be the maximum number of vehicles, probably only three releases could be made after which the brake could not be released by means of the lever; but with the Slip Carriage only there might be as many as six or seven separate applications and satisfactory releases.

3. The Slip Guard must, before starting, see that the slipping lever is firmly secured in the "Main Train" or "running" position by the cotter being placed in the end of the catch rod, and in the case of double-ended slips, that the levers at both ends are so placed and secured.

Slipping Lever to be secured before starting

A small padlock is provided in every Slip Guard's compartment, attached by means of a chain to the slipping apparatus. Holes are also drilled in the ends of the cotters which hold the catches down, and, by means of padlocks, the cotters can be locked in position.

Provision of Padlocks on Slipping Apparatus.

Each Slip Guard will be supplied with a key fitting the padlocks, which he must retain in his possession and always have with him when on duty.

4. (a) EXCEPT IN CERTAIN SPECIAL CASES AUTHORISED BY THE SUPERINTENDENT OF THE LINE, slip carriages must be coupled to the train in the ordinary way by the screw coupling of the slip coach until the train arrives at the last station at which it is HOOKED REGULARLY TO STOP prior to passing the station where the slip portion has to be detached. At this point, the coupling must be adjusted and placed in the slip hook.

Coupling of Slip Carriages.

(b) GREAT CARE MUST BE EXERCISED WHEN SCREWING UP THE COUPLING TO THE SLIP HOOK IN ORDER TO AVOID THE SCREW COMING SO NEAR TO THE HOOK AS TO PREVENT IT TURNING OVER EASILY WHEN THE COACH IS SLIPPED. BEFORE COUPLING UP, THE SCREWS AT BOTH ENDS OF THE COUPLING SHOULD BE EXAMINED AND ADJUSTED, SO THAT THE SAME LENGTH OF SCREW SHALL BE INSIDE EACH OF THE SHACKLES. IF THIS IS NOT DONE, IN SOME CASES THE SCREW ON THE SLIP CARRIAGE SIDE WILL PROJECT SO FAR INTO THE SHACKLE BEFORE IT IS TIGHTENED THAT WHEN THE TIGHTENING PROCESS HAS BEEN COMPLETED THE SCREW WILL PRESS AGAINST THE SLIP HOOK AND PREVENT IT FROM TURNING OVER. The coupling must be screwed up in the same manner as when connecting ordinary coaches together.

Adjustment of screw coupling.

(c) At the point where the Slip apparatus is adjusted, the Slip Guard must be present and make the necessary movements with the slipping lever to enable the connector to couple the vehicle. The Slip Guard must then, by means of the cotter, secure the lever in the correct running position and lock the cotter by means of the special padlock provided for the purpose, and must not unlock the padlock until the train has started from the station at which it last stops before reaching the slipping point. In those cases where the slip couplings are adjusted at the starting point of the train, the padlock must not be unlocked until the train has proceeded some distance on its journey. The Slip Guard must also be careful to see, before giving the signal to start, that the cotter is in its proper place and padlocked.

Adjustment of Slip Couplings

On arrival at the slipping station the padlock must be left hanging on its chain until it is again required to be used on a slipping journey.

5. Before leaving the last stopping place prior to slipping, the Slip Guard must see:—

Slip Guard's duties before starting from the last stopping place.

(1) That the couplings and the hose pipe adaptors are properly connected, and that the flexible pipes are securely looped up by means of the chains and brackets provided.

(2) That the slipping lever is secured and padlocked in the "Main Train" or "Running" position.

(3) That 23 inches at least is indicated by the vacuum gauge in the slip compartment.

(4) That the proper Slip signals are carried, including a white head light after dusk.

(5) If 23 INCHES IS NOT REGISTERED ON THE VACUUM GAUGE THE SLIP PORTION MUST NOT BE SLIPPED, BUT THE TRAIN MUST STOP AT THE SLIPPING STATION.

NOT TO SLIP IF less than 23 inches of Vacuum before leaving last stopping place of Slipping

PASSENGER TRAIN INSTRUCTIONS.

5. Carriage Working—page 91.

Delete paragraph (a) of clause 6 and substitute the following:—

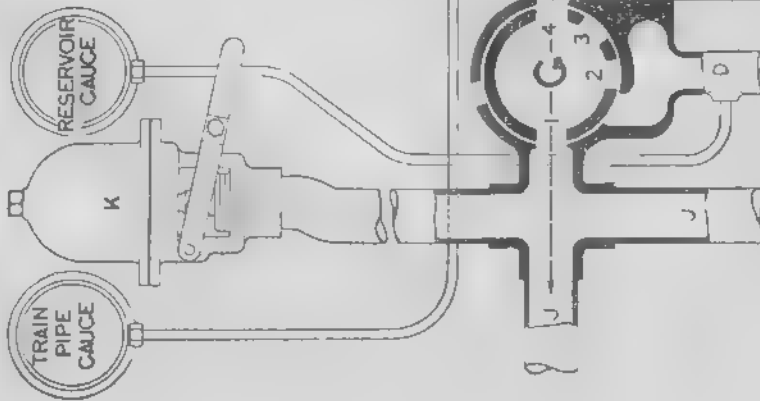
(a) The Guard of each Slip portion will be responsible for seeing by examination and testing before the train starts or at the station authorised for the adjustment of the Slip where he joins the train otherwise than at the starting point, that the brake, coupling and other apparatus are in good order. He must also see that the prescribed Slip Carriage signals are attached at the point of adjustment.

(G.A.27.Op. 1 51. O.M. Min. 12556.)

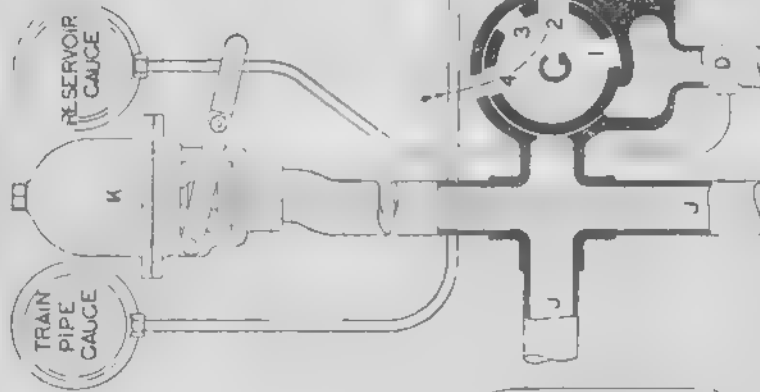
PASSENGER TRAIN INSTRUCTIONS.
SLIP CARriage WORKING—*Continued.*

—SLIP COACH WORKING— • —DIAGRAM N° 2—

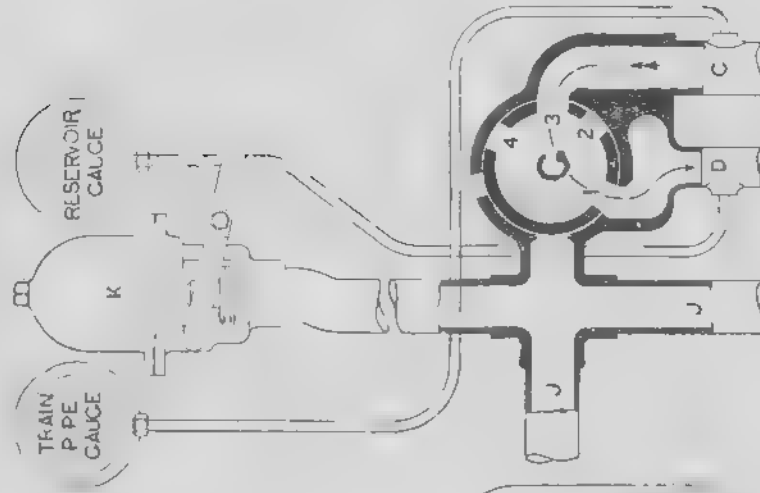
—TRAIN RUNNING—



—COACH SLIPPED—
BRAKE ON.



—COACH SLIPPED—
BUT 'BRAKE' RELEASED.



C. TRAIN PIPE.

D. RESERVOIR PIPE.

C. SLIP VALVE.

J. HOSE CONNECTING PIPE.

K. BRAKES SETTER.

(b) Should the slip hook or other apparatus including the vacuum brake, be defective, the slip portion must be attached by the screw coupling of the slip coach to the main train, and the train must stop at the shipping station.

When the Slip
Coupling or other
apparatus is
defective.

Shipping of
Carriages.

6. The ship board must be careful to maintain a safe interval between the ship position and the train, as is the usual rule of the ship position, in the event of the latter backing up, and to bring it to a stand from any cause before reaching the platform.

before reaching the platform. The Driver or Fuel Attendant and the Guards must look back over the Ship portion as it is pulled in, and on this it was stated by the Senior Guard that to satisfy themselves about correct coupling, they had to get out of the car and attempt to get in to stop the train unless the Ship Guard has pointed the clock provided below the main gate.

[illegible]

Bells on Ship
Carriages.

When a carrier is passing ship cars and it is necessary to give warning of the approach of a yard or they must sound the bell or whistle several times. This should, in any case, always be done before passing level crossings or entering stations.

When unable to
Sip.

If a machine is vacuum rated, it is registered on the gauge, no attempt must be made to suck out the SO₂ until the vent opens, the brake cock is closed, and the gauge is set to the vacuum to be needed. As filling, with a knowledge

4889] But the Sox (Goodman) spot on the brake cock
brake, on the fact that the engine is in need of shifting, will acknowledge
the fact by giving three short snaps at the whistle, or by giving the acknowledgment
signal that means the cook and the driver to proceed, and the
brake light to a short wave in a large of the platform at the shipping
the necessary stop signals have been given.

The same action must be taken by the Ship Guard and the Driver in the event of a signal at a shipping station worked by one signal box, or the lower distant signal, or the stop signal immediately to the rear of the platform at a station where there are two or more signal boxes, being passed by the Ship Guard at Danger, in consequence of which the ship portion cannot be shipped.

in consequence of which the ship portion cannot be shipped
should the ship portion of a train be delayed beyond the station at which it
should have been shipped, the Ship Guard must not take a full action in regard
to the shipping apparatus, but the Signaller must advise the Cox in advance by
talking instrument, where, unless the section is short in which case the advice
must be sent on immediately to the next Signal Box, by speaking instrument, the

PASSENGER TRAIN INSTRUCTIONS.

SLIP CARRIAGE WORKING—Continued.

train must be stopped and the Driver and Main Train Guard advised of the circumstances. The slip lever must be secured and padlocked in the Main Train (running) position and the key handed to the Main Train Guard. At the same time the Tail Lamps must be altered to agree with the proper tail signals normally carried by the train over that section of the line, and the slip portion taken on to the most convenient station, the Driver and Guards being instructed where the vehicles are to be detached.

11. On bringing the Slip Carriage to a stand after slipping, the Slip Guard must, before leaving the Slip compartment, screw the hand brake on tightly.

Guard to secure
Slipping Lever
after slip has
stopped.

12. Immediately after the slip portion has come to a stand the tongue of the slip hook must be replaced in its normal position. The Guard must then secure the Slip Lever in Main Train or Main Portion position by means of the cotter provided for the purpose.

Means of dealing
with adaptors
after slipping.

13. Two vacuum hose pipe adaptors are required for each S.C., and they will be fixed by a Carriage Department man at stations where that Department has a staff. The adaptor and chains left on the train portion of the train are to be removed by a Traffic Department man at the first stopping place after slipping. The box for carrying the adaptor and chains will be found in the van, and this, with the adaptor and chains, must be promptly returned to the station to which it belongs, by first available train, booked as a parcel. The adaptor, or adaptors, and chains left on the hose pipes of Slip Carriages after slipping must be removed by a Traffic Department man after arrival of the vehicles at the platform or appointed place and placed in the boxes provided for the purpose, which will be found in the slip ends of the coaches, and returned promptly to the station to which they belong by the first available train, booked as parcels.

Hose pipe adaptors are also provided for use in connection with the steam heating pipes, when the latter are in use, and these must be dealt with in a similar manner to the vacuum pipe adaptors.

Great care must be exercised in handling the adaptors so as to avoid damage, and every effort must be made to guard against loss and ensure that the articles are properly dealt with.

Two vacuum
gauge reservoirs
Carriages

14. A second vacuum gauge lettered in red "Reservoir," is fitted in the Guard's compartment. It is to be used in connection with the vacuum reservoir in the additional vacuum reservoirs.

Slip Guards should observe this gauge, as if from leakage or other cause it records a lower amount of vacuum than the main train gauge the vacuum brake will not be as readily released as usual on application after slipping.

Slip carriage to be
accompanied by a
Guard
Slip Carriage
Indicators.

15. (a) Every slip carriage or portion of a train to be slipped must be accompanied by a Guard.

(b) When Slip carriages are run on a train, Slip carriage indicators, as prescribed in Rule 125, must be carried.

Special Tail
Lamp for use
on trains to
which Slips
are attached.

16. THE LAST VEHICLE OF THE MAIN PORTION OF A TRAIN CONVEYING SLIP COACHES WILL CARRY A SPECIAL TAIL SIGNAL CONSISTING OF A DOUBLE WHITE TAIL LAMP SHOWING RED LIGHTS BY NIGHT PLACED VERTICALLY AS SHOWN BELOW BETWEEN THE STATION AT WHICH THE SLIP COUPLING IS ADJUSTED AND THE STATION AT WHICH THE TRAIN IS FIRST BOOKED TO STOP AFTER SLIPPING:—



GUARDS IN CHARGE OF SUCH TRAINS MUST, BEFORE THE TRAINS START FROM THE STATION AT WHICH THE COUPLING IS ADJUSTED, SEE THAT THE SPECIAL DOUBLE WHITE TAIL LAMP WITH RED LENS IS PLACED ON THE REAR VEHICLE OF THE MAIN PORTION OF THE TRAIN INSTEAD OF THE ORDINARY TAIL LAMP, AND ON ARRIVAL AT THE FIRST STATION AT WHICH THE TRAINS ARE BOOKED TO STOP AFTER SLIPPING, THE SPECIAL TAIL LAMP MUST BE REMOVED AND THE ORDINARY TAIL LAMP SUBSTITUTED FOR IT.

THE ORDINARY TAIL LAMP MUST BE CARRIED IN THE REAR GUARD'S VAN OF THE MAIN PORTION OF THE TRAIN FROM THE STATION AT WHICH THE SLIP COUPLING IS ADJUSTED, IN ORDER THAT NO DELAY MAY ARISE IN PROMPTLY REPLACING IT ON THE REAR OF THE MAIN PORTION OF THE TRAIN AT THE FIRST BOOKED STOPPING STATION AFTER SLIPPING. THE MAIN TRAIN GUARD WILL BE RESPONSIBLE FOR LIGHTING THE ORDINARY TAIL LAMP IN SUFFICIENT TIME BEFORE REACHING THE FIRST BOOKED STOPPING STATION AFTER SLIPPING TO ENSURE THE LAMP BURNING PROPERLY WHEN PLACED ON THE REAR OF THE MAIN OR THROUGH PORTION OF THE TRAIN.

THE DOUBLE WHITE TAIL LAMP WITH RED LENS WHEN REMOVED FROM THE MAIN PORTION OF THE TRAIN MUST BE RETURNED TO THE STATION TO WHICH IT BELONGS AS INDICATED BY THE TRAIN BOOK.

1867.

PASSENGER TRAIN INSTRUCTIONS

SLIP CARRIAGE WORKING—Continued.

THE REAR OF THE MAIN PORTION OF THE TRAIN AT THE FIRST BOOMED STOPPING STATION AFTER SLIPPING THE MAIN RAIN GUARD WHO BE RESPONSIBLE FOR PLACING THE CARRIAGE IN POSITION. THE DETAIL OF CHANGING THE FIRST BOOMED STOPPING STATION AFTER SLIPPING TO ENSURE THAT IT OCCURS PROPERLY WHEN PLACED IN THE REAR OF THE MAIN PORTION OF THE TRAIN.

THE FOLLOWING DETAIL LAMP WHEN REMOVED FROM THE MAIN PORTION OF THE TRAIN MUST BE RETURNED TO THE STATION TO WHICH IT BELONGS AS INDICATED ON THE REVERSE SIDE OF THE LAMP BY THE FIRST AVAILABLE PLAIN.

THE DIVISIONAL SUPERINTENDENTS AND DISTRICT T. S. S. MANAGERS MUST ADVISE THE SPECIAL TRAINS AT THE VARIOUS STATIONS TO BE ALERTED TO SEE THAT THE ARRANGEMENTS MADE ARE CARRIED OUT.

A LIST OF THE SPECIAL TRAINS AND SPECIAL LAMPS WORK AND THE TRAINS BY WHICH THEY ARE RETURNED TO THE FORWARD STATION, IS SHOWN IN THE THROUGH COACH PROGRAMME.

17. EXCEPT FOR THE PURPOSE OF SLIPPING, THE COUPLING MUST ONLY BE USED IN THE SLIP HOOK IN CASES OF NECESSITY. GREAT CARE MUST BE EXERCISED WHEN SHUNTING SLIP VEHICLES HAVING SLIP CONNECTIONS IN ORDER TO AVOID DAMAGE TO THE SLIP HOOKS. Use of Slip Hook.

18. SLIP LOCKS ON SLIP CARRIAGES ARE NOT TO BE STUCK AND DAMAGED THROUGH VEHICLES BEING SHUNTED WITH THE VACUUM BRAKE. BEFORE ANY ATTEMPT IS MADE THEREFORE, TO MOVE SUCH VEHICLES THE HAND BRAKE MUST BE PROPERLY TAKEN OFF AND THE VACUUM BRAKE RELEASED BY MEANS OF THE CORD OR WIRE WHEN THE TONGUE OF THE SLIP HOOK IS IN ITS NORMAL POSITION AND THE SLIP LEVER IS IN THE "Main Train" or "Running" position. Damage to Slip Hooks, etc.

19. WHEN IT IS NECESSARY TO PUT TWO OR MORE SLIP CARRIAGES COUPLED TOGETHER, IT IS OF THE GREATEST IMPORTANCE THAT THE VACUUM BRKES BE PROPERLY WORKED BY MEANS OF THE CORD OR WIRE AT THE PROPER POINT OF COUPLING. IF THE VACUUM BRKES ARE NOT SET TOGETHER, A VACUUM IN THE RESERVOIRS, THE NECESSARY MOVEMENT OF THE SLIPPING LEVER DURING THE PROCESS OF COUPLING APPLIES THE BRAKE. Should be two SLIP CARRIAGES coupled together.

Clause 20 to be amended to read:

(a) Unless instructions to the contrary are issued by the Superintendent of the Line, when a train which conveys slip carriages, has to be shunted, or is divided at ANY STATION AFTER IT HAS COMMENCED ITS JOURNEY, the first portion must not carry the vacuum brake, and the vacuum brake must be released by means of the cord or wire at the proper point of coupling. The shunting station and the intermediate stations and signal boxes must be advised by notice when practicable, or when this cannot be done, then by speaking instrument. IN THIS JOB.

(b) WHEN A TRAIN WHICH CONVEYS A SLIP CARRIAGE OR CARRIAGES IS RUNNING LATE, AND AN EXTRA TRAIN HAS TO BE DISPATCHED IN ADVANCE OF IT FROM ANY INTERMEDIATE STATION, THE FIRST PART MUST BE CONSIDERED THE SPECIAL, AND CARRY AN OBSCURE SINGLE TAIL LAMP. THE SLIPPING STATION AND THE INTERMEDIATE STATIONS AND SIGNAL BOXES MUST BE ADVISED BY SPEAKING INSTRUMENT, THE SPECIAL IF NOT MORE THAN TWENTY MINUTES IN ADVANCE OF THE REGULAR TRAIN AT THE STATION WHERE THE SLIP IS DETACHED MUST RUN THROUGH THE SLIP STATION OR STATIONS IN THE SLIPPING POSITION. IF THE REGULAR OR SECOND PORTION OF THE TRAIN WILL BE MORE THAN TWENTY MINUTES BEHIND THE SPECIAL OR FIRST PORTION OF THE TRAIN AT THE SLIP DETACHING STATION, THE LATTER MUST STOP AT THE SLIP DETACHING STATION IF THERE ARE ANY PASSENGERS TO PUT ON. THE SECOND OR LATE PORTION OF THE TRAIN IN EITHER CASE MUST CARRY THE SLIP CARRIAGE OR CARRIAGES (FURNISHED WITH CARRIAGE TAIL LAMPS), TO BE SLIPPED AT THE PROPER STATION IN THE USUAL WAY.

Slip Carriage
trains running
late

When it is necessary (CLAUSE 14, R.C.H. Op. Sup. Min 7946.) at a station where one is not usually slipped, an advice must be issued by the Divisional Superintendent to all Stations and Signal Boxes between the preceding station and the station where the carriage has to be slipped. The Station Manager in receipt of such advice, must immediately advise all persons concerned, including Permanent Way Gangers working in the neighbourhood of the station where the carriage is intended to be slipped, but in no case should it be arranged to slip the carriage without ample time being afforded for complying with the instructions contained therein.

PASSENGER TRAIN INSTRUCTIONS.

SLIP CARRIAGE WORKING—Continued.

When Permanent
Way operations
are in progress at
slipping
stations, etc

22. (a) If any engineering work should be in progress of a character to render it undesirable to slip a train, or if, owing to fog, snow or other cause, the Station Master or person in charge at a place where a carriage is usually slipped considers it desirable that the train should stop instead of the carriage being slipped, he must give INSTRUCTIONS THAT THE SIGNALS APPLICABLE TO THE SLIPPING TRAIN ARE NOT TO BE LOWERED UNTIL THE TRAIN HAS COME TO A STAND AT THE FIRST STOP SIGNAL AFTER PASSING THE POINT AT WHICH THE SLIP CARRIAGES ARE USUALLY SLIPPED, WHEN THAT SIGNAL MAY BE LOWERED AND THE TRAIN ADMITTED TO THE PLATFORM. IN SUCH CASES SLIP CARRIAGES MUST NOT BE DETACHED UNTIL THE TRAIN HAS COME TO A STAND AT THE FIRST STOP SIGNAL.

22 (b) When practicable the station at which the train last stops must be advised by telegram or telephone of the circumstances. The Station Master, or other person in charge receiving this message, must immediately acknowledge it to the sending station, issue form 2079 and specially instruct the Driver, the Slip Guard, and the other Guards of the train that the train must stop at the slipping station instead of slipping the carriages and he must see that the Slip Guard locks the slipping lever in the running position and hands the padlock key to the Main Train Guard before the train is allowed to proceed. On arrival at the slipping station the Main Train Guard must return the key to the Slip Guard to allow the latter to unlock the padlock and release the slipping lever. The lever must remain in the "brake on" position until the main portion of the train has started. The slip hook must then be replaced in the normal position, and the slip lever in the running position. The flexible pipes and adaptors will be dealt with at the usual stations.

If it is not practicable to wire the station at which the train last stops in sufficient time to advise the trainmen there, the advice must be sent to the station next in rear of the slipping station. This must be immediately acknowledged, and the train must be stopped there and the trainmen advised by the Station Master or other person in charge and form 2079 issued. The slipping lever must be locked in the running position and the padlock key handed to the Main Train Guard, who must retain it in his possession until the train arrives at the slipping station.

If train is stopped
in rear of slipping
station

When a train is stopped in rear of the slipping station where the slip carriage should be attached, must instruct the Driver and guards that the train must stop at the slipping station, and the Station Master or person in charge, at any intermediate station at which the train is booked to call, must satisfy himself that the trainmen understand they are required to stop at the slipping station. If from any cause arising at the last stopping station a train is required to stop at a station in rear of the slipping station, the Station Master or person in charge at such last stopping station must instruct the Driver and Guards that the train must stop at the slipping station. The Driver must see that the slip carriage or other vehicle is attached to the train at the slipping station, and the running position and HANDS THE PADLOCK KEY TO THE MAIN TRAIN GUARD BEFORE THE TRAIN IS ALLOWED TO PROCEED. ~~When the train is stopped in rear of the slipping station, the Driver must see that the slip carriage or other vehicle is attached to the train at the slipping station, and the running position and HANDS THE PADLOCK KEY TO THE MAIN TRAIN GUARD BEFORE THE TRAIN IS ALLOWED TO PROCEED.~~ (See paragraph 31).

In all such circumstances as those referred to above, or whenever vacuum adaptors have been fixed, the Slip Guard must ride in the brake compartment of the slip carriage until the vehicle is detached.

Maximum number
of cars to be
slipped.

23. When the slip portion consists of more than six 4- or 6-wheel vehicles, or four 8-wheel vehicles, the slip portion must not in such circumstances be placed in the slip-hook, and the train must be stopped at the station at which the slip portion is usually detached, that station being also advised by speaking instrument that the train will stop, unless it is decided to run the train in two parts, for instructions as to which see Paragraph 20 (a).

For the purpose of this regulation, four 70-foot vehicles may be considered as equivalent to four 8-wheel vehicles, and not on the basis of 10 wheels per 70-foot coach.

Slipping at
stations where
there is more than
one signal box.

24. (a) IN THOSE CASES WHERE THERE ARE TWO OR MORE SIGNAL BOXES AT THE SLIPPING STATION, THE SLIP GUARD MAY SOMETIMES FIND THE DISTANT SIGNAL, AT OR NEAR WHICH HE IS INSTRUCTED TO SLIP, AT "CAUTION." HE MUST, IF THIS IS SO, REFRAIN FROM SLIPPING UNLESS AND UNTIL HE CAN PLAINLY SEE THAT THE STOP SIGNAL AHEAD OF HIM, IMMEDIATELY IN THE REAR OF THE PLATFORM, AND THE DISTANT ARM BENEATH IT HAVE BEEN LOWERED FOR THE TRAIN TO RUN PAST THE PLATFORM. WHEN SLIPPING IN SUCH CIRCUMSTANCES, SLIP GUARDS MUST MAKE DUE ALLOWANCE FOR THE REDUCED DISTANCE THEY WILL HAVE TO RUN BEFORE REACHING THE PLATFORM.

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The last sentence of the first paragraph of Clause 22 (c) to be amended to read:
The Station Master must also issue Form 2079 and advise by speaking instrument the slipping station.
(See paragraph 31.)

(G.A. 18. 11/47. L.K. 1/8326/Gen. 4.)

SLIP CARRIAGE WORKING—Pages 87 to 98.

Clause 31. Delete second sentence in the first paragraph and substitute the following —
THE SLIP CARRIAGE TAIL SIGNALS MUST BE REMOVED AT THE USUAL SLIPPING STATION IN THOSE CASES WHERE THE TRAIN STOPS INSTEAD OF SLIPPING AND THE DOUBLE WHITE TAIL LAMP SHOWING RED LIGHTS PUT ON. THE DOUBLE WHITE TAIL LAMP MUST BE CARRIED AS FAR AS THE STATION AT WHICH THE TRAIN IS FIRST BOOKED TO STOP AFTER LEAVING THE SLIPPING STATION (G.A. 23—7 49 LK 1 9379 37)

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PASSENGER TRAIN INSTRUCTIONSSLIP CARRIAGE WORKING—Continued.

(b) **CLAUSS (A) OF DOUBLE LINE BLOCK REGULATION 4A OF THE DOUBLE LINE** Block Regulation 4a
BLOCK INSTRUCTIONS WITH REFERENCE TO THE WORKING OF LOWER DISTANT SIGNAL
 ARMS, MUST NOT BE APPLIED TO THE WORKING OF TRAINS SLIPPING CARRIAGES AT STATIONS. UNLESS "LIVE CLEAR" HAS BEEN OBTAINED SUFFICIENTLY FAR AHEAD OF THE SLIPPING STATION TO ENABLE ALL THE SIGNALS APPLICABLE TO THE SLIPPING TRAIN TO BE LOWERED, "LIVE CLEAR" MUST NOT BE RETURNED TO THE SIGNAL BOX WORKING THE DISTANT-SIGNAL WHICH SERVES AS AN INDICATION THAT ALL IS RIGHT TO SLIP.

25. At stations where the train runs through the middle road, and the slip portion is diverted to the platform road, the slip portion must be stopped at the facing point home signal, and the signalman must not reverse the points or lower the platform home signal until he has assured himself that the slip portion has come to a stand to the rear of it. Where the slipping train does not run through the platform road.

26. If, after the distant signal has been passed at "All Right" and the slip portion has been detached, the home signal at a slipping station should be put to "Danger" before the train has passed, or should any other necessity arise for the driver to reduce speed or stop the train after the slip portion has been detached, he must immediately whistle for the brakes of the slip portion to be applied by sounding the brake whistle, and the Slip Guard must apply the vacuum brake to prevent the slip portion running into the train. The Slip Guard must not, however, rely upon the sounding of the whistle in such an emergency, but must keep a good look out after having detached the slip portion, so that in the event of any danger arising he may be prepared to prevent the slip portion from colliding with the train. When Home signal put to Danger or train has to be stopped. If it is necessary to stop the slip portion as outlined.

27. At certain places where the Distant signals cannot be easily seen, repeating Distant signals working with the ordinary Distant signals, or separate auxiliary Distant signals, are provided in such positions that the Trainmen can more readily observe them. These additional signals are provided primarily for the Slip Guards, but the Engine-driver must be advised of their responsibility of observing them. If it is found that it will be necessary to stop his train at the slipping station to take water or for any other essential purpose, he must bring his train to a stand at the station before the rear of the slipping point or at a signal box intermediate between such station and the slipping point. If the train has already passed such station and inform the Station Master, person in charge or Signaller, of his requirements. The person so informed must advise the slipping station accordingly, in order that the slip signal may be maintained at Caution, form 2079 must be issued, and the provisions of the first paragraph of Clause 22 (b) must also be complied with. Repeating Distant Signals. If Driver finds it necessary to stop.

If the Driver finds it necessary to stop his train otherwise than in obedience to a Danger signal after he has passed the station or signal box next in the rear of the slipping point, he must take care that he does not stop until the train is well clear of the platform at which the slip portion will have to stop, in order to avoid the slip portion coming into collision with the train.

(b) Where there is a Public Level Crossing between the place where the slip portion is detached and the station, the one must not be folded between the train and the slip portion. Where a Public Level Crossing exists.

30. Should there be any irregularity in the working of the slip portion, the driver must immediately report full particulars to the Superintendent, and also record the facts in his journal, and call the attention of the carriage examiner at the first examining station. Irregularities to be reported.

31. EXCEPT IN THE CIRCUMSTANCES SHOWN IN CLAUSE 32, THE SLIP CARRIAGE INDICATORS WHETHER THE TRAIN IS ORDERED TO STOP AT THE SLIPPING STATION MUST ALWAYS BE CARRIED BY THE TRAINS ON WHICH SLIP CARRIAGES ARE ATTACHED. Slip carriage indicators to be always sent by trains on which they are attached to stop.
~~THE SLIP CARRIAGE INDICATORS MUST BE REMOVED AT THE FIRST EXAMINING STATION IN THOSE CASES WHERE THE TRAIN IS RUN AS A SPECIAL AND THE ORDINARY TRAIN LAMP PURCH.~~

WHEN SLIP COACHES ARE NOT ATTACHED TO A TRAIN WHICH IS SCHEDULED TO CONVEY SLIP PORTIONS, THE REAR COACH MUST CARRY THE USUAL SLIP LAMPS AND THE LAMPS FOR THE OTHER SLIP PORTIONS MUST BE PLACED IN THE VAN, SO THAT THEY MAY BE ATTACHED AT THE VARIOUS STOPPING PLACES, A WIRE TO BE SENT TO THE SLIPPING STATIONS ACCORDINGLY.

32. IN THOSE CASES, HOWEVER, WHERE THE SLIP CARRIAGES WHICH ARE USUALLY RUN ON A TRAIN ARE RUN AS A SPECIAL INSTEAD, AND NOT SLIPPED FROM THE TRAIN, THE TRAIN WHICH USUALLY SLIPS THE CARRIAGES MUST NOT CARRY THE SLIP INDICATORS OF THE PORTION RUN AS A SPECIAL. IN SUCH CIRCUMSTANCES, When train has to call at Slipping Station.

PASSENGER TRAIN INSTRUCTIONS.

SLIP CARRIAGE WORKING—Continued.

THE TRAIN MUST CARRY ORDINARY TAN. LAMPS IF THERE ARE NO CARRIAGES TO BE SLIPPED, AND IF THERE IS ONE SET OF SLIP CARRIAGES THEN THE TRAIN MUST CARRY THE PRESCRIBED LAMPS FOR TRAINS SLIPPING AT ONE STATION, AND THE DIVISIONAL SUPERINTENDENTS WILL ISSUE A NOTICE TO THE STATIONS CONCERNED TO THAT EFFECT.

Station Master and Inspectors to see that Guards work slip carriages, are thoroughly competent, accustomed to the working of the slip carriage, and well acquainted with the duty and the places where they have to detach slip carriages.

Return of Lamps or Lamps. 34. The Special Lamps must in every case be returned with the Slip Carriage unless otherwise specially ordered and the Vacuum Brake and Steam Heating Hose Pipe Adaptors must be booked to the forwarding station from the station at which the train first stops after slipping a coach. Any irregularity in working them must be immediately reported to the Superintendent.

REGULATIONS FOR WORKING THE VACUUM BRAKE.

1. **Description.** (a) The operation of the vacuum brake depends upon the creation and maintenance of the required vacuum.

THE NORMAL VACUUM ON THE G.W.R. IS 25 INCHES. IT SHOULD IN NO CASE BE MORE THAN 26 INCHES. ON PASSENGER TRAINS IT SHOULD BE NOT LESS THAN 23 INCHES AND, ON FREIGHT TRAINS, NOT LESS THAN 20 INCHES.

(b) The vacuum brake is continuous throughout the fitted and piped portion of the train, the amount of vacuum is indicated by the train pipe pressure gauge. The brake is engaged when the gauges on the engine and in the brake vans. On many engines and in certain vans belonging to other Companies the gauge has two pointers, one indicating the amount of vacuum in the train pipe and the other the amount in the reservoir.

A "fitted" vehicle must be understood to mean a vehicle which carries its own brake apparatus connected by a hose or pipes to the train pipe, and on which the brake blocks are operated by the vacuum brake.

A "piped" vehicle must be understood to mean a vehicle which is equipped with a continuous pipe only, i.e., without brake blocks operated by the vacuum brake.

(c) ~~The vacuum train pipes at the ends of "fitted" vehicles are connected to the vacuum train pipe at the end of the train by means of a coupling.~~ 4A 30

2. **Operation of Brake.** (a) The brake is applied by admitting air into the train pipe and is taken off by closing the air valve and restoring the vacuum to the required amount.

(b) The brake is primarily applied by the Driver, but it may also be applied by the Guard. A partial application of the brake can also be made, in case of emergency, by pulling the passenger communication chain.

3. **Before Starting.** (a) When the engine is attached to the train it is the duty of the person attaching it to see that the hose pipes of the engine and of the train and the Driver must satisfy himself that these pipes have been properly connected. The hose pipes between the vehicles must also be properly connected, and the hose pipes at the leading end of the engine, and at the rear end of the last fitted or piped vehicle, must be fitted with the coupling and must be placed on the stop pins. When an additional engine is attached in front of the train, the Driver of the leading engine must satisfy himself by personal inspection that the hose pipes between the two engines are properly connected.

When the hose pipes have been connected the Driver must create the required vacuum. He must take place at the train pipe pressure gauge post and satisfy himself by observing the gauge that there is no undue leakage. Should he not be able to create the necessary vacuum, or should he find such undue leakage as will prevent him being able to maintain the necessary vacuum, he must, after satisfying himself that the brake apparatus of his engine is in proper order, at once inform the Guard or Station Staff, who must take steps to have the train examined. Should the brake on any vehicle be able to give the brake or such other service as to render it operative or the vehicle detached, or any other steps taken as may be necessary.

(b) When the engine has been attached to a train, or an engine is changed, or an additional engine or vehicle attached, or a vehicle detached, the Driver must take place at the train pipe pressure gauge post and satisfy himself that the necessary vacuum is created in the gauge in the rear van, and then open the brake service valve on the brake van. If there is a leak, and he knows that the hose pipes are properly coupled up between this van and the engine. If no mishap takes place, he must inform the Driver and also take steps to ascertain the defect, and have it remedied.

If there are vehicles behind the rear brake van the Guard must himself see that the brake pipes are properly coupled up at the rear end of the last fitted or piped vehicle in the stop place.

The Guard must see that the necessary vacuum is registered in the gauge in his van before giving the Driver the signal to start.

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REGULATIONS FOR WORKING THE VACUUM BRAKE.—Page 98.

Clause 1 (c) of these instructions to be amended to read:—

The vacuum train pipes at the ends of "fitted" vehicles are painted red, the vacuum train pipes at the ends of "piped" vehicles are painted white.

(G.A.30 Op.—9/54 R1/5662)

The following to be added after the third paragraph of Clause 3 (b) :—

Guards in charge of Auto trains must open the driver's Automatic Train Control
Cut-out switch before testing the vacuum brake and satisfying themselves that the
vacuum is registered on the gauge. When this has been done the Cut-out
switch must be closed immediately, showing "Not in use"

This instruction will also apply in the case of trailers formed in trains to act as a

(G.A. 8.—5/41. LK1/5512/6.)

PASSENGER TRAIN INSTRUCTIONS.

VACUUM BRAKE REGULATIONS—Continued.

Guards must screw the hand brakes off before starting, as laid down in Rule 129/iv c, and on the chain or strap where provided.

(c) The Driver must, before starting, satisfy himself that the gauge on the engine indicates the necessary vacuum.

The Driver must accept the signal to start given by the Guard, not only as an indication that the train is ready to proceed, but also as an assurance that the hose pipes are properly connected throughout the train and that the gauge in the rear van indicates the necessary vacuum.

(d) The Guard must, before starting, and at places where the engine is changed or any vehicle is attached or detached, inform the Driver the number of vehicles there are on the train, and in the case of passenger trains, the weight of the train in tons. In the event of the brake not being in operation on any vehicle of the train, the Guard must also inform the Driver upon how many it cannot be applied. At certain places, where authorized, the Station Master or other appointed person will give the Driver this information. In each case the Driver must have this information before he proceeds on his journey.

The instructions contained in the preceding paragraph do not apply to trains running daily with the formation completed and the brake complete; if, however, the formation is altered or any vehicle is attached or detached, the Guard must advise the Driver.

(e) Care must be taken that the proportion of vehicles fitted with pipes only and not with brake apparatus does not exceed one in four in any passenger train running a distance of ten miles or under without a stop, nor one in six in any passenger train running more than ten miles without a stop.

For the purposes of this instruction the number of vehicles forming a passenger train must be counted as follows:

Motor box, carriage truck, fish van, or other 4-wheeled vehicles not carrying passengers	As $\frac{1}{2}$ vehicle.
Coaching vehicles 4- or 6-wheeled	1
Coaching vehicles, 8- or 12-wheeled	2
Articulated coaching vehicles, each bogie	1
Tank engine, 4- or 6-wheeled coupled	2
Tender engine, 4-coupled	3
Tender engine, 6- or 8-coupled	4

TABLES SHEWING THE PROPORTION IN WHICH PIPED VEHICLES MAY BE RUN IN PASSENGER TRAINS IN ACCORDANCE WITH REGULATION 4, CLAUSE (e) OF THE REGULATIONS FOR WORKING THE VACUUM BRAKE.

Table shewing what proportion of piped vehicles may be attached to a Passenger train running not more than 10 miles without a stop.

To a train (including engine) consisting of braked vehicles equal to:—

3	may be added	Unbraked vehicles equal to—
3	1
3 $\frac{1}{2}$	1
4	1
4 $\frac{1}{2}$	1 $\frac{1}{2}$
5	1 $\frac{1}{2}$
5 $\frac{1}{2}$	1 $\frac{1}{2}$
6	2
6 $\frac{1}{2}$	2
7	2
7 $\frac{1}{2}$	2 $\frac{1}{2}$
8	2 $\frac{1}{2}$
8 $\frac{1}{2}$	2 $\frac{1}{2}$
9	3
9 $\frac{1}{2}$	3
10	3
10 $\frac{1}{2}$	3 $\frac{1}{2}$
11	3 $\frac{1}{2}$
11 $\frac{1}{2}$	3 $\frac{1}{2}$
12	4
12 $\frac{1}{2}$	4
13	4
13 $\frac{1}{2}$	4 $\frac{1}{2}$
14	4 $\frac{1}{2}$
14 $\frac{1}{2}$	4 $\frac{1}{2}$
15	5

Table shewing what proportion of piped vehicles may be attached to a Passenger train running more than 10 miles without a stop.

To a train (including engine) consisting of braked vehicles equal to:—

3	may be added	Unbraked vehicles equal to—
3	$\frac{1}{2}$
3 $\frac{1}{2}$	$\frac{1}{2}$
4	$\frac{1}{2}$
4 $\frac{1}{2}$	$\frac{1}{2}$
5	1
5 $\frac{1}{2}$	1
6	1
6 $\frac{1}{2}$	1
7	1
7 $\frac{1}{2}$	1 $\frac{1}{2}$
8	1 $\frac{1}{2}$
8 $\frac{1}{2}$	1 $\frac{1}{2}$
9	1 $\frac{1}{2}$
9 $\frac{1}{2}$	1 $\frac{1}{2}$
10	2
10 $\frac{1}{2}$	2
11	2
11 $\frac{1}{2}$	2
12	2
12 $\frac{1}{2}$	2 $\frac{1}{2}$
13	2 $\frac{1}{2}$
13 $\frac{1}{2}$	2 $\frac{1}{2}$
14	3
14 $\frac{1}{2}$	3
15	3

Note. The term "Brake Van" includes any vehicle fitted with a brake compartment.

PASSENGER TRAIN INSTRUCTIONS.

VACUUM BRAKE REGULATIONS—Continued.

(f) Should the brake not be in operation on any vehicle conveying passengers formed behind the rear brake van, such passengers, unless in charge of live stock, must be transferred to the vehicles in front of it unless another fitted vehicle on which the brake can be applied by the Driver is attached in rear.

4. During Journey. (a) The required vacuum should be maintained throughout the journey except when it is necessary to apply the brake.

Should the Guard find during the journey that the gauge in the van shows less than the required vacuum (unless he is satisfied that this is caused by the Driver applying the brake), he must be prepared to apply his hand brake as may be required.

(b) The Driver and Guard must report any irregularity in connection with the working of the brake or defect in its action, or other special circumstances, and call the attention of a Carriage Examiner to the matter at the first opportunity. The Guard must also note the particulars on his journal.

(c) When vehicle is detached or attached the vacuum in the train pipe must be destroyed by opening the air valve on the engine or in the nearest brake van.

(d) Should the Driver find that his train is being retarded owing to the brake blocks on all the vehicles not being off, he must stop, under the protection of fixed signals if practicable, and have the brakes properly released.

(e) When two engines are attached to a train, the Driver of the leading engine must work the vacuum brake, as laid down in Rule 135 (a).

(f) When it is necessary for the Guard to apply the brake to attract the Driver's attention in an emergency, or when he is being aware that the Driver is not going to make a booked stop, or from any other cause, he must apply the brake by opening the cock in his van and keep it fully open until he is satisfied that the Driver has become aware that the train is required to stop. He should then leave the Driver to bring the train to a stand at the most convenient situation.

5. Stopping. (a) The vacuum brake must be used for the ordinary stopping of the train by the Driver. The brake should be so operated during the stop that it is not necessary to make a powerful application of the brake when the train is travelling at a low speed, but, in case of emergency, the vacuum should be destroyed as rapidly as possible.

(b) Before the vacuum is destroyed the vacuum should be partially re-created so as to prevent a rebound of the vehicles, or undue strain on the couplings.

(c) After the brake has been applied, the steam supply to the train must not be applied to move the train until the brake has been released throughout the train.

(d) The working of the vacuum brake by the Driver will not relieve the Guards from the responsibility of observing Rule 148.

(e) In the case of a train being delayed the Guard in the rear van must put his hand brake hand on and secure it with the chain or strap, where provided, before leaving the van. He must also take any other measures that may be necessary to prevent the rear portion moving.

6. Testing Brake when Running. In addition to tests laid down in Regulation 3, Drivers must also test the brake in good time before reaching their full braking distance when approaching

(i) Steep falling gradients.

(ii) A terminus.

(iii) A principal station at which the train has to stop.

(iv) A crossing place on a single line at which the train has to stop.

(v) AFTER RECEIVING A "WARNING" SIGNAL, THE DISTANT SIGNAL APPLICABLE TO THE HOME SIGNAL AT WHICH THEY WILL HAVE TO STOP IF IT IS AT DANGER.

The speed of the train must be reduced by the test and Drivers must enter such stations or a dead end bay at any station at a speed which will enable them to stop the train at the proper place.

Unless the vacuum brake is working properly when this tested, the Driver must whistle for the Guard's hand brake, stop the train, and inform the Guard that the vacuum brake is out of order, and that the hand brakes must be relied upon for controlling the train. Special care must be taken to regulate the speed of the train.

7. Release of Brake by Hand. To release the brake on any vehicle the cord or wire the position of which is indicated by star, arrow, or the letter "A" or "V" on the solbar of each vehicle, must be pulled and the release valve held open until the brake is sufficiently released. The cord or wire must never be fastened. On vehicles where two brake cylinders are provided the cords or wires of both must be pulled and the release valves held open.

WHERE OTHER COMPANIES' VEHICLES DETACHED FROM THE TRAIN ARE REQUIRED TO HAVE THE BRAKES FULLY RELEASED, IT IS IMPORTANT THAT THIS SHOULD BE DONE BEFORE THE HOSE PIPES ARE PUT ON THE STOP PLUGS AS, IF SUCH VEHICLES ARE RELEASED WITHOUT ONE END AT LEAST OF THE CONTINUOUS PIPE BEING OPEN TO THE ATMOSPHERE, THERE IS A RISK OF THE BRAKE SUBSEQUENTLY RE-APPLYING ITSELF.

Clause 8.

The third paragraph to be amended to read :

If the Driver, is from any cause, unable to work the vacuum brake, it must be cut off and released throughout the train. The train should then be worked by hand brakes only, the speed being so regulated as to enable the Driver to have full control of the train by the hand brakes. a Guard must travel in the rearmost brake van on the train, be on the alert, and assist in stopping the train with the hand brake.

The following to be added as the final paragraph :

Unless the train is being assisted in the rear, passengers must not be conveyed in vehicles on which the continuous brake is inoperative if such vehicles are marshalled behind the brake van in which a Guard rides. If the defect cannot be quickly remedied, any passengers in these vehicles must be detrained or transferred to the vehicles in front of the rear brake van, unless the vehicles on which the brake is inoperative can with safety and without causing serious delay be re-marshalled inside the rear brake van or an additional brake van can be attached to the rear of the train. If it is not practicable to remarshal this passenger stock or to attach an additional brake van to the rear of the train, the vehicle or vehicles must be detached at the nearest convenient point. Should, however, the failure occur between stations, the train may be worked forward to the next station where the foregoing procedure must be observed.

MAY, 1945.

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PASSENGER TRAIN INSTRUCTIONS.

VACUUM BRAKE REGULATIONS—Continued.

When porters and others are dealing with horse boxes, carriage trucks, fruit trucks, etc., detached from passenger trains at stations situated on gradients they should be very careful not to release the vacuum brake until they have first ascertained that the vehicle is provided with a hand-brake, as some of these vehicles, belonging to other companies, are not provided with hand brakes, and there is considerable risk of such vehicles running away if the vacuum brake is released before they are safely scotched.

8. Failure of Brake.—In case of failure of the brake, provided the defect is not in the train pipe, the defective cylinder or cylinders should, where practicable, be put out of action and the vehicle worked as one with through pipes only; the brake will then be effective on the remainder of the train.

If the defect is in the train pipe, or in the arrangement of fittings for putting the cylinder or cylinders out of action, the hose pipe at the leading end of the defective vehicle must be disconnected and the hose pipe at the rear end of the vehicle next in front placed on the stop pipe. The brake on the front vehicle can then be worked as usual. The brake on the defective vehicle, and those in rear of it must be released and the train worked forward under the control of the hand brakes with the assistance of the Driver can give with the vacuum brake on the vehicles still connected to the train, to the next station where the defective vehicle can be conveniently detached or the defect remedied.

If the Driver is, for any cause, unable to work the vacuum brake, it must be cut off and released throughout the train. The train should then be worked by hand brakes only, the speed being regulated as to enable the Driver to do full duty of the train by the hand brakes. Guards must be on the alert, and assist in stopping the train with their hand brakes.

9. Detaching of Engines or Vehicles.—a. When an engine is detached it is the duty of the person detaching to put the hose pipe of the engine and that of the first vehicle on the stop pipe.

b. Vehicles fitted with the vacuum brake are provided with the vacuum brake must be released and when it is necessary that brakes should be applied for the purpose of keeping the vehicles from moving, the hand brakes, where provided, must be used, supplemented by stretches if necessary.

10. Frosty Weather. In frosty weather special attention must be paid to the vacuum brake. Any injectors with leaky steam valves must be changed or repaired, all drip traps must be examined daily and ascertained to be free from water or ice, and the drip valves clear and working properly, and pipes between engine and tender (if not fitted with screened connections) must be kept clear and as free as possible from water or ice. Drivers must also use special care in approaching terminal stations and other places specified in Section 6 places where a speed restriction is in force and a flag carrying a "Warning" signal.

11. Working of Brake.—As a result of experiments made to ascertain the cause of the rough stops that are sometimes made with passenger trains, it has been found that these invariably occur whenever a sudden and heavy reduction of vacuum is made just before the train comes to rest.

Attention is called to the instructions in Section 5 (b) that "Before finally coming to a stand the vacuum should be partially recreated so as to prevent a rebound of the vehicles, or undue strain on the couplings." When it is not possible to do this, the vacuum should be steadily reduced as much as possible before the final application is made, and this application should not be made by suddenly throwing the handle right over.

A train should always be stopped without closing up the buffers between the carriages, as when this takes place, although nothing may be noticed on the engine a severe jar is always felt in the train.

12. Improper Use of Vacuum Brake. Guards and others must not make use of the brake setters in the guard's vans to bring trains to a stand for such purposes as the more convenient loading or unloading of luggage, or because some persons may have failed to alight from or enter the train before it began to move. Such applications of the vacuum brake are strictly prohibited, unless the brake setters are required to be used in case of emergency in order to avoid an accident.

It is to be understood that the cock should in ordinary circumstances be used by guards when it is required to attract the Driver's attention for the purpose of stopping the train. If no cock is provided the chain communication should be used.

13. Regulations for Working Chain Communication between Passengers and Guard and Driver by means of the Vacuum Brake.

(a) All G.W. Carriages and Horse Boxes have been fitted with the chain communication, which is connected with the vacuum brake.

(b) (i) A passenger requiring to communicate with the Guard and Driver will pull down the chain provided for the purpose. This will cause the partial application of the brake, which will be indicated by the gauges on the engine and in the Guard's van.

(ii) A small disc which is normally in a horizontal position on each side of that carriage from which the communication has been made will be turned and exhibited in a vertical position.

(iii) The Guard can ascertain from which compartment of the carriage the alarm has been given by the blackness of the communication chain.

PASSENGER TRAIN INSTRUCTIONS.
VACUUM BRAKE REGULATIONS—Continued.

(c) (i) WHEN THE DRIVER OBSERVES THAT THE BRAKE IS BEING APPLIED, HE MUST INFER THAT THE COMMUNICATION HAS BEEN USED AND MUST STOP HIS TRAIN WITH AS LITTLE DELAY AS POSSIBLE, HAVING DUE REGARD TO RULE 187 IN THE RULE BOOK. HE MUST, HOWEVER, EXERCISE DISCRETION IN STOPPING, IT BEING UNDESIRABLE TO BRING THE TRAIN TO A STAND ON AN OVERBRIDGE, OR A VIADUCT, ON CATCH POINTS, OR IN A TUNNEL, OR OTHER PLACES OF A SIMILAR CHARACTER.

(ii) WHEN THE GUARD OBSERVES THAT THE COMMUNICATION HAS BEEN APPLIED HE MUST ACT AS CIRCUMSTANCES REQUIRE.

(d) WHEN THE TRAIN HAS BEEN BROUGHT TO A STAND IT MUST BE PROTECTED AS PRESCRIBED IN RULE 181 (f).

(e) THE GUARD MUST TAKE STEPS TO ASCERTAIN WHY, AND BY WHOM, THE COMMUNICATION HAS BEEN APPLIED, AND, SHOULD THE ALARM HAVE BEEN MISCHIEVOUSLY GIVEN, OR FOR AN INSUFFICIENT CAUSE, THE NAMES AND ADDRESSES OF ALL THE PASSENGERS IN THE COMPARTMENT MUST BE TAKEN IN ORDER THAT THE OFFENDING PASSENGER MAY BE PROPERLY DEALT WITH.

(f) THE GUARD MUST, IN ADDITION TO THE NOTES IN HIS JOURNAL, SPECIALLY REPORT ANY USE THAT MAY HAVE BEEN MADE OF THE COMMUNICATION ON THE JOURNEY, OR ANY FAILURE IN ITS ACTION.

(g) BEFORE THE TRAIN COMMENCES ITS JOURNEY, THE GUARD MUST SATISFY HIMSELF THAT THE DISCS ARE IN THEIR NORMAL POSITION.

14. Engines fitted with Vacuum Reservoirs to Automatic Steam Brake Attachment. AUTO ENGINES AND ENGINES HAVING TRIP COCKS OR AUTOMATIC TRAIN CONTROL APPARATUS, WHEN STEAM-BRAKED, ARE FITTED WITH VACUUM RESERVOIRS TO THE AUTOMATIC STEAM BRAKE ATTACHMENT. THE AUTOMATIC ACTION IS THEREBY MADE CONSIDERABLY MORE SENSITIVE THAN USUAL, AND THE STEAM BRAKE WILL BE APPLIED BY ORDINARY LEAKAGE OF THE VACUUM IN THE TRAIN PIPE.

WHEN RUNNING, THE EJECTOR MUST ALWAYS BE WORKED AS ON A VACUUM BRAKE ENGINE, *i.e.*, THE BRAKE MUST BE BLOWN OFF UNTIL THE ENGINE IS MOVING FAST ENOUGH TO ENABLE THE PUMP TO MAINTAIN THE VACUUM.

SHOULD THE BRAKE FAIL TO COME OFF ON RECREATING THE VACUUM AFTER AN APPLICATION, A MOVEMENT IN EITHER DIRECTION OF THE HANDLE OF THE RELEASE VALVE ON TOP OF THE RESERVOIR WILL REMEDY THIS.

WHEN THERE IS A VACUUM IN THE RESERVOIR, AN APPLICATION OF THE BRAKES EITHER BY MEANS OF THE EJECTOR HANDLE OR OTHERWISE, DOES NOT FULLY APPLY THE STEAM BRAKE. TO APPLY THE STEAM BRAKE WITH FULL FORCE THE ENGINEMAN SHOULD DESTROY THE RESERVOIR VACUUM BY MEANS OF THE RELEASE VALVE IN ADDITION TO PUTTING THE EJECTOR HANDLE INTO THE FULL "BRAKES ON" POSITION.

WHEN SHUNTING, OR WORKING NON-VACUUM FITTED FREIGHT TRAINS, IN ORDER TO AVOID THE SUDDEN APPLICATION OF THE STEAM BRAKE, IT HAS BEEN THE PRACTICE TO REMOVE THE FLEXIBLE VACUUM TRAIN PIPE FROM THE PLUG AND SO PREVENT THE APPLICATION OF THE STEAM BRAKE BY THE VACUUM ATTACHMENT, AND ALLOW OF IT BEING APPLIED EITHER FULLY OR GRADUALLY BY MOVEMENT OF THE EJECTOR HANDLE.

THIS PRACTICE OF REMOVING THE FLEXIBLE PIPE MUST NOT BE ADOPTED ON ENGINES FITTED WITH THE VACUUM RESERVOIRS REFERRED TO IN THE FIRST PARAGRAPH WHEN RUNNING ON MAIN LINES OR ON THE METROPOLITAN OR HAMMERSMITH AND CITY RAILWAYS, AS TO DO SO WOULD INTERFERE WITH THE OPERATION OF THE AUTOMATIC TRAIN CONTROL APPARATUS. A FULL OR GRADUATED APPLICATION OF THE STEAM BRAKE MUST BE MADE BY OPERATING THE RELEASE VALVE ON THE RESERVOIR AND BY MOVING THE EJECTOR HANDLE TO THE "BRAKES ON" POSITION.

Engines fitted with Vacuum Reservoirs must on no account be moved on the Main Lines or the Metropolitan and Hammersmith and City Lines until a vacuum has first been created in both the reservoir and the train pipe.

IT IS IMPORTANT THAT A FULL VACUUM SHOULD BE MAINTAINED IN THE RESERVOIR, AND DRIVERS SHOULD IMMEDIATELY REPORT ANY LEAKAGE.

15. Engines fitted with Vacuum Retaining Valves. Object. THE OBJECT OF THIS VALVE IS TO SECURE A MORE EFFICIENT ACTION OF THE ENGINE BRAKE, AT THE SAME TIME REDUCING THE COST OF MAINTENANCE.

Action.—FIGURE 1 SHOWS THE LATEST TYPE OF VALVE IN RUNNING POSITION. IT IS CONNECTED AT A TO THE PUMP, AT B TO THE TRAIN PIPE AND AT C TO THE RESERVOIR. THERE IS THUS ALWAYS TRAIN PIPE VACUUM IN THE SPACES ABOVE THE SMALL PISTON D AND BELOW THE LARGE PISTON E (THESE TWO SPACES BEING CONNECTED BY MEANS OF THE HOLE THROUGH THE PISTON ROD), AND RESERVOIR VACUUM IN THE SPACE BETWEEN THE PISTONS. WHEN RUNNING, AIR IS EXTRACTED FROM THE TRAIN PIPE BY THE PUMP IN THE USUAL WAY AS SHOWN BY THE ARROWS.

WHEN THE BRAKE IS APPLIED, AIR FROM THE TRAIN PIPE FLOWS UNDERNEATH THE LARGE PISTON E AND RAISES THE PISTONS TO THE POSITION SHOWN IN FIGURE 2. THE PISTON D THEN SEALS THE PASSAGE B AND, BEING RAISED ABOVE THE PASSAGE A, OPENS COMMUNICATION BETWEEN THE LATTER AND THE PASSAGE C, AND AIR IS THEN SUCKED BY THE PUMP FROM THE RESERVOIR, THUS MAINTAINING THE VACUUM THEREIN. A "PEPPER BOX" VALVE ON THE CONNECTING PIPE PREVENTS THE RESERVOIR VACUUM FROM RISING TO AN EXCESSIVE AMOUNT AND SO PREVENTING THE PISTONS RETURNING TO THEIR NORMAL POSITION WHEN THE BRAKE IS BLOWN OFF.

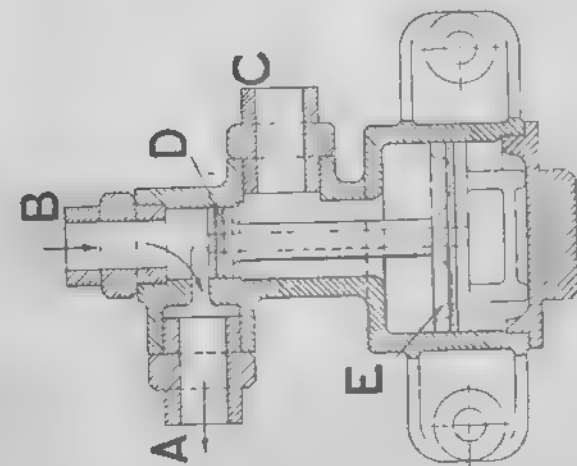


FIG. 1

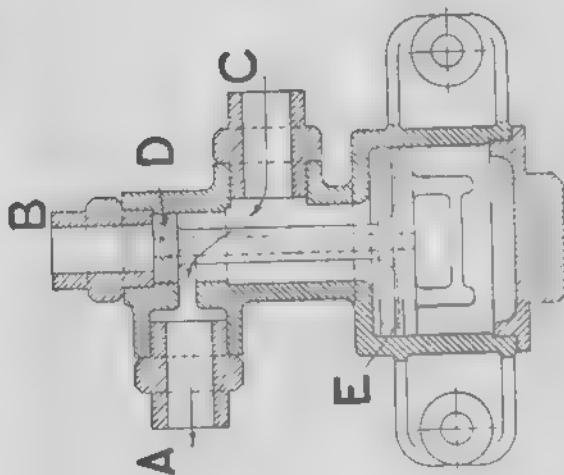


FIG. 2

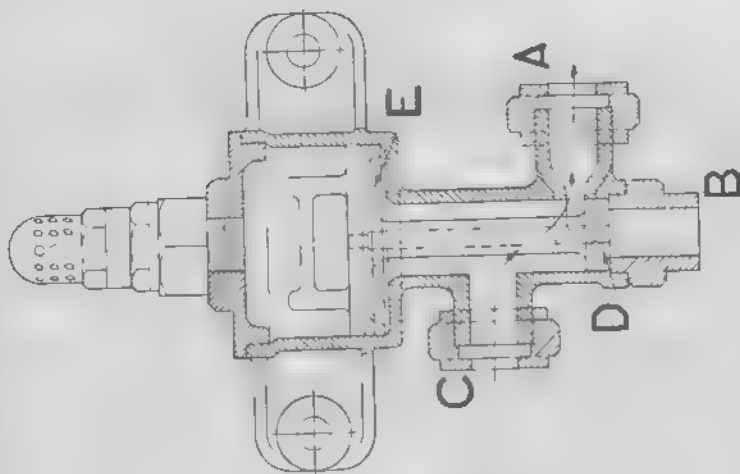


FIG. 3

PASSENGER TRAIN INSTRUCTIONS.

VACUUM BRAKE REGULATIONS—*Continued.*

IN THE OLDER TYPE OF VALVE THE RESERVOIR AND TRAIN PIPE CONNECTIONS ARE REVERSED AND THE VALVE IS PLACED THE OTHER WAY UP. FIGURE 3 SHOWS ONE OF THESE VALVES IN RUNNING POSITION. THE "PEPPER-BOX" VALVE, IN THIS CASE, IS PLACED ON THE CHAMBER ABOVE THE LARGE PISTON, WHICH COMMUNICATES WITH THE RESERVOIR BY MEANS OF THE HOLE THROUGH THE PISTON ROD. THE VALVE IS CONNECTED AT A TO THE PUMP, AT B TO THE RESERVOIR AND AT C TO THE TRAIN PIPE. WHEN THE BRAKE IS APPLIED, AIR FROM THE TRAIN PIPE ENTERS THE SPACE BETWEEN THE PISTONS AND RAISES THEM SO THAT THE SMALL PISTON D COMES ABOVE THE PASSAGE A. AIR IS THEN EXTRACTED FROM THE RESERVOIR THROUGH THE PASSAGE B.

Inspection when running.—WHEN EVERYTHING IS IN ORDER THE RESERVOIR VACUUM GAUGE NEEDLE WILL BE SEEN, ON APPLYING THE BRAKE, FIRST TO FALL SLIGHTLY (DOWING TO THE MOTION OF THE PISTON IN THE BRAKE CYLINDER COMPRESSING THE RESIDUAL AIR), AND THEN REMAIN STATIONARY OR RISE SLIGHTLY.

THE INITIAL DROP IN VACUUM SHOULD NOT BE MORE THAN ABOUT FIVE INCHES AND, WITH THE HANDLE IN "RUNNING" OR "BRAKES ON" POSITION, THE NEEDLE SHOULD, IF IT FALLS BELOW 22 INCHES, RISE AGAIN TO AT LEAST THAT FIGURE. AN EXCESSIVE INITIAL DROP IS A SIGN THAT THE BRAKE GEAR IS SLACK AND REQUIRES TAKING UP. IF THE RESERVOIR NEEDLE SHOULD CONTINUE FALLING THERE IS PROBABLY A LEAK INTO THE RESERVOIR. THIS WILL MOST LIKELY BE FOUND EITHER IN THE CONNECTIONS OF THE SMALL FLEXIBLE PIPE BETWEEN ENGINE AND TENDER OR IN THE PIPE TO THE RELEASE COCK OR VACUUM GAUGE, THOUGH IT MAY BE ELSEWHERE. IF NO LEAK CAN BE FOUND, OR IF A LEAK IS FOUND AND STOPPED, AND THE TROUBLE STILL CONTINUES, THE RETAINING VALVE IS PROBABLY OUT OF ORDER AND SHOULD BE EXAMINED OR CHANGED.

ENGINEERS MUST SEE THAT THE BRAKE IS WORKING PROPERLY AS ABOVE DESCRIBED, and must at once report any defect.

Inspection in the Shed—A ~~twelve monthly~~ ^{twelve monthly} inspection of the condition of the brakes must be made in the shed as follows: A vacuum must be blown up and the brakes applied. IF THE NEEDLE OF THE VACUUM GAUGE SHOULD SHOW THAT AN ENDURE LEAKAGE IS TAKING PLACE INTO THE RESERVOIR THE RETAINING VALVE MUST BE TEMPORARILY BLANKED OFF AT A, B AND C. IF THIS STOPS THE LEAKAGE THE VALVE IS DEFECTIVE AND MUST BE CHANGED. IF IT DOES NOT THE DEFECT LIES ELSEWHERE AND THE BRAKE MUST BE EXAMINED AND THE DEFECT FOUND AND REMEDIED.

IN ALL CASES THE RETAINING VALVE SHOULD BE CLEANED AND OILED WHEN THE TEST IS MADE (THIS SHOULD ALSO BE DONE OCCASIONALLY BETWEEN INSPECTIONS).

IF ONLY OF THE NEW PATTERN VALVES SHOULD FAIL AND ONLY OLD PATTERN VALVES ARE AVAILABLE TO REPLACE IT, ONE OF THESE CAN BE USED BY REMOVING THE "PEPPER-BOX" VALVE AND SUBSTITUTING A BLANK.

Blanking Off in case of Failure.—IF A RETAINING VALVE SHOULD FAIL IN SUCH A WAY AS TO PREVENT THE ENGINEER FROM BEING ABLE TO RELEASE HIS BRAKE PROPERLY, OR OTHERWISE INTERFERE WITH THE WORKING OF THE TRAIN, ON THE FIRST OPPORTUNITY THE CONNECTION TO THE RESERVOIR (AT THE CONNECTION C IN THE NEW TYPE OF VALVES FIGURES 1 AND 2, AND THE CONNECTION B IN THE OLD TYPE, FIGURE 3) SHOULD BE BLANKED. THE COVER MUST BE UNSCREWED AND THE PISTONS REMOVED, THE COVER BEING THEN REPLACED.

IF POSSIBLE THE "PEPPER-BOX" VALVE SHOULD BE REMOVED AND A BLANK SUBSTITUTED. IF THIS IS NOT DONE, ONLY 22 INCHES OF VACUUM WILL BE MAINTAINED BY THE PUMP WHILE RUNNING.

16. Coaches fitted with Direct Admission Valves. Object. THE OBJECT OF THE DIRECT ADMISSION VALVE (COMMONLY CALLED THE "D.A." VALVE) IS TO OBTAIN A MORE RAPID ACTION OF THE BRAKE IN CASES OF EMERGENCY BY REDUCING THE TIME REQUIRED TO FILL THE TRAIN PIPE AND CYLINDERS WITH AIR.

Action.—FIGURES 1 AND 2 SHOW DIAGRAMMATICALLY THE VALVE AND ITS CONNECTIONS. FIGURE 3 SHOWS THE VALVE IN SECTION.

REFERRING TO FIGURE 3, ON BLOWING UP A VACUUM AIR IS DRAWN FROM THE CYLINDER PAST THE NON-RETURN VALVE A, A VACUUM BEING FORMED AT THE SAME TIME IN THE CHAMBER B BY MEANS OF THE PASSAGE C.C. AND THE FLAT D ON THE SPINDLE.

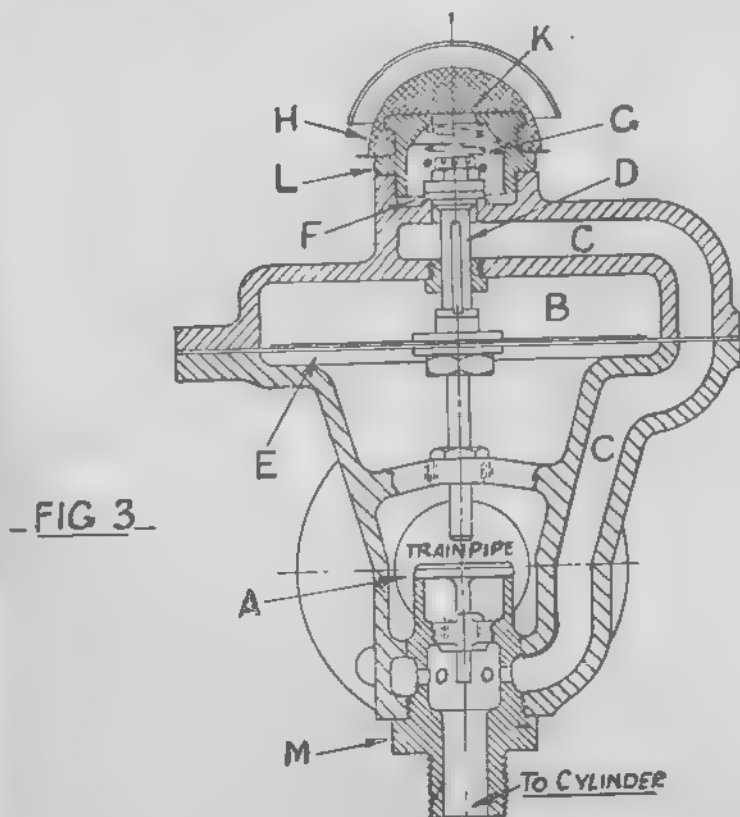
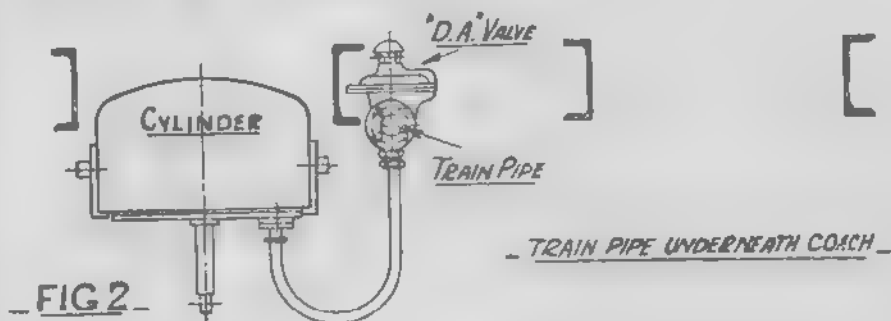
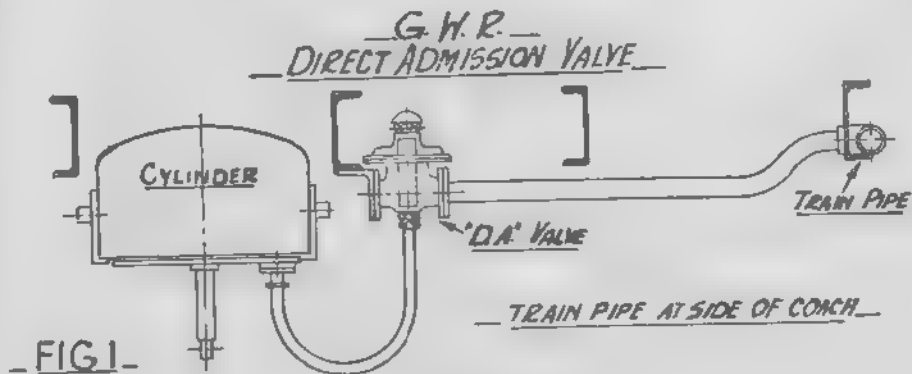
ON REDUCING THE VACUUM IN THE TRAIN PIPE TO APPLY THE BRAKE THE DIAPHRAGM E IS RAISED, THUS OPENING THE VALVE F AND ADMITTING AIR TO THE CYLINDER BY WAY OF THE PASSAGE C.C. AT THE SAME TIME AIR IS ADMITTED PAST THE FLAT D ON THE SPINDLE TO THE CHAMBER B. WHEN SUFFICIENT AIR HAS BEEN ADMITTED TO REDUCE THE VACUUM IN THE CYLINDER TO THE SAME AMOUNT AS IN THE TRAIN PIPE, THE DIAPHRAGM, BRING IN EQUILIBRIUM, IS BROUGHT DOWN BY THE SPRING G AND THE VALVE F IS CLOSED, THUS PREVENTING THE ADMISSION OF MORE AIR TO THE CYLINDER.

Inspection.—EXAMINERS MUST GIVE ATTENTION TO THESE VALVES AND SATISFY THEMSELVES, AS FAR AS POSSIBLE, THAT THEY ARE WORKING PROPERLY. THE BRAKE BLOCKS SHOULD BE FELT, AND, IF THEY ARE COLD, THE COACH SHOULD BE "GREEN CARDED" FOR EXAMINATION OF "D.A." VALVE.

ON ARRIVAL AT THE YARD THE VALVE SHOULD BE TESTED BY ATTACHING AN ENGINE, CREATING A VACUUM AND THEN DESTROYING IT AND SEEING THAT THE BRAKE GOES ON PROPERLY. IF IT DOES NOT DO SO THE VALVE IS PROBABLY DEFECTIVE.

Blanking Off in Case of Failure. IF THE VALVE F SHOULD BE FOUND LEAKING, THE SHIELD H, IF FITTED, SHOULD BE REMOVED AND A CORK INSERTED IN THE $\frac{1}{2}$ IN. HOLE K IN THE CAP L. THE SHIELD CAN THEN BE REPLACED IF THE CORK DOES NOT PROJECT TOO FAR. THE INSERTION OF THE CORK WILL STOP THE LEAK BUT WILL, AT THE SAME TIME, CUT THE CYLINDER OUT OF ACTION.

PASSENGER TRAIN INSTRUCTIONS.
VACUUM BRAKE REGULATIONS



PASSENGER TRAIN INSTRUCTIONS.

VACUUM BRAKE REGULATIONS—*Continued.*

IF TIME WILL ALLOW, THE FLEXIBLE PIPE TO THE CYLINDER SHOULD BE DISCONNECTED, THE VALVE CAGE **M** UNSCREWED AND THE VALVE **A** REMOVED. ON REPLACING THE CAGE AND COUPLING UP THE CYLINDER CONNECTION THE BRAKE WILL ACT AS ON A COACH NOT FITTED WITH A "D.A." VALVE.

IN ALL CASES THE COACH MUST BE "GREEN CARDED" TO DESTINATION, AND THE VALVE **A**, WHEN REMOVED, SENT TO SWINDON.

DEFECTIVE VALVES.—DEFECTIVE VALVES MUST BE SENT TO SWINDON AND REPLACED BY OTHERS.

17. Engines fitted with Four Cone Ejectors. Modern four cone ejectors, often referred to as "Three one" ejectors are arranged with a small extra steam valve on top of the main steam and air valve casting shown at **A** Figures 1 and 2, which operates one cone only, the main steam valve **B** operating the other three. These ejectors can thus be used either as single cone three cone, or four cone ejectors. In the older type of four cone ejector the steam valve **B** operates all four cones and steam valve **A** is not fitted.

The small valve when provided, should always be used in place of the large one to maintain the vacuum while waiting to start or when it is required to assist the pump. By this means less steam will be used and there will be less risk of blowing up an excessive vacuum. The large valve should only be used when it is required to create a vacuum quickly, or to clear the exhaust pipe of water as explained below.

When the ejector has not been used for some time, it may occasionally, when first blowing up, throw a little water out of the chimney. Enginemen should, therefore, when approaching a station at which the train is to stop, open the large steam valve for a few seconds, thus ejecting any water while the engine is still away from the platforms.

When an engine has been standing for some time in a station, the steam valve should be opened very gradually when blowing up a vacuum before moving, so that any water which may fall back into the smokebox instead of being thrown out of the chimney.

Should the needle of the vacuum gauge fall rapidly after the steam valve has been closed, the check valve should be examined. This is done by unscrewing the cap **C** Figures 1 and 4 on top of the valve box, the valve being then accessible.

Drains are provided for the ejector **D** at **E** Figure 1, and drain holes on the exhaust pipe in the smokebox at **F** and **G** Figure 1, and on the cylinder at **G** Figure 2. In the older form of check valve box, which slopes away from the cylinder, the drain hole is a separate hole at **H**. All these drains must be kept clear and the air valves at **D**, **E**, **F**, **G**, and **H** kept in proper working order.

Fig. 3.

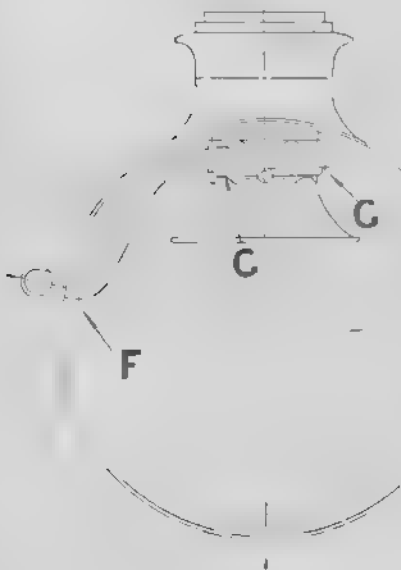
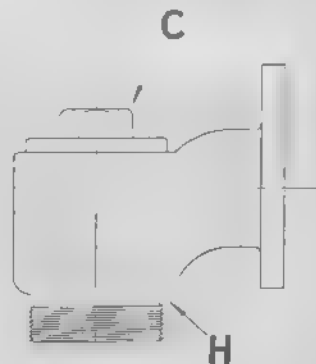


Fig. 4.



PASSENGER TRAIN INSTRUCTIONS
VACUUM BRAKE REGULATIONS—*Continued*

Fig. 2.

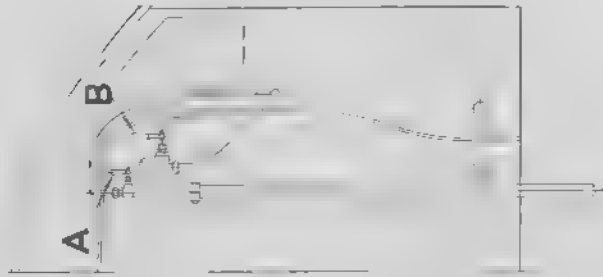
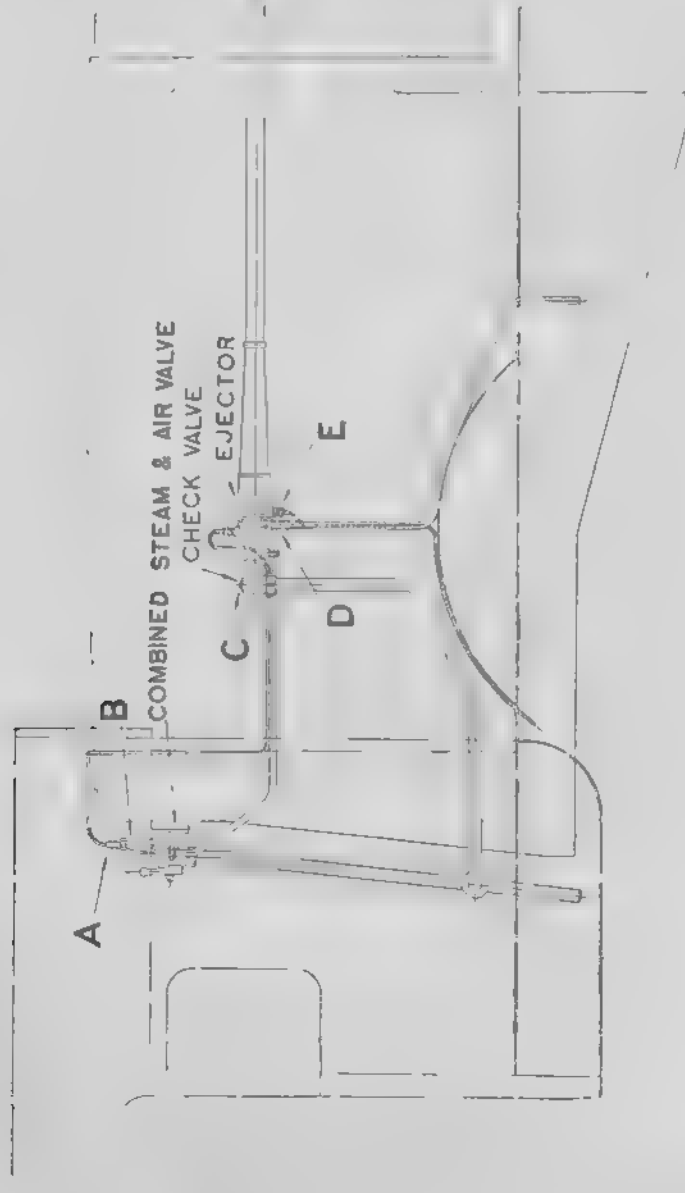


Fig. 1.



PASSENGER TRAIN INSTRUCTIONS.
VACUUM BRAKE REGULATIONS—Continued.

18. Securing of Vacuum Brake Hose Pipe Connections.—THE VACUUM BRAKE HOSE PIPE CONNECTIONS ARE FITTED WITH A CHAIN AND SPLIT PIN. THE PINS MUST BE INSERTED IN THE HOLES OF THE CONNECTIONS WHEN THE PIPES OF ADJOINING VEHICLES ARE CONNECTED, AND WHEN A PIPE IS PLACED ON A STOP PLUG, THE PIN MUST BE USED TO SECURE THE PIPE INTO THE PLUG.

19. General. (a) Vacuum hose pipes when put on must not be left loose, but must be properly secured on the stop plugs. When safety pins are provided together with holes in the stop plugs, the pins must be inserted in the holes.

(b) To couple the hose pipes, they must be taken one in each hand and lifted sufficiently high to hook the bottom horns of the couplings together first and then, when lowered, the top horns of the couplings will fall in the slots.

(c) To uncouple the hose pipes, they must be lifted straight up, when the horns at the top will come out of the slots, and the couplings will separate.

Safety pins, where provided, must be put in the holes of the hose pipes and left in. Hose pipes on goods wagons must be disconnected when a train is to be uncoupled by lifting them with a slanting pole.

(d) When connecting or disconnecting hose pipes, care must be taken that the washers are not damaged, displaced or lost, and that no dirt or waste or any other material that might be drawn into the pipes is taken on.

(e) Employees passing between vehicles must not stop, or interfere with, or damage, hose pipes or couplings.

(f) Any snow or ice with which the hose pipes or couplings may have become coated must be removed before attempting to connect or disconnect hose pipes or stop plugs, in order that the necessary connections may be air tight.

(g) Guards and others connected with the train must see that the hoses placed in brake vans cannot come into contact with the handle of the Brake Setter.

(h) Station Masters, Inspectors, and others concerned, should satisfy themselves by personal observation, whenever possible, that the foregoing Regulations are being properly observed.

NOTE.—The term "Brake Van" includes any vehicle fitted with a brake compartment.

20. Operation of Vacuum Brake on Trains conveying other Companies' Stock.—THE BRAKE CYLINDERS ON OTHER COMPANIES' STOCK MUST BE RELEASED AT THE FIRST SIGNALING STATION, AND, IN CONNECTION THEREWITH, THE VACUUM BRAKE MUST BE RELEASED AT THE FIRST SIGNALING STATION.

CARE MUST BE TAKEN WHEN WORKING TRAINS CONVEYING THESE VEHICLES THAT NOT MORE THAN 2" INCHES OF VACUUM IS CREATED BY THE FIRST ENGINE, AND THAT THE SAME AMOUNT OF VACUUM IS MAINTAINED WHEN RUNNING.

NOT MORE THAN 25 INCHES OF VACUUM BE CREATED WHEN WORKING OTHER COMPANIES' STOCK, THE DRIVER MUST INFORM THE EXAMINER AT THE STATION WHERE THE ENGINE IS DETACHED, AND THE BRAKES MUST BE PROBABLY RELEASED. THE FOREIGN COACHES RELEASED ARE TAKEN ON BY ANOTHER ENGINE. THE MATTER MUST ALSO BE REPORTED AT THE LOCOMOTIVE SIGN, WHEN THE RELEASE-BOX VALVE SHOULD BE CHANGED.

TO AVOID RISK OF THE VEHICLES STANDING WITHOUT ANY BRAKE POWER, THE BRAKES MUST NOT BE RELEASED UNTIL AFTER THE SECOND ENGINE HAS BEEN ATTACHED.

21. Coupling of G.W. and other Companies' Engines. G.W. engines must not be coupled to other Companies' engines for the purpose of coupling to the vacuum created by the respective engines. When however it is necessary to couple a G.W. engine to a vehicle, the driver must be careful not to create more vacuum than is necessary to destroy the vacuum in the engine and train. When the vacuum is destroyed in the G.W. engine, the driver must be careful not to create more vacuum than is necessary to destroy the vacuum in the engine and train. The driver must watch his vacuum gauge and use the release cocks as necessary to prevent the brake dragging.

G.W. light engines may be coupled to other Companies' light engines for short distances, such as between Shed, Station, etc., and in such cases the vacuum pipes must be connected in accordance with Rule 134, Clause 2. The G.W. driver must carefully watch his vacuum gauge and use the release cocks as necessary to prevent the brake dragging.

EXCEPTION. Southern Railway engines, which are fitted with three or four pumps may be coupled to G.W. engines without restriction, but, when the Southern Railway engine is leading, the G.W. Driver must be prepared to assist with his ejector, when required, to obtain 25 inches of vacuum.

22. Vehicles fitted with Westinghouse Automatic Brake attached to Trains fitted with Vacuum Automatic Brake. THE HANDLE OF THE COCK AT THE END OF EACH VEHICLE FITTED WITH THE WESTINGHOUSE BRAKE MUST BE TURNED ACROSS OR AWAY FROM THE PIPE WHICH IS PAINTED BLACK; THE RELEASE CORD OR WIRE UNDER THE VEHICLE MUST THEN BE PULLED TO ALLOW ALL THE AIR TO ESCAPE FROM THE CYLINDER.

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(h) (i) If a vehicle has to be detached and the vacuum pipes cannot be uncoupled, the Guard or other competent person must verbally instruct the Driver to destroy the vacuum or must open the air valve in the nearest brake van.

(ii) When it is necessary to intimate to the Driver that the vacuum requires to be created so that the Guard may comply with the provisions of Regulation 2(b), the Guard, Shunter, or other person in charge, must either so inform the Driver verbally or exhibit a hand signal to him as follows:—

By day	..	Arm moved vertically up and down above shoulder level.
By night	.	Red light moved vertically up and down above shoulder level.

G. W. R.

Divisional Superintendent's Office,
Bristol. G. W. 7.

Ref:- Al/-

2nd June, 1942.

Dear Sir,

FORMATION OF PASSENGER TRAINS.
Guards riding in rear Brake Vehicle.

Referring to the instruction on Page 100 of the General Appendix, Clause 4 regarding "The Formation of Passenger Trains including provision of Guards and handbrakes", there appears to be some misunderstanding in regard to the interpretation of the instruction.

This was framed to cover the attachment of additional vehicles at intermediate points en route, and was so worded to cover cases in which such vehicles did not include a brake.

It is desirable that the Head Guard shall ride in the rear Brake Vehicle on all possible occasions.

Will you please note and instruct your staff accordingly.

Yours truly,

F. R. G. P. G.
J. J.

PASSENGER TRAIN INSTRUCTIONS

FORMATION OF PASSENGER TRAINS, INCLUDING PROVISION OF GUARDS AND HAND

1. The brakes referred to below are:—

- Continuous Automatic brake, viz., either Vacuum or Westinghouse. (See pages 98 and 108).
- Guard's hand brake, viz., the screw brake in the Guard's compartment.
- Outside hand brake on certain vehicles and applied from the ground.

2. The "piped" vehicles referred to are vehicles without continuous automatic brakes, with or without outside hand brakes, but with pipes through which the automatic brake on vehicles each side of them can be operated.

3. The Continuous Automatic Brake pipe connection, between vehicles, including the engine and tender must all be closed up and the Continuous Automatic Brake must be capable of being applied by the Driver and Guard to every vehicle in the train, except as follows:—

(1) A proportion of "piped" vehicles may be formed in the train as shewn in Clause 3 (e) and (f) of Vacuum Brake Instructions.

(2) THE LAST VEHICLE OF EVERY PASSENGER TRAIN MUST BE FITTED WITH THE CONTINUOUS BRAKE COMPLETE, except that, where necessary to avoid delay, one vehicle, not being a passenger-carrying vehicle, may be attached in the rear of a BRANCH OR LOCAL PASSENGER STOPPING TRAIN without being fitted with the continuous brake or pipe, PROVIDED SUCH VEHICLE IS ATTACHED BY MEANS OF THE SCREW COUPLING OF THE LAST PASSENGER TRAIN VEHICLE, BUT IN THIS RESPECT AN EMERGENCY SCREW COUPLING SHOULD BE USED WHEREVER AVAILABLE. GROOMS OR ATTENDANTS TRAVELLING IN HORSE BOXES OR OTHER SUCH VEHICLES NOT ORDINARILY USED FOR THE CONVEYANCE OF PASSENGERS ARE NOT, FOR THE PURPOSE OF THIS INSTRUCTION, REGARDED AS PASSENGERS.

4. Unless special instructions are given to the contrary by the Superintendent of the Line, the maximum number of wheels allowed behind the brake in which the rear Guard rides to be as follows:—

Ruling Gradient.	Number of wheels of vehicles of all descriptions.	Number of wheels of vehicles containing passengers.*
Rising not steeper than 1 in 100 or falling or level	40	40
Rising steeper than 1 in 100, but not steeper than 1 in 40	40	24
Rising steeper than 1 in 40	16	1 vehicle.

* Provided the vehicles containing passengers are in all cases fitted with Continuous Brake complete.

WHERE PRACTICABLE, THE VEHICLES CONTAINING PASSENGERS SHOULD BE FORMED IMMEDIATELY BEHIND THE VAN.

Note. When the train is travelling on a gradient steeper than 1 in 200 the Guard or Guards must be prepared to apply instantly the hand brake.

5. (a) Passenger and Empty Coaching Stock Trains should be worked by one Guard only, except when the work on the train or at stations or other exceptional circumstances, necessitates the employment of an additional man or men. When two Guards are employed the assistant Guard must ride in the front of the train, and the Guard in charge of the train at the rear.

(b) Loaded passenger trains must not exceed 136 wheels.

The total of empty passenger stock train must not exceed the equivalent of 20 eight-wheel coaches.

Parcels, Perishable, Fish and Milk trains must not exceed a gross weight of 550 tons (excluding engine or engines but inclusive of the brake van), whether assisted or unassisted.

(c) Loaded passenger trains having more than 96 wheels (G.A. 8.—5/41. T.10.M.)

NOTE.—Either one or two horse boxes, carriage trucks, or such like vehicles which do not carry the public, may be attached to a train of 96 wheels without a second brake being attached.

page 109.

Delete paragraph (c) of clause 5 and the additional sentence shewn in Circular G A 6 and substitute the following:—

a. A brake van or vehicle with brake compartment leading should, as far as practicable be marshalled next the engine of all passenger trains, except where the former on the line is specified in the Carriage Working Instructions or delay will be caused at stations by the brake van or vehicle with brake compartment trailing should, as far as practicable be marshalled at the rear of passenger trains.

When passenger trains are attached en route to either the front or rear of the train, they should be marshalled with the brake van where this can be done without causing delay to the working. (G A 27 Op— 51 L.K. 9823 Gen E)

PASSENGER TRAIN INSTRUCTIONS.

PROVISION OF GUARDS AND HAND BRAKES—Continued.

Persons in charge of stations to see that trains have sufficient brake power.

Guards to see that hand-brakes are in good order.

Certain vehicles not to be coupled out of main coaches.

Coupling of Passenger vehicles.

6. Persons responsible for starting trains from stations where the journey is commenced, and the Guards, must see that brake power is provided in accordance with the instructions, and such persons are similarly responsible at stations where vehicles are added.

7. Guards must see that hand brakes are in working order and secured in the "off" position before starting.

8. Vehicles containing passengers, or stock 9' 3" to 9' 7" wide over body and brake vans with guards' projections, must not be formed in front of Mail vehicles which have to use the packing up and setting down Mail apparatus on the journey.

9. (a) To obtain good and steady running it is most important that a train should move as one unit, and there should be no slackness between any of the coaches.

(b) In forming trains it is necessary for the vehicles to be brought together until the buffers just touch, two intermediate turns being then given to the coupling, which results in the springs being compressed about half an inch. If in fact two turns are given, the friction between the buffers is too great and prevents the comfortable sliding motion which steadies the vehicles with it jar. When coupling on a curve the screw coupling should be so adjusted as to bring about the above mentioned conditions as nearly as possible.

(c) Guards, Train Ticket Collectors, Restaurant Car Conductors and Sleeping Car Attendants must promptly report all cases of rough riding and excessive oscillation and give particulars of the vehicle and the locality. Any slackness between coaches must also be reported and steps taken at the first opportunity to have the couplings tightened and attended to. The supervisory staff concerned must also give particular attention to this matter, and take steps to have the instructions rigidly carried out.

10. Train Ticket Collectors or other men assisting on Passenger trains worked by one Guard, must, in addition to their special duties, assist the Guard of the train as far as possible, both on the journey and while at stations. Men so working, with the exception of Travelling Post Office Porters, must carry with them a set of flags, not less than 12 detonators, and a trimmed hand lamp.

On a Train
Passenger
Trains

(d) When coupling an engine to a passenger train the engine coupling should be used unless a slip coach is formed as the leading vehicle in the train.

In the event of failure of the engine coupling, the coupling of the leading vehicle must be used.

(G.A. 5.—2/39. T.28419.G.(2.)

Four wheeled vehicles, either coaching, or brake-fitted freight stock, requiring to be run in Passenger trains, must comply with the following requirements:

(i) Vehicles to be fitted with:

Oil axle boxes, automatic brakes or through pipes, screw couplings and long buffers.

(ii) Minimum tare to be 6 tons.

(iii) Vehicles with a wheelbase of less than 9 ft. to be restricted from running in express Passenger trains.

(iv) All four-wheeled vehicles to be marshalled at the back of the train in rear of any bogie vehicles carrying passengers, and if they are unsuited for traffic working they may be placed next to the engine or in rear of six-wheeled Non-Passenger carrying vehicles provided such vehicles are attached in front of bogie vehicles, but vehicles not fitted with through steam pipes, having to be conveyed in trains, and if not heated or if heated at rear of steam heated vehicles. Four-wheeled vehicles must not be conveyed between bogie vehicles except in the following circumstances, when they may, if absolutely necessary, be marshalled in the positions indicated:

(a) Between bogie fish or other vehicles carrying vehicles when such are marshalled behind the last vehicle conveying passengers in the train.

(b) Between empty bogie passenger coaches and the rear brake.

(c) Vehicles containing Theatrical, Naval, Military or Air Force Traffic may be marshalled according to destination when formed in trains composed entirely of such traffic.

Sub-section (iv) does not apply to four-wheeled vehicles having a wheelbase of 14 ft. or over, the position of which on Passenger trains need not be restricted if the vehicles comply with Sub-sections (i) and (ii).

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GREAT WESTERN RAILWAY.

Alterations and Additions to the General Appendix to the Rule Book

To come into operation forthwith.

REGULATIONS GOVERNING THE RUNNING OF FOUR-WHEELED VEHICLES IN PASSENGER TRAINS—Page 110.

The **NOTE** at the end of Clause 1 (c) of these Instructions to be amended to read

NOTE. All four wheeled non passenger carrying coaching stock and braked freight stock of 10 feet and under wheelbase suitable for running in passenger trains carrying "A" headings will be marked "XP" on the right hand corner of each vehicle or the right hand corner of the solebar. The wheelbase will be shewn underneath the letters "XP".

Any passenger train and any other train composed of coaching stock (i.e. empty stock train, parcels train, newspaper train, milk train, horsebox train or pigeon train or any fish, meat, fruit or perishable train if not signalled as fitted freight train) conveying one or more four wheeled vehicles of less than 11 feet wheelbase **must not exceed 60 m.p.h. at any point.**

In every case when four-wheeled vehicles of less than 10 feet wheelbase are marshalled in the train the Guard must advise the Driver before starting so that the speed of 60 m.p.h. may not be exceeded at any point whilst such vehicle is, or vehicles are attached to the train.

The load of any four wheeled vehicle marked "XP" and with less than 11 feet wheelbase must be restricted to 11 tons if it is to be conveyed on any passenger train and any other train composed of coaching stock (i.e. empty stock train, parcels train, newspaper train, milk train, horsebox train or pigeon train or any fish, meat, fruit or perishable train if not signalled as a fitted freight train). This loading restriction will apply to such vehicles loaded on this Company's system only.

(G.A.17. 10 46.LK1 /7217/10.)

JAMES MILNE,
General Manager.

October 1946 5442C—45,000

Each member of the Staff receiving a copy of this Circular is required to read carefully and note the contents, and, if supplied with a copy of the General Appendix, to alter, or cancel in ink the present instructions on the subject appearing therein, afterwards pasting the amendments in their proper places in the General Appendix.

Station and Depot Masters are responsible for seeing that copies of the General Appendix supplied to Signal Boxes, etc., under their supervision, are corrected in accordance with this Circular.

PRIVATE AND NOT
FOR PUBLICATION

District Operating Superintendent's Office,
BRISTOL (D.O.) W.R.7
27th May 1953

FRIDAY, 29th MAY

5-55pm PADBINGTON TO C. E. H. A. 11.11.53
PRINTED NOTICE NO. 950.

The FIRST PART will now run t

	arr. a.m.	dep. a.m.		arr. a.m.	dep. a.m.
Bedwyn		1/45	Clink Road Jen.	2/21	
Patney & C.		2/1	Blatchbridge Jen.	2/24	
Heywood Rd. Jen.		2/14	Castle Cary	2/39	
Fairwood Jen.		2/17			

7-50am TAUNTON TO PADB.
PAGE 46. PRINTED N°

trains at Bristol (D.O.)

Load 13 from Swindon.

PAGE 47 PRINTED NOTICE NO. 950.
Will run to Manchester (D.O.)
to be labelled "Manchester".

10-30am PADBINGTON TO C. E. H. A. 11.11.53
PRINTED NOTICE NO. 950.

A Restaurant Car will be conveyed on the first and second parts.

PAGE 49 PRINTED NOTICE NO. 950.
The FIRST PART will also call at Teignmouth and Kingskerswell.
Will carry TRAIN NO. 761.
The SECOND PART will carry TRAIN NO. 760.

(continued)

NOTICE NO. 928

- 2 -

27/5/3

WEDNESDAY, 30th MAY

3-45pm (P.P.E.S.) FENJANCE TO P

Will run and be signalled as a passenger train ('B' Headcode)
from Taunton to Reading.

4-25pm (P.P.E.S.)

Old Oak Common.

USING -
ON EXPRESS

vehicle had then to be formed inside the four-wheel vans.

In the event of a breakdown, the
vehicles must be attached to the front of passenger trains.

RECEIPT OF THIS

WIRE IMMEDIATELY TO HSTROP

'ARNO 928'.

ARRANGE AND ADVISE ALL CONCERNED

THIS NOTICE TO BE RETAINED FOR REFERENCE PURPOSES.

FOR L. EDWARDS

up

PRIVATE AND NOT
FOR PUBLICATION

BRITISH RAILWAYS (WESTERN REGION)

CIRCULAR NO.

District Operating Superintendent's Office,
BRISTOL (TEMPLE MEADS)

23th January 1951.

GX.3718.

MISC
S.R. STOVE VANS

Any S.R. Stove Vans in the series 370 to 410 must be withdrawn from service via the nearest junction. Control R.S. B. must be advised the details of despatch in all cases and also the names of vehicles forwarded home.

still not being received and immediate action must be taken to ensure

INSULATED FISH VANS

August 29th 1950,
Chops. all Western
express passenger trains which
exceed 50 m.p.h. in run

arrangements.

A1/72051.

BRISTOL (T.M.) TELEPHONE EXCHANGE

Following extension has been connected to the
Bristol Telephone Exchange.

Extension 515 - District Office

A4/71014

3 and March 31st 1951 are
required to be on Saturday, February 3rd 1951.
Any van despatched to stations and
employed and any Station or Yard
required to receive it.
who has not received this letter, should
contact the District Office.

REPLY TO BE MADE BY PASS NO. 28359. AVAILABLE IN THE
AND FILTON JUNCTION. ISSUE IN FAVOUR
OF MR. C.J. TREMLETT.

AF NO.

29/1/51

SCHOOL UNIVERSITY

the minimum titular

20 years and awarded triennially.

It shall be open to persons employed by the
not less than 18 and not more than 23 years

of age and who have been so employed for not less than twelve months, the examination is held,

be awarded upon the results of a
nomination to be held by the University.
It shall be paid by the examiners to the

nomination

(1) Pure Mathematics.

(2) Heat Engines.

enics.

(3) Mechanics and Electricity)

or Chemist.

4. The scholar shall be eligible for the degree of
Bachelor of Engineering or for the Certificate in Engineering.

Examiners must

Registrar
Staff

to the notice of all concerned and to the Registrar. These details
of any
application forms
will be supplied.

SR/60601/2

IN FAVOUR
OF BOWMAN
- BRIDPORT.

The above periodical free ticket has been lost and I shall be
glad if all concerned will please take the necessary steps to prevent
misuse.

All Station Masters on the "C" Group Stores Distribution list
are reminded that the requisition books are due in this office on
Thursday next, February 1st 1951.
Please arrange accordingly.

ARRANGE AND ADVISE ALL CONCERNED.

FOR L. EDWARDS.

1/18/51

REGULATIONS GOVERNING THE RUNNING OF FOUR-WHEELED VEHICLES IN PASSENGER TRAINS—page 110.

These the whole of the instructions under this heading and substitute the following:—
CONVEYANCE OF FOUR-WHEELED NON-PASSENGER-CARRYING COACHING STOCK AND BRAKED FREIGHT STOCK IN PASSENGER TRAINS.

1 Four-wheeled vehicles, either non-passenger-carrying Coaching Stock or Braked Freight Stock, running in passenger trains must comply with the following requirements:—

- (a) Oil axle boxes.
- (b) Automatic brake or through pipes.
- (c) Screw couplings and long buffers.
- (d) A minimum tare weight of 6 tons. (The minimum tare weight of 6 tons does not apply to Container wagons when such wagons are carrying containers either loaded or empty and the total load, i.e. tare weight of wagon plus weight of container, is 6 tons or over.)

Four-wheeled vehicles conforming to the above requirements and having a wheelbase of 10 feet or over are marked "X.P." together with the wheelbase.

The term "non-passenger-carrying Coaching Stock" refers to stock not constructed for conveying passengers but includes Horse Boxes and Cattle Boxes.

NOTE.—This requirement does not modify the authority (where given) to attach at the extreme end of a vehicle not conveying passengers and not fitted with the continuous brake or through pipe the instructions headed **Formation of Passenger Trains, Cause 3 Sub-section (2)**.

When a four-wheeled vehicle is attached to a passenger train next to a bogie vehicle the screw coupling of the four-wheeled vehicle must be used and must be screwed up tightly. It must not be used for attaching a vehicle to a passenger train and the emergency screw coupling or screw coupling of the adjacent vehicle must be used.

Four-wheeled vehicles marked "X.P." and having a wheelbase of 5 feet or over may be attached to passenger trains without restriction, unless otherwise specially prohibited.

2. (a) Passenger trains conveying vehicles with a wheelbase of less than 15 feet must not exceed a speed of 60 m.p.h. at any point. In any case where four-wheeled vehicles of less than 15 feet wheelbase are marshalled at the Guard's office the Driver before starting so that the speed of 60 m.p.h. may not be exceeded at any point when such vehicles or vehicles are attached to the trains.

(b) Four-wheeled vehicles with a wheelbase of less than 10 feet must not be conveyed on Express Passenger Trains.

3 Four-wheeled vehicles with a wheelbase of under 5 ft. should, as a general rule, be marshalled at the back of passenger trains and at the rear of the train. When this is impracticable in the interests of traffic working, they may be marshalled as shown below:

(i) Next engine.

(ii) Between bogie non-passenger carrying vehicles or bogie passenger coaches not conveying passengers.

(iii) According to destination when conveying Theatrical, Naval, Military or Air Force traffic when formed in trains composed entirely of such traffic.

4 Four-wheeled vehicles may be conveyed next to the engine during the period when steam heating is in operation, unless the vehicle or vehicles are fitted with steam pipes.

5 Four and six wheeled vehicles may be conveyed provided they are marshalled at the front of the train behind vehicles of the following description:—A 20 Op 550, R 1 Op Cur, Min 228) underneath the letters "X.P."

(vi) Except in the special circumstances enumerated in sub-section (vii) below all four-wheeled vehicles of less than 15 feet wheelbase (including all with a wheelbase of 9 ft. and under 10 ft. wheelbase), conveyed by passenger trains other than those carrying "A" head end traffic, must be marshalled in the rear of any bogie vehicle at the rear of the train. In the case of traffic working they must be placed next to the engine, or mixed with bogie vehicles, or carried in passenger trains provided all such vehicles are attached in front of bogie vehicles, but vehicles fitted with through steam pipes, having to be conveyed in trains which are steam heated, must be formed at the rear of steam heated vehicles.

(vii) Four-wheeled vehicles with a wheelbase of less than 10 ft. must not be conveyed between bogie stock except in the following circumstances, when they may, if absolutely necessary, be marshalled in the position indicated but in all cases when such vehicles are marshalled between bogie stock the Driver of the train must be informed by the Guard and the speed of the train must be reduced to a maximum of 40 m.p.h. at all points:—

(a) Between bogie stock or other non-passenger carrying vehicles when such are marshalled behind the last vehicle conveying passengers in the train.

(b) Between empty passenger vehicles and the rear brake.

(c) Vehicles conveying Theatrical, Naval, Military, Air Force or Horse Traffic may be marshalled according to destination.

WHEN FORMED IN TRAINS COMPOSED ENTIRELY OF SUCH TRAFFIC, four-wheeled vehicles having a wheelbase of 15 ft. or over, may be marshalled without restriction provided the train complies with the conditions (i) and (ii).

Four-wheeled non-passenger carrying Coaching Stock, other than Rail Motor Cars and four-wheeled under-frames (e.g. Motor Cars, Motor Trucks, and Braked Freight Stock) conveyed at the rear of passenger trains must be short buffered provided the vehicles otherwise conform with Cause 3 sub-sections (1), (a) and (iii).

(d) If the first vehicle (not fitted with screw couplings) is attached to the train by means of the screw coupling of the last Passenger train vehicle, or by an emergency screw coupling. The latter should be used whenever possible.

(e) When more than one four-wheeled vehicle is attached at the rear of a Passenger train, the second vehicle and subsequent vehicles if not fitted with a screw coupling must be attached by means of emergency screw couplings.

The provision of Cause 3 sub-section (2) of the instructions in regard to the Formation of Passenger Trains must be observed.

Except where instructions exist to the contrary, knuckle couplings must not be used for attaching vehicles to the rear of a Passenger train. Where necessary screw couplings must be used.

A.F.V. TRAINS PROTECTIVE ARRANGEMENTS IN THE EVENT OF A BREAKAWAY.

Whenever a loaded A.F.V. train is brought to a stand where the train stops for where it is important the train should not move back and it is necessary for the guard to leave his van, he must, before doing so, apply his hand brake.

If for any reason, the engine is detached from the train, or the train is divided, either by intention or accidentally, on gradients of 1 in 10 or steeper the guard must take immediate steps to apply the hand brakes on all wagons, warratts or other brake vehicles, working forward from the brake vehicle in which he was working.

(GA.15—12/44 L.K.1/7429/6A)

GRAND
STATION
BRISTOL

Divisional Superintendent's Office,

Temple Meads Station,

BRISTOL

P2/

4th November, 1938

Dear Sir,

Regulations Governing the running of
Four-wheeled vehicles in Passenger
trains.

With reference to the instructions contained in
Clause (iv) of Circular G.L.4 dated 1st July 1938, vehicles
with a wheel base of 10 feet and under 12 feet may be
employed next on line of the following services between Bristol
Temple Meads and Newport as under:-

5- 0pm Penzance to Crewe
Vehicles for the North of England.

10-15 . Bristol to Crewe (Sundays)
Vehicles for the North of England.

5-50pm Bristol to Crewe (Sundays)
Vehicles for South Wales.

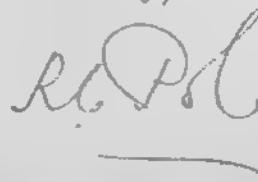
11-50pm Bristol to Crewe (Sundays)
Vehicles for the North of England.

Authority has also been issued by the Divisional
Superintendent at Newport for such vehicles to be formed next
engine between Newport and Bristol (Temple Meads) on the
under-mentioned trains:-

8-17pm Crewe to Bristol (EM)
1-25am Crewe to Bristol (EM) (M.O.)
11-55am Crewe to Bristol (EM) (Sundays)
7- 3pm Hereford to Bristol (EM) (Sundays).

These instructions will not apply to vehicles
which are not fitted with through steam pipes. These
vehicles must be formed at the rear of steam-heated vehicles
as shewn in the printed Circular.

Yours truly,

.....

.....

PASSENGER TRAIN INSTRUCTIONS.

RUNNING OF FOUR-WHEELED VEHICLES IN PASSENGER TRAINS—*Continued.***CONVEYANCE OF FOUR-WHEELED NON-PASSENGER-CARRYING COACHING STOCK AND BRAKED FREIGHT STOCK IN EMPTY COACHING STOCK AND PARCELS, ETC., TRAINS.**

Any train (other than Passenger) composed of coaching stock, i.e. empty coaching stock train, parcels train, newspaper train, milk train, horse box train, pigeon train, fish, meat, fruit or perishable train conveying one or more four-wheeled vehicles of less than 15 feet wheelbase must not exceed 60 m.p.h. at any point. In every case where four-wheeled vehicles of less than 15 feet wheelbase are marshalled on the train, the guard must advise the driver before starting, so that the speed of 60 m.p.h. may not be exceeded at any point while such vehicle is, or vehicles are, attached to the train. (G.A. 26 Op. 5.50 R.F. Op. Com. Min. 228.)

The provisions of Clause 1, sub-section (2) of the District Regulations regarding the formation of Passenger Trains, etc., must be observed.

Except where instructions exist to the contrary, instant couplings must not be used for attaching vehicles to the rear of a Passenger train, but emergency screw couplings must be used.

Reference to the following to be made on page 111 :—

CONVEYANCE OF RAIL MILK TANKS.

"Six-wheeled milk tanks, loaded or empty, may be formed in any position in express passenger trains, except between coaches conveying passengers."

(G.A. 24—11.49. L.K.I. 8533 Gen. 4.)

The couplings to be screwed up tightly in accordance with the instruction in Clause 9 (b), page 110.

Milk tanks must not be formed in Freight trains except by the special authority of the Operating Superintendent. (G.A. 23—7.49. L.K.I. 8533 Gen. 4.)

Flat Container Wagons.

The Regulations governing the running of four-wheeled vehicles in Passenger trains will not apply to Flat Container Wagons numbered 1, 20000, etc., so far as the weight of 6 tons is concerned. These wagons, although also less than 6 tons tare, considerably exceed that weight with the container and may be conveyed by Passenger trains when loaded with a container which is either empty or loaded.

Vacuum Fitted Goods stock on Passenger Trains.—Cattle wagons having a wheelbase of 11' 3", 11' 3", or 11' 6", and fitted with the vacuum brake and screw couplings, may be run in Passenger trains on which they are authorised to be conveyed in accordance with Clause 1 of the Regulations governing the running of four-wheeled vehicles in Passenger trains, except that when such cattle wagons have to be conveyed on trains which are steam heated they must be formed at the rear of the steam-heated vehicles.

Any vacuum fitted grease box goods vehicles which may be required to work by Passenger train must be first specially examined and passed by the Locomotive Department examiners and attached to trains which do not run more than 10 miles without stopping.

DETACHING OF ENGINES FROM, AND DIVISION OF, PASSENGER TRAINS ON GRADIENTS.

In the event of a passenger train coming to a stand on a gradient which is steeper than 1 in 100, owing to the inability of the engine to work the train, the engine may be detached or the train may be divided and the first portion taken forward, provided that, after the continuous brake has been applied, the hand brakes on all brake vans are screwed hard on and the weight of the train left standing does not exceed that shewn in the following table :—

GRADIENT.	LOAD (INCLUDING WEIGHT OF BRAKE VAN OR VANS).
1 in 30	Twice weight of brake van or vans.
1 in 40	Three times weight of brake van or vans.
1 in 50	Four " " " " "
1 in 60	Five " " " " "
1 in 70	Six " " " " "
1 in 80	Seven " " " " "
1 in 90	Eight " " " " "
1 in 100	Nine " " " " "

NOTE.—The term "Brake Van" includes any vehicle provided with a brake compartment.

PASSENGER TRAIN INSTRUCTIONS.

COMPUTATION OF LOADS OF PASSENGER, PARCELS, MIXED AND FISH TRAINS.

The loads of all Passenger, Parcels, and Fish trains are calculated in tons.

To enable Guards and others to calculate the loads in tons, the tare weight of the vehicle, in 2½ inch metal figures, has been painted on the side of all passenger, parcels, and fish trains formed in trains coming within the category of those mentioned above, and the method of computing the loads is as follows:—

- PASSENGER TRAINS .. By adding together the tare weights, irrespective of whether the vehicles, passenger or otherwise, are loaded or empty.
 PARCELS TRAINS .. By adding together the tare weights of the vehicles plus one ton for each loaded vehicle.
 FISH TRAINS .. By adding together the tare weights of the vehicles plus three tons for each loaded vehicle.
 MIXED TRAINS .. Where trains are authorised to be run as "Mixed" trains, the total

weight of the train is to be obtained as follows:—

Tonnage of Passenger stock.

Tare weight of Goods Brake Van (where provided).

Tonnage of freight vehicles to be calculated as under:—

Class 1 traffic = 16 tons per wagon.

" 2 " = 13 " "

" 3 " = 10 " "

Empties (4-wheel stock) = 6 tons per wagon.

Standard allowances to be made for vehicles exceeding 10 tons capacity.

In the case of another Company's vehicle not marked with the tare weight, being formed in a train, the weight of the vehicle must be ascertained by the Guard or other small vehicle; twenty tons for a four or six-wheeled passenger-carrying vehicle or brake van; thirty tons for a four-wheeled passenger-carrying vehicle or goods wagon, and forty tons for a dining car, sleeping car, or 70 feet passenger-carrying vehicle or brake van.

The loads of the trains are to be ascertained by the Guard or other small vehicle at the start of the train, and the weight of the vehicles to be added to the tonnage of the train.

Guards must be aware of the weight of the vehicles to be added to the tonnage of the train at starting points and of the weight of the vehicles to be added to the tonnage of the train at stations when working passenger trains must give the weight and number of vehicles for each portion, viz

" 1.30 p.m. Paddington, 141 tons, 4 vehicles Penzance, 72 tons, 2 vehicles Kingswear, etc.

THE TONNAGE LOADS OF PASSENGER, PARCELS AND FISH TRAINS FOR ENGINE WORKING PURPOSES ARE SHOWN IN THE SERVICE TIME TABLES.

THE LOADS REPRESENT THE CAPACITY OF THE ENGINE IF THE STANDARD POINT TO POINT TIMING IS TO BE MAINTAINED. ON SECTIONS WHERE THE GRADIENTS WILL PERMIT THE LOADS MAY BE EXCEEDED WITH A SUITABLE INCREASE IN THE POINT TO POINT TIMING, BUT ON SECTIONS WHERE THERE ARE STEEP RISING GRADIENTS IT WILL BE NECESSARY TO PROVIDE AN ASSISTANT ENGINE.

NOTE: The instructions in this Appendix are to be read in conjunction with the Appendix respecting the formation of passenger and mixed trains.

PASSENGER TRAIN VEHICLES BRANDED "NOT TO RUN IN THROUGH FAST TRAINS."

A number of passenger train vehicles, principally stock for use by the Atterdon Companies, is still in use, and is being put on the side of the line. Not to run in through fast trains." The vehicles are constructed with wooden under-frames, and it is undesirable they should be formed with heavy metal frame vehicles and subjected to severe stresses on stopping.

These vehicles must not be allowed to work on trains which run more than 30 miles without stopping, nor on any passenger train, and it is recommended that they should be confined to local services only, and should not be used on any other service. In the event of any of the vehicles referred to getting out of order, they should be sent to the Rolling Stock Department, Office of Superintendent of the Line, in order that they may be traced and restored to their regular service.

CARE OF PASSENGERS' LUGGAGE IN COMPARTMENTS.

Care must be exercised in dealing with luggage conveyed in compartments with passengers.

Guards, Train Porters, and Collectors must exercise all possible vigilance over the property of passengers and are especially liable to search the compartments. Very careful search of compartments at stations where coach working terminates must be made.

Luggage should not be placed in compartments except in the presence of the owner, and if it is known that passengers purpose leaving the compartment after the train is fully started it should be parted out of the compartment and not left in any room or box in the event of the owner's absence.

When it is known that passengers intend travelling in a Restaurant Car for a considerable part of the journey it should be suggested that suit cases be labelled and placed in the luggage van.

1. **Distribution of Working Notices.** Station Masters or persons in charge, must personally distribute copies of the Special Force Notices to the staff in their Signaling, and every person supplied with a copy is held responsible for reading it and for conveying the general information in it to his staff as far as it affects their own department. No excuse of want of knowledge of Special Arrangements can be admitted for any failure or neglect of duty.

The Station Master, District Inspector, or other person employed in the Station must in all cases, take the men's receipts at the time the Notices are handed to them.

It is important that the work be done in a safe manner and that the power supply be adequate for the necessary lighting.

3. **Brake power.** Station Masters (must not be any other person) started with the duty of starting engines or, if the engine is not started, they are expected to start the train made up at, or starting from, the station. A record will be kept of the brake power required on the gradients over which the train traveled. For list of gradients, see Appendix 1. See Table Tables.

5. **Relief trains.** Relief trains, when not at prescribed times, must not leave any station more than ten minutes before they are scheduled to, unless a new schedule and routing book is for connecting trains; but if the ordinary trains which the Special are supposed to follow be late, the Special trains may be kept back, where necessary, to RUN AT THE ADVERTISED TIMES.

6. **Equipment of trains.** All vehicles must be properly loaded and lavatory compartments equipped before leaving to start a station, and the Guard must examine the water, gas indicators and electric lighting before starting, en route, and before commencing the return journey.

7. **Luggage.** Care must be taken in labeling and stowing luggage at the various stations, and especially with through trains which are divided. A person concerned must be very careful to see that passengers and luggage are loaded in the proper portions of the divided trains.

If, for any special reason, baggage or other traffic is accidentally loaded, or in the wrong part of the train, a sufficiently early advice as to the exact position in which the traffic is stowed must be sent to the station ahead, where the traffic has to be unloaded or transferred.

PASSENGER TRAIN INSTRUCTIONS.

RUNNING OF SPECIAL PASSENGER, EXCURSION, ETC., TRAINS -Continued.

Luggage must be sorted on barrows on the platform before the arrival of the trains and placed in charge of Porters opposite the van into which it has to be loaded.

Where special kinds of labels are provided for luggage to be conveyed by Excursion trains, such labels must be used.

8. **Telegraphing trains.**—Particular attention must be paid to telegraphing the trains forward to the appointed stations; and the departure from a station of every train, the running of which is telegraphed forward, must be immediately reported to the Telegraph Office at that station. See Telegraph Appendix and train telegraphing regulations.

The stations starting an additional Special must in all cases immediately advise the principal stations ahead by wire, in addition to the usual Box-to-Box message.

9. **Guards.**—Guards working special trains must assist at the terminal station on the forward journey as required, and on no account leave the station without first obtaining permission from the Station Master or person in charge, who must give the Guards clear instructions as to the time they are to return to duty. They must also assist the Ticket Collectors by opening and closing the carriage doors, and when specially called upon to do so by the person in charge, assist in the collection and examination of tickets.

The Station Masters or persons in charge of the stations where trains are started from, will be held responsible for providing and supplying the necessary Guards for the trains. The Station Master must provide the trains starting from their stations at least three days before the Guards are required, and must make special application to the District Superintendent of the District in which the train is required beyond those who can be furnished out of their own staff.

[illegible][illegible]

Guards must see that windows of unoccupied compartments are kept closed.

10. Severn Tunnel. All the way, the guide up an escalator with the special standard instructions for working through the Severn Tunnel.

that set A is not the intersection of a β -ray of N with M and $A \cap N$ contains the tunnel T which is a β -ray of N through the Tunnel.

No passenger train must be allowed to run through the Tunnel without lighted roof and tail lamps.
See Severn Tunnel Instructions

11. **Up trains to Paddington** The station Master must be advised the nature of vehicles and weight to be carried on the up train by the station clerk, and, if a special train, also by station clerk, if a special vehicle is used, when the load of such train exceeds 16 8-wheel vehicles.

12 **Bank Holiday traffic.** Station Masters at the principal stations must report to the Superintendent, in the form of the General Appendix, and also whether the traffic has been more or less heavy than usual and whether any particularly noticeable increase has been experienced from want of business on other causes.

LADIES' COMPARTMENTS.

One or more compartments of each class, as required, must be provided for "ladies only" on semi-fast trains, other than business services, when are fitted with non-corridor stock, also on the express night trains.

The compartments must be labelled before the commencement of the journey, and Guards and Ticket Collectors should be instructed that they must inform lady passengers who are unaccompanied that there are Ladies' compartments in the train.

Intermediate doors between First and Third Class Compartments in corridor composite vehicles *with end doors only* must be left unlocked in all cases.
(G.A. 10. 3 42 LK 1 '6625/5)

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PASSENGER TRAIN INSTRUCTIONS.

PASSENGER TRAINS NOT TO STOP ON BRIDGES.

Wherever the Railway passes over a bridge or viaduct near a stop signal, and the bridge or viaduct is provided with a high parapet, no passenger train shall be brought to a stand on the bridge or viaduct, but should, if possible, stop clear of it.

FASTENING CARRIAGE DOORS.

Station Masters or persons in charge of stations or platforms must see that all doors of vehicles on passenger trains are closed and fastened.

It is the duty of the staff to see that the doors of all vehicles are properly fastened and that the doors of compartments are closed and fastened. If compartments are then empty, the windows should also be closed and during the months when the heating pipes are connected up the handle of the regulator in the compartment should be placed to the "On" position.

At all stations guards must co-operate in closing and fastening the doors of vehicles on their trains, and must see that all doors of passenger vehicles, carriage trucks or motor vehicles which are formed in their trains or attached on the journey are secured.

"SLAM" LOCKS ON G.W. PASSENGER COACHES.

The doors of passenger coaches fitted with slam locks will automatically catch when pushed to the upright position. The door will not turn to the horizontal position, but remain at an angle of about 45 degrees.

The handles of doors fitted with slam locks are of a different pattern from the standard handle, and the staff must be careful to see that the doors are properly closed and completely fastened.

Where trains are made up of vehicles fitted with both types of locks, care must be taken to see that the doors are properly closed and fastened, and that the doors are turned to the horizontal position.

CLOSING WINDOWS IN BRAKE AND LUGGAGE COMPARTMENTS OF COACHES IN WHICH GUARDS ARE NOT RIDING.

To prevent fire from engine spark, the windows of brake or luggage compartments, in which guards are not riding, must be closed at the starting point and kept closed throughout the journey.

INSTRUCTIONS TO GUARDS AND TICKET COLLECTORS ON CORRIDOR TRAINS.

INSTRUCTIONS TO GUARDS AND TICKET COLLECTORS ON CORRIDOR TRAINS.—Page 115.

The existing paragraph 1 of Clause 1 to be deleted and the following substituted:—

Corridor and gangway doors should be left unlocked so as to provide free access through the train, except as shown below —

- (1) Gangway doors at the extreme ends of the trains. Care must be taken to ensure that when vehicles are detached from a train en route that the gangway doors at the point of detachment are locked.
- (2) Brake Vans or Luggage Vans at the extreme ends of the train. Where, however, a guard is riding in the van or it is empty, the doors should be unlocked. Should the guard have occasion to leave his van whilst the train is in motion he must lock the door.
- (3) Brake Vans or Luggage Vans intermediate in the train. Where, however, there is a restaurant or buffet car on the train or a guard is riding in the van or the latter is empty, the doors should be unlocked.
- (4) Where the gangway connections cannot be made.
- (5) The gangway doors at both ends of sleeping car accommodation on trains. Where, however, it is necessary to admit passengers to their berths or to give access to a restaurant car during the time the restaurant service operates, the doors should be unlocked.

Locking of
corridor and
gangway doors

In laying down the marshalling of trains, arrangements should be made, if possible, to avoid a passenger carrying vehicle being isolated from the remainder of the train by being marshalled between the brakevan in which a guard is not riding and the sleeping car accommodation. In cases where, in the interests of the working, this is not desirable the door leading to the sleeping car must be left unlocked.

The Guard will be responsible for carrying out these instructions but Travelling Ticket Staff, where provided, should assist. In the case of Sleeping Cars, the Sleeping Car Attendant will be responsible.

PASSENGER TRAIN INSTRUCTIONS.

TO GUARDS AND TICKET COLLECTORS ON CORRIDOR TRAINS. *Continued.*

Passengers
passing through
Luggage Vans.

4. When it is necessary for passengers to pass through vans and luggage compartments in the train, the Guard or Luggage Van must see that the luggage is securely stowed and that sufficient space is given to enable passengers to pass through readily and safely.

Train Lavatory
Equipment

5. Lavatories must be kept clean and properly equipped, and the Guard must report on the condition and equipment to the Chief Mechanical Engineer.

Train Ticket Collectors and Guards when no Train Ticket Collector is provided, must see that the lavatories are fully equipped at the commencement of the journey and that a report to the Chief Mechanical Engineer is made in all cases where the equipment is not complete.

6. Lavatories must, subject to the modifications shown below, be equipped, with soap, sanitary paper and 3 towels at starting point, immediately before departure, when starting from the Chief Mechanical Engineer's Department.

Lavatories of Ocean Specials, Dining Car units and slip coaches must be equipped with 6 towels.

Lavatories of third class sleeping cars to be equipped with soap, sanitary paper and 3 towels at the starting point, immediately before departure, and in addition the Sleeping Car Attendant to be supplied with sufficient towels to enable him to hand one to each third class sleeping car passenger.

Lavatories of special trains or coaches reserved for the conveyance of Government forces, ships' crews or other similar parties, must not be provided with towels, except the first class lavatory compartments, which must be equipped with 3 towels.

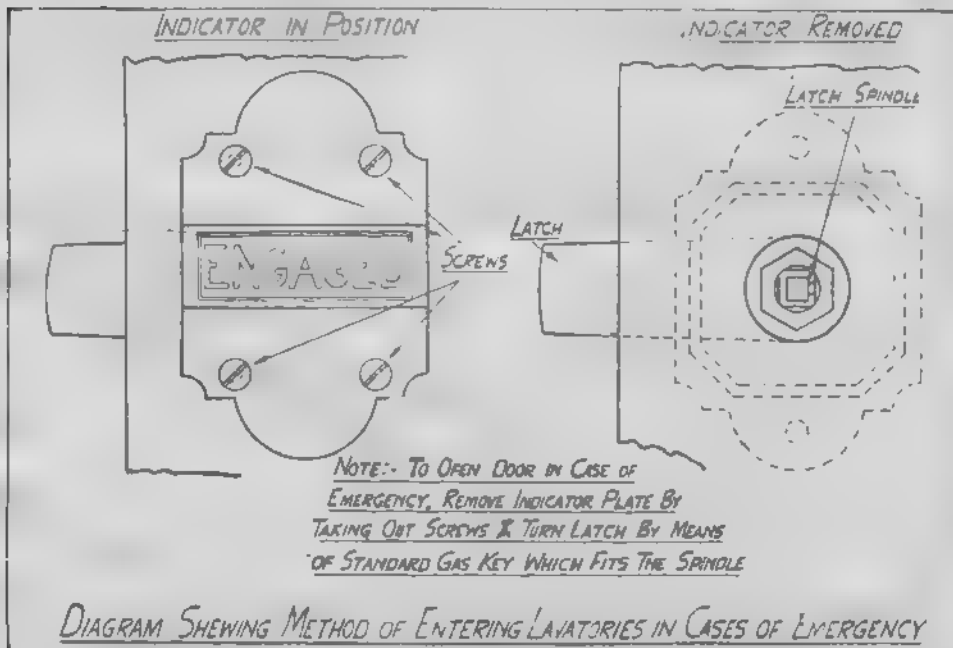
A supply of clean, spare towels must be provided at starting point by the Chief Mechanical Engineer's Department for replenishment purposes en route, as shown in the current supplement to these Instructions. These towels must be placed in the custody of the Train Ticket Collector, or Guard, if a Collector is not provided. If the Train Ticket Collector or Guard is relieved en route, the spare towels must be duly transferred to the relieving men.

Lavatory Door
Locks.

7. In the event of it being necessary to obtain access to the locked lavatory in an emergency, the following method should be adopted. By removing the indicator plate, the latch can be reached, and by using the end of the spare key, the door can be opened.

An explanatory drawing is given below.

G F 30



The following additional paragraphs to be added to Clause 5:

On arrival at the terminating point of a journey Guards must satisfy themselves that all lavatories, except those marked 'Lavatory Out of Use', are unoccupied. In any instance where it has not been possible for Carriage Examining Staff to give attention to the securing of the door of a defective lavatory, it is the responsibility of the Guard to specially inspect such lavatories at completion of the journey to ensure that they are unoccupied.

Guards, when in charge of passenger trains, must have with them a supply of labels reading 'Lavatory out of Use' and lavatories which are defective and in need of repair must be labelled with one of these labels. Should a Travelling Ticket Collector or Guard have his attention drawn to a defect in a train lavatory, he should make it desirable for the lavatory to be put out of use by affixing the label reading 'Lavatory out of use' to the door. In such circumstances steps must be taken to ensure that the gangway or corridor door between the defective lavatory and the next usable one is left unlocked.

In cases where it is necessary to give effect to this instruction, a record must be made on the Guard's journal and the incident reported at the first opportunity to a Carriage & Wagon Examiner or other responsible person.

The following to be added to Clause 7:

When it is necessary to put a lavatory out of use through defect or other causes, in addition to affixing a gummed label as required by Clause 5, the following instructions must be carried out:

- (a) The inside handle and square spindle should be removed from the segment lock, leaving the outside handle only in position.
- (b) The door should be locked by reversing the methods shown in the above paragraph for unlocking.
- (c) The coach itself should be labelled with a green "for repairs" label giving the station, date and defects.

(G.A.16. 5/46. L.K.2/9657/F.)

The first paragraph of Clause 7 amended to read:—

In the event of it becoming necessary to obtain access to the locked lavatory in a coach of the former G.W.R. design in the case of an emergency, this can be done by removing the indicator plate, which is secured by four screws, when the end of the spindle becomes exposed. This spindle will take the standard gas key and by using the same, the door can be readily opened.

The following to be added as the third paragraph of Clause 7:—

When it is necessary to release a lavatory door lock (with Indicator set at "Engaged") belonging to a British Standard Carriage, the three screws which secure the indicator plate should be withdrawn, the plate removed, and the indicator driving pin will then become exposed. Upon sliding this pin towards the lock handle it will be possible to gain access by turning the lock handle.

(G.A.30 Op.—9/54 LK1/6624/Gen. E.)

Paragraph 2 of the instructions under this heading to be deleted and substituted by the following:—

2. The Guard in charge must see that the inside sliding doors of luggage compartments where such are provided, and the steel grilles of luggage compartments fitted in British Railways Standard Stock, are kept closed and locked whilst the train is travelling.

(G.A.31—7/56—T.50, 639G/7 L.K.1/13138/420)

DAMAGE TO CARRIAGE WINDOWS, ETC.—page 117.

The following to be added to the list of amounts to be collected from persons responsible for the breaking of carriage windows

	s	d.
New type coaches		
Fixed window, corridor side First Class	32	2
Fixed window, corridor side Third Class	9	3
Drop window, corridor side First and Third Class	12	4
Door window, sliding door, corridor partition	11	9
Window, sliding ventilator, compartment side	11	9
(G.A.3.—12/37. C.M.L. 2009)		
	31	

No.

The foregoing rates to be regarded as a general guide basis, and Station Staff must use their discretion in the case of unusual climatic conditions.

G.A.3

PASSENGER TRAIN INSTRUCTIONS ROBBERIES FROM LUGGAGE VANS.

In order to prevent robberies from luggage vans the attention of Guards, Train Ticket Collectors, and Restaurant and Sleeping Car Attendants is directed to the importance of seeing that the following instructions are strictly observed:—

(1) Unauthorised persons are not permitted to enter vans.

(2) The Guard in charge must see that the inside sliding doors of luggage compartments, where such are provided, are kept closed and locked ~~at the train is travelling.~~

(3) Guards are warned against the practice occasionally adopted of an accomplice decoying a train early away from the vans or detaining them in conversation on some pretext, to afford the thieves time to operate.

SECURITY OF MAILS AND PARCEL POST RECEPTACLES.

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ATTACHING COMPANY'S LADDERS, POLES, &c, TO STEPS OR ROOFS OF CARRIAGES.

Ladders, poles, &c, must not be conveyed on the footboards of vehicles on passenger trains, and under no circumstances must these articles be placed upon the roofs of carriages.

STEAM HEATING OF PASSENGER TRAINS—Page 117.

Cause 1 of the existing instructions to be cancelled and the following substituted

1 The following dates are those which normally must be followed in the application and discontinuance of steam heat for passenger trains:

Commencement.

- | | |
|---|---------------|
| (i) All steam heater pipes to be fitted and coupled for use by 25th August. | |
| (ii) Heat to be applied: | |
| Sleeping Car trains | 1st September |
| Other Express trains while running after 5.0 p.m. and before 10.0 a.m. | |
| All other passenger trains | 1st October |

Discontinuance.

- | | |
|--|-----------|
| (i) Heat to be discontinued: | |
| All trains except as shown below | 1st May |
| Sleeping Car trains | |
| Other express trains while running after 5.0 p.m. and before 10.0 a.m. | 15th June |
| (ii) Pipes to be removed as soon as possible after | 15th June |

The foregoing dates for the application and discontinuance of steam heating are to be regarded as a general guide, but guards and others concerned must use their discretion in the event of unusual climatic conditions.

(G.A.30 Op.—9/54 T33320 G/2)

climatic conditions.

(G.A.23—7 49. R.E. Stand :—T.33320. G 2)

DAMAGE TO CARRIAGE WINDOWS, ETC.—Page 117.

The following to be substituted for the 1st of amounts that must be collected from persons responsible for the breakage of windows or carriage fittings, if possible at the time of the incident, otherwise the identity and address of the person(s) should be established and reported

Old Type Coaches					
	s.	d.		s.	d.
Top side light	6	0	Large light in corridor of saloon coaches under 34 inches wide...	35	0
Ordinary door light	16	0	Large light in corridor of saloon coaches, 34 inches wide and not exceeding 50 inches wide	52	6
Corridor door light	14	0	Large light in corridor of saloon coaches over 50 inches wide	56	6
Ordinary quarter light	22	0	Ventilators, large in Auto cars	86	0
Corridor quarter light	17	0	Ventilators, small in Auto cars	52	6
Lavatory drop light	8	6			
Lavatory fixed light	10	6			
Quarter light "Smoking"	22	0			
Door light frame including the lights	69	6			
New Type Coaches					
	s.	d.		s.	d.
Door light frame (with glass)	71	6	Window under ventilator, compartment side	52	6
Fixed window, corridor side			Window side of ventilator, compartment side	10	0
First Class	72	6	Fixed window, corridor partition, First and Third Class	16	6
Fixed window, corridor side, Third Class	62	6	Door window, sliding door, corridor partition	19	6
Drop window, corridor side, First and Third Class	17	6	Window, sliding ventilator, compartment side	10	0
Door drop window	19	0			
Blinds, First Class			Rack netting, First Class	28	0
Door 19½ inches	12	0	Rack netting, Third Class	27	0
Quarter light	9	0	Wash basin, old type	75	0
Sliding door	10	6	Wash basin, N.P. without pedestal	78	6
Blinds, Third Class			Pedestal N.P. without wash basin	72	0
Door 19½ inches	12	0	Pedestal and wash basin N.P. complete	144	0
Quarter light	10	6	Cushions, Third Class corridor	138	0
Sliding door	10	0	Cushions, Third Class, non corr.	220	0
Carriage Fittings			Cushions, First Class corridor 6 per compartment	74	6
Curtains, Third Class	26	6	Cushions, First Class corridor 4 per compartment	128	0
Curtains, First Class	25	6			
Electric light bulbs	2	0			
Window straps, First Class	12	6			
Window straps, Third Class	5	6			
Mirrors, Lavatory O.P. (inc. frame)	33	0			
Mirrors, Lavatory O.P. (Glass only)	8	0			
Mirrors, N.P.	17	6			
Mirrors, Compartment	19	0			

The same amount should also be charged in the event of breakage of windows and carriage fittings in other Region's stock working on the Western Region. (G.A.30 Op.—9 54 C.S.O.—C.B./D)

PASSENGER TRAIN INSTRUCTIONS.

STEAM HEATING OF PASSENGER TRAINS—*Continued.*

flexible pipes must be connected throughout—the stop-cocks being opened by bringing the handles in line with the pipes. The stop-cock at the rear of the last vehicle must be closed by bringing the handle in line with the end of the vehicle.

The flexible pipe at the rear must be properly secured with the hook and chain provided for the purpose, the hook to be attached to the clip at the lower end of the pipe and not in the mouth of the connection.

3. Steam must be passed through for a sufficient time to warm the whole train, and the driver must regulate the pressure of steam in the train according to the weather and length of the train, i.e., for trains not exceeding 5 coaches, 40 lbs. pressure must be maintained, for trains consisting of 6 to 8 coaches, 60 lbs. pressure must be maintained, and for trains consisting of more than 8 coaches, 70 to 80 lbs. pressure must be maintained.

In severe weather the full pressure of 60 lbs. must be maintained in all trains formed of more than eight coaches.

4. The engine should, whenever possible, be attached to the train at least ten minutes before starting, and the pipes coupled. The Driver must, unless otherwise instructed by the Guard, turn on steam at once.

At terminal or other stations, where the empty trains are drawn or backed into the station, the pilot engine must be used to warm the train before the train engine is attached.

5. The Driver must report to the Guard the condition of the appliances used, as reported by the Carriage Examining Staff, and must call to the Guard's attention any defects in the train. The Driver must also report to the Guard any defects in the train which are reported to him by the Carriage Examining Staff.

6. To enable the Driver to locate any defects, the Driver must not shut off steam until the engine is about to be uncoupled.

7. All concerned should see that the handles of the regulating valves in compartments are placed in the "On" position. If already in that position, they should be tried to see that they work freely; if not, the matter should be reported to the nearest Carriage Examiner.

8. At stations where Carriage Cleaners are employed, they must place the handle of the regulating valve in each compartment to the "On" position when cleaning the carriages.

9. Prior to the commencement of the journey the Guard in charge of the train must, as far as possible, see that the regulating valve in each compartment is turned to the "On" position, and in the case of corridor trains, Guards and Train Ticket Collectors should be on the alert when passing through their trains to see that the handles of the regulating valves in empty compartments are placed in the "On" position.

10. When uncoupling steam pipes, the stop-cocks at the ends of the vehicles (or engine and vehicle when the engine is being detached) must be closed by placing the handles in line with the ends of the vehicles, or vehicle and engine as the case may be, and away from the draw bar hook. If there is any steam in the pipes it will escape through the stop-cock.

Special
Provisions
to be observed

11. The Driver must see that the steam pipes are properly coupled and uncoupled, and that the stop-cocks are properly closed, and that the flexible pipes are properly secured, at the start and end of the journey.

12. Special arrangements have been made locally for preventing the steam-pipes of coaches running on important trains from freezing, and these must be put into force by the Locomotive and Carriage Department when they consider it necessary to do so.

13. In the case of coaches not covered by Clause 12, the following arrangements must be carried out:—

(1) Steam must be kept continuously on the train whilst in traffic.

(2) As far as possible the engine must remain coupled to the train whilst in traffic, and if the engine has to be detached for any reason, the Locomotive Department staff must see that it is allowed to be recoupled to the train as quickly as possible.

(3) When putting away trains or coaches, the men responsible for uncoupling the steam-pipes must open the cock at the end of the train and satisfy themselves

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5. Defects or irregularities in the working of the apparatus must be reported specially by the Guard on a memorandum, in duplicate, with particulars of the pressures recorded on the front and rear gauges, one copy to be handed to an Examiner calling his attention to the matter at the first opportunity the other copy to be attached to the train signal. The Junior Guard or Train Ticket Collector will report to the Head Guard if necessary.

PASSENGER TRAIN INSTRUCTIONS.

STEAM HEATING OF PASSENGER TRAINS—*Continued.*

that steam is passing through the rear vehicle before the engine is released. If steam does not issue from the cock, the nearest Locomotive and Carriage Department Examiner or responsible man must be informed at once, in order that he may make arrangements for putting the train in order before its next trip.

(iv) After uncoupling the engine, the stop-cocks must be opened and the flexible pipes between each coach must be uncoupled and left hanging down.

(v) When necessary, arrangements must be made for engines to join their trains half an hour earlier for the first trip in order to warm their carriages. The men who couple the pipes should leave the cock at the rear end of the train open until dry steam blows from it.

14. Slip Coaches.—To admit of slip coaches being steam heated, an adaptor is provided whereby steam heating pipes can be separated automatically when the coaches are slipped. The adaptors must be fixed by the Carriage Examiners and removed by the Traffic Department staff. Boxes are provided for conveying adaptors back to their home stations, the name of the station being stamped on a brass plate affixed to the lid.

When there is only one slip portion on the train, one set of adaptors will be used, being fixed between the Main train and slip.

When two or more slip portions are attached, additional sets of adaptors will be required, and fixed between each slip portion.

The box for the adaptor on the end of the Main train will be handed to the rear Guard, who must hand it to the staff at the station at which the adaptor is removed. The boxes for the adaptors on the slip coaches will be handed to the Slip Guards. The adaptors should be placed in the boxes on arrival at the slipping stations and booked back to their home stations by the first available train. The adaptors and boxes must be kept in a specially appointed place at the home station, so that the Locomotive and Carriage Department may have ready access to them.

REGULATION OF STEAM-HEATING APPARATUS ON BANANA VANS.

All banana vans are fitted with the steam-heating apparatus, and, when loaded, must be formed next to the engine and steam heated when required.

LIGHTING OF TRAINS.

1. Station Masters, Inspectors and others at starting stations must see that trains timed to arrive at their destination after dark, or where required, or which are likely to work back after dark, are properly supplied with gas at the starting stations.

Coaches fitted with lamps to burn gas must have the globes and reflectors properly cleaned and the reservoirs charged with gas.

The gas must be lighted on the bye-pass before leaving the starting station if the lights will be required before the train arrives at its destination, or for Tunnel purposes. The lights must be turned full on at the last stopping place, unless otherwise specially provided for, before entering the tunnels named below. They must be extinguished in each corridor coach after passing through the tunnel for which the Train has been lighted or at the first stopping place, provided the Train will reach its destination before dark. Train Ticket Collectors, when going through the trains, must assist Guards in extinguishing lights.

If, owing to a bye pass defect, the gas will not light on the bye-pass it must burn full on.

2 When there is an interval of more than half an hour at stations at either end of, or at intermediate points on Branch lines before trains are again required for use, the lights in the compartments should be placed on the bye passes. When the trains are again required the lights should be turned up only in as many coaches as will comfortably accommodate the passengers, further vehicles being lighted up as the necessity arises.

3. Guards and others concerned must see that the cocks on all vehicles, other than passenger-carrying, fitted with incandescent gas, are turned down when the lights are not required for use, and between long distance points, the lights should always be on the bye-pass.

PASSENGER TRAIN INSTRUCTIONS.

LIGHTING OF TRAINS—*Continued.*

4. Fish, Fruit and Milk trucks fitted with gas lighting are provided with a by-pass cock beneath the bodies of the vehicles to enable the gas to be turned up or down as may be necessary.

Electricity.

5. Every effort must be made to economise the electricity stored in the batteries of electrically-lighted coaches, and Station Masters, Inspectors and others must see that the lights are burning only when required. The lamps in electrically-lighted coaches must not be switched on more than 30 minutes before the train leaves the station.

Electric light
failures

6. In cases where the electric light fails, or any electrical defect is developed in a coach in the train they are working, particulars giving number of coach and compartment, with brief details of failure must be given by Guards at the next stopping-place at which there is an Examiner, and similar particulars must be shewn on their journals.

Electric Through Control Couplers.—When not in use, electric through control couplers must be placed in the pockets provided, and not allowed to hang loose.

Electric
Lighting of trains
through tunnels.

7. All passenger trains must be lighted through all tunnels irrespective of length where the Guard has facilities for switching the lights on and off, while running.

(b) All passenger trains must be lighted through the following tunnels during daylight.

Ardley.	Marley.
Ballingham (between Holme Lacy & Fawley).	Merthyr.
Bincombe (between Dorchester and Weymouth).	Mitcheldean.
Birkenhead.	Newport.
Box.	Old Hill.
Brimacombe.	Patchway.
Bryn (P.T. Section).	Pencader.
Caerphilly.	Pencaedrain (Vale of Neath).
Campden.	Penllergaer.
Chipping Sodbury.	Perran (Falmouth Branch).
Clevedon.	Perridge (between Longdown & Christow).
Cockett.	Pitsea (Ferry Road).
Colwall.	Poundbury (near Dorchester).
Cymmer.	Pontypridd.
Dainton.	Quaker's Yard.
Dinmore.	Rhondda.
Dudley.	Sapperton.
Eversholt.	Severn.
Fox's Wood.	Shaugh (Launceston Branch).
Grenofen (Launceston Branch).	Somerton.
Grimstone.	Sparnick (Falmouth Branch).
Halton.	Tidenham.
Ledbury.	Wenvoe.
Llangyfelach.	Whiteball.
Loulas.	Yelverton (Launceston Branch).

The electric lights must be switched on during daylight immediately before entering the tunnel where the Guard has the means of doing this while running, otherwise they must be switched on at the last stopping place before entering the tunnel.

The lights must be switched off immediately after passing the tunnel where the Guard has the means of doing this while running, otherwise they must be switched off at the first stopping station after passing through the tunnel.

The Guard of the train (Head Guard when there is more than one Guard) will be responsible for seeing that the instructions are duly observed.

7. ELECTRIC THROUGH CONTROL COUPLERS.

Through control cables must be coupled on all coaches before the train starts and when any alteration in the formation of the train is made there must be taken to see that the wires of the lights on the train are switched on from the Battery, and that the Goids to ride. The Goids of the train must satisfy themselves that this instruction is followed. When in use, electric through control couplers must be placed in the pockets provided, and not allowed to hang loose.

Clause 7 of the existing instructions to be renumbered 8 and the first paragraph lettered (a).

(GA. 18. 11 47. L. K. 1 8235 Gen. 4)

Reference to the following to be made on page 121:—

FIRES IN PASSENGER TRAINS.

If any member of the staff, either in the employ of the British Transport Commission or the British Transport Hotels and Catering Services, becomes aware of a fire on the train on which he is travelling he should take appropriate steps to extinguish it. If, however, he is unable to do so promptly he must arrange for the train to be stopped immediately so that suitable action can be taken.

After the train has been stopped the services of a Carriage and Wagon Examiner should be obtained, if possible. Attention is also drawn to the fact that if the fire occurred in a roof with a ceiling, it may be necessary to remove part of the ceiling to ensure that the fire is properly extinguished. When the train proceeds on its journey a member of the staff should, if possible, travel in the affected compartment or carriage for the purpose of observation and he should be prepared to deal with any subsequent outbreak of fire. Should a member of the staff not be available to ride in the compartment or carriage, the Guard should examine the affected compartment or carriage as frequently as possible.

Passengers should not be permitted to travel in the vehicle concerned until it has been passed fit for service by a Carriage and Wagon Examiner. (G A.30 Op.—9/54 LK1/11343/365E)

PASSENGER TRAIN INSTRUCTIONS.

WATER CANS FOR USE IN LAVATORY COMPARTMENTS IN CASE OF EMERGENCY.

During exceptionally severe weather in winter months, the water tanks in lavatory compartments fitted with warming apparatus, should where possible, be emptied at night and refilled in the morning.

To avoid the risk of damage to lavatory fittings by frost in the case of coaches sent to various points to stables for long periods during the winter months, it is very important that the tanks, pipes, etc., should be completely emptied of water, and the following arrangements should operate.

In coaches not fitted with hot water apparatus tanks to be emptied by means of the flushing valve and valve over basin, but with coaches fitted with hot water apparatus, the drain tank below the boiler must also be opened. This to be done by the Department concerned before vehicles leave a station or depot for stabling.

When the vehicles have finally come to rest at the stabling point, the valves over basins and pistons must be operated to clear the pipes of any residual water in the tank which may have entered the pipes through oscillation when running. This work to be performed by the Traffic Department staff of the station nearest to the stabling point, or as may be otherwise specially arranged.

A sufficient number of water cans will be supplied each winter to all stations from which carriages equipped with lavatories commence their journey. These cans are only to be used in case of emergency, such as burst or frozen pipes.

At the close of the winter season all the cans must be returned to the Stores Department, Swindon.

As a guide to the man responsible for filling the tanks on coaches with live heating apparatus under the lavatory tanks, the letter "H" is fixed in the tank, (flange over on the roof of the vehicle). This arrangement will gradually supersede the old method of painting "H.W." on the end of the vehicle.

LAVATORY COMPARTMENTS

Station Masters at terminal stations and Masters of the Traffic Staff should take steps to satisfy themselves by frequent personal inspection that all lavatory tanks are kept full, the lavatories are clean, and in thorough working order, also that they are provided with a good supply of water, sanitary paper, soap and clean towels.

EMERGENCY APPLIANCES, &c., IN PASSENGER TRAINS

Emergency appliances and "First Aid" requisites are provided on passenger trains, as shown below, for use in the event of an accident.

Description.	Where Provided.
HAND FIRE EXTINGUISHER	As far as possible two No. 1 "Pressure" extinguishers are provided on all Passenger trains (one at each end of the train, in Guards' brake compartment), two in each sleeping car (one at each end of corridor) and two in each Restaurant Car (one in kitchen and one in vestibule). One No. 4 "Hand Cup" extinguisher will be provided as far as possible in a third rail vehicle at one end of the train.
SET OF TOOLS, &c.	One case in the Guard's compartment of each passenger brake vehicle.
FIRST AID REQUISITES	One case in each of the Guard's compartment of each passenger brake vehicle.

NOTE: The FIRE EXTINGUISHERS AND TOOLS must only be used in the case of fire, or in the event of a train accident.

1. In the event of an accident to a train conveying passengers, it is of the first importance that immediate steps should be taken to relieve any who may have sustained injury, as well as to adopt all precautionary measures, which the circumstances may render desirable, in the direction of limiting the effects of the occurrence. The following important points are therefore set out as supplementing the instructions of the Rule Book and those detailed above, and as being necessary to be borne in mind:—

(a) Examine train to see if there is any sign of fire from live engine coal or broken gas cylinders or connections, and if so take prompt steps to extinguish it by means of the Fire Extinguishers provided on the train, or any other available means.

Immediately call for assistance from the nearest Fire Brigades, who

Special Instructions to Station Masters in respect of accidents.

PASSENGER TRAIN INSTRUCTIONS.

EMERGENCY APPLIANCES, ETC.—Continued.

should, if possible, be informed the exact situation of the trains and the means of access.

(b) Ascertain where the greatest damage has occurred, and take instant steps to release any passengers who may be entangled in the wreckage.

Tools and other appliances provided in the Brake Compartments to be handed out to the most experienced men available.

(c) Insure prompt telegraph or telephone communication with the nearest likely places for Doctors, Nurses, Ambulances, Refreshments, &c., according to circumstances.

The assistance of all available "First Aid" men to be obtained, and the best possible use made of the "First Aid" Outfits provided in the Brake Compartments.

At stations where Ambulance Sections are formed, in the event of a train accident in the vicinity, the Station Master must arrange immediately to despatch one or more of the organised Ambulance Stations, with their equipment, to the scene of the accident on receipt of a telegraphic or telephone request.

(d) Enlist the services of any uninjured and willing passengers or other persons to assist in the rescue, and to assist in the direction of the Railway Staff.

(e) Use caution taken from the engines in such a manner as to form a fire triangle, and to prevent the escape of gas.

(f) Advise the nearest Station Master and other officials, and also the Permanent Way men, at the earliest possible moment.

Should gas be escaping from a broken pipe, the pipe should be bent away from timber or inflammable wreckage clear of any source of ignition and, if practicable, in the same direction as the wind. The pipe can be easily bent or hammered flat to prevent the escape of gas.

Should the gas be ignited at a broken pipe and it can be extinguished by directing a stream of liquid from the No. 1 Pressure Extinguisher on to the end of the pipe

Broken
Gas Pipe

EMERGENCY APPLIANCES, ETC., IN PASSENGER AND PARCELS TRAINS.—Page 121.

The following to be inserted after "Diesel Cars" in the table in the first paragraph

New Type Diesel Cars.

Power Cars.—One water/CO₂ pressure extinguisher in the brake compartment and two CO₂ gas extinguishers in the driving cab.

Note.—A chlorobromomethane (C.B.) extinguishing system is fitted to each diesel engine, with detectors to give the driver warning of fire and indicate which motor is affected.

Trailer Cars.—One water CO₂ pressure extinguisher at one end of the corridor.

(G.A. 31—7/56 A.1/72034/102)

Instructions
as to use,

TO OPERATE:—

1. Screw down wheel to right.
 2. Turn valve to control jet.
- (On the turn valve the "open" and "shut" positions are shown by raised letters on the body of the valve.)

On the top of the wheel are the words "Screw right down," the direction being shown by an arrow.

NOTE. The wheel on the top of the No. 1 Pressure Extinguisher must not be screwed down nor the appliance made use of except in the case of fire.

TO OPERATE:—

1. Screw down wheel to right.
 2. Press valve to control jet.
- (On the press valve the word "press" is stamped.)

EMERGENCY

EMERGENCY

EMERGENCY

HAND FIRE

OUT OF TO

AD

Note—The

1000

Instructions to
be used in the
case of fire.

EMERGENCY APPLIANCES, &c., IN PASSENGER TRAINS.—Page 121.

The instructions under this heading to be cancelled and the following substituted —

EMERGENCY APPLIANCES, &c., IN PASSENGER & PARCELS TRAINS.

Emergency appliances and first aid requisites are provided in vehicles on passenger and parcels trains for use in the event of fire or accident in accordance with the following:—

Description	Where provided
HAND FIRE EXTINGUISHERS	Sleeping Cars .. Two water CO ₂ pressure extinguishers, one at each end of corridor, or alternatively the two extinguishers in a recess at one end of the corridor.
Restaurant Cars	
Twin Units ..	One foam and one CO ₂ gas extinguisher in kitchen. One water CO ₂ pressure extinguisher in corridor at end of kitchen and one water/CO ₂ pressure extinguisher in corridor at end of dining car.
Single Units ...	One foam and one CO ₂ gas extinguisher in kitchen, and one water/CO ₂ pressure extinguisher in corridor at end of car.
Corridor Brake Vans	Standard Stock —One water CO ₂ pressure extinguisher and two buckets in brake compartment. One water/CO ₂ pressure extinguisher in corridor. Non-Standard Stock —One water/CO ₂ pressure extinguisher in brake compartment.
Other Corridor Vehicles	One water/CO ₂ pressure extinguisher or W.R. No. 4 "Hand Cup" extinguisher at end of corridor.
Non-Corridor Brake Vans	Standard Stock —One water/CO ₂ pressure extinguisher and two buckets in each brake compartment. Non-Standard Stock —One water/CO ₂ pressure extinguisher in each brake compartment.
Diesel Cars	One water/CO ₂ pressure extinguisher and one W.R. No. 5 "Vessel and Syringe" C.T.C. extinguisher
Post Office Vans ...	Two W.R. No. 4 "Hand Cup" extinguishers and two buckets of sand.
SET OF TOOLS &c.	One case in the guard's compartment of each passenger brake vehicle.
FIRST AID REQUISITES	One cabinet in the guard's compartment of each passenger brake vehicle

Note—The fire extinguishers and tools must only be used in the case of fire, or in the event of a train accident.

Special instructions to Station Masters, Firemen and others.

1. In the event of an accident to a train conveying passengers, it is of the first importance, after protection as required by the Rules, that immediate steps be taken to relieve any who may have sustained injury, and to adopt all precautionary measures which the circumstances may render desirable in the direction of limiting the effects of the occurrence. The following further important points which are supplementary to the instructions of the Rule Book must be borne in mind:—

(a) Examine train to see if there is any sign of fire from live engine coal or broken gas cylinders or connections, and if so take prompt steps to extinguish it by means of the fire extinguishers provided on the train, or any other available means.

Immediately call for assistance from the nearest Fire Brigade, who should, if possible, be informed the exact situation of the train and the means of access. The correct method for calling the brigade by telephone is included in the Fire Notice exhibited at every station, yard and signal box.

(b) Ascertain where the greatest damage has occurred and take instant steps to release any passengers who may be entangled in the wreckage.

Hand out tools and other appliances provided in the brake compartments to the most experienced men available.

(c) Ensure prompt telegraph or telephone communication with the local police officer and with the nearest likely places for doctors, nurses, ambulances, refreshments, etc., according to circumstances.

Obtain the assistance of all available first aid men and make the best possible use of the first aid cabinets provided in the brake compartments.

If a serious train accident occurs in the vicinity of a station, the station master or person in charge must despatch immediately as many staff qualified to render first aid as possible, with equipment.

(d) Enlist the services of any uninjured and willing passengers or other persons to convey messages, and to assist generally, under the direction of the railway staff.

(e) Use cushions taken from the coaches in such a manner as to form comfortable couches for any injured persons until such persons can be removed.

(f) Advise the nearest station master and other officials, also the permanent way men, at the earliest possible moment.

(g) Should gas be escaping from a broken pipe, bend the pipe away from timber or inflammable wreckage clear of any source of ignition and, if practicable, in the direction to which the wind is blowing. If possible, the pipe should be hammered flat to prevent the escape of gas.

Should the gas be ignited at a broken pipe end and it is not possible to flatten it, remove all inflammable material from the vicinity of the flame. If the end of the pipe is not accessible the adjacent woodwork should be wetted. Provided persons are not trapped in the wreckage the flame may be extinguished by directing a stream of liquid from a water/CO₂ pressure extinguisher to the end of the pipe in the SAME DIRECTION as the issuing flame. Naked lights should not be allowed in the vicinity until the gas has been dispersed.

2. Fire Extinguishers

The W.R. No. 1 pressure extinguisher is fitted with a quick-acting press valve, which is controlled by thumb pressure and closes automatically to reserve the liquid.

The following proprietary extinguishers are provided in passenger vehicles for use as shown:—

Name	Type	Use
Conquest 55	Water/CO ₂ pressure	All fires except electrical, petrol and oil
Firesnow	do.	do.
Waterloo	do.	do.
Phomene	Foam	Petrol, oil and fat fires.
Kidde (Lux)	CO ₂ gas	Electrical, petrol, oil and fat fires
Pyrene CO ₂	do.	do.

In addition to the instructions shown on each appliance, booklet B.R.7006—"Portable Fire Extinguishers on Trains" illustrates the various types of extinguishers and their methods of operation.

All train staff must be in possession of this booklet and be fully acquainted with its contents.

Lead seals marked "B.R. (W)" and "S.F.B." are provided on W.R. No. 1 water/CO₂ pressure, W.R. No. 4 "Hand Cup," C.T.C. and CO₂ gas extinguishers.

Guards, ticket collectors, sleeping car attendants, restaurant car attendants and travelling carriage cleaners will be instructed in the use of each type of extinguisher, as may be appropriate according to the types they are likely to encounter. Where possible they will have actual experience in handling the appliances.

The periodical examination of trainmen in the rules and regulations must include the use of fire appliances.

Train staff must not on any account test the appliances by operating them and drawing off small quantities of liquid. When seals are provided they must not be broken unless the extinguishers are required for a fire or when permission for doing so is given by an authorised instructor.

3. Tool Cases.

The tool cases are painted red with the words "FOR EMERGENCY USE ONLY" lettering, black edged, on the front of the cases and a rectangular piece of reinforced glass is inset in the lid to enable the contents to be seen. The lid is taped and sealed at the top and bottom on one side, and in the event of an accident it is only necessary to cut or break the tape in order to open the lid and obtain the tools and appliances.

Each tool case in Western Region stock contains the following articles:—

- 1 Saw, hand, 26 inches, rough tooth.
- 1 Saw, hand, 22 inches, rough tooth.
- 1 Felling Axe.
- 1 Axe, hand, small.
- 2 Crowbars, 3ft. 5in. and 2ft. 5in. long respectively.
- 1 Extension Tube.
- 1 Chain, 12ft. 0in. long, with hook at one end and ring at the other.
- 1 Spade.
- 1 Set of Splints (4 to set).
- 1 Metal Sleeve (for extension of splints)

Tool cases in B.R. standard stock contain the following articles:—

- 1 Saw, hand, large.
- 1 Saw, hand, small.
- 1 Felling Axe.
- 1 Axe, hand, small.
- 1 Crowbar, 3ft. 0in. long.
- 1 Crowbar, 4ft. 6in. long (in clips on left-hand side of tool case).
- 1 Crowbar Extension Tube.
- 1 Sledge Hammer.
- 2 Steel Wedges.
- 1 Coil of Rope, approx. 13 yards long and $\frac{1}{2}$ in. diameter.
- 1 Coil of Rope, approx. 12 yards long and $\frac{1}{4}$ in. diameter.
- 1 Inspection Lamp, electric, fitted with 200ft. flexible lead and plug.
- 2 Lamps, hand, paraffin.

4. Examination and Maintenance of Fire Extinguishers and Tool Cases

The Chargehand Carriage & Wagon Examiner or other authorised member of the Carriage & Wagon Engineer's staff at the various depots will be held responsible for seeing the fire extinguishers and tool cases are in their proper places and that seals (where provided) are intact.

Should an extinguisher be damaged or missing or be found with a broken or missing lead seal when it should have one, such extinguisher must at once be replaced. A supply of spare extinguishers and tool case replenishments will be kept on hand for renewal purposes at the following depots:—

Fire Extinguishers.

Aberdare H.L.	Oxford
Aberystwyth	Paddington
Barry	Penzance
Birmingham, S.H.	Plymouth
Bridgend	Pontypool Road
Bristol (Dr. Days)	Pontypridd (To draw on Cardiff Q. St.)
Caerphilly (To draw on Cardiff Queen St.)	Pwllheli
Cardiff General	Reading
Cardiff Queen Street	Ruabon
Carmarthen	St. Blazey
Cheltenham St. James	St. Erth
Exeter	Severn Tunnel Junction
Goodwick	Slough
Helston	Southall
Hereford	Stratford-upon-Avon
Leamington	Swansea High Street
Liskeard	Swindon
Marylebone	Taunton
Neasden Shops	Trowbridge
Neath	Truro
Newport (Ebbw) Carriage Shed	Tyseley
Newquay	Westbury
Neyland	West London
Old Oak Common	Wolverhampton Cannock Road
Oswestry	Wolverhampton L.L.
Oswestry Shops	Worcester
	Wrexham

Tool Cases.

Aberystwyth
Barry
Bridgend
Bristol
Cardiff General
Carmarthen
Cheltenham St. James
Fishguard & Goodwick
Hereford
Marylebone
Neasden Shops
Neath
Newport (Ebbw) Carriage Shed

Neyland
Old Oak Common
Oswestry
Penzance
Plymouth
Pwllheli
Swansea High Street
Taunton
Tyseley
West London
Wolverhampton Cannock Road
Wolverhampton L.L.
Worcester

Duty
Attendants,
Sleeping Car
Attendants
Restaurant Car
Attendants

Guards in charge of trains must satisfy themselves that extinguishers are installed in accordance with the introductory paragraph to these instructions and that the seals on extinguishers (where provided) and tool cases are intact. In the case of restaurant and sleeping cars the attendants will be responsible for examining the extinguishers and reporting defects, etc., to the guard.

Duty of
Cleaning Staff.

Carriage cleaning staff observing deficiencies, damage or broken seals when carrying out interior cleaning must advise the Carriage & Wagon Engineer's staff, so that replacements can be installed with a minimum of delay.

The
Guard or
Attendant
Responsible
for the
Train

The guard will be responsible for reporting to the Carriage & Wagon Examiner on duty at the station where the train terminates its journey any cases of extinguishers being damaged, missing or having broken seals. Where restaurant or sleeping cars are formed in the train the guard must confer with the attendants.

In cases where the guard does not proceed with the train to its destination he must report any defects, etc., to the guard who receives the train. The latter will be responsible for advising the Carriage & Wagon Examiner at the terminating station.

Any such report made by the guard must be recorded on his journal and the District Operating or District Traffic Superintendent in view of whom the journal is filed must immediately pass an extract of the report to the District Outdoor Carriage & Wagon Engineer. The latter must then report any special features or defects in connection with fire appliances to the Carriage & Wagon Engineer, Swindon.

Reporting
Extinguishers.

If an extinguisher is damaged or the seal provided is broken or missing it must at once be exchanged and the defective appliance returned to one of the crates provided to the Fire Station, M. & E. Engineer's Department. See also the Carriage & Wagon Examiner or other authorised member of the Carriage & Wagon Engineer's staff at the depots mentioned being responsible for this duty.

5. First Aid Cabinets.

Contents
of Cabinets.

The First Aid Cabinet is marked "BR(W) FIRST AID No. 3" and contains the following requisites:—

Sterilised Dressing (small)	2
" " (medium)	2
" " (large)	2
Mines Dressing (large)	6
" " (Medium)	3
Sterilised Cotton Wool, ½oz. packet	3
Antiseptic No. 5—2oz. bottle	1
Sal Volatile—2oz. bottle with screw cap (also to be used as smelling salts)	1
Triangular bandages (individually wrapped)	9
Roller bandages 2ins. x 2 yards	6
Safety pins—set of 6	1
Splints, with junctions—set	1
Lint—white, ½oz. packet	3
Eye drop No. 1A (castor oil)—½oz. bottle	1
Card of Instructions	1
Report Form	1

The First Aid Cabinet is primarily intended for use in "Train Accidents."

The top of the cabinet is sealed but the seal may be broken in event of emergency as indicated above, when the contents will be obtainable.

6. Examination and Maintenance of Train First Aid Cabinet.

Station and Yard Masters will be held responsible for ensuring that the examination, replenishment and sealing of First Aid Cabinets is carried out by the Carriage Cleaning Staff

Should a First Aid Cabinet be damaged or missing, or found with a broken or missing seal, it must be replaced or replenished without delay and a supply of spare First Aid requisites will be maintained for this purpose at certain Stations and Depots. When a First Aid Cabinet has been replenished and sealed a small coloured adhesive label must be affixed to the lid to indicate when the contents were last inspected. The labels will be forwarded annually, without requisition, to District Officers by the Stationery Department and will be used in the following sequence:—

Colour	For use during 3 months commencing
Pale Blue	January 1st
Brick	April 1st
Apple Green	July 1st
White	October 1st

7. Train First Aid Cabinets—Replenishment Cupboards.

Stations at which Replenishment Cupboards are provided.

Operating District	Station or Depot	Grade	Operating District	Station or Depot	Grade
LONDON	Didcot ...	C	CARDIFF	Barry ...	C
	Henley-on-Thames ...	C		Bridgend ...	B
	Old Oak Common ...	A		Caerphilly ...	C
	Oxford ...	C		Cardiff (Bute Road)	C
	Paddington ...	B		Cardiff (Carriage Sdgs)	A
	Reading ...	B		Cardiff (General)	C
	Slough ...	C		Cardiff (Queen St.)	C
	Southall ...	C		Dowlas (Caeharris)	C
	West London ...	A		Llantrisant ...	C
				Maerdy ...	C
BRISTOL	Bristol (Dr. Days) ...	A	SWANSEA	Penarth Town ...	C
	Chippenham ...	C		Pontypridd ...	C
	Frome ...	C		Porthcawl ...	C
	Swindon ...	B		Rhymney ...	C
	Trowbridge ...	A		Tondu ...	C
	Westbury ...	C		Treherbert ...	C
	Weston-Super-Mare ...	B			
	Weymouth ...	B		Aberavon Town ...	C
	Yeovil (Pen Mill) ...	C		Carmarthen ...	B
				Fishguard Harbour	C
EXETER	Exeter (St. Davids) ...	C	WORCESTER	Swansea (High St.)	A
	Kingswear ...	B		Swansea (Victoria)	B
	Paignton ...	C		Whitland ...	C
	Newton Abbot ...	B			
	Taunton ...	B		Evesham ...	C
PLYMOUTH	Bodmin ...	C		Kidderminster ...	C
	Helston ...	C		Worcester (Shrub Hill)	B
	Kingsbridge ...	C	BIRMINGHAM	Banbury ...	C
	Laira ...	C		Birmingham (Snow Hill)	B
	Liskeard ...	C		Leamington Spa ...	C
	Newquay ...	C		Stourbridge Jct.	A
	Penzance ...	B		Stratford-upon-Avon	C
	Plymouth (Millbay) ...	A		Tyseley (Carriage Sdgs)	A
	St. Ives ...	C		Wolverhampton	
	Truro ...	B		(Cannock Road)	B
				Wolverhampton (L.L.)	A
GLoucester	Cheltenham (St. James)	C			
	Gloucester (Central)	C			
	Lydney ...	C			
	Ross-on-Wye ...	C			

Operating District	Station or Depot	Grade	Operating District	Station or Depot	Grade
NEWPORT	Aberbeeg ...	C	CHESTER	Hereford ...	B
	Aberdare (High Level)	C		Ruabon ...	C
	Abergavenny Junction	C		Shrewsbury ...	B
	Ebbw Jct. (Carriage Shed)	A		Wellington (Salop)	C
	Merthyr ...	C		Wrexham ...	C
	Newport (High Street)	C	CENTRAL WALES	Aberystwyth ...	C
	Pontypool Road ...	C		Brecon ...	C
	Severn Tunnel Junction	C		Oswestry ...	C
	Tredegarr ...	C		Pwllheli ...	C

Replenishment Cupboards—Details of Stock to be held.

Details of the stock to be held should be pasted on the inner side of the replenishment cupboard door and the stock must be maintained as near as possible to the quantities stated.

Excessive stocks must not be held.

Spare empty cabinets when required should be obtained from the nearest Carriage Repair Depot.

Replenishments to be ordered on form "BR.9016."

Standard Item Number	Description of Material	Stock Unit	Stock permitted at Stations		
			Grade of Station		
			"A"	"B"	"C"
102-204	Bandages Roller 2in. x 2 yards ...	doz.	4	2	1
102-207	Bandages Triangular—Wrapped ...	each	72	36	18
102-211	Blocks Wood ...	"	1	1	1
	Dressings Sterilised				
102-222	Compressed, Mines type, large ...	"	48	24	12
102-223	Compressed, Mines type, medium ...	"	24	12	6
102-224	Ordinary, Finger ...	"	16	8	4
102-225	Ordinary, Large, wound ...	"	16	8	4
102-226	Ordinary, Medium wound ...	"	16	8	4
102-228	Drops Eye, No. 1A (castor oil) ...	"	8	4	2
102	Forms 5042 (Guard's Report) ...	"	24	12	6
102-237	Lint, White ½ oz. packets ...	doz.	2	1	½
102-238	Lotion, Antiseptic, 2oz. bottles ...	each	8	4	2
102-245	Pins, safety (sets of six) ...	set	8	4	2
102-250	Sal Volatile—2oz. bottles ...	each	8	4	2
102-253	Seals—Lead ½ in ...	each	150	100	50
102-255	Splints—with joints ...	set	3	2	1
102-259	Tape, Sealing ...	doz. yds.	4	3	2
102-262	Wool, Absorbent ...	each	24	12	6
	Cards of Instructions ...	"	8	4	2

Duties of
Guards

8. Guards in charge of trains must satisfy themselves that the seals of the First Aid Cabinets are intact and if any of the equipment is damaged or missing, or the seals broken or missing, must report the matter to the person in charge at the station at which the train terminates its journey, who must advise the Carriage Cleaning Staff that a First Aid Cabinet requires attention.

In cases where a Guard does not proceed with the train to its destination he should report any defects to the Guard who receives him and the latter will be responsible for advising the person in charge as stated above.

Entry on
Guard's
Journal

Any such report must be recorded on the Guard's journal and the District Operating Superintendent or District Traffic Superintendent in whose office the journal is filed must ensure that the necessary replenishment and re-sealing is carried out.

Record of
use.

In the event of a First Aid Cabinet having been used the Guard must make an appropriate entry on the form provided and leave the form inside the Cabinet for the information of the Carriage Cleaning Staff at the terminating station or depot.

9 Pilferage from First Aid Cabinets.

Numerous losses from First Aid Cabinets in trains have been brought to notice and there is reason to believe that these occur during the night or whilst the vans are standing in isolated sidings.

Guards when taking charge of trains should satisfy themselves that the Cabinets are in order and call attention of the Station or Yard Master to any irregularity, which should also be reported on the Journal.

Station and Yard Masters should report all irregularities to the District Operating Superintendent and, if necessary, the B.T.C. Police.

The co-operation of the staff is desired to prevent pilferage from First Aid Cabinets.

(G.A.30 Op.—9/54 AI/7 1163/102 S2/AMB/7)

EMERGENCY APPLIANCES, ETC., IN PASSENGER TRAINS—pages 121 to 127.

On page 122 at the end of Clause 2—Fire Extinguishers.

The design are now being supplied for use on passenger trains.

"Waterlon" which are for use in carriage stock, are of the

type that it is necessary to remove the plunger cover carry the

strike the knob and direct the jet at the base of the fire

or as a liquid fire in connection with burning liquids. To

be used on a fire the lever at the head of the cylinder

should be allowed to fall gently on the

and Phenomenal extinguishers do not bear wire and

(G.A.29 Op.—5 52 AI/71163 3.)

of fire appliances to their brackets or supports as

(G.A.29 Op.—5 52 AI/71163 3.)

green tape and lead seal" to be amended to read

(G.A.29 Op.—5 52 AI/71163 3.)

be deleted and the following substituted:

be sealed with wire and lead seal marked "BR(W)"

(G.A.29 Op.—5 52 AI/71163 3.)

to be amended to read "wire and lead seal."

(G.A.29 Op.—5 52 AI/71163 3.)

Case 7 to be deleted and the following substituted—

with a sufficient number of

and crates to meet requirements.

(G.A.29 Op.—5 52 AI/71163 3.)

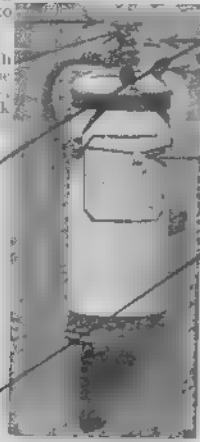
PASSENGER TRAIN INSTRUCTIONS. EMERGENCY APPLIANCES, ETC. *Chained*

TURN VALVE.

PRESS VALVE.

Screw down
wheel to
right.

Sealed with
green tape
and lead
seal marked F.R.S.



Hand valve on
control jet.

Handle to carry Screw down
appliance (on wheel to
shoulder if at
a distance).

Support with
brown tape &
lead seal
at a distance.

Support with
brown tape &
lead seal
at a distance.

Hose to
control jet.



Sealed with
green tape and
lead seal
marked F.R.S.

Hand valve to
control jet.

Sealed to sup-
port with
brown tape &
lead seal
marked F.R.S.

No. 4. "Hand Cup" Extinguisher.

The No. 4 Hand Cup Extinguisher consists of a vessel containing fire extinguishing liquid and a hand cup for applying it.

The outlet of the vessel is closed with a bung to which a hand cup is attached for the purpose of withdrawing liquid.

The hand cup is inserted over the outlet of the vessel.

To Operate: If small quantities of liquid in hand cup and DASH FORCIBLY ON SEAT OF FIRE.

Should the fire extinguishing liquid be exhausted before the fire is extinguished, supplies of water should be drawn from a tender, stream, ditch, or other available source.

Instructions
as to use.

Attach
supplies of
water.

Break off top, twist
round to break seal.

Place with
L.R.S. both sides
Green tape

Place with
L.R.S. both sides
Green tape

Place with
L.R.S. both sides
Green tape

Place with
L.R.S. both sides
Green tape



Hand cup for applying liquid
on outlet of vessel.

Twist and draw up to
close case.

Case closed with bung.

Extinguisher sealed to support here
with brown tape and lead seal,
marked L.C.D.

Vessel containing 2 galls. of fire
extinguishing liquid.



Pouring small quan-
tities of liquid
from hand cup.

Using the
thumb
to break the seal.



The use of the
hand cup.

Dash forcibly
on seat of fire.

PASSENGER TRAIN INSTRUCTIONS.

EMERGENCY APPLIANCES, ETC. *Continued.*Seals on
Extinguishers.

The appliances are sealed with green tape and lead seal marked "G.W.R." "F.B.S.," and are sealed in position with brown tape and lead seal marked "G.W.R." "L.C.D."

Arrangements
for
Instructing
Guards.

Guards will be instructed in the use of each type of extinguisher, and will have actual experience in handling the appliances.

NOTE—Guards must not on any account test the appliances by operating them and drawing off small quantities of liquid from time to time, and the seals of the extinguishers should not be broken unless required for extinguishing a fire.

Tool Cases.

Description.

3. The Tool Cases are painted red, with the words:

"TOOLS."

FOR EMERGENCY USE ONLY"

in white lettering on the front of the case, and a glass square is inset in the lid to enable the contents to be seen. The lid is taped and sealed at the top and bottom on one side, but at the centre of the lid it is only necessary to tear or break the tape in order to open the case and obtain the tools and appliances.

Contents of
Case.

Each Tool Case contains the following articles:—

- 1 Felling Axe.
- 1 Small Bench Axe (with hammer end).
- 2 Single handed rough-toothed Saws.
- 2 Crowbars 3 ft. 5 in. and 2 ft. 5 in. long respectively, with one extension.
- 1 Chain 12 feet long with hook at one end and ring at the other.
- 1 Short handed Spade.

The First Aid Outfit is marked "G.W.R. First Aid Outfit" and contains the following requisites:

- Card of Instructions.
- "First Aid in a Few Words."
- Bi-Carbonate of Soda (1 tin).
- Swansdown Bandage (1).
- Petroleum Jelly (1 tube).
- Splints (1 Set).
- Bandages (3 Roller, $\frac{1}{2}$ in. wide).
- " (2 Roller, 2 ins. wide).
- " (1 Roller, 3 ins. wide).
- " (1 Triangular).
- Bandage for Joints.
- Cotton Wool (2 packets).
- Cotton Wool (1 packet).
- Cotton Wool (1 packet).
- Form 342 Guards Report Form.

- Plaster (1 spool).
- Safety Pins (6).
- Tourniquet in Box (2).
- Antiseptic Tablets (12).
- Eye Loop.
- Scissors.
- Washing Bowl.
- Tincture of Iodine (2 ozs.).
- Measuring Glass (1).
- Smelling Salts (1 bottle).
- Castor Oil (1 bottle).
- Sal Volatile (4 tubes).
- Burn Dressings, No. 8 (2).

In wood
block

Use of Outfit.

The "First Aid" outfit is primarily intended for use in "Train Accidents," but may be utilised when occasion requires for any personal injury when Station or Depot appliances are not available.

How Sealed.

The lid of the Outfit is taped and sealed, but the tape may be broken in event of emergency as indicated above, when the contents will be obtainable.

Examination and Maintenance of Fire Extinguishers, Tool Cases and First Aid Outfits.

Appliances to be
in proper places
with seals intact.

5. The Chargehand Carriage Cleaners at the various depots will be held responsible for seeing that the Extinguishers, Tool Cases, and "First Aid" Outfits are in their proper places with the seals intact.

PASSENGER TRAIN INSTRUCTIONS.

EMERGENCY APPLIANCES, F&C — *Continued*.

Replenishment Cupboards Details of Stock to be held.

This form to be posted on the inner side of replenishment cupboard door.

Stock kept in this cupboard to be maintained as near as possible to the quantities stated below according to grade.

Excessive stocks must not be held

Replenishments to be ordered on form "5097."

Article	Denomination	Stock permitted at Stations.		
		Grade "A"	Grade "B"	Grade "C"
Antiseptic Tablets, tins of 12 each	B. boxes	6	3	2
Bandages, Roller, 1 in. by 1 yd.	Doz.	3 doz.	3 doz.	3 doz.
" " " " " "	"	4 "	2 "	2 "
" " " " " "	"	2 "	1 "	1 "
" " " " " "	"	12 "	6 "	6 "
" " " " " "	"	4 "	2 "	1 "
B. carbonate of Soda, 1 lb. tins	1 lb. tins	6	3	2
Black Wood " " " "	each	1	1	1
Boric Acid, 1/2 oz. packets	Doz.	2 doz.	1 doz.	1 doz.
" " " " " "	"	2 "	1 "	1 "
" " " " " "	each	10	6	3
B. pills, Washing Kidney	"	2	1	1
Boxes, Tourniquet	"	4	2	1
Card of Instructions	"	4	2	2
Castor Oil, 1/2 oz. bottles	"	6	3	2
Corks, I.R.	"	6	3	2
Cotton Wool, 1/2 oz. packets	"	24	12	6
Dressings, Burn, N.	Doz.	1 doz.	1 doz.	1 doz.
Eye loops (in envelope)	each	12	6	3
First Aid in a few words	"	4	2	2
Form 5042	"	24	12	6
Glasses, Measuring, graduated	"	4	2	1
Iodine, 2 oz. bottles	"	6	3	2
Mops, Cotton Wool, box of 6	Boxes	6	4	2
Petroleum, 1 lb. can, 1/2 oz. tubes	each	6	3	2
Plaster, 1/2 in. wide, 5 yds. on reel	Reels	6	3	2
Safety Pins, sets of 6	Sets	12 sets	6 sets	3 sets
Salt V. bottle, tea spoon of 1/2	each	30	24	12
S. S. S.	Para	4	2	1
Smelling Salts, 1 oz. bottles	each	6	4	2
Splints, sets of 5 each	Set	2 sets	1 set	1 set
Tourniquets	each	4	2	1
Tape	Y	6	3	2
Seals, Lead	each	50	50	50

Duties of Guards and Conductors.

6. Guards in charge of trains must satisfy themselves that the seals of the Extraordinary, Tool Cases and First Aid Kits are intact. In the case of Restaurant and Shopping Cars the Conductors will be responsible for examining the Extraordinary.

If a passenger complains of a defect in the carriage, the Guard or Conductor must report to the Carriage Examiner in charge.

A Guard or Conductor on discovering any of the appliances are damaged or missing, or the seal broken, or the stock missing must report the matter to the Carriage Examiner in charge at the station where the train is located, and the Carriage Examiner in charge must at once advise the Chargehand Carriage Cleaner.

In cases where a Guard does not proceed with the train to its destination he should report any defects, etc., to the Guard who receives him, and the latter will be responsible for advising the Carriage Examiner as shown above.

Any such report made by a Guard or Conductor must be recorded on his journal, and the Divisional Superintendent or District Traffic Manager in whose office the journal is filed must pass an extract of the report to the Divisional Superintendent. The latter must then report any special matters or defects in connection with fire appliances to the Chief Mechanical Engineer, Swindon.

(G.A.3 12/97. L.K. 147128)

PASSENGER TRAIN INSTRUCTIONS.

EMERGENCY APPLIANCES, ETC.—Continued.

In the event of the contents of a "First Aid" Outfit having been used the carrier must fill up one of the printed forms provided in each case, and have the form inside the case for the information of the Chargeman Carriage Cleaner at the depot at which the train terminates.

Use of First Aid Outfit.

7. If an Extinguisher is damaged or the green time seal broken or missing, it must be at once exchanged and the defective one retained in a crate provided to Fire Station, Leamington, for repair. Swinton, the Chargeman Carriage Cleaner at the depot at which the train terminates, must provide for this duty.

Replacing Extinguishers

Each of these depots will be supplied with a sufficient number of Extinguishers and must not be kept out of service by loss of seals and green tape for sealing Extinguishers in position when repairs are made.

Square Extinguishers Type Seals, etc.

On being advised that tools are missing or damaged, the Locomotive and Carriage Repairer must make arrangements for repairs to be sent from Swinton.

Replacing Tools

First Aid Cabinets in Trains.

Valuable losses from Accident Cabinets have been reported to date, and there is reason to believe that these are continuing. The following instructions are issued to ensure that the contents of these cabinets are not lost or damaged. When material is lost for any of the purposes mentioned in this sheet a report must be sent on the form provided in each case.

Station Masters should report all irregularities to the Divisional Superintendent or District Traffic Manager.

The co-operation of the staff is desired to prevent pilferages from the Cabinets.

GAS RINGS AND ELECTRIC HEATERS IN PASSENGER BRAKE VANS.

Guards and others concerned must see that waste paper, or other inflammable material, is not left on or near gas rings or electric heaters in passenger brake vans.

ELECTRICAL COMMUNICATION ON RAIL MOTOR CARS, DINING CARS, Etc.

The Chief Mechanical Engineer's Department is responsible for the maintenance of the electrical communication on all rolling stock fitted with electrical communication.

FAMILY, SALOON, AND INVALID CARRIAGES.—Pages 127 and 128.

These instructions are amended as follows:—

1. These vehicles are under the control of the Superintendent of the Line. Applications for their use are to be made through the Divisional Superintendent or District Traffic Manager and must be stated for whose use the coaches are required, the number of passengers, by what train, date of journey, and where it is to work from and to. Family carriages must not be supplied for composite parties of first and third class passengers attending race meetings, horse shows, and similar events.

2. When one of these vehicles is ordered to be sent away from a Depot for use at another station, it must be gassed, the tanks filled with water, and otherwise fully equipped ready for use; it must also be taken to destination with the special label provided for the purpose, and the Guard instructed as to where it is required. Should a vehicle arrive at a station not properly equipped, the circumstance must be reported to the Divisional Superintendent or District Traffic Manager.

3. As soon as the journey is completed the vehicle must be sent by Passenger or Parcel train, properly labelled, as ordered; until this can be done it must be kept under cover wherever possible.

4. In order that the best possible use may be made of such vehicles and to prevent their being detained when returning empty, a telegram must be sent to the Divisional Superintendent or District Traffic Manager stating the number and train by which any of these vehicles are being forwarded.

5. When it is necessary to send one of them to the shops for repairs, the Superintendent in whose Division the vehicle is stationed must advise the Superintendent of the Line. All concerned are requested to see that care is taken of these vehicles.

6. Applications on hand in Division or worked away to be shown on Daily Coaching Reports (Form 6379) sent by Divisional Superintendent or District Traffic Manager to Superintendent of the Line.

and of these vehicles.

PASSENGER TRAIN INSTRUCTIONS.

FAMILY, SALOON AND INVALID CARRIAGES *-Continued.***7. Securing Invalid Couches.**

The invalid couch must be suspended from the roof with the chains provided, the spring ends of which should be attached nearest the roof. The loose adjustable springs underneath must be coupled up to the couch and floor afterwards and adjusted to suit the weight of the patient. Particular care must be taken to see that the tension springs are correctly and evenly adjusted.

There are three of these adjusting springs at each end of the couch, one to take up the longitudinal movement, the other two to be crossed to prevent undue lateral sway.

When it is necessary to carry the couch the brass knobbed handles immediately underneath the suspension rings may be pulled out.

The bedding of the invalid couches, when not in use, is to be carefully stored in a dry place, and kept aired.

(C.A. 1. 3/37. R6/1618.A.)

HORSE AND CARRIAGE TRAFFIC.

A Circular relating to the conveyance of Horse and Carriage Traffic, and the services which are available for it is issued as necessary from time to time by the Superintendent of the Line, and the instructions contained therein must be carefully observed.

This Circular is not issued concurrently with each change of the public time tables, and copies of each issue must be retained until the next issue is circulated.

GENERAL INSTRUCTIONS TO BE OBSERVED IN CONNECTION WITH AUTO-CAR SERVICES.

Passengers alight and board car

1. Guards must use every care in getting passengers into and out of the cars quickly and safely. Special care and consideration must be paid to children and elderly or infirm passengers. Where it is necessary for passengers to enter cars at halts provided with raised platforms by the door at the engine end, the Fireman will assist by opening and closing the door. It will be arranged for passengers to enter and leave the cars by separate doors where the traffic is heavy, and in circumstances amounting to a congestion. Where necessary the Fireman will attend to the door at the Engine end. Passengers must not be allowed to travel on the platforms at the ends of the cars, or in the centre vestibule.

If necessary, passengers may be permitted to leave Cars through the door leading to the driver's compartment, provided that it is not opened until the car has come to a standstill at the platform.

Cars with Centre Vestibules

In the case of cars with centre vestibules, the door leading to the Driver's compartment must be locked, except at starting stations when it is found necessary to load the cars quickly.

Doors giving access to the luggage compartment must be kept normally locked. Guards must use all means provided for signalling to the Driver, by the means of the electric bell communication provided for the purpose, and not by hand signal. In case of failure of the bell communication, the Guard must use a green flag by day and green light by night, and must report to his Station Master that the bell communication has failed, the Station Master to immediately notify the Telegraph Lineman.

In the event of bell communication between the Guard and Driver not being available from any cause the Guard must ride in the vestibule portion on the Auto-Car, where the Driver's application valve is fixed, in order to be able to apply the brake in case of emergency. Before giving the Driver a signal to start the Guard must examine the brake application valve and satisfy himself that the cotter pins can easily be removed in case of necessity. The Guard must also see that the cotter pins are in proper position before he vacates the vestibule end.

Trailer Coach.

2. When an ordinary coach is attached to a car, passengers without tickets joining at halts must not be allowed to travel in the coach, provided there is room for such passengers to ride in the car, the object being to enable the Guard to issue tickets en route.

Where Guards are to ride.

3. On Services worked by cars with centre vestibules, Guards must ride in the centre vestibule.

Guards on Auto-Cars must not, in any case, ride in the same compartment as the Driver, and, unless other accommodation is provided, they must ride in the passenger compartment of the car.

Cars run light.

4. When cars are not being run as passenger trains, it will not be necessary to provide a Guard, but they will be treated as light engines. Except where there are instructions to the contrary, when a trailer or other vehicle is attached, it will be necessary to provide a Guard or other man to act as Guard, who must ride in the rear vehicle. It will not, however, be necessary to provide a Guard for an auto engine and one trailer without passengers.

PASSENGER TRAIN INSTRUCTIONS.

AUTO-CAR INSTRUCTIONS *Continued.*

5. Passengers without tickets joining the car at halts will be booked by the Guard after entering the car. Each ticket issued by the Guard must, before being handed to the passenger, be punched by a special punch opposite the name of the station (on the Up or Down side of the ticket, as the case may be) to which it is available. Special care must be exercised to punch the tickets correctly so that they may not be made use of beyond the proper station or halt. Ordinary card tickets (where in use) must be dated by the Guard before being handed to the passenger. **Tickets.**
- Tickets for stations must be collected from passengers by the ticket collecting staff at stations in the usual way, unless otherwise specified, but tickets issued for halts where there is no staff must be collected by each Guard when passengers leave the car. **Examination of tickets by Guard.**
- Tickets collected by Guards must be cancelled and placed in a box provided on the car. Unless instructions are given to the contrary, this box must be cleared after each journey by men appointed for the purpose. **Collected tickets**
6. Stock tickets for the use of Guards and men in charge of halts will be ordered and kept by Station Masters as required. Application must be made to the Station Master for a fresh supply in good time. Car tickets must be kept apart and recorded separately in the station stock ticket register. The Guards and men in charge of halts must keep a record of all tickets supplied to them. They must be careful to see that they have a sufficient number of tickets of each series ready for use, and will only issue tickets to places at which the car is booked to stop. **Stock tickets.**
Ordering tickets
7. A list of fares must be exhibited in the cars wherever tickets are issued on the cars. At halts where staff is employed, fare lists must be exhibited. **Fares and fare lists.**
8. Cash collected by the Guard must be paid at the stations to the ticket clerk. Each Guard will hand in form M17 with the cash, the form to be initialed by the person to whom it is paid. **Cash collected by Guards**
9. Guards must announce the names of stations and halts to the passengers in the cars in a CLEAR AND DISTINCT VOICE just before reaching the stations or halts. **Announcing names of stations and halts.**
- When a car and trailer or trailer coach, not connected by vestibuled gangways, are coupled together, and run in charge of one Guard, he must ride on the car and announce the name of the stopping place from the platform.
10. When staff is not employed at halts, Guards will be held responsible for dealing with passengers at those places. **Where no staff at halts.**
11. The lighting and extinguishing of lamps at halts must be carried out in accordance with instructions. **Lighting and extinguishing lamps at halts.**
12. District Inspectors must see that the platforms at the halts are kept clean by the Permanent Way staff, who will also clear snow from halt platforms and approaches whenever necessary. **Platforms at halts to be kept clean.**
Clearing halts of snow.
- Guards must report on their journals any omission in these respects.
13. The movable steps on cars are only to be used at halts not provided with raised platforms. **Movable steps at halts not provided with raised platform.**
- The steps at the engine end must not be used, neither must the doors at this end be used for the purpose of allowing passengers to enter or leave the cars. Care must be taken to see that these doors are kept locked.
- After the car has stopped at a halt not provided with a raised platform, the step must be released, the steps put into position, and securely fastened by key pin or spring catch (whichever is provided) and the door then opened. The door must not be opened until this has been done. **Manipulation of movable steps.**
- Before giving the signal to the Driver to start, the Guard must close the door and then withdraw the steps, and be careful to see they are securely fastened in running position.
14. Guards must see that the cars are lighted as, and when, required. **Lighting of cars.**
- When there are long intervals between trips, the gas must, whenever possible, be turned down low, and turned up again in reasonable time, and the Guard will be held responsible for this.
15. The Driver will be responsible for changing the lamp at the leading end of the car, and the Guard for the lamp at the rear, except when an Auto engine is at the rear, in which case the tail lamp will be fixed by the fireman. **Lamps.**

Tail lamp to be carried on stand at bottom of chimney during the time the lamp is not lighted and in the centre of buffer plank when alighting.

PASSENGER TRAIN INSTRUCTIONS.

AUTO-CAR INSTRUCTIONS—Continued.

Fireman to deal
with train staff,
etc., on some
lines.

16. On Single lines the Driver must see the Electric token, or staff in all cases before proceeding into the onward section, but the Fireman will be held responsible for picking up and setting down the electric token, staff or ticket in the case may be, and in those instances where the Driver is travelling alone in the leading compartment the Fireman, each time he takes or gives up the electric token, staff or ticket, must sound the engine whistle so as to make the Driver aware that he has done so. If the Driver does not hear the whistle sounded he must give four rings on the electric bell, and immediately stop the car to ascertain what is wrong.

Where electric bell communication is not available, the Driver must give four short whistles.

Destination or
Route Boards.

17. Guards must see that the proper Destination or Route Boards are exhibited and placed in the proper position on the cars.

Cleaning cars and
train.

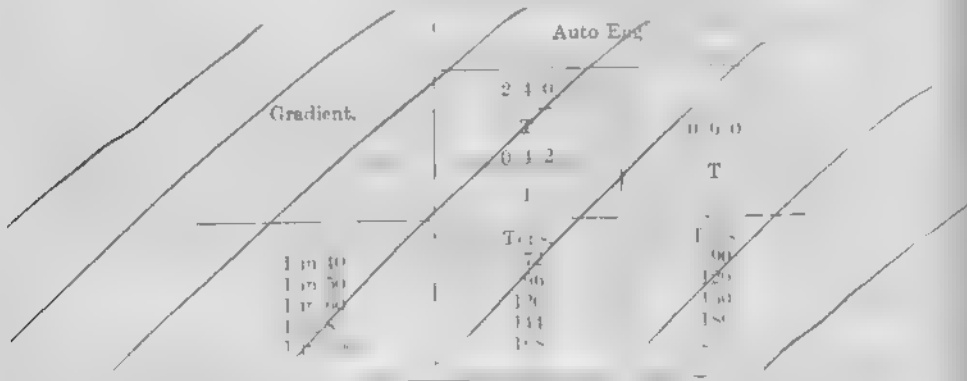
18. The cars must be well cleaned daily. This will be done by the Locomotive Department, but the Station Master at the terminal stations must see that the cars to be swept out as often as is necessary to keep them thoroughly clean during the time they are in service. The Guards to report on their journals when this is not done.

Whenever possible the Ticket Inspectors must examine the tickets of the passengers in the cars.

Brake and Locomotive
Services with
Auto-cars.

19. Whenever it is necessary to attach auto-cars to trains on trips when the auto-car would be hauled by the Auto-Engine, the number of cars to be attached in front, but may be allowed to propel not more than two vehicles provided the regulator is capable of moving the cars forward and backward. It must be remembered also, provided the wheels, during these periods, would make up a total of 100 wheels below:

The maximum loads of Auto-Cars can be computed on a tonnage basis as under:



The vehicles attached to Auto Engines should be calculated for maximum load purposes at their tare weight, plus the usual allowances when conveying milk, fish, parcels, etc.

The following diagram will illustrate what is meant:—

ORDINARY VEHICLES (Drawn).

(PROPELLED).

Driver to ride in
front at ♦ when
Auto-car is pro-
pelled.



An Auto-Engine may be uncoupled for shunting purposes when necessary.

When the wheels behind the auto-engine exceed the number shown in the Table on page 109 a brake vehicle must be placed at the rear with a Guard in it.

GENERAL
CAR
The

GENERAL INSTRUCTIONS TO BE OBSERVED IN CONNECTION WITH AUTO-CAR SERVICES.—Pages 128-131.

The following to be substituted for the table shown in paragraph 19 on page 130 —

Gradient	Auto Engines	
	2-4-0	2-6-2
	$\frac{1}{T}$	$\frac{1}{T}$
	0-4-2	0-6-0
	$\frac{1}{T}$	$\frac{1}{T}$
	Tons	Tons
1 in 40	72	90
1 in 50	96	120
1 in 60	120	150
1 in 80	144	180
1 in 100	168	210

(G.A.30 Op.—9/54 T.D./P.R.P.)

PASSENGER TRAIN INSTRUCTIONS.

AUTO-CAR INSTRUCTIONS—Continued.

20. It often becomes necessary for auto-trains with auto-cars attached to take water when the auto-car is being prepared, and so it is that the auto-engine is drawn up opposite the water tank, the auto-car standing in advance of a stop signal and fouling other lines or crossings.

In such cases it is to be distinctly understood that the auto-car must not be propelled past the stop signal until the signal is lowered, and the authority of the Signalman, or the lowering of the stop signal, and the last water tank has been cleared. The Driver must, on receipt of a hand signal from the Guard or other authorised person, set back with all vehicles clear of the stop signal and when he has done so, whistle three times as an intimation to the Signalman that the vehicles are all inside the signal unless he receives instructions to proceed on his journey and the signals are right for him to do so. When a signal is lowered for this purpose it must not be replaced at "Danger" until the Driver has whistled to signify all vehicles have been set back clear.

Signalmen must be careful not to accept trains on lines which are liable to become fouled by shunting operations. If this is the case, they have satisfied themselves the vehicles have been drawn to the rear of the stop signal, or have proceeded on their journey.

21. When auto-trains with auto-cars attached are working as passenger trains, the Driver must always work the engine at the leading end. When any other vehicles are added to a train, and there are no passengers on the cars, it will not be necessary for the Driver to be at the leading end. The Driver must be in the leading end of the leading vehicle, keeping a good look-out and prepared to hand signal to the Driver. At places where the shunting operation involves several reversing movements and difficulty or delay would result if the Guard or Shunter had to be present, special instructions will be issued.

INSTRUCTIONS TO BE OBSERVED BY DRIVERS AND FIREMEN IN WORKING AUTO TRAINS WHEN THE DRIVER IS DRIVING FROM THE VESTIBULE END AND THE FIREMAN REMAINS ON THE FOOTPLATE.

It is essential that there should be a proper understanding between the Driver and Fireman as to the working of the engine.

The Driver must satisfy himself that the Fireman properly understands the working of the reversing gear, and the brake lever, and the action of the fire and boiler.

On receipt of a signal to start from the Guard, the Driver must sound the whistle which the Fireman must acknowledge, and the Driver must not start until the Fireman has acknowledged his signal. Before reversing the Driver's signal, the Fireman must satisfy himself that the brake is off and that the reversing lever is in the correct position.

When approaching signals or terminal stations, the Fireman must be on the look out and be prepared to act in case of emergency from any cause.

If a Fireman discovers any fault in the working of the engine he must inform the Driver at the next stopping place, but it is to be understood that it is necessary to stop before reaching the stopping place the Fireman must be informed of the fault by applying the reversing brake.

The Fireman must not leave the footplate or the engine-room without the consent of the Driver.

The following is the code of hand signals between the Driver and Fireman and Guard.—

1 ring	..	..	..	To start.
2 rings	..	..	..	Fireman to blow brake off.
3 rings	..	..	..	To stop.

PASSENGER TRAIN INSTRUCTIONS.

WORKING OF AUTO TRAINS ON BRANCH LINES, WITHOUT A GUARD.

On certain Branch Lines, where specially authorised, a Guard will not be employed on auto trains provided the number of coaches on the train does not exceed three vacuum fitted vehicles two of which must be fully vacuum brake fitted and one vehicle may be piped only.

When auto trains are being worked without a Guard the following General Rules and Regulations are modified as shewn below:—

Rules 120 and 129. The Station Master, or other authorised person, must see that the tail lamp is in position and burning properly when necessary before the train leaves the starting point.

Rule 141 (b).—The signal for starting will be given by the Station Master or other authorised person.

Rule 141 (e).—In the event of the train being stopped by accident, or other exceptional cause, the Driver must satisfy himself that it is in order before again proceeding with the train.

Rule 179.—In the event of an accident, or failure, the train must be considered as coming within the category of a light engine.

Regulations for Working the Vacuum Brake.

Clause 3.—General Appendix, page 98.

The duty of ascertaining whether the brake is properly coupled up and in working order throughout the train, and between the vehicles and the engine or rail motor, and of testing the brake, must be performed by the Station Master, or other authorised person.

Regulations for Working Chain Communication between Passengers and Guard and Driver by means of the Vacuum Brake.

The duties devolving on the Guard as shown in the instructions under the above heading on page 101 of the General Appendix must be carried out by the Fireman.

Conveyance of Mails, etc.

Mails, parcels and miscellaneous traffic must be loaded in the luggage compartment.

Diesel

GA19

WORKING OF ~~STANDARD~~ ~~TRAIL~~ CARS.

The instructions under this heading on page 128 to 131 are to be cancelled and the following substituted:—

In connection with the running of ~~Standard~~ ~~Trail~~ cars the instructions for Auto Car Services shewn on pages 128 to 131 must be observed except as varied below:—

Clause 1. (Fifth paragraph.)

Guards must in all cases give the signal to the Driver to start by means of the bell communication provided for the purpose, and not by ear signal. In case of failure of the bell communication, the Guard must give a verbal message and must repeat it to the Station Master that the bell communication has failed, the Station Master to satisfy the person who is in charge of the Bell communication on Auto Trains, Dining Cars, etc., in the district. (Guard should not speak to the Driver.)

Clause 4.

A Guard or other man to act as Guard must always be provided for standard trail cars.

Clause 16.—Exchange of Electric Train Token.

As the Driver is unable to exchange the train token whilst the car is in motion, it must be brought to a stand at the Signal Box or other convenient point, and the Signaller must get the car and exchange the token with the Driver.

When issuing a token for the car the loop carrier must not be used.

Clause 21.

When any movement with passengers in the car is required, the Driver must always be at the leading end.

When there are no passengers in the car and bell signals can be given in the rear driving compartment by the Guard or Shunter from the leading end, the shunting movement may be performed with the Driver at the rear end, but the Guard or Shunter must at the leading end keep a sharp look-out and be prepared to signal to the Driver by means of the bell push provided for the purpose.

The bell codes to be used are:—

"Go ahead"	1 ring
"Set back"	2 rings.
"Stop"	3 rings.

In the event of the bell communication failing the Driver must ride at the leading end.

Rule 55. Diesel

~~Standard~~ ~~trail~~ cars must not be relied upon to operate track circuits and Guards or Shunters will always be held responsible for carrying out Rule 55 in regard to reminding the Signaller of the presence of the car where it is stopped at signals, irrespective of whether the line is track circuited or not. The car must not be allowed to draw up to Advanced Starting points or Starting signals in advanced positions to await acceptance from the Box in advance, but must be brought to a stand as near the Signal Box as practicable well clear of junction points and within the protection of the Home signal as soon as possible.

Clause (c).

The duty of going to the Signal Box must be performed by the Guard or Shunter, and to enable this to be done a Guard or Shunter must be provided for standard trail cars.

Rule 55. Diesel

~~Streamlined~~ rail cars must not be relied upon to operate track circuits, and Guards or Shunters will always be held responsible for carrying out Rule 57 in regard to reminding the Signman of the presence of the car where it is stopped at signals, irrespective of whether the line is track circuited or not. The car must not be allowed to draw up to Advanced starting signals or starting signals in advanced positions to await acceptance from the Box in advance, but must be brought to a stand as near the Signal Box as practicable well clear of Junction points and within the protection of the Home signal as soon as possible.

Clause (c).

The duty of going to the Signal Box must be performed by the Guard or Shunter, and it is enable this to be done a Guard or Shunter must always accompany the car.

Use of "Vehicle on Line" Switch.

In the event of the ~~streamlined~~ ^{Diesel} car having to stand in a particular line where a "Vehicle on Line" switch is provided it must be protected by means of the switch and the Station Master concerned will be responsible for seeing that this is done.

Instructions to Signmen.

~~Streamlined~~ rail cars must be brought within the protection of the Home signal as soon as possible. At Signal Boxes where additional Home signals are provided, the Signman must not allow the Diesel to pass the Home signal to the train ahead of the car, but must wait until the car has passed the Inner Home signal. ~~Streamlined~~ rail cars must not be allowed to pass the furthest Home signal except in case of emergency.

Where Intermittent Flash signals are fitted for the Signal Box, the car must not be allowed to leave the Signal Box in rear until the Flash signal has been cleared by the Signman at the Box in accordance with the instructions of the Signman. The car must not be allowed to pass the flashing signal until the flashing signal has been cleared for it.

Trains Stopped by Accident, Failure or Obstruction or Other Exceptional Cause Rules 179 to 181.

The car must be dealt with in accordance with the Rules and where necessary the protection usually performed by the train must be carried out by the Driver. Clauses (g) and (h) of Rule 179 and (h) of Rule 181 will apply to streamlined rail cars.

Should it be necessary for the Guard or Driver to leave the car unattended, the Driver, before leaving the car, must stop the engine and apply both hand brakes. In failing gradients the forward gear must be engaged in reverse, and select and engage bottom gear. On rising gradients the forward gear must be engaged. The gear selector lever must be locked up by the Driver in the train box. All doors giving access to the Driver's compartment must be locked before the Driver leaves the car to carry out the provisions of these Rules.

Driving Compartments.

No one is allowed to ride with the Driver in the leading driving compartment of a streamlined car unless he is in possession of an engine pass.

No one but an official of the Company holding an engine pass or a guard in the performance of his duty is allowed to travel in the rear driving compartment.

When it is necessary for anyone to travel in this compartment, care must be taken not to interfere with any of the controls.

The following to be added after the word "run" in the two of the last paragraph of these instructions (See G.A.25)

or if a Diesel Car is required to work in a service which is normally scheduled to be worked by a train or auto-car on a route over which Diesel Cars are authorised to be worked.

(G.A.30 Op.—9/54 LK1/8847/372)

The following paragraph to be added to the end of these instructions :—

It is absolutely necessary for a Diesel car to work over a section of line where it is not normally scheduled to run, and a printed or stenciled notice cannot be issued in sufficient time to ensure that all concerned are given notice. The Drivers of such Diesel cars must be notified of the circumstances and must then sound the horns when entering and emerging from tunnels, also when approaching curves, level crossings, narrow crossings, overbridges, gangers' huts and other buildings adjacent to the line upon which the car is run.

(G.A.25—1/50. L.K.1/8847/40.)

The instructions under this heading to be amended to read—"**WORKING OF DIESEL CARS.**"

The reference in these instructions to "Streamlined Rail Car" to read "Diesel Car" throughout.

The following to be added at the end of the instructions :—

When a Diesel car is required to work over a section of line where it is not normally scheduled to run, prior advice must be issued to all concerned, including the staff of other Departments e.g. Permanent Way men, of the intention to make such movement.

When scheduled Diesel car working has been suspended for a short period, i.e., not exceeding seven days, in connection with repairs, etc., a train or Auto service being substituted temporarily, it will not be necessary for a special advice of resumption of Diesel car working to be issued to all concerned.

When scheduled Diesel car working has been suspended for a period in excess of seven days, a notice to all concerned must be issued before Diesel car working can be resumed.

(G.A.19—10/48. L.K.1/8847/40.)

WORKING OF DIESEL CARS.—Page 132.

The following to be added at the end of the second paragraph under the heading "Instructions to Signalmen." :—

"The Train Entering Section" signal for the Diesel Car must be sent when the car leaves the signal box controlling the Intermediate Block Signal.

(G.A.31—7/56 L.K.1/12510/372)

Diesel
WORKING OF ~~STRAIGHT~~ RAIL CARS. page 132.

The following to be inserted last in a diagram under heading "Head and Tail Lamps"

As a condition precedent to the use of the Diesel engine in a rail car, it is necessary that the car be equipped with a current transformer and a current meter. The current transformer should be connected with a current transformer having a ratio of 100 to 1. The current meter should be connected with the current transformer so that the current is properly transformed.
(G A 1211 and L. K. 1854 H.)

WORKING OF DIESEL CARS—Page 132.

The following additional paragraph to be inserted after the instructions headed "Use of emergency coupling—Cars 1 to 17 inclusive":—

Tail Traffic.

Tail traffic may only be hauled by Diesel Cars numbered 18 upwards.

The maximum tail load in all circumstances must not exceed 60 tons. In the case of the Twin Diesel Units formed with an intermediate coach, the maximum tail tonnage must not exceed 30 tons.

The tail load must be restricted to 30 tons where there is a rising gradient of 1 in 60 or steeper, except where the restriction is lifted for Engineering Department tunnel inspections when the restriction applies on rising gradients of 1 in 40 or steeper.

When steam heating is used not more than one passenger carrying vehicle may be attached to the Diesel Car or Twin Diesel Units.

(G.A.30 Op.—9/54 LK1/12009/372 T.D.288)

The following to be inserted at the end of the clause headed "Instructions to Guards".—

All passenger Diesel Cars with the exception of Diesel Car No. 1, may carry additional passengers up to a maximum of one-third of the normal seating capacity.

Exceptions :

- (a) The normal seating of Diesel Car No. 1 (69 passengers) must not be exceeded.
- (b) The loading of Diesel Cars passing through the Severn Tunnel must not exceed the seating capacity.
- (c) The maximum permissible loads in Luggage compartments of Diesel Cars must not exceed the maximum indicated on the Notice exhibited in the Car.

(G.A.30 Op.—9/54 T.39,119 G/1)

~~The following additional paragraph to be inserted on page 133.—~~

~~Working over Permissive Lines.~~

~~When empty Diesel Cars are required to be worked over permissive lines the following instructions must be observed~~

~~Trains which are admitted to a permissive line immediately following a Diesel Car must be brought to a stand at the Signal Box in accordance with clause 9 of the Permissive Block Regulations and the Driver being told that a Diesel Car is in the section ahead. The Driver of a train which is so admitted must proceed with caution and must take care not to suffer up to the Diesel Car.~~

~~(G.A.3.—12/57 O.M.1.1)~~

PASSENGER TRAIN INSTRUCTIONS.

WORKING OF STREAMLINED CARS *Continued.***Failure of Car on a Single Line.—Electric Train Token Regulation 14.**

In the event of a streamlined rail car becoming disabled on a section of a single line between two Token Stations the Driver must take the token to the nearer token station for the purpose of obtaining assistance and as he proceeds place detonators on the rail as laid down in Rule 179, the Guard must proceed in the opposite direction, protecting his train as directed in this Rule.

Use of Emergency Coupling, Cars 1 to 17 inclusive.

No additional vehicle can be attached to these cars.

A special coupling is carried in the above cars, for use in emergency only, by means of which the car can be pulled or propelled by an engine. Whenever the car has to be pulled or propelled the special coupling must be used.

The special emergency coupling must not be used except for the purpose of hauling or propelling the loaded car to the first available station, at which it can be shunted clear of the running line. If, however, serious delay or inconvenience to passengers would result from the car being shunted clear at the first available station which might be avoided by hauling or propelling the car to another station reasonably near, this may be done.

When the special coupling is used the streamlined rail car may be drawn by any type of engine, tender or coach and, where the line is straight this may be done at normal speed. On sharp curves the speed must not exceed 5 m.p.h. Propelling movements must be made very carefully and must not, in any case, exceed 5 m.p.h.

Head and Tail Lamps.

The car will not carry the standard G.W. head lamps but the standard tail lamp will be carried.

After sunset or during darkness or when proceeding through a tunnel, the tail lamp must be alight, and the two white lights at each end of the car must be exhibited to the leading end of the train. When A head lamps and B tail lamp are used, the white light at top centre will be used.

The switching on of head lamps after sunset during fog or falling snow or before passing through tunnels, must be performed by the Driver.

The Guard or man acting as Guard will be responsible for seeing that the tail lamp is placed in position and lighted when necessary.

Instructions to Guards.

The "right-away" signal must invariably be given by means of the vestibule bell and must not be given until all doors are closed and securely fastened.

The doors next to the adjoining running line must not be used except under unavoidable circumstances, and in such cases special care must be exercised to see that no other train is approaching.

The number of passengers in and out of each station must be recorded on the Guard's journal.

Warning of the Approach of Car.

These cars are fitted with two pairs of horns at each end, the two horns of each pair sounding notes of different tones, which are operated alternately. The warning is distinctive and not liable to be mistaken for that given by a road vehicle.

All concerned must warn men employed under their supervision who may be required to work on the permanent way to walk upon or cross running lines of the importance of observing the warning and that they must be prepared for the car to approach quietly and at high speed. Upon hearing the warning the Driver should be given an acknowledgment whenever possible.

It is important that men engaged on permanent way work, etc., shall move promptly to a point of safety upon sighting or receiving audible warning of the approach of the car.

Working Over Permissive Lines.

Trains which are admitted to a permissive line immediately following a streamlined rail car must be brought to a stand at the Signal Box in accordance with clause 9 of the Permissive Block Regulations and the Driver verbally told that a streamlined rail car is in the section ahead. The Driver of a train which is so admitted must proceed with caution and must take care not to buffer up to the streamlined rail car.

Automatic Train Control Apparatus.

Streamlined rail cars are equipped with A.T.C. apparatus at each end. Before leaving one end to drive from the other the Driver must take the apparatus out of use by operating the lever provided. The movement of this lever indicates that the apparatus is both the battery and the brake system puts the vacuum gauge in that vestibule out of use, and changes the "Not in use" flag into view. Upon entering the vestibule at the other end, the Driver must bring the A.T.C. apparatus into use and until this duty has been performed no reading will be obtainable on the vacuum gauge in that vestibule.

Gas and Water.

Gas will be required for the buffet car and a supply of water for lavatories and buffet where provided.

(G.A. 5—2/39. L.K. 14839 41.)

PASSENGER TRAIN INSTRUCTIONS.

WORKING OF STREAMLINED CARS—Continued.

Automatic Train Control Apparatus.

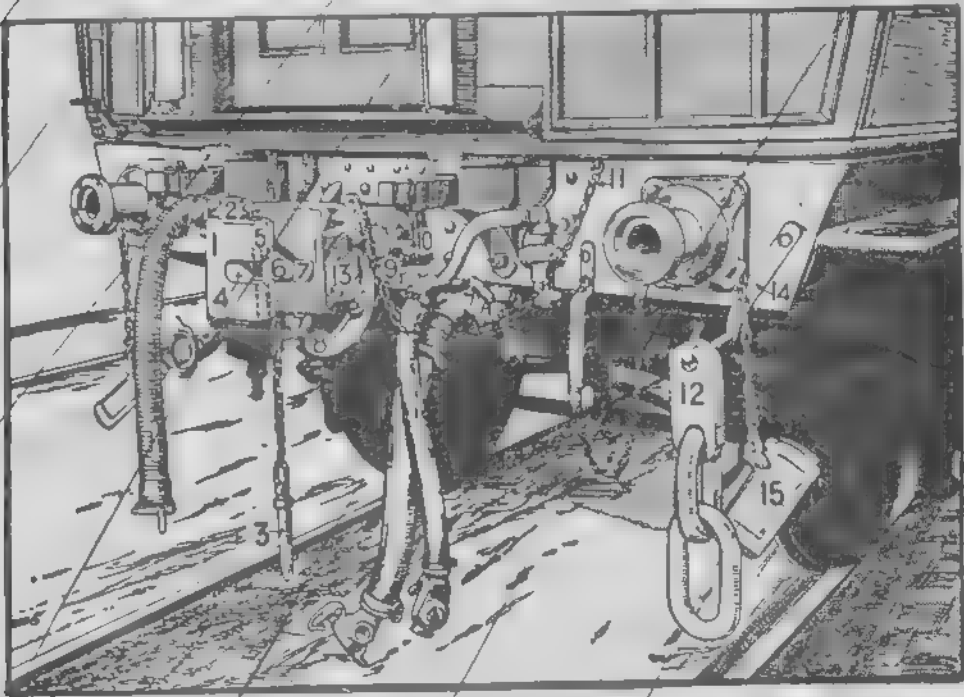
Streamlined Passenger Cars are equipped with A.T.C. apparatus at each end. After a car has been supplied for use, the A.T.C. is set in the reservoir in the car body with the A.T.C. handle in the "Set" position. The A.T.C. handle is set in the "Set" position by the driver to start a new run. The A.T.C. handle is set in the "Set" position by the driver to start a new run. The A.T.C. handle is set in the "Set" position by the driver to start a new run.

Gas and Water.

Gas will be required for the Buffet Car and a supply of water for lavatories and buffet when provided.

AUTOMATIC COUPLERS.

1. The following instructions apply to vehicles fitted with automatic couplers and Pullman type ganeways.
2. The automatic coupler and auxiliary fittings are shown in the illustration below:—



- | | | |
|--|---|---|
| <ol style="list-style-type: none"> 1. Knuckle 2. Knuckle Pin. 3. Emergency Coupling Pin. 4. Slot for Emergency Coupling. 5. Head in Knuckle for Emergency Coupling Pin. | <ol style="list-style-type: none"> 6. Uncoupling Lever. 7. Coupler Support Pin. 8. Pin in Head. 9. Head in Head. 10. Head in Head. | <ol style="list-style-type: none"> 11. Uncoupling Chain. 12. Emergency Tank Coupling. 13. Coupler Head. 14. Head in Head. 15. Buffer Saddle. |
|--|---|---|

3. The vehicles are fitted with movable buffers instead of those of the ordinary pattern. These buffers must be placed in the "Short" position when the Automatic Coupler is used and in the "Long" position when attaching a coach fitted with the ordinary screw coupler to Automatic Coupled stock.

4. Changing Position of Movable Buffers.

To change the side buffers from the "Short" to the "Long" position, pull the buffers out as far as they will go and then pull the buffer head out as far as it will go, taking care to see that the saddle enters the buffer head and that the ends of the saddle are placed towards the buffer sockets.

The instructions on pages 134 to 136 under headings **AUTOMATIC COUPLERS** and **METHOD OF USING EMERGENCY COUPLINGS** to be deleted and the following substituted —

"BUCKEYE" AUTOMATIC COUPLERS AND PULLMAN GANGWAYS.

In dealing with vehicles fitted with "Buckeye" automatic couplers and Pullman gangways the following instructions must be observed. The illustration will provide a convenient means of reference to the various parts mentioned in the instructions.

WARNING.—When coupling together vehicles fitted with automatic couplers, or coupling vehicles so fitted to an ordinary carriage or engine, the staff employed must not in any circumstances stand between the vehicles, but must wait until they have been brought together before passing under the buffers to connect the brake pipes, etc.

On ex L & NE "Buckeye" fitted stock the emergency coupling pin, emergency link coupling and buffer saddle, items 3, 12 and 15 respectively are fixed on the headstock to prevent rattling, as shown in the illustration and are not allowed to hang. An emergency screw coupling is carried in each guard's compartment.

On ex S.R. standard "Buckeye" fitted stock emergency link couplings are not used and an emergency screw coupling is carried in each guard's compartment.

On B.R. standard carriages fitted with "Buckeye" couplings, emergency link couplings are not used and an emergency screw coupling is carried at one end of each coach fixed to the underframe headstock. In addition, an emergency screw coupling is carried in each guard's compartment.

(1) Changing Positions of Movable Buffers.

(a) TO CHANGE THE SIDE BUFFERS FROM THE "SHORT" TO THE "LONG" POSITION, pull the buffers out as far as they will come and place the saddles flat on top of the buffer spindles, taking care to see that the saddle enters the grooves of the buffer sleeve and that the lips of the saddle are placed towards the buffer sockets.

(b) TO CHANGE THE SIDE BUFFERS FROM THE "LONG" TO THE "SHORT" POSITION, remove the saddles and push the buffers back as far as they will go.

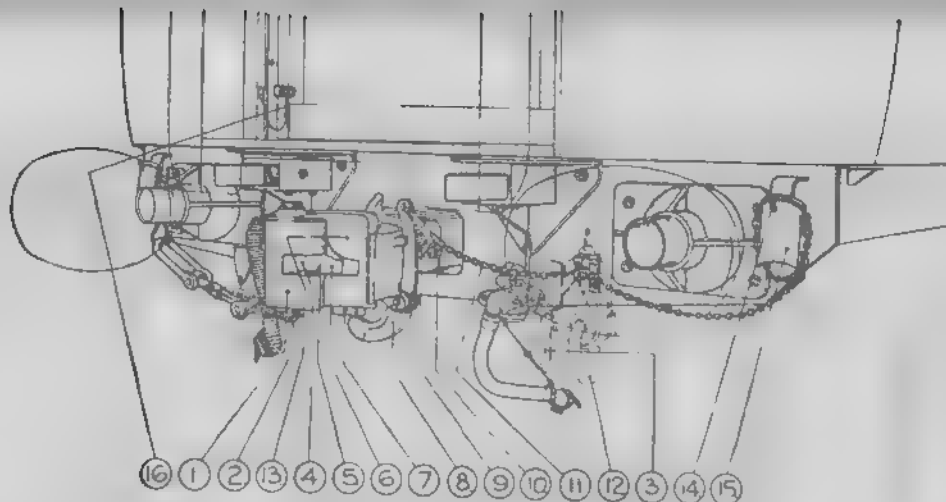
(c) When not in use, the saddles must be hung on the hooks provided for them on the headstocks.

(2) Coupling Instructions.

(a) WHEN THE VEHICLES TO BE COUPLED TOGETHER ARE BOTH FITTED WITH THE AUTOMATIC COUPLER, the procedure is as follows:—

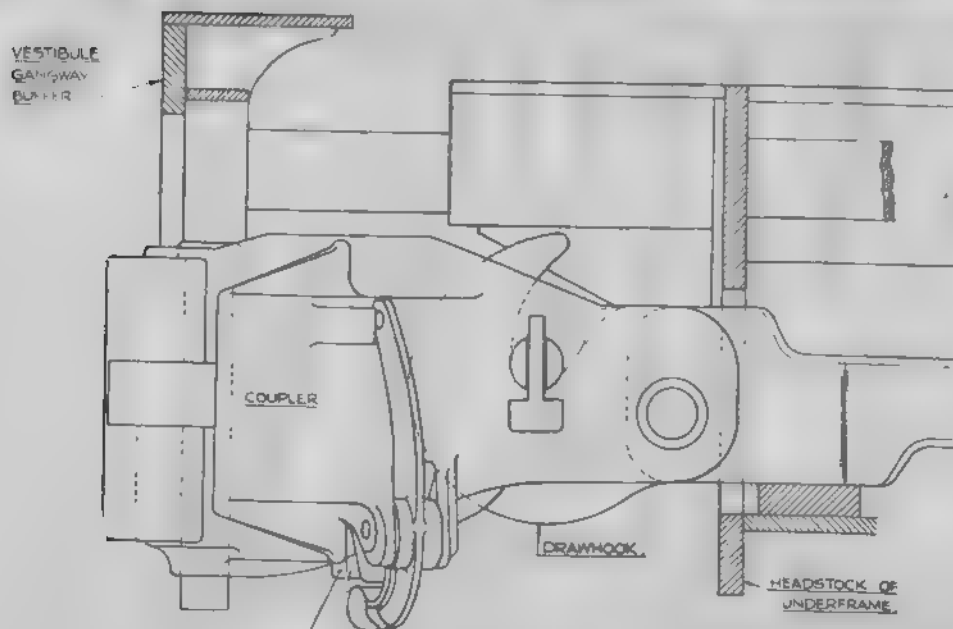
The gangway shields must be off and the buffers in the "short" position.

Each coupler head must then be secured in the position shown in the illustration by withdrawing the support pin, lifting the coupler head as high as it will go, and replacing the support pin. The utmost care must be taken to ensure that the TAIL-PIECE of the pin falls downwards.



- 1 KNUCKLE
- 2 KNUCKLE PIN
- 3 EMERGENCY COUPLING PIN
- 4 SLOT FOR EMERGENCY COUPLING
- 5 HOLES IN KNUCKLE FOR EMERGENCY COUPLING PIN
- 6 KNUCKLE TONGUE
- 7 VERTICAL LOCK,

- 8 UNCOUPLING LEVER
- 9 COUPLER SUPPORT PIN
- 10 PIVOT PIN
- 11 UNCOUPLING CHAIN
- 12 EMERGENCY LINK COUPLING
- 13 COUPLER HEAD
- 14 HEADSTOCK HOOK
- 15 BUFFER SADDLE
- 16 FRENCH PIN,



NOTE THE LOCK BOLT PROJECTS BELOW
BOTTOM OF THE COUPLER

The knuckle of the coupler head of ONE of the vehicles must then be opened by pulling the uncoupling chain, which operates the lock.

The knuckle of the coupler of the OTHER coach should be kept closed, EXCEPT WHEN THE COUPLING IS DONE ON A CURVE, in which case it may be necessary to open the knuckle of both couplers.

Automatic couplers will NOT engage on ACUTE curves.

The stationary vehicle must have the brake hard on or be prevented otherwise from moving.

The vehicle to be attached should be brought up steadily against the stationary vehicle and pressed against it without shock, until the gangways are compressed sufficiently to allow the coupler knuckle to close and the lock to drop.

Staff must satisfy themselves that the couplers have engaged properly by looking or feeling underneath to make certain that the vertical lock on each coupler is projecting below the coupler head, and that the knuckles of the coupler are clasp ing each other. In addition it is necessary to make a test by a slight pull from the engine. //

The brake, and, when necessary, the heating pipes, bell and lighting cables, GANGWAY CURTAINS (or wind guards), must then be connected.

(b, WHEN COUPLING A VEHICLE FITTED WITH SCREW COUPLINGS OR AN ENGINE, TO A VEHICLE FITTED WITH AUTOMATIC COUPLERS, first place the buffers of the vehicle which is fitted with the automatic couplers in the "long" position, then slightly lift the coupler head, withdraw the coupler support pin, lower the coupler head, and replace the support pin in the coupler head, taking care to see that the tail-piece of the support pin is turned properly backwards. Place the screw coupling shackle on the "Buckeye" hook and screw up in the usual way.

(3) Uncoupling Instructions.

Before uncoupling two vehicles fitted with automatic couplers, the gangway curtains (or wind guards) and electric bell couplings inside the gangways must be disconnected, the brake and heating pipes and electric cables must also be uncoupled.

The driver should then be signalled to set back slightly and the uncoupling chain firmly pulled and held. This will release the lock and allow the knuckle to open when the engine draws ahead.

After the coupling has been disengaged the vehicles should be drawn a few yards apart sufficiently clear to allow of a safe working margin.

When the necessary gap between the vehicles has been made the shunter must exhibit a hand danger signal to the driver to indicate that he wishes to proceed between the vehicles and he must obtain an acknowledgment of this hand signal working in close co-operation with the Driver before going between the vehicles.

If, in the operation of dividing, there is any easing backwards or forwards, no attempt must be made to prevent the "Buckeye" couplers from re-engaging, and the train must again be divided.

(4) General Instructions.

(a) The vehicles must not be fly-shunted, and they must not be bumped against each other or against buffer stops, as this causes damage to the gangways.

(b, WHEN VEHICLES FITTED WITH AUTOMATIC COUPLERS ARE BEING SHUNTED AGAINST STOCK FITTED WITH SCREW COUPLINGS, OR ARE LEFT IN A POSITION WHERE STOCK FITTED WITH SCREW COUPLINGS COULD BE SHUNTED AGAINST THEM, THE SADDLES MUST BE ON THE BUFFERS, AND THE COUPLER HEAD IN THE "DOWN" POSITION.

(c, WHEN A VEHICLE FITTED WITH AUTOMATIC COUPLERS BECOMES THE REAR VEHICLE OF A TRAIN, THE SADDLES MUST BE PLACED ON THE REAR BUFFERS AND THE REAR COUPLER HEAD MUST BE IN THE "DOWN" POSITION.

(d) in cases where vehicles fitted with automatic couplers are being shunted for connecting purposes to a vehicle similarly fitted, the saddles must be off the buffers, and the coupler head in the "up" position.

(e) In the event of a buffer sticking on a vehicle fitted with automatic couplers or the couplers falling, the carriage examiner's attention should be called immediately to the matter. All cases of difficulty must be reported to the C & W Engineer's Outdoor Assistant by the C. & W. Department Inspector.

5) Emergency Screw and Emergency Link Couplings.

An emergency screw coupling is carried in the guard's compartment of all brake vehicles fitted with automatic "Buckeye" couplers, and on ex L. & N.E. vehicles an emergency link coupling is carried on the headstock at one end of all the vehicles fitted with automatic "Buckeye" couplers.

Ex S.R. standard vehicles do not carry an emergency link coupling.

All B.R. standard vehicles fitted with automatic "Buckeye" couplers carry an emergency screw coupling on one headstock in addition to the emergency screw coupling carried in the guard's compartment of brake vehicles.

When, from any cause, the "Buckeye" couplers cannot be used, the emergency SCREW coupling must be utilised whenever possible, and screwed up tightly. The gangway doors must be locked.

IN THE EVENT OF TWO VEHICLES OR VEHICLE AND ENGINE FITTED WITH THE "BUCKEYE" COUPLER BECOMING UNCOUPLED, ON NO ACCOUNT MUST ANY ATTEMPT BE MADE TO COUPLE UP AGAIN WITH THE "BUCKEYE" COUPLER.

When the emergency SCREW coupling is used, due to mishaps as detailed below, the station where it is put on the carriage must advise, by wire or telephone, the destination station where the defective vehicle will be taken out of the train and the staff at the latter point must replace the emergency SCREW coupling in the brake compartment or on the headstock of the vehicle to which it belongs.

METHOD OF USING EMERGENCY COUPLINGS.

Nature of Mishap.	Coaches to be joined.	Emergency Coupling to be used. -	Method of Coupling.
One or both couplers defective and draw-hook broken.	One ex L. & N.E. and any other coach fitted with "Buckeye" Couplers.	Ex L. & N.E. LINK Coupling (to withdraw coach with broken hook from train).	<p>If sufficient hook is left on the vehicle with the broken draw-hook to hold the coupler in the "Up" position, leave the coupler in that position and carry out the following operations —</p> <p>EXTEND SIDE BUFFERS ON BOTH VEHICLES.</p> <p>Place the oval end of the emergency link coupling on the sound draw-hook. Place the flat end of emergency link into the slot of vehicle with the broken draw-hook (the slot referred to is shown by figure 4 on the illustration). The hole in the flat link must be in line with the hole which runs through the coupler knuckle and when the emergency coupling pin (No. 3 on illustration) is inserted through this hole the operation of joining the two vehicles is then complete.</p> <p>LOCK THE GANGWAY DOORS ON EACH VEHICLE and, in the case of ex S.R. and B.R. Coaches, use french pin (No. 16 on illustration).</p> <p>If there is not sufficient hook left to support coupler head, it must hang on link coupling.</p>
One or both couplers defective and draw-hook broken.	Two ex S.R. or B.R. Coaches or one ex S.R. and one B.R. Coach fitted with "Buckeye" Couplers.	---	<p>Drop Couplers.</p> <p>Extend side buffers on both vehicles. Lock gangway doors of each vehicle, and in the case of ex S.R. and B.R. coaches, use french pin (No. 16 on illustration). Withdraw from service.</p>

PASSENGER TRAIN INSTRUCTIONS.

AUTOMATIC COUPLERS—Continued

(b) To change the side buffers from the "LONG" to the "SHORT" position, remove the saddles and push the buffers back as far as they will go.

(c) When not in use, the saddles shall be hung on the hook provided for them or the headstocks.

Automatic coupling

[illegible]

If two valves coupled by means of a single coupling are fitted with springs which

When coupling vehicles fitted with Automatic Couplers the Guard or Shunter performing the work must not under any circumstances stand between the vehicles, but must wait until they have been brought together and coupled before proceeding to connect the vacuum brake pipes, etc.

4. **Uncoupling.**--Before the automatic coupling is made, the captain of the tug must be taken to see that the electric cable connects properly and that the points of the gangways are folded back and secured in position.

After the vacuum brake and steam heating hose-pipes have been connected the driver must be instructed to "ease up," when the uncoupling chain must be pulled to unlock the coupling. This operation will lift the lock, allow the knuc to open, and pull the coaches to be separated.

the mobility losses are $\Delta b = \Delta b_{\text{max}} - \Delta b_{\text{min}}$ for the vehicle speed

After this has been done the coupler support pin must be ~~cut~~ ^{cut} ~~loosely~~, when the coupler will hang down on the pivot pin to enable the ordinary hook to be used. The support pin must not be allowed to hang loosely, but be replaced in the holes in the coupler.

As stated will be given by the Attorney General's Department Staff, where was a person, and, asking the buffers and coupes.

The same procedure must be carried out when attaching Automatic Coupled ~~STUCK TO A VEHICLE~~
fitted with ordinary screw couplings.

A limited number of G.W. valves are fitted with special gangway levers for coupling to Pullman type gangways.

Vehicles not fitted with special equipment must have rear door locked and main door fitted.

A stock of gangway adaptors is kept at Swindon

9. Emergency Screw and Emergency Link Couplings. Vehicles fitted with Automatic Couplers must also be equipped with emergency screw and emergency link couplings. When both couplings are used, the screw coupling is carried in the brake compartment of the vehicle and the link coupling is carried in the headstock, as shown in the illustration. When the emergency screw coupling is provided it is carried on the headstock.

~~When, from any cause, the Automatic Couplers cannot be used, the emergency Screw coupling must be utilized whenever possible and screwed up tightly to keep the vestibules together for use~~

In the event of two vehicles fitted with the Automatic Coupler being coupled on no account must any attempt be made to couple up with the 1st vehicle.

When the emergency SCREW or emergency PIN is used the station where it is put on the carriage must advise by wire the destination station, the retarding vehicle and be removed out of the train, and the staff at the latter point must remove the emergency coupling which has been used in its correct position on the vehicle which it belongs.

PASSENGER TRAIN INSTRUCTIONS.
METHOD OF USING EMERGENCY COUPLINGS.

Nature of defect	Coaches to be joined.	Emergency Coupling to be used.	Method of Coupling
One or both couplers defective and draw-hook broken.	Two coaches fitted with Automatic couplers.	Link.	<p>If sufficient hook is left on the vehicle with the broken draw-hook to hold the coupler in the "up" position, leave the coupler in that position and carry out the following operations:-</p> <p>Extend side buffers on both vehicles.</p> <p>Place the oval end of the emergency coupling link on the sound draw-hook. Place the flat end of the emergency link into the slot of the vehicle with the broken draw-hook. (The slot referred to is shown by figure 4 on the illustration.) The hole in the flat link must be in line with the hole which runs through the coupler knuckle (Figure 1) and when the emergency coupling pin (Figure 3) is inserted through this hole the operation of joining the two vehicles is then complete.</p> <p>Lock the vestibule doors on each vehicle.</p> <p>If there is not sufficient hook left to support coupler head, it must hang on link coupling.</p>
Draw-hook broken on Automatic Coupled Vehicle	One vehicle fitted with Automatic Coupler and one ordinary Screw Coupling.	Link.	<p>If sufficient hook is left on Automatic Coupled vehicle to support coupler, extend side buffers in "up" position. Extend side buffers on Automatic Coupled vehicle and place the oval end of emergency coupling link on the draw-hook of the ordinary vehicle.</p> <p>Place the flat end of the emergency link into the slot of the vehicle with the broken draw-hook. (Figure 4). The hole in the flat link must be in line with the hole which runs through the coupler knuckle (Figure 1), and when the emergency coupling pin (Figure 3) is inserted through this hole the operation of joining the two vehicles is then complete.</p> <p>Lock the vestibule doors on each vehicle.</p> <p>If there is not sufficient hook left to support coupler head it must hang on link coupling.</p>
One or both couplers defective.	Two coaches fitted with Automatic Couplers.	Screw.	<p>Drop couplers, extend side buffers on one vehicle only, place short link of screw coupling on hook first, then long link on opposite hook, and screw up as tightly as possible.</p>
Broken or defective Screw Coupling on ordinary vehicle	One vehicle fitted with Automatic Coupler and one ordinary Screw Coupling.	Screw.	<p>Shorten side buffers on Automatic Coupled vehicle, place short link of screw coupling on hook first, then long link on opposite hook, and screw up as tightly as possible.</p>

CENTRAL BUFFERS AND DRAWGEAR ON COACHES.

Sets of Coaches fitted with Central Buffers and Drawgear are provided for local working on certain sections of the Line.

The coaches are coupled together by a fixed coupling bar which passes through the central buffers and is secured by a coupling pin in each coach. The leading and rear ends of each set of coaches are provided with ordinary buffers and drawgear for connecting to the engine or ordinary vehicle.

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METHOD OF USING EMERGENCY COUPLINGS—continued.

Nature of Mishap.	Coaches to be joined.	Emergency Coupling to be used.	Method of Coupling.
Draw-hook broken on "Buckeye" vehicle	One ex L. & N.E. vehicle fitted with "Buckeye" Coupler and one with ordinary Screw Coupling.	ex L. & N.E. Link Coupling (to withdraw coach with broken hook from train).	<p>If sufficient hook is left on "Buckeye" vehicle to support coupler-head, fix latter in "Up" position. Extend side buffers on "Buckeye" vehicle and place the oval end of emergency coupling link on the draw-hook of the ordinary vehicle.</p> <p>Place the flat end of the emergency link into the slot of the vehicle with the broken draw-hook. (The slot referred to is shown by figure 4 on the illustration). The hole in the flat link must be in line with the hole which runs through the coupler knuckle and when the emergency coupling pin (No. 3 on illustration) is inserted through this hole the operation of joining the two vehicles is then complete.</p> <p>LOCK THE GANGWAY DOORS ON EACH VEHICLE and, in the case of ex S.R. and B.R. Coaches, use french pin (No. 16 on illustration).</p> <p>If there is not sufficient hook left to support coupler head it must hang on link coupling.</p> <p>Lock gangway doors of each vehicle and, in the case of ex S.R. and B.R. Coaches, use french pin (No. 16 on illustration).</p> <p>RETAIN SIDE BUFFERS ON "BUCKEYE" VEHICLE, in extended position. Withdraw from service.</p>
Draw-hook broken on "Buckeye" vehicle.	One ex S.R. or B.R. Coach fitted with "Buckeye" Coupler and Coach fitted with ordinary Screw Coupling.	—	<p>Drop couplers. EXTEND SIDE BUFFERS ON BOTH VEHICLES, place SHORT link of screw coupling on one hook first, then long link on opposite hook, and screw up as tightly as possible.</p>
One or both couplers defective	Any two Coaches fitted with "Buckeye" Couplers.	SCREW	<p>Drop the gangway doors on both vehicles if both are gangway fitted and, in the case of ex S.R. or B.R. Coaches use french pin (No. 16 on illustration).</p> <p>RETAIN SIDE BUFFERS ON "BUCKEYE" VEHICLE, in extended position. Place short link of SCREW coupling on one hook first, then long link on opposite hook, screw up as tightly as possible.</p>
Broken or defective Screw Coupling on ordinary vehicle.	Any vehicle fitted with "Buckeye" Coupler and one with ordinary Screw Coupling.	SCREW.	<p>Drop the gangway doors on both vehicles if both are gangway fitted and, in the case of ex S.R. or B.R. Coaches use french pin (No. 16 on illustration).</p> <p>RETAIN SIDE BUFFERS ON "BUCKEYE" VEHICLE, in extended position. Place short link of SCREW coupling on one hook first, then long link on opposite hook, screw up as tightly as possible.</p>

(6) Automatic Couplers Separating.

When the Automatic Couplers become divided in service they must be kept as far as possible in the condition in which they are found so as to assist in the discovery of any defect.

(G.A.28.Op.—4 51. LK1/9172 Gen E.)

Special Instructions Relating to "Graves"
 "Deadends" are cancelled GA 24
 See circular O/R.R. in front of GA

SECTION II. (b).

INSTRUCTIONS CONCERNING BOTH PASSENGER AND FREIGHT TRAINS.

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Reference to the following to appear on page 137 :—

SPECIAL INSTRUCTIONS TO BE OBSERVED IN CONNECTION WITH THE WORKING OF TRAINS DESIGNATED BY THE CODE WORD "DEEPDENE."

GENERAL INSTRUCTIONS TO STATION MASTERS, INSPECTORS, ENGINE DRIVERS, GUARDS, SIGNALMEN, LENGTHMEN AND OTHERS CONCERNED.

1.—ENGINE HEAD LAMPS.

The Engine, or where more than one is used, the leading Engine, must carry express passenger train headlamps.

2.—TRAIN TAIL LAMPS.

Two tail lamps must be carried on the rear vehicle of the train. Block Regulation 19 "Train passed without Tail lamp" need not be carried out unless both lamps are missing or both lights are out when they should be burning.

3.—BLOCK SIGNALLING.

The train must be signalled on the Block Instrument by a special "Is line clear?" signal of 12 beads on the bell given thus :—

4-4-4

4.—TRAINS RUNNING UPON, CROSSING, OR FOULING, THE LINE ON WHICH THE TRAIN CODED "DEEPDENE" RUNS.

(a) A train running in advance of the train coded "Deepdene" on the same line of rails must not be allowed to leave or pass any place at which it can be shunted, unless there is time for the train to stop at the next place and be shunted 5 minutes before the coded train is expected to pass the signal box in the rear. In the case of a train not continuing on the same line of rails as the coded train, unless such train can pass the Junction at which it will foul the line upon which the coded train will run 5 minutes before the coded train is expected to pass the signal box in the rear.

(b) No train or vehicle must be allowed to occupy, cross or foul, the line upon which the coded train will run for 5 minutes before the coded train is expected to pass the signal box in the rear.

(c) A train which cannot be permitted to have precedence over the coded train must not be allowed to approach the Junction from a conflicting direction until after the coded train has passed unless there are trap points or other points set for another line to prevent the line upon which the coded train is being run being fouled, or Outer and Inner Home signals are provided on the converging line, in which latter case the train must be held at the Outer Home signal.

(g) On single lines Block Regulation 4 must apply. If no engine is authorised, this must not be used in connection with the coded train or any train requiring to cross the coded train.

5.—SHUNTING OPERATIONS.

Shunting operations on any lines or sidings next adjoining the line on which the coded train will run must be suspended for 5 minutes before the coded train is expected to pass the signal box in the rear and until it has passed.

6.—ROAD LEVEL CROSSINGS.

(a) All Occupation and Accommodation Level Crossings must be specially protected.

(b) At all Level Crossings where Crossing Keepers are stationed, the person in charge must be on duty 30 minutes before the coded train is due to pass and remain until 10 minutes after it has passed.

At Public Road Level Crossings where a Crossing Keeper is not regularly employed, Permanent Way staff provided with the necessary hand signals and detonators must be placed in charge 30 minutes before the train is due to pass until 10 minutes after it has passed, unless instructions are issued for the gates to be locked.

(c) Nothing must be allowed to cross the line at any Public Road Level Crossing for 10 minutes before the coded train is expected to pass and until it has passed, and at any Occupation or Accommodation Level Crossing for 30 minutes before the coded train is due to pass and until it has passed.

(d) The District Permanent Way Inspectors will be responsible for providing the extra men required.

7.—SUSPENSION OF ENGINEERING WORKS.

All Engineering work on the line upon which the coded train will run must be stopped 15 minutes before the train is due to pass and until it has passed.

8.—STATION MASTERS.

Station Masters at all stations on the route of the coded train must be on duty, and watch the passage of the train. They must satisfy themselves that the whole of their staff concerned thoroughly understand these instructions.

- (d) A train must not be run on to a diverging line, nor must a train be allowed to stand on a platform, bay, or other converging line (except at terminal stations) for the coded train to pass unless there are trip points or other points set for another line, to prevent the line upon which the coded train will run being fouled.
- (e) Should advice be received that the coded train is running out of course the above-mentioned instructions as to trains not being allowed to precede it or cross the line upon which it will run must be carried out according to the time it is expected to pass.
- (f) Permission must not be given for a train following the coded train to approach on the same line of rails or on a converging line at a junction until the "Train out of Station" signal has been received for the coded train from the signal box in advance, unless there are Facing Points which are set for another line.
- (g) On single lines Block Regulation 4 must apply and at signal boxes where Regulation 5 is authorised this must not be used in connection with the coded train or any train requiring to cross the coded train.

Reference to the following to be made on page 139 :—

ENGINES FITTED WITH ELECTRIC HEADLAMPS.

Deleted G.R. 24

Certain engines have been equipped with small electric lamps for the purpose of headlamp indication. The electric headlamps, white light, are fixed permanently at each end in all positions in which they may be required. Brackets on which oil lamps or discs can be placed for use in the circumstances described below are also provided.

The appropriate electric lamps must be illuminated under all the conditions in which ordinary oil headlamps should be lighted as provided for in Rule 122.

In the event of the failure of individual electric lamps required for the prescribed code they should be replaced temporarily by lighted oil lamps placed on the appropriate brackets.

When it is not necessary for the electric lamps to be illuminated the appropriate headlamp code indication must be given by the display of discs placed on the brackets provided.

Engines fitted with electric lighting equipment must carry—

- (a) An oil tail lamp which must be used at all times when it is necessary to carry out Rule 122.
- (b) An oil headlamp to replace, when necessary, an individual electric headlamp that may have failed.
- (c) A red shade to enable the oil headlamp referred to in clause (b) to be used for single line working, etc., purposes.

Should it prove impossible to display the full lamp code required either by electric lamps or temporarily with the assistance of oil lamps, such measures must be taken as will enable the full code to be displayed at the earliest possible moment.

Certain engines are equipped with electric headlamps at the front end only, and in the case of these engines traveling tender first the headlamp code must be given by means of oil lamps.

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

ENGINE HEAD LAMPS.

(1) In thick type letter shown on each class of Head Lamp indicating that "A, B, C," etc., is placed at the end of the class to indicate what head lamps that particular train is to carry.

NOTE. The Train men will be responsible for seeing that their trains carry the proper Head Lamps according to class.

(2) The same system of head lamps and marks applies on relief lines as on main lines.

(3) The head lamp signal is given by the train men, and is sufficient to advise the engine driver of the signal, and that the train is to be stopped at the signal. The signal is given by the train men in accordance with the head lamps, and the appropriate "Is Line Clear?" bell codes.

(4) Head Lamps carried by trains as shown by an engine as under

For distances up to 12 miles ... head lamp only to be carried at foot of chimney by the engine. Train engine to carry its own light, i.e. broken




For distances over 12 miles ... Front engine only to carry ... and those to be the head lamps applicable to the train.

(5) Except where specified to the contrary, other Companies' Engines with ... where this is not done

(6) ...

(7) ...








DIAGRAM SHOWING CLASSIFICATION, HEAD LAMPS AND CORRESPONDING BELL SIGNALS.

Class of Train	Head Code (White Lights)	Description of Train	Beats on Bell	How to be Given
A		Express passenger train, newspaper train or breakdown van train or snow plough going to clear the line or light engine going to assist disabled train ...	4	Consecutively.
		Officers' Special train not requiring to stop in Section ...	4	Consecutively.
		Express diesel car ...	8	4 pause 1 pause 3.
B		Ordinary passenger train, mixed train or breakdown van train NOT going to clear the line or loaded rail motor train†	4	3 pause 1.
		*Branch passenger train ...	4	1 pause 3
		Ordinary passenger or parcels diesel car	9	5 pause 1 pause 3.
C		Parcels, fish, fruit, horse, livestock, meat, milk, pigeon or perishable train composed entirely of vehicles conforming to coaching stock requirements ...	5	1 pause 3 pause 1.
		Express freight, livestock, perishable or ballast train pipe fitted throughout with the automatic brake operative on not less than half of the vehicles ...	5	3 pause 1 pause 1.
		Empty coaching stock train (not specially authorised to carry "A" headcode) or empty rail motor train† ...	5	2 pause 2 pause 1.

(continued)

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

DIAGRAM SHOWING CLASSIFICATION, HEAD LAMPS AND CORRESPONDING
BELL SIGNALS—*continued.*

Class of Train.	Head Code (White Lights)	Description of Train.	Beats on Bell.	How to be Given.
D		Express freight, livestock, perishable or ballast train partly fitted with the automatic brake operative on not less than one-third of the vehicles	5	Consecutively.
E		Express freight, livestock, perishable or ballast train partly fitted with NOT less than four braked vehicles connected by vacuum pipe to the engine Express freight, livestock, perishable or ballast train with a limited load of vehicles NOT fitted with continuous brake	5 5	1 pause 2 pause 2. 1 pause 2 pause 2.
F		Express freight, livestock, perishable or ballast train NOT fitted with continuous brake	5	3 pause 2.
G		Light engine or light engines coupled ... Engine with NOT more than two brake vans	5 5	2 pause 3. 1 pause 1 pause 3.
H		Through freight or ballast train not running under class "C," "D," "E" or "F" headcode	5	1 pause 4.
J		Mineral or empty wagon train	5	4 pause 1.
K		Freight, mineral or ballast train stopping at intermediate stations †*Branch freight train Freight, ballast or Officers' Special train, requiring to stop in section	3 3 7	Consecutively. 1 pause 2. 2 pause 2 pause 3.

†—To be used only where authorised by the Operating Superintendent.

*—"B" Head Codes are subject to alteration in the case of through trains working over Branches, as shewn in the Working Time Tables.

‡—The term "rail motor" includes "auto-train."

Signalmen entering trains in the Train Register Book must, in addition to any information that may be necessary to identify the train, insert in the first column the proper "Is Line Clear?" bell signal.
(G A 26 Op.—5 50 R E Op. Com Min 176)

DIAGRAM SHOWING CLASSIFICATION, HEAD LAMPS AND CORRESPONDING BELL SIGNALS.—Page 139.

Insert § against D headcode trains and the following at the foot of page 140

§ The proportion of revenue on which the automatic brake must be operative to be based on the equivalent load, Class 3 traffic

(G.A.30 Op.—9/54 E.84669 H(2-C)).

Engine Head Codes.

The following to be added to the "description of Trains" carrying "E" Head Codes:—

	Beats on Bell	How to be given
5	1 pause 2 pause 2	
Weed killing trains when both running and spraying		

(G.A.30 Op.—9/54 LK1/10661/417E).

Reference to the following to be made on page 141:—

REVERSING OF ENGINES TO START.

Whenever it is necessary for a Driver to reverse his engine to start and in setting back is likely to foul any other movement another running line, or a level crossing, proper authority must be obtained before setting the train back.

In cases where after a detachment from a train composed of coaching stock, the engine or engine with vehicles has moved forward and come to a stand and it is subsequently necessary for the Driver to reverse his engine in order to start, he must before setting back for that purpose, satisfy himself that the person who performed the uncoupling has moved clear of the vehicles.

(G.A.29 Op.—5/52. LK1/E.)

TIMING OF EMPTY COACHING STOCK TRAINS.—Page 141.

The following to be added at the end of the 2nd paragraph:—

"on sections of the line where the ruling gradient is less than 1 in 100 rising."

(G.A.30 Op.—9/54 T.38964 G/7).

Reference to the following to be made on page 141:—

WORKING OF GAS TURBINE ENGINES.

When a gas turbine engine is required to work over a section of line where it is not normally scheduled to run, prior advice must be issued to all concerned, including the staff of other Departments, e.g., Permanent Way men of the intention to make such movement.

When the scheduled working of a gas turbine engine has been suspended for a short period, e.g., not exceeding seven days, in connection with repairs, etc., it will not be necessary for a special advice of resumption of normal working to be issued to all concerned.

When the scheduled working of a gas turbine engine has been suspended for a period in excess of seven days, a notice to all concerned must be issued before normal working is resumed.

If it is absolutely necessary for a gas turbine engine to work over a section of line where it is not normally scheduled to run, or if a gas turbine engine is required to work in a service which is normally scheduled to be worked by a steam locomotive on a route over which the gas turbine engines are authorised to be worked and a printed or stenciled notice cannot be issued in sufficient time to ensure that 48 hours' notice is given to all concerned, the Drivers of such engine must be notified of the circumstances and must then sound the siren when entering and emerging from tunnels, also when approaching curves, level crossings, barrow crossings, overbridges, gangers' huts and other buildings adjacent to the line upon which the gas turbine engine is run.

(G.A.30 Op.—9/54 LK1 8847,372)

Approaching Geographical Junctions and requiring to proceed through Junction.

†On Main line and requiring to proceed to left	1 long 1 short
†On Main line and requiring to proceed to right	1 long 2 short
†On Slow or Goods line and requiring to proceed to left	2 long 1 short
†On Slow or Goods line and requiring to proceed to right	2 long 2 short
†The appropriate route code whistle to be given at Signal Boxes enumerated in the local Appendices.	

To or from Goods line or Slow line or Loop and Main line	5 short
To cross from Main to Main	4 short
To or from Bay or Platform lines	1 crow 1 long
Down Main or Fast, Slow or Goods or Loop to Down Sidings	1 crow
Down Main or Fast, Slow or Goods or Loop to Up Sidings	2 short pause 3 short
Up Main or Fast, Slow or Goods or Loop to Up Sidings	3 short pause 1 short
Up Main or Fast, Slow or Goods or Loop to Down Sidings	3 short pause 2 short
Up Sidings to Down Sidings or vice versa	3 short pause 3 short
Train ready to leave Sidings	2 short pause 1 short
Shunt from Sidings to Main line	2 short pause 2 short
To or from Loco.	2 short
Express trains requiring fresh engine at next stopping place	3 crows
†Fire on lineside	1 crow 1 long 1 crow
†To be repeated when passing next Permanent Way men, Station, Signal Box or Crossing	1 crow 1 long 1 crow
†Keeper's hut.	
Engine requiring water	1 long pause 3 short
To indicate light engine is clear of points which require to be turned	1 short
To indicate that train or light engine has been shunted clear of points leading from one running line to another—(Rule 69)	1 crow 1 short
To indicate that train or light engine has been shunted clear of all running lines—(Rule 69)	3 short
Before starting train assisted by engine in rear—(Rule 133 clause c)	2 crows

(G.A.30 Op.—9/54 LK1/9593/E).

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

The following items to be inserted on page 141.

PROVISION OF ENGINE CONDUCTORS.

In all cases of engine conductors being requisitioned, and particularly when required from an intermediate point in order to prevent delay in carrying out the operation, all concerned should be notified precisely where the conductor is to be picked up.

(G.A.28 Op -4/51 LK1 512/30)

COUPLING AND UNCOUPLING OF ENGINES OF PASSENGER TRAINS.

and in connection with the Traffic and Locomotive Department.

DUTIES OF ENGINE CONDUCTORS.

If a Driver, a passed Fireman acting as a Driver, or Motorman, is not thoroughly acquainted with any portion of the Line over which he has to run, he must obtain the services of a competent Conductor.

When the Conductor is familiar with the type of engine employed, he must work the engine.

When the Conductor is not familiar with the type of engine employed, he will give to the Train Driver the necessary instructions in regard to the signals, curves, gradients, speed restrictions, etc., applicable to the line over which they are working, and leave the actual driving entirely in the hands of the Train Driver.

The Conductor will be responsible for the due observance of signals, speed restrictions, etc., and safe working of the train.

In every case the Train Driver must study the signals, speed restrictions, etc., for that part of the line over which he is being conducted.

The Conductor will be responsible in cases where it is necessary for the Fireman to carry out the provisions of Rule 55, for seeing that this is done.

(G.A.28 Op.-4/51. LK1/10600/20.E.)

STANDARD WHISTLE CODE.

Drivers requiring Assistant Engine or Fresh Engine en route.

In order to minimise delays to trains because of partial or total engine failure, the attention of all Drivers and Signalmen is directed to the standard instructions in the Sectional Appendices which provide for the use of the following whistle code.

Express trains requiring a fresh engine at next stopping place — 3 crows

The sounding of this whistle code will denote to Signalmen that another engine is required at the next station, if available there, or at the next Motive Power Depot, and

Signalmen must immediately pass forward an advice, giving title and head code of train in conformity to the appropriate Control Office, who will decide what action is necessary and issue instructions accordingly. In cases where a fresh engine is required from a Motive Power Depot, an advice must be passed forward by the Control without delay, and, on receipt of such an advice, Motive Power Depot staff must take steps at once to provide the required fresh engine.

(G.A.31-7/56 T.38 113 G 738 417 G 4-2)

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

ENGINE HEAD LAMPS FOR FREIGHT TRAINS WITH THROUGH LOADS.

When an ordinary or special Freight train carrying ~~its~~ ^{its} head lamps makes up a full load through to destination at an intermediate point *en route*, the Guard should inform the Engineman of the fact, who should then alter the engine head lamps to "A" ^{GA26}

The person in charge at the point where the train is made up to a full load should advise the Signaller of the change in the character of the train, and also advise any places in advance where the train would call in the ordinary course to exchange traffic, so that as far as possible trains stopping intermediately may be kept clear.

TIMING OF EMPTY COACHING STOCK TRAINS.

Trains composed of 8-wheeled main line stock in normal running condition and with-in Standard Passenger Load for engine.	4-6-0 engines—Express Passenger Speeds.
	2-6-0 and 4-4-0 engines with 5 ft. 8 in. wheels—Ordinary Passenger Speeds.
Trains composed of 8-wheeled main line stock in normal running condition, with loads not more than 20 per cent. in excess of Standard Passenger loads for engine.	4-6-0 engines—Ordinary Passenger Speeds.
	2-6-0 and 4-4-0 engines with 5 ft. 8 in. wheels—"D" Head lamps—Freight train speeds.

Note.—The code "Pug" should be used in applying to the Locomotive Department for power, stating approximate tonnage when practicable and headlamps to be carried.

COUPLING AND UNCOUPLING OF ENGINES OF PASSENGER TRAINS.

At certain Stations, which have been agreed upon between the Traffic and Locomotive Departments, arrangements have been made for the Locomotive Department to perform the work of coupling and uncoupling the engines of Passenger Trains in certain circumstances. (See Table Appendix for full details.)

ENGINES TAKING WATER AT WATER COLUMNS SITUATED AT BOXES WHICH ARE SWITCHED OUT.

When it is necessary for Engines to take water at Water Columns situated near Boxes which are switched out, Engine Drivers, before proceeding to such Water Columns, must stop at the nearest Box in the rear where a Signaller is on duty, and inform the latter that they intend to stop at the section for that purpose.

ENGINE WHISTLES.

ENGINE WHISTLES—Page 141.

The table and instructions under this heading to be deleted and the following substituted —

STANDARD CODE OF ENGINE WHISTLES

The following code of engine whistles applies at all stations, junctions and sidings, not otherwise specially provided for in the local Code of Engine Whistles shown in the table following the main heading.

In order to avoid annoyance to passengers at stations and residents in the neighbourhood of the Stations, Drivers are hereby directed not to make more frequent use of the engine whistles than is necessary to ensure safe and efficient working in compliance with the Rules and Regulations.

Note: The term "Slow line" includes Relief line.

Description	Whistles
*Main or Fast lines	1 long
*Line next to Main line (Slow or Goods)	2 long
*Line next to Slow or Goods	3 long

One additional whistle to be given for each additional line farther away from the Main line.

*These whistles to be given when approaching signals at Danger or when necessary to indicate when ready to proceed on same line.

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(D) 4-6-0 60xx "King" Class Engines.

On all routes authorized for 60xx "King" class engines (except the goods loops scheduled for use in emergency by a "King" class engine singly at a maximum speed of 5 m.p.h., two "King" class engines may run coupled together or may be assisted by any other Western Region engine, subject to the following regulations:—

- (1) Where any tender engine is run coupled to a "King" engine they must not be coupled in any other way.
- (2) When a tank engine or the locomotive propels or assists a "King" engine, the tank engine must be coupled to the "King" engine.
- (3) Not more than one engine may be coupled to a "King" engine without special authority.
- (4) The foregoing permissions are subject to the observance of the special overall speed restrictions laid down on certain routes as applicable to the working of a "King" class engine singly.

(G.A. 9-10-48. C.E.—K.2 46819.)

WORKING OF ENGINES IN STEAM COUPLED TOGETHER.—Page 142.

The instruction under heading (B) "**Over Royal Albert Bridge, Saltash**" (see Supplement G.A.30) to be cancelled and the following substituted :—

- (1) The speed of all engines passing over the structure must not exceed 15 m.p.h.
- (2) Not more than two permitted engines may work coupled together.
- (3) The instructions shewn on pages 144 and 145 of the General Appendix to the Rule Book in respect of the assisting or double heading of trains must be observed.

(G.A.31—7/56. T.D.95/41)

~~Both groups of engines may also be assisted by engines of the "Yellow" and "Uncoloured" classes.~~

~~Assisting tender engines with a leading bogie may be coupled either in front of the train engine or between the train engine and the train. In all other respects the general instructions for assisting or double heading of trains apply.~~

(G.A.30 Op.—9/54 TD95/41)

(C) OVER RIVER WYE BRIDGE, CHEPSTOW.

- (1) The maximum speed of all trains, auto cars and light engines is 15 m.p.h.
- (2) Two "Red" tank engines must not run coupled together.
- (3) When a tank engine and a tender engine (both of the "Red" classification) are coupled together, the tank engine must be coupled to the tender of the other engine.

(G.A.18.11 17. C.E.—K2 34299.)

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

MIXED TRAINS -Continued.

All Mixed trains must stop at stations to avoid a longer run than 10 miles without stopping, but in the case of stations more than 10 miles apart it will not be necessary for a stop to be made between such stations.

5. When trains are run for the conveyance of horses, cattle, or other stock, and vehicles are added for the conveyance of passengers, the vehicles containing the passengers must be placed in front of all goods vehicles, be provided with the continuous brake worked from the engine, and the train must be run subject to the conditions applying to Mixed trains. Drivers, firemen, or other persons travelling in charge of live stock are not for the purpose of this instruction regarded as passengers, but when a separate passenger vehicle is provided for their accommodation, it must be formed next to the engine and be provided with the continuous brake worked from the engine.

6. The regular trains authorised to be run as Mixed trains are marked Mixed in the Service Time Tables.

7. If the goods work entails more than a shunt at any station the vehicles containing passengers must, if practicable, be placed on another line or shunt and secured before the shunting is commenced. If it is not practicable to do so, the vehicles containing passengers are attached to the engine.

8. When running a Mixed train, if the first vehicle to the passenger end is fitted with a Garrett-Lok coupling, the passenger end must be used, but when the leading vehicle is fitted with a screw coupling, the proper emergency screw coupling must be used in accordance with the instructions on page 145.

9. When the number of vehicles on a train is such that the continuous brake can be applied from the engine, it is less than one-fifth of the total weight of the train, the continuous brake must be applied. Mixed trains must stop at stations to avoid a longer run than 10 miles without stopping, but in the case of stations more than 10 miles apart it will not be necessary for a stop to be made between such stations.

10. Tank wagons can only be conveyed by Mixed trains, subject to the instructions shown on page 177.

11. These Regulations do not apply to troop trains.

INSTRUCTIONS TO BE OBSERVED WHEN ARRANGING THE ASSISTING OR DOUBLE HEADING OF TRAINS.

1. ASSISTING OR DOUBLE HEADING.

Assisting or Double heading may be arranged only over those sections of the line and with those types of engine which have been approved by the Board of Railways, or see Main Line Regulations. Working of engines coupled together is dealt with in the Standard Section Service Time Tables for Routes other than the Main Line, and is not always permitted, except where otherwise specially authorised.

2. PASSENGER TRAINS.

Assisting from the bottom to the top of inclines. -As a general rule, any type of engine with multiple wheels not less than 1' 6" diameter may be used to assist a passenger train from the bottom to the top of an incline. Except where stated, the engine assisting must be placed in front of the train engine and must be detached at the signal box at the summit of the incline.

(b) Assisting or Double heading on the level or on falling gradients. Subject to Clause 1, when the points between which a passenger train requires assistance are so situated that it is necessary for the assisting train to run in the same direction as the train being assisted, it is necessary for the assisting train to run in the same direction as the train being assisted, or after having worked the incline, or when it is necessary for any purpose to attach a second engine to one of these trains, the following instructions must be observed:

(i) Engines of the 4-6-0 and 4-4-0 types may be coupled in front of any engine (subject to special regulation respecting the "King" class), and, if available, an engine of one of these types should always be the leading engine.

~~When the train is to be assisted from the bottom to the top of an incline, the engine of the 4-6-0 or 4-4-0 type must be placed in front of the train engine.~~

(ii) Except where shown below, engines of the 2-6-0 and 2-4-2 types with wheels 5' 8" diameter may assist in front of any engine except the 60XX "King" class between the following points only:—

Bristol to Badminton.
Severn Tunnel Junction and Badminton.
Pitling and Severn Tunnel Junction.
Stroud to Sapperton Siding.

Newton Abbot and Brent.
Totnes and Newton Abbot.
Stourbridge Junction and Birmingham.
Stratford-on-Avon and Earlswood Lakes.

* Millbay or North Road to Hemerdon -2-6-0 type (5' 8" wheels) only.

* Kemble to Brimscombe -2-6-2 (5' 8" wheels) type only. (G.A. 10. 3/42. T.29,880. G/1)

Taunton to Burlescombe

(c) Assisting or Double heading "King" Class Engines. No engine must run coupled to a "King" 60XX class engine except as indicated on page 142. Working of Engines in Steam coupled together and then the position of the engine must conform to clause 2 (b), sub section (ii) of these instructions.

In certain cases individual engines of a colour group superior to the route colouring are authorised to work, but it must be understood that such authorisation is not intended to cover these engines working coupled together or assisted by any other engine. Specific authority must be given in each individual case before such working is permitted.

(G.A.13. 9/43. E. 83395/H.)

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Clause 2—Passenger Trains (b) (ii) to be amended to read :—

... of an engine with a pony truck of the 2-R-0, 2-6-0, 2-4-0, 2-8-2T, 2-8-0T or 2-6-2T type, should be the leading engine. Otherwise the more powerful engine must be placed in front. (G.A.19—10 48., L.K.1 9/174 8.)

INSTRUCTIONS TO BE OBSERVED WHEN ARRANGING THE ASSISTING OR DOUBLE-HEADING OF TRAINS—Page 144.

2. Passenger Trains.

• The following to be added as paragraph (iv) to Clause (b):—

... of the 43xx 2-6-2T types and of the 43xx 2-6-0 type may assist in front of any authorised engine between the following points —

Par-Newquay
Newquay-Par.

(G.A.30 Op.—9/54 TD.95/166).

Reference to the following to be made on page 145:—

WORKING OF LOCOMOTIVES WITH TENDER LEADING.

Tender locomotives must not exceed a speed of 45 m.p.h. when running with the tender leading either when attached to a train or when running light. (G.A.30 Op.—9,54 LKIE).

(c) Vacuum fitted or partly vacuum fitted Freight trains.

Except where authorised by the Operating Superintendent and the Motive Power Superintendent vacuum fitted or partly vacuum fitted freight trains must not be double headed except for the purpose of assisting from the bottom to the top of an incline and through the Severn Tunnel.

NOTE — For the purpose of this instruction the following classes of trains come within the category of "Freight" —

Vacuum fitted or partly vacuum fitted livestock and perishable (including fish, meat and fruit), freight, mineral, ballast, sleeper and Engineering Department trains. (G.A.26 Op.—5,50.)

For sections of line over which passenger trains may be assisted in rear, in clear weather, see Local Appendices.

Appendices.

NW. For the purpose of this instruction the full name of classes are transcribed with the category of "Passenger".

NAME. Middle Name. Last Name. Sex. Race. Period.

⁶⁴ *Passenger* :—

[illegible]

3. FREIGHT TRAINS.

3. **FREIGHT TRAINS.** *As a general rule*—As a general rule any type of engine with
in Assisting from the bottom to the top of inches.—As a general rule any type of engine with
coupled wheels, as that 10" diameter may be used to assist a freight train from the bottom to
the top of a train.

The assistant engine must be placed as shown in the Appendix to the Service Time Table.

Assisting or Double Heading on the level or on falling gradients. Subject to clause 1, when the points between which a freight train or express assisting locomotives are used as to make it necessary for the assisting train to run on a distance with level or falling gradient or after having stopped at the valley, or when a locomotive for any purpose is attached to a second engine to one of these trains, over such gradients the assisting engine or engines of the same type as the train engine, or of the following 4 classes, must be used.

Partly Vacuum-fitted Freight trains—Except when authorized by the Superintendent of the Line, the Chief Mechanical Engineer, partly vacuum fitted freight trains must not be double-headed except for the purpose of assisting from the bottom to the top of an incline and through the Severn Tunnel.

~~NOTE: The purpose of this part act on the following cases appears once within the category of~~

19. apply AFV

4. GENERAL INSTRUCTIONS

4. GENERAL INSTRUCTIONS

When the train has to be assisted at the rear it must be stopped at the appointed place and the engine must be backed up to the rear of the train. The engine must be backed up to the rear of the train and the train must start from a state of rest.

Rule Book, the train must start from a state of rest.
 (c) Arrive at the appointed place at the top of the incline; the train must stop for the assigned engine to be uncoupled, exactly where the assigned engine is to be uncoupled, in which case the assigned engine will cease to push at the top of the incline and the train will run forward without stopping.

without stopping.

During a falling snow pass, rear trucks must never be assisted at the rear, but the assisting engine must be attached in front of the engine at the provisions of case 2. When with city limits, the engine should make all trucks to be assisted at the rear, the assisting engine must during fog or falling snow be coupled to the rear of the train IN EVERY CASE.

ASSISTING ENGINES RETURNING LIGHT TO THE BOTTOM OF INCLINES.

ASSISTING ENGINE RETURNING LIGHT TO THE BOTTOM OF DECKHEAD.

TRAINS ENTERING REFUGE SIDINGS ENGINE WHISTLES.

TRAINS ENTERING REFUGE SIDINGS ENGINE WHISTLES. The first step provided for is for the purpose of clearing the main line of the short jump whistles as soon as the whole of his train is placed clear of the siding. The whistle is sounded as the train may be, in order to give the signalman a warning of the train's position, and that they may be turned so as to prevent the train from running on to the running line.

EMERGENCY SCREW COUPLINGS.

as of loose screw couplings are in use, as follows:

For connecting Carriage stock (2 ft. 11 in. maximum length extended).

For connecting Carriage and Wagon stock and wagons to wagons if either or both are fitted with screw couplings. (2 ft. 7½ in. maximum length extended.)

For connecting Wagon stock when both are fitted with 3 link couplings (2 ft. 1½ in. maximum length extended).

Proper care must be used for their various purposes, and the screw, shackle pins, etc., of the emergency couplings must be well lubricated. Station Masters, Inspectors and Foremen must satisfy themselves that screw couplings are in good condition and work freely before they are put into use.

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

INSTRUCTIONS FOR DEALING WITH FIRES OR ACCIDENTS INVOLVING OR THREATENING
EXPLOSIVES, INFLAMMABLE LIQUIDS, DANGEROUS CHEMICALS, COMBUSTIBLE
MATERIAL, ETC.

When accidents or fires occur involving or threatening dangerous goods in the possession of the Railway Company, the first aim must be to prevent injury to life, and the next to prevent the damage extending to merchandise or to other property.

The steps to be taken depend upon the nature of the goods involved, and this should be, if possible, ascertained at the outset. Trucks containing inflammable liquids are labelled with the "Inflammable" label, and those containing other dangerous goods with the "Dangerous" label, as per specimens shown below: -

GREAT WESTERN RAILWAY. (3140 A)

INFLAMMABLE		3
DATE _____	TO _____ TRAIN _____	
FROM _____	_____	
TO _____	_____	
Via _____	_____	
Sheets <u>IN</u> or <u>ON</u> Wagon. Total No. _____		
Owner & No. of Wagon _____		KEEP LIGHTS AWAY FROM THIS TRUCK. LOAD and UNLOAD OUTSIDE GOODS SHEDS.
Consignee _____		

PLACE AS FAR AS PRACTICABLE FROM ENGINE, BRAKE AND VEHICLES LABELLED "EXPLOSIVES"

This Label to be used for INFLAMMABLE LIQUIDS

GREAT WESTERN RAILWAY. (3140)

SHUNT WITH CARE	DANGEROUS		3
	DATE _____	TO _____ TRAIN _____	
	FROM _____	_____	
	TO _____	_____	
	Via _____	_____	
	SHEETS <u>IN</u> or <u>ON</u> Wagon. Total No. _____		
Owner & No. of Wagon _____			

THIS LABEL TO BE USED FOR ACIDS AND ALL OTHER DANGEROUS
GOODS EXCEPT INFLAMMABLE LIQUIDS AND EXPLOSIVES.

The following to be inserted as paragraph 5 under the heading "General Procedure".—

5 In the event of a fire occurring in a Yard and there being explosives in the vicinity, the Fire Brigade must be notified of the presence of explosives in the area in which the outbreak of fire has occurred. (G.A.23—7.49 L.K. 6133.13.)

INSTRUCTIONS FOR DEALING WITH FIRES OR ACCIDENTS INVOLVING OR THREATENING EXPLOSIVES, INFLAMMABLE LIQUIDS, DANGEROUS CHEMICALS, COMBUSTIBLE MATERIAL, ETC.—Page 147.

The following to be inserted as the third paragraph of the "Special Instructions of the Yard".

In the event of a serious leakage occurring from a damaged or overfilled Petrol Tank or if such a damaged Petrol Tank has been involved in a fire, the driver must be immediately and requested to attend and stand by with the vehicle until the leakage has been stopped or the tank has been repaired or replaced. If the leakage is not stopped, the vehicle must be isolated and the driver must leave the vehicle and report to the nearest person in charge of the yard. The vehicle must be isolated until such time as the leaks dry out, when the vehicle may be reforwarded. (G.A.23—7.49 L.K. 6133.13.)

Reference to the following to be made on page 147:—

INSTRUCTIONS FOR DEALING WITH HIGH DENSITY HYDROGEN PEROXIDE IN RAIL TANK WAGONS.

1 Identification.—A longitudinal band 8 inches wide made up of white outer bands 1 1/2 inches wide with a light blue or green centre band 5 inches wide painted on the bottom edge of the cowl (or on an approximate similar position on the tank where no cowl is fitted) at each side, and the words "Corrosive Liquid" inserted in the band in 4-inch white letters.

(2) The tanks bear one star.

(3) Each vehicle bears the "Dangerous" label.

(4) The vehicles must not be marshalled next to wagons known to contain explosives or inflammable liquids.

(5) Loose shunting, either with the tanks concerned, or with other vehicles against the tank wagons, is prohibited.

(6) Spillage or Leakage of Contents.—In the event of accident or mishap involving spillage or leakage, water must be used where available. Ashes or ballast must not be used.

The instructions as to the method of dealing with fires or accidents involving acids and corrosive liquids as shown on this page, must be observed. (G.A.29 Op—5.52. E77314 H.)

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

INSTRUCTIONS FOR DEALING WITH FIRES OR ACCIDENTS INVOLVING OR THREATENING EXPLOSIVES, ETC. *Continued.*

GENERAL PROCEDURE.

1. Isolate the wagon on fire at once, and unless the fire can be effectually dealt with by buckets of water from the engine tender or by sand or other such means, remove to the nearest source of water, such as water column or fire station.
2. When possible, the packages on fire should be removed from the wagon before the fire is extinguished, so as to avoid unnecessary damage to other goods by water.
3. After the fire has been extinguished, the wagon or packages involved must be kept under observation as a precaution in case of any further outbreak.
4. When, as the result of a fire, any goods are left at the side of the line, the Station Master at the nearest station must be advised immediately, and he will become responsible for their removal to his station without delay.

SPECIAL.

Motor spirit, petroleum and other inflammable liquids are generally lighter than water and will not mix with it, and consequently such fires may continue to burn on the surface of the water. Instead of extinguishing the fire, water may thus extend the area of the fire when motor spirit, petrol, etc., are alight. **Foam-producing extinguishers, or sand and earth** are specially useful in putting out such fires.

When inflammable liquids are burning in the open, use sand or earth plentifully to prevent the flowing away of the burning liquid, especially towards watercourses.

Acids and Corrosive Liquids. Nitric acid may be set on fire or straw alight. Water can be used to extinguish any such fire, but it is especially dangerous with sulphuric acid and acetic acid, and especially when they come into contact with water. If such acids are involved, the water must be directed on the fire from a safe distance.

(Corrosive liquids in contact with the body are especially dangerous when burning. Wash the hands or parts of the body affected with plenty of water.)

Cylinders of compressed gases are liable to explode if near a fire, and there are dangers, both from the cylinders themselves and from the escaping gas. If the fire is very near such cylinders, keep everyone far away.

Celluloid fires become suddenly very fierce. Use water or chemical extinguishers promptly.

Charcoal may be spontaneous, especially if damp, but the fires are not sudden. Unload the portion alight but put it out by water. Load the portion containing dry and undamaged to destination. Store the wet portion in open pending instructions.

Matches. So long as the outside package remains sound it is impossible for a fire to follow the ignition of matches inside boxes. Generally any such fire smother itself. **Do not open the case or attempt to extinguish unless the contents are continuing to burn.**

Spent oxide of iron from gas works sulphur. Such traffic, like charcoal, is liable to fire spontaneously, and fires are not sudden. The sulphur as fires are objectionable. Water may be used, but the load must be afterwards watched, as fires tend to recur.

Saltpetre, nitrate of soda, and other nitrates do not burn, but if they come into contact with a fire they may become very fierce. Special instructions will be given to remove such materials from the zone of fire.

Cotton or wool in bales or bags. Keep the bales shut and do not open out the material. After putting out the fire keep under observation, as such fires tend to break out again.

Lime itself does not burn, but if it is not sufficiently cool when loaded, or if it comes into contact with water or other water, it may produce so much heat as to set other goods alight. The heated material should be removed from the train and care should be taken to prevent water reaching the remaining lime.

DON'T USE WATER to extinguish fires involving: -

Calcium Carbide,
Metallic Sodium,
Metallic Potassium,
Metallic Calcium,

but use sand, dry powder, or, where available, the Standard No. 5 Extinguisher.

The following instructions must be observed in dealing with fires, etc., are based on the chemical properties of the materials dealt with.

The following instructions will be ascertainable from the Fire Warden.

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

INSTRUCTIONS FOR DEALING WITH VEHICLES, CONTAINING EXPLOSIVES, DERAILED OR OTHERWISE INVOLVED IN A MISHAP.

1. All vehicles containing explosives are labeled with Special Explosives labels (see specimen below):—

GREAT WESTERN RAILWAY.

(3140 B)

DATE 19 19 19

FROM

TO

VIA COY

SHEETS IN or ON WAGON. Total No.

Owner & No. of Wagon

Consignee

**SHUNT
WITH
GREAT
CARE.**

**LOAD and UNLOAD
OUTSIDE
GOODS SHEDS.**

In the event of vehicles becoming derailed or breaking down in transit every possible care must be taken in handling them.

The following precautions are the most important:—

2. The first precaution is to prevent fires whether from lights or sparks.
3. Smoking in the neighbourhood is strictly prohibited.
4. Matches must not be struck near the scene.
5. If naked lamps or lights are necessary they should be placed on the windward or windy side of the vehicles, and far away from explosives and inflammable liquids.
6. Any vehicle containing Explosives which has derailed or been involved in a mishap, must be sent to the nearest Goods Depot or Siding, to be reloaded, and if necessary reloaded in accordance with existing instructions before going forward.
7. Avoid all shocks in moving the Explosives. Packages containing Explosives must as far as practicable be passed from hand to hand and not rolled upon the ground. They must not be thrown or dropped down.
8. Every vehicle to which Explosives are to be reloaded must first be swept quite clean. Packages of Explosives from one vehicle must not be mixed with those from another. Packages must be kept apart both when placing on the side of the line and in re-loading into other vehicles.
9. Should packages of Explosives be scattered on the line place them carefully on the side of the line and protect them with sheeting.
10. A responsible person must remain with the Explosives until the arrival of a competent representative to supervise their re-loading.
11. All wagons into which Explosives are re-loaded must be labelled with the "Explosives" labels.
12. If any loose Explosive has been spilt on the line, drench it thoroughly with water and scatter earth over it.
13. These Instructions are taken from the Bye Laws relating to explosives embodied in the Special Classification of Dangerous Goods.

Liskeard (Looe Branch)

Six-wheeled stock with base wheelbase exceeding 7 feet must not work over this Branch.

(G.A.15-12/44 R.6/3921A)

Liskeard (Looe Branch)

The following stock not conveying passengers may be worked over the connection from the Main Line to the Liskeard and Looe Branch, and on to Looe, provided a special speed restriction of 5 m.p.h. over the connection Main Line to the Looe Branch Line is observed :-

All types of four-wheeled vehicles.
Six-wheeled stock not exceeding 34ft. 6ins. total length and 21ft. wheelbase.

(G.A.12. 4/43. R.6/3471A.)

RESTRICTION AS TO THE WORKING OF STOCK OVER CERTAIN BRANCH LINES—page 149.

The instructions under heading of Princetown Branch to be amended to read:

BRANCH	RESTRICTION
Princetown Branch	Six-wheeled coaching and freight stock must not work over this Branch.

(G.A. 7.—8/40. R.6/2757/A.)

RESTRICTION AS TO WORKING OF STOCK OVER CERTAIN BRANCH LINES. Page 143.

The instructions on page 149 are amended as follows —

Branch.	Restriction.
Highworth	No vehicle of greater length than 60 ft. over buffers, nor exceeding 9 ft. in width over body, 2 ft. 6 in. high in centre, and 11 ft. at sides, must be worked over this Branch. (G.A.1. 3/37. C.E.12/34395.)
Eastern and Western Valleys	Add—“except as between those junctions and beyond” (G.A.1. 3/37. C.E.12/34395.)
Quaker's Yard H.L. and Merthyr..	Coaches must not be wider than 9 ft. 3 in. over panels at waist nor exceed 63 ft. 6½ in. in length over buffers at this width, but coaches 9 ft. wide are unrestricted as to length. (G.A.1. 3/37. R.6/1543A.)
Ross and Monmouth	Trailers 12 ft. 6 in. from rail to roof, together with 73 ft. long by 9 ft. 6 in. wide stock must not work over this Section. (G.A.1. 3/37. R.6/355.S.B.)
Bridport Branch	Passenger stock not exceeding 64 ft. 5½ in. in length over buffers is permitted to work over this Branch. Passenger stock exceeding 64 ft. 4 in. in length over buffers may be worked into Bridport Station for or arranged with the Station Master, who will be responsible for ensuring that the adjoining road is not occupied by any stock during the period the long vehicles are at the station. (G.A.1. 3/37. R.6/1387A.)
The following to be added :— Blackmill Branch	Coaches 9 ft. wide or over must not pass a train conveying similar stock over the curve between Tondle Middle and Tondle Ogmores Jct. Signal Boxes, nor at Brynamman Station. (G.A.1. 3/37. R.6/1444A.)

Branch.	Restriction.
Brynamman	W.R. Coaching Stock 9 feet wide, may work to Brynamman West Station, subject to service restrictions, and excluding Brynamman Loading Bank Siding. Such stock must not be allowed to work under the Brynamman Road Overbridge, unless specially authorised.

(G.A.26 Op.—5 50 L.K.2 36310 Gen.)

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.
RESTRICTIONS AS TO WORKING OF STOCK OVER CERTAIN BRANCH LINES.

BRANCH.	RESTRICTION.
Ealing and Shepherd's Bush Railway.	Electric Stock only to be worked over this line between Wood Lane Station and Fyover Bridge, and over platform lines at Ealing Station.
Weymouth Quay ...	<p>Buffers of Passenger Stock must be a minimum of 1ft. 3 ins. in width</p> <p>Coaching Stock (W.R. or other Regions) not exceeding the following dimensions can work to Weymouth Quay.—</p> <p>63ft. 6ins. in length over buffers by 9ft. 3ins. in width over body.</p> <p>64ft. 6ins. in length over buffers by 9ft. 0ins. in width over body.</p> <p>W.R. stock 66ft. 8ins. in length over buffers by 8ft. 11ins. in width over body.</p> <p>Non-corridor sets permanently close-coupled together by special type buffing and drawgear must not be attached to a train for Weymouth Quay under any</p>
Easton	<p>PASSENGER TRAIN VEHICLES OF THE LIGHT OR CLASS WORKED OVER THE TRAMWAY MUST HAVE ALL THE GANGWAYS AND HOOBS OPENLY DISCONNECTED, AND THE COUPLERS SECURED BY THE WHICH THE SCREW COUPLINGS ARE TO BE SLACKENED OUT AND TAKEN OFF FOR THE LOOSE COUPLINGS TO BE IN PLACE WHEN THE TRAIN LEAVES WEYMOUTH QUAY.</p> <p>THE STATION DESPATCHING A TRAIN FORMED WITH COACHES OR VANS OF THIS DESCRIPTION, OR IS WORKED FROM A FOREIGN LINE, THE LAST STOPPING STATION BEFORE REACHING WEYMOUTH, MUST BE THE WEYMOUTH STATION, AND MUST BE TO ARRANGE SUFFICIENT TIME TO SECURE THE WORK OF UNCOUPLING.</p> <p>COACHES OR VANS WITH CLOSE BUFFERS AND CONNECTIONS MUST ATTACH TO A TRAIN FOR WEYMOUTH QUAY UNDER ANY CIRCUMSTANCES.</p> <p>All train must be composed of four-wheel or bogie vehicles only. In all cases the last vehicle when leaving Portland must be a brake van or brake coach. No restriction for goods vehicles, except six-wheelers with a wheel base of 22 ft.</p>
Culm Valley	Special eight wheel passenger coaches are provided to work on the Branch. Ordinary eight wheel stock for passenger use, vehicles over 60 ft. in length and also six wheel stock (with the exception of six wheel motor trucks) are prohibited.
Devonport Dockyard Lines ..	Six wheel vehicles are not to be left with in the lines of H.M. Dockyard at Devonport unless previous arrangements have been made for their reception.
Princetown Branch ..	Six-wheeled coaching stock must not work over this branch.
Coleford	Four or eight wheeled vehicles to work over this line as a rule, but six wheelers may be run in cases of necessity.
Eastern and Weymouth ..	Passenger coaches 9 ft. 0 in. in width not to work over these lines <i>EXCEPT AS RESTRICTED</i> by the notice at an overbridge situated at 4 miles from Machen. No coaches of 60 ft. long by 9 ft. wide or over must be worked by another train of any description between Machen South and Machen Junction.
Machen (B. & M. S. & ..	Light wheel stock not exceeding 60 ft. over buffers by 9 ft. wide may work over the branch and use the loop siding at Brynmawr Station.
Quakers Yard and Werthyr ..	Coaches longer than 60 feet over buffers and 9 feet over body must not travel between these points. 70 feet stock may not travel over this Branch.
Cricklade (M. & S.W. Section	<p>Passenger Stock work over these branches must not exceed 163' 6" in length over buffers and 9' 3" in width over body.</p> <p>Goods stock exceeding rail motors and trailers 70 feet long are prohibited from working on the Brynmawr Branch.</p> <p>Coaching Stock 9 ft. 0 in. in width over buffers, having a greater distance than 42 ft. 0 in. between bogie centres is prohibited from working into the Goods Shed Siding at Cricklade, and this type of stock must not be placed on the siding referred to.</p>
Wood Lane Milk Depot	Vehicles exceeding 61 ft. 6 in. in length over buffers and "Red Triangle" stock must not work to this Depot.

Weymouth Quay

Buffers of Passenger Stock must be a minimum of 1ft 5 ins in width
Coaching Stock (W.R. or other Regions) not exceeding the following dimensions can work to Weymouth Quay

63ft 6 ins in length over buffers by 9ft 3 ins in width over body

64ft 6 ins in length over buffers by 9ft 0 ins in width over body

W.R. stock 60ft 8 ins in length over buffers by 8ft 11 ins in width over body

Non corridor sets permanently, close coupled together by special type buffing and drawgear must not be attached to a train for Weymouth Quay under any circumstances.

Automatic coupled **corridor** stock not exceeding 67ft 3 in in length over extended buffers and 9ft 3 in extreme width may work to Weymouth Quay but must be fitted on all occasions with gangway curtains

(G.A.23.7.49 R 6 1619 A.)

Paddington to Penzance via Westbury, including:—

West Ealing to Greenford.
 Hanwell Loop.
 Uxbridge Vine Street Branch.
 Staines Branch.
 Windsor Branch and Slough West Curve.
 Maidenhead to High Wycombe.
 Marlow Branch.
 Henley Branch.
 Basingstoke Branch.
 Brentford Branch.
 Newbury to Didcot.
 Lambourn Branch.
 Winchester Branch.
 Westbury Loop.
 Frome Loop.
 Salisbury Branch.
 Castle Cary to Weymouth Town Station.
 Abbotsbury Branch.
 Bridport Branch (to Bridport).
 Yeovil to Curry Rivell Junction.
 Chard Branch.
 Alton Branch.
 Bournemouth (to Bournemouth).
 Totton Branch.
 Stoke Canon to Morebath Junction.
 Teign Valley Branch (includes Exeter Railway).
 Moretonhampstead Branch.
 Kingswear Branch.
 Brixham Branch.
 Ashburton Branch.
 Kingsbridge Branch.
 Launceston Branch.
 Plymouth North Road to Millbay Pier.
 West Cornwall Loop.
 Bodmin Branch (to Bodmin Town).
 *Fowey Branch.
 Newquay Branch (to Newquay).
 Chacewater to Newquay.
 Falmouth Branch.
 Helston Branch.
 St. Ives Branch.

*Lostwithiel to Fowey (Subject to speed restriction of 10 m.p.h. between 282 m.p. and 282m 65c)

* Station staff to call attention to 1' in gap between centre of continuous footboard and Fowey platform.

Paddington to Penzance via Bristol, including:—

Bristol Relief Lines.
 Wellingford Branch.
 Faringdon Branch.
 Calne Branch.
 Thingley Junction to Westbury.
 Holt Junction to Patney and Chilton.
 Bathampton to Holt Junction.
 Bradford West Junction to Bradford South Junction.
 Limply Stoke to Camerton.
 Bristol to Frome.
 Portishead Branch via Parson Street Junction.
 Clevedon Branch.
 Yatton to Witham.
 Blagdon Branch.
 Weston-super-Mare Loop.
 Narrowways Hill Junction to Ashley Hill Junction.

Yaffton to Withams.
Blagdon Branch
Weston super Mare.
Ashley Hill Jet. and Avonmouth Docks Station
on Joint Line.

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

RESTRICTIONS ON THE WORKING OF GREAT WESTERN WIDM. STOCK *Continued*

Pallington to Fistguard, Newland and Milford Haven and Penryn Dock
 Waterbury to Bristol and Exmouth Dock
 Malmesbury Branch.
 Bristol Loop.
 Patchway to Filton.
 Filton West Loop.
 Pilning Junction to Hallen Marsh Junction.

Trains composed of or containing any of the above mentioned stock must not be passed by trains on the alignment between the Boxes set out below and can only be dealt with by pre-arrangement.

Cardiff General to Llanach via Ystrad Mynach and Cardiff East Branch. When travelling in either direction between Cardiff General and Queen Street Stations.

Llandough Lower and Cogan Sidings.

Barry Junction and Barry Sidings.

Llantwit Major and Llandow

Scutcherdown Road in Up Middle and Up Platform Lines.

Duch v Quarry and Ewenny Quarry.

Cowbridge Road Junction and Bridgend East.

Paddington to Oxley Sidings, via Bicester or Oxford.

Uxbridge (High Street).

Oxford to Princes Street.

Earford Branch.

Abingdon Branch.

Wokingham Branch.

* Oxford to Churchdown via Kingham.

Kingham to King's Sutton.

Kingham to Honeybourne.

* Honeybourne to Churchdown.

Honeybourne to Worcester.

Honeybourne to Hatton.

Honeybourne to Tyseley.

Worcester to Leominster.

Worcester to Leominster.

Hartlebury to Handsworth.

Stourbridge Junction to Priestfield.

Worcester to Hereford.

Swindon to Gloucester.

Kemble to Cirencester Town.

Gloucester to St. Ann's Tunnel Junction.

Gloucester to Hereford.

Gloucester to M. Junction.

Ross to Little Mill Junction.

St. Ann's Tunnel Junction to M. Junction.

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M. Junction to M. Junction.

63 ft. 6 ins.
 long by
 9 ft. 3 ins. wide.

The following is the position in regard to the acceptance of this Stock by other

Companies:—

Southern Company—Will not accept.

L. & N.E. Company—Will not accept.

L.M. & S. Company—Will accept on following Sections:—

Western Section.

Shrewsbury and Crewe.

Crewe and Holyhead.

Crewe and Liverpool (Lime Street).

Crewe and Manchester (London Road).

Chester and Liverpool via Hatton Junction.

Chester and Manchester (Exchange) via Warrington.

Crewe and Glasgow (Central).

Crewe and Edinburgh (Princes Street).

Crewe and Blackpool (Central).

Crewe and Blackpool (Talbot Road Station only, not Excursion Platform).

9 ft. 3 ins. wide Stock may work over Sirhowy Line via Nine Mile Point.

Midland Section.

Standish Junction and Yate Junction.

Bristol and Birmingham.

Bristol and Bradford.

Gloucester and Birmingham.

Cheltenham (Lansdown) and Birmingham.

Such stock is also prohibited from working on the following Sections and Branches:—

Highworth Branch.

M. & S.W. Junction Railway Section.

Culm Valley Branch.

Bassaleg to Barry Junction (B. and M.).

Machen to Caerphilly.

Vale of Glamorgan.

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RESTRICTIONS ON WORKING OF WESTERN REGION WIDE STOCK—page 151.
64 ft 8 in. long by 8 ft. 11 in. wide

Paddington to Fishguard (via Landore and or Swansea High St.), Neyland, Milford Haven and Pembroke Dock, including:—

Swindon to Gloucester via Kemble.
Kemble to Cirencester Town.
Gloucester to Severn Tunnel Junction.
Gloucester to Hereford.
Newent Branch (Over Junction to Ledbury).
Cirencester to Monmouth.
Ross to Little Mill Junction.
Little Mill Junction to Shrewsbury.
Winterbourne to Bristol.
Bristol to Avonmouth Dock Station (Joint) via Hallen Marsh Junction.
Stapleton Road to Ashley Hill Junction.
Malmesbury Branch.
Bristol Loop.
Patchway to Filton.
Filton to Bristol Loop.
Bristol Loop Junction to Hallen Marsh Junction.
District Lines. Court Sart Junction to Llanelli via Skewen East Junction, and Llanlas Junction (also via Neath Junction R. & S.B. and Neath Loop, Llanlas Junction). Morlais Junction South and Llandilo Junction.
Mormston Branch. Felin Fran West to Hafod Junction.

Trains containing any of the above stock must not pass nor be passed by any stock on the area between the boxes set out below in either direction and can only be dealt with by pre-

Section	Boxes
Cardiff West and Penarth C. via South (inclusive)	Cardiff West and Penarth C. via South (inclusive)
and Barry	Llandough Lower and Cogan Sidings.
	Bighs Junction and Gas Works Junction at Barry Docks.
	Barry Junction and Barry Sidings.
	Llantwit Major and Llandow.
	Southern Down Road in Up Middle and Up platform lines.
	Duchy Quarry and Ewenny Quarry.
	Cowbridge Road Junction and Bridgend East (inclusive).
Cardiff Central to Hereford H. or Llan-	Cardiff East to Cardiff Central Street South
Cardiff East Junction Branch	Cardiff East—over double junctions Main to Relief
and Dinas Mawddwy	Lines
	Carmarthen West—over double junctions of Relief lines
	with Main Lines.

Paddington to Saltney Junction via Bicester or Oxford and Aynho Junction, including:

Uxbridge High Street Line.
Abingdon Branch.
Oxford to Princes Risborough.
Woodstock Branch.
Fairford Branch.
Oxford to Gloucester via Kingham.
Kingham to Kings Sutton.
Kingham to Honeybourne.
Honeybourne to Gloucester and Cheltenham St. James.
Honeybourne to Worcester.
Honeybourne to Hatton.
Honeybourne to Tyseley.
Alcester Branch.
Worcester to Leominster.
Worcester to Hartlebury.
Hartlebury to Handsworth.
Stourbridge Junction to Priestfield.
Worcester to Hereford.
Shrewsbury to Buttington.
Ruabon to Barmouth Junction.

Must not pass out of gauge loads over LMS. Company's Maintenance between Churchdown and Gloucester.

Central Wales.

Moat Lane to Brecon and Dolygaer.
Whitchurch to Aberystwyth.
Oswestry to Gobowen.
Aberystwyth to Bryn Teifi.
Pwllheli to Dovey Junction (Speed restricted to 5 m.p.h. through tunnels Aberdovey to Dovey Junction).
Wrexham to Ellesmere.
Lampeter to Aberayron.

RESTRICTIONS ON THE WORKING OF GREAT WESTERN WIDE STOCK. page 151.

Reference to the following to be inserted on page 151:

66 ft. 8 in. long by 8 ft. 11 in. wide.

Lines prohibited:

- Ealing and Shepherds Bush.
 Hammersmith and City Line.
 Highworth Branch.
 M. & S.W. Junction Line throughout.
 Culm Valley Branch (Tiverton Junction to Hemyock).
 Princetown Branch (Yelverton to Princetown).
 Liskeard and Looe Line.
 Ex Brecon and Merthyr (Bassaleg Junction to Duffryn Isaf).
 Caerphilly Branch (Machen Junction to Caerphilly (exclusive)).
 Burry Port and Gwendraeth Valley Railway and Branches.
 Llanelly Up Bay Platform Line.
 Brynamman Branch.
~~Swansea and Wye Junction Line throughout~~ GA 23
 Golden Valley Line (Ponttril to Hay).
 Gloucester Docks. Permitted with the exception of the following sidings:
 Nos. 3 and 4 (Coal sidings).
 No. 5.
 No. 6 (Marshalling sidings).
 No. 7.
 Shipston-on-Stour Branch.
 Pontcysyllte Branch (Rhos to Trevor).
 Wrexham and Minera (Brymbo to Coed Poeth).
 Cleobury Mortimer and Ditton Priors Light Railway.
 South Wales Docks Lines.

Lines on which restrictions are necessary.

Weymouth Tramway	Speed to be restricted to 4 m.p.h. and screw couplings to be slackened to 4 threads
Lostwithlee and Fowey Line	Speed to be reduced to 10 m.p.h. between Lostwithlee and Fowey Station.

Passing Restrictions. M. & S.W. Junction Line. Permitted. Train or Train conveying passengers. The following Box-Box Stations

From	To	Line or Lines affected
Pontypool Road Station	Between the platforms at Ponthur Station	Mains and Reliefs
Mandee Junction North	Mandee Junction East	Mains
Pontypool Road Station South	Trumpton Junction	Mains
Cefn Crib	Crumlin Junction	Mains
Crumlin H.L. (inclusive)	P. & S.W. Junction	Mains
Hengoed H.L.	Penalltau Junction	Mains
Nelson and Llancniach	Quakers Yard H.L.	Mains
Cresselley Crossing	Middle Duffryn	Mains
Pontymister South	Risca Junction	Reliefs
Aberbeeg Junction	Aberbeeg North	Mains
Risca Junction (inclusive)	Brickworks South	Mains and Reliefs
Rock View North	N. & S.W. Junction	Mains
Pontnewydd Junction	Aberystwyth L.L.	Mains
Hengoed H.L.	Ystrad Mynach North	Mains
Long Dyke Junction	Tyrdal Street Crossing	Mains
Riverside North	Cherry Road	Mains
Cardiff East	Cardiff (Queen Street) South	Mains
Cardiff West	Penarth Curve South	Mains

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

RESTRICTIONS ON THE WORKING OF WESTERN REGION WIDE STOCK

—page 151.

66 ft 8 ins. long by 8 ft 11 ins. wide

Delete the following entry from the list of lines prohibited :—

M. & S.W. Junction line throughout

The following new entry to be inserted :—

This stock may work over the M. & S.W. Junction line subject to the undermentioned stipulations :—

Prohibition.

Cricklade—Goods Shed Sid ing (in accordance with the entry on page 149).

Passing Restrictions.

Not to pass nor be passed by similar passenger stock and or freight trains on adjoining lines at the following points :—

Swindon Town, between Boxes " A " and " B ".

Chiseldon, except in loops at Marlborough end of station.

Marlborough, within platforms

Colingbourne, within platforms.

Ludgershall, within platforms.

(G A.27.Op.—I 51. R.6 3926.)

Passing Restrictions.

The last paragraph of these Restrictions dealing with the Newport Engineering District as listed in G A 26 Op. to be deleted and the following inserted :—

These coaches are prohibited from passing other coaching stock and out-of-gauge loads between the following points in the Newport Engineering District :—

Maesycwmmmer Station (inclusive) to Pengam Station

The existing restrictions so far as other Regions are concerned will continue to apply as already shown in G A 18

63 ft. 6½ ins. long by 9 ft. 3 ins. wide.—Page 152.

Delete the following from the list of lines over which this stock is prohibited :

Vale of Glamorgan

(G A 31—7/56 R 6/3926)

... H.L. inclusive and Penar Junction.

... H.L. and Penar Junction.

... & Llancaiach and Quaker's Yard H.L.

... Crossing and Middle Duffryn

... not pass, nor be passed by, any Passenger Train or Train conveying passenger stock, or

... gauge load on the opposite (or adjacent) running line (either direction) between the

... Box to-Box sections —

From	To	Line or Lines affected.
...er South	Risca Junction	Reliefs.
... Junction	Aberbeeg North	Mains.
... (on inclusive)	Brickworks South	Mains and Reliefs.
... North	Nine Mile Point	Mains.
...ydd Junction	Abersychan L.L.	"
...	Ystrad Mynach North	"
... Junction	Tyndall Street Crossing	"
... North	Clarence Road	"
...	Cardiff (Queen Street) South	"
...	Penarth Curve South	"

... are prohibited from passing other coaching stock and out-of-gauge loads between
... in the Cardiff Valleys Engineering Division —

... to Cogan Sidings

... to Cadoxton, Nos. 1 and 2 lines (inclusive),

... (exclusive) to Gas Works Junction

... to Barry Island.

... Station (inclusive) to Black Lion.

... to Trehafod Junction.

... to Rhondda Cutting

... Line portion at Maerdy.

... to Mountain Ash

... to Groeswen.

... to Hengoed L.L.

... inclusive to Pengam Station.

... to Treforest Junction

... (exclusive) to Radyr Station (inclusive).

... so far as other Regulations are concerned will continue to apply as already

... GALE

(G.A.26 Op—5 50. R 6 3926.A.)

RESTRICTIONS ON WORKING OF WESTERN REGION WIDE STOCK—page 151.
66 ft. 8 in. long by 8 ft. 11 in. wide.

The restrictions which apply, so far as the Western Region is concerned, have been revised, and the following should be substituted for those set out in G.A. 18.

Lines Prohibited :—

Ealing and Shepherds Bush.

Hammersmith and City Line.

Harrow Branch.

~~London and Tilbury Branch.~~

Claydon Branch (Tiverton Junction to Hemyock).

Princetown Branch (Yelverton to Princetown).

Linkeard and Loose Line.

South Wales Docks Lines.

Ex. Brecon and Merthyr (Bassaleg Junction to Duffryn Isaf).

~~Ex. Brecon and Merthyr~~ Branch (Machen Junction to Caerphilly (exclusive)).

Dowlais Junction and Dowlais Cae Harris (Taff Bargoed Branch).

~~Ex. Brecon and Merthyr~~ and Gwendraeth Valley Railway and Branches.

~~Ex. Brecon and Merthyr~~ Up Bay Platform Line.

~~Ex. Brecon and Merthyr~~ Valley Line (Pontreflas to Hay)

~~Ex. Brecon and Merthyr~~ Docks Prohibited with the exception of the following sidings :—
Nos. 3 and 4 (Coal Sidings)

No. 5.

No. 6. (Marshalling Sidings)

No. 7.

Shepton-on-Stour Branch.

~~Ex. Brecon and Merthyr~~ Branch (Rhos to Trevor)

Wrexham and Minera (Brymbo to Coed Poeth).

~~Ex. Brecon and Merthyr~~ Mortimer and Ditton Priors Light Railway.

Lines on which restrictions are necessary :—

~~Ex. Brecon and Merthyr~~ Tramway

Lostwithiel and Fowey Line Speed to be restricted to 4 m.p.h.

Speed to be reduced to 10 m.p.h. between 282 m.p. and 282m. 65c. Fowey Station

Passing Restrictions :—

Stocks not to be passed by, stock of identical dimensions, or by trains conveying passenger stock longer than 9 feet 6 inches wide, or by stock 73 feet long by 9 feet wide, also by trains conveying stock of similar dimensions and/or out-of-gauge loads, between the following Box-to-Box

Stockpool Road Station South and Trosnant Junction (Clarence Street)

Carn Crib and Crumlin Junction inclusive (This section includes Glyn Tunnel.)

These coaches are prohibited from passing other coaching stock and out-of-gauge loads between the following points in the Cardiff Valleys Engineering Division :

Cogan Junction to Cogan Sidings.
 Biglis Junction to Cadoxton, Nos. 1 and 2 lines (inclusive).
 Barry Dock (exclusive) to Gas Works Junction.
 Barry to Barry Island.
 Quakers Yard L.L. Station inclusive to Black Lion.
 Eurw Branch Junction to Trehafod Junction.
 Gyfeillon Lower to Rhondda Cutting.
 Double Line portion at Maerdy.
 Nixon's Crossing to Mountain Ash.
 P.C. and N. Junction to Groeswen.
 Dowlas Junction to Dowlas Cae Harris.
 Ystrad Mynach North to Hengoed L.L.
 Maescymmer Station (inclusive) to Pengam Station.
 Over junction curves at Treforest Junction.
 Radyr Junction Box, inclusive, to Radyr Station (inclusive).

L & N.E. Railway.

Lines Prohibited :

Ferryhill to Hartlepool.
 North Leith Branch.
 Penicuik Branch.
 Eyemouth Branch.
 Newcastle Central.

To following sections by prior arrangement only :

Between Sheffield and Manchester
 Between Marks Tey and Long N
 Between St. Dunstons
 Between Darlington and Shild
 Between Durham and Bishops Auckland
 Between Wormit Leuchers and Treport.
 Newbroom Branch.
 Richmond Station.
 Whitby Station.

LMS Railway.

Lines Prohibited :

Between Carnforth and Whitehaven.
 Between Maryport (exclusive) and Carlisle.
 Between Bingley Junction Shipley and Bradford Junction Shipley.
 Between Halifax East Junction and Ovenden.
 Between Beaufort S.B. and Ebbw Vale Junction.
 Dursley Branch.
 Thornbury Branch.
 Nailsworth Branch.

Loop Line L.
 Euston Branch.
 Hatfield Branch.
 Widnes and St. Helens Line.
 Penistone Station.
 Dunstable Bay Line.
 Swansea Vale Line and Branches.
 Swansea Victoria Station.
 Brynmawr, Blaenavon Bay.

(A. Line) Up Line through station.
 Platform.
 Walsden High Level Up Fast Platform.
 No. 1 Down Steam Line.
 St. Pancras Station.
 Euston Station Poplar Branch.
 Euston Street.
 Euston Station Down Platform.
 (Tilbury Junction Bow to Bramley Junction Line)
 Walsden (Central) Station platform roads (LMS and L. & N.E. Joint).
 No. 2 Bay Platform Line.

LMS Railway

Lines Prohibited :—continued.

Bradford (Exchange) Nos. 1 and 10 Platforms.
Keighley (Worth Valley Branch Platform Lines).
Invergordon Harbour Branch.
 Branch.
Balerno Branch.
Dumbarton Central Loop Lines.
Barncluth Tunnel.
Severn and Wye Joint Line.

Southern Railway.

Lines Prohibited :

Between Tonbridge and Battle.
Between Deptford and Woolwich Arsenal.
Between Dormans and Kingscote.
Between Canterbury West and Whitstable Harbour.

Stock 60ft. long by 9ft. wide Page 153.

Stock 60ft. long by 9ft. wide - Page 153.
The following to be added to the entry under heading 'L M. & S Company (Western Section)'
Swansea Victoria and Craven Arms (provided the length over buffers does not exceed 63ft. and the width over projections does not exceed 9ft 4in) subject to the following prohibitions:—
Work road in the Carriage Sheds.

Swansea, Victoria—Wash road in the Carriage Sheds.

(G.A 30 Op -9 54 R.6).

Reference to the following to appear on page 1 -

COACHING STOCK OF CROSS COUNTRY DIMENSIONS

New cables constructed from ~~aluminum~~ ~~steel~~ ~~cables~~ ~~and~~ ~~polyethylene~~ ~~plastic~~ sent into traffic.

[illegible]

L. & N. E.

Accept generally over Main Lines.

L.M.&S.

Can work generally, except over the following restricted sections:—

Dursley Branch.

Thornbury Branch.

Nailsworth Branch.

Halesowen Branch

Churnet Valley Line Platforms at Uttoxeter Station

Loop Line Etruria to Kidsgrove.

Loop Line Enters to Ridge Grove,
Disley Tunnel on Buxton Branch.

Widnes and St Helens Line.

Barrow (Ramsden Dock) and Coniston

Whitehaven, Bransty Station (except by special arrangement)

Halifax (Nos 4, 6 and platform lines).

Penistone (Through station).

Penistone & Thro Raiemo Branch

Balerno Branch
Maryport (exclusive) and Carlisle,

Maryport (exclusive) and
Hampstead Junction Line.

Leadhills Branch.

Bingley Junction, Shipley and Bradford Junction, Shipley

Bingley Junction, Str.
Dunstable Bay Line.

Macclesfield (Central) Station Platform Roads.

On the Hereford, Hay and Brecon section must work over main running lines.

Southern

Accept generally, except between : —

Cambridge and Battle

~~Caspar Holten & Crystal Palace Low Level~~

(Charlton and Plumstead)

Dartford and Strood.

Canterbury West and Whitstable Harbour

(7 A) (R.G 13.8.A)

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the following:—

Colne Valley Branch

 $(G_A \otimes I_{\mathbb{B}}) R^{(6)}_{n(A)}$
$$\frac{d}{dt} \left(\frac{\partial L}{\partial \dot{x}} \right) = \frac{\partial L}{\partial x}$$

and Holyhead.

chester (London Road).

- er and Liverpool (via Halton Junction).
- ter and Manchester Exchange (via Warrington).

• we and Edinburgh (Princes Street).

~~Passengers may not walk over the Schoway Lane via Nine Mile~~

Yate notation.

in Lansdown and Birmingham.

(North Stafford Section) accept as shewn below:—

d Macclesfield.

wich and Norton Bridge.

Branch.

L. & S.W. Section) accept for general working

S.R. & C. Section cannot accept for general working.

Joint Line accept, providing vehicles have not to be taken into
ent to their Doors.

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.
DIMENSIONS OF VEHICLES TO STATIONS ON S.E. & C. AND L.B. & S.C. SECTIONS OF
SOUTHERN RAILWAY.

"Double ended" Beetles (B & C), Horse Boxes, Patos B & C, brake coaches with side look-outs, large Milk Trucks, Sphares G & J, and other vehicles which exceed the dimensions set out below cannot work over the following sections of the Southern Railway:

Tonbridge & Cr. wharf.	The High Crystal Palace Low Level Tunnel.
Hastings and Winchelsea.	Streatham Hill & West Norwood.
Whitstable Harbour Branch.	Between Charlton and Plumstead.

Length of body	50 feet 0 inches.
Width of body at waist	8 " 0 1/2 "
Width across side cornices of roof	8 " 4 "
Width across top step boards	8 " 10 "
Width across bottom stepboards on body	8 " 4 "
Width across bottom stepboards on bogie truck	8 " 6 "
Height of top of side cornice from rail level	10 " 8 1/2 "

This restriction does not apply to Tunbridge Wells West (L.B. & S.C. Section) via Oxted, nor to Hastings via Polegate.

The "double-ended" beetles, luggage bodied and horse boxes, with luggage compartment, also brake coaches with side look-outs are not within these dimensions.

Coaches with an elevated or dome roof, or covered carriage trucks, must not work through via Victoria to the L. & N.E. and L.M. & S. Lines, but the latest pattern G.W. Horse Boxes may be used for traffic to the L. & N.E. and L.M. & S. systems via Victoria.

G.W. brake coaches with elevated or dome roofs, or covered carriage trucks, to the L.B. & S.C. section of the Southern Railway. Such vehicles, if they are not used, therefore, may be used for stations on the L.B. & S.C. section of the Southern Railway.

PASSENGER TRAIN STOCK FOR ILFRACOMBE BRANCH. Vehicles with lower centre step boards will be accepted from the Great Western Company at Barnstaple for working over the Southern line to Ilfracombe.

The Southern Co. have agreed to L. & N.E. Co. seaching stock 30 feet long and ft. 7 ins. over buffers, and 9 feet wide working to Ilfracombe via Barnstaple.

VEHICLES MUST NOT BE LOADED AS UNDER :

STATION TO	FIRM.	VEHICLES NOT TO BE LOADED.
Gloucester (Docks, L.M. & S. Side)	Ashbee & Co. Fielding & Platt Gloucester Joinery Co. Griggs & Co., Ltd. Haime & Corry, Ltd. H. W. Ingram & Co., Ltd. Matthews & Co., Ltd. Meggitt & Jones Nicks & Co. Price, Walker & Co. J. Romans & Co., Ltd. Sessions & Sons, Ltd. Western Trading Co., Ltd.	30-ton rail and timber trucks. ("Macaws B," "C," "D" & "E.")
London :— East India Dock	All firms	Six wheeled vehicles (passenger or goods) with shipment traffic. Four-wheeled vehicles must be used. Vehicles with wheelbase over 11 ft. Well trollies ("Crocodile," "Crocodile A," &c.), Bogie rail and timber trucks ("Macaws B," "C," "D" and "E"), girder and boiler trucks ("Pollen," "Pollen A," &c.), and "Loriot."
Millwall Dock	All firms	Six wheeled vehicles (passenger or goods) with shipment traffic; four-wheeled vehicles must be used. Vehicles with wheelbase over 29 ft. or 3 ft. 3 in. wide. Well trollies ("Crocodile," "Crocodile A," &c.) bogie rail and timber trucks ("Macaws B," "C," "D," and "E"), girder and boiler trucks ("Pollen," "Pollen A," &c.), and "Loriot."
*Royal Albert Dock South Dock	All firms * (Except Motor Packing Co., South Dock)	Six wheeled vehicles (passenger or goods) with shipment traffic. Four-wheeled vehicles must be used.
Swansea :— King's Dock Prince of Wales Dock South Dock	.. .. .. .. .. .. .. .. .. ..	Fruit vans. Fruit vans.
Gloucester (Docks L.M.S. side)	Ashbee Son, & Co., Ltd. Gloucester Joinery Co., Ltd. Griggs & Co., Ltd. Meggitt & Jones, Ltd. Nicks & Co. Price, Walker & Co. Romans & Co., Ltd.	Cattle wagons. Covered vans with wooden bodies. Bogie Boiler vehicles and vehicles with wheel base exceeding 15 ft. 6 in.

Reference to the following to appear on page 155 :—

CONVEYANCE OF ROAD-RAIL MILK TANK TRAILERS BY RAIL.

Road rail milk tank trailers are fitted with an adjustable screw spring stop above the centre of each bearing spring to prevent further deflection of the springs, and thus avoid rolling during transit. These stops must be screwed down until they are in contact with the springs before commencing a journey by rail, and the loading staff must see that this is done in all cases, whether the tanks are loaded or empty.

Care must also be taken to see that the chains provided for securing the road trailer to the rail vehicle are not twisted when finally tightened up, in order to ensure that they will not become slack en route.

When an empty road rail tank is required to be loaded with milk whilst on the rail vehicle, the adjustable screw spring stops must first be screwed back sufficiently clear of the springs to allow for any deflection of the tank springs likely to take place during loading, and after loading has been completed the screw stops and chains must be properly tightened and adjusted. In the case of a loaded road-rail tank requiring to be emptied whilst on the rail vehicle, the chains must be slackened back before emptying in accordance with the printed instructions on the underframe of the rail vehicle.

(G.A. 5.—2/39. C.M.E.—31299)

Reference to the following to appear on page 155 :—

SIDE CHAINS ON WARFLATS AND WARWELLS.

Side Chains on Warflats and Warwells must not be coupled between vehicles.

(G.A.15—12 44 L.K1/7688 6A

The following to be substituted for the fifth paragraph of these instructions —

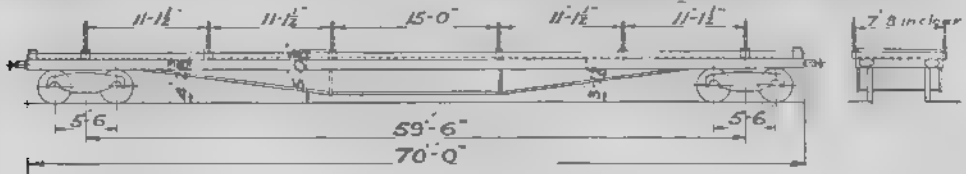
When coaching stock is propelled on running lines or from running lines to sidings the Guard or Shunter should ride on the leading vehicle or the nearest suitable vehicle, keep a good lookout and be prepared to hand signal to the Driver or Fireman. If this is impracticable the Guard or Shunter should place himself in such a position on the ground that he can plainly signal to the Driver.

(G.A.30 Op.—9/54 O.M.12725).

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

MACAW C Nos. 84995 and 84996.

These wagons must not be loaded to any other Company's line or sent loaded or empty on to the C.W. Docks Lines without the authority of the Chief Goods Manager being first obtained. Chief
(C.A.16. 5/46. C.G.M. W.T./X. 99864)



TAIL LAMP ADAPTORS FOR L. & N.E.R. VEHICLES.

Adaptors to enable Great Western tail lamps to be carried on the tail lamp brackets of L. & N.E.R. vehicles have been supplied to various stations. The adaptors are borrowed before being returned to the Station, and must be returned to the home station as booked parcels in the same way as hose pipe adaptors for slip carriages.

SHUNTING INSTRUCTIONS.

SHUNTING, &c., IN STATIONS AND STATION YARDS.

Signalmen must exercise every care in working in station yards where the points are frequently used.

A train or portion of a train, must never be shunted in a yard (except at the points) where the points are close to a signal, until the last vehicle is over the points, and the Signalman has given the necessary signal for the purpose of seeing that the last vehicle is over the points, and the Signalman has given the necessary signal to turn the points; the Signalman must not turn the points until he has received and signalled the Shunter that the whole of the train is over the points, and the Engine man move his engine until he has received a signal from the Shunter that he is to do so.

If there is a Shunter at the yard, this duty must be performed by the Guard of the train, whether passenger or freight.

Passenger stock must be provided with hand brake must not be used, but must remain attached until the movement has been brought to a stand, the train must be secured.

When empty coaching stock is prepared for shunting, as for running lines to sidings, the Guard or Shunter must, on the ground, which is the most suitable place therefor, keep a good look out and be prepared to hand signal to the Driver.

The Driver of a shunting engine, not accompanied by a Guard or Shunter, must satisfy himself that the points are in proper position before moving his engine over them. After moving over points the Driver must also satisfy himself that the engine is clear, and give an indication to the Signalman by three short, sharp whistles.

The person conducting the shunting must see that the points are in proper position before giving the signal for the train to move in either direction.

Signalmen must satisfy themselves that the points and signals are set in their proper position for shunting operations, as well as for the safe passage of trains, and they will be held responsible for reporting every case of an error in the part of Shunters or Guards to comply with these instructions.

Persons must give notice to all persons removing goods into or out of trucks and to men engaged in repairing wagons, by whistle once before moving wagons that are standing in message sidings, or which may be under repair, and Shunters and others, before giving the signal to the Engine men to move such wagons, must always walk the whole length of wagons and personally caution each individual who is engaged as described, and at the same time make him understand at what time it will be safe to resume his work; any neglect to obey this regulation will be severely dealt with.

Signalmen, Foremen, Shunters and Guards, when at work in a yard with engines, must instruct Drivers verbally, as far as possible, with respect to the movements of the engines, and not trust to hand signals.

When an engine is shunting between a siding and a running line, the Signalman must not reverse the points until he has had an understanding with the person in charge of the shunting, and, in the event of there being no one in charge, then an understanding must be arrived at with the Driver.

Shunters, Shunt Horse Drivers and Capstan men must exercise care in the movement of trucks into or out of goods sheds. Trucks must not be moved until it has been ascertained that the doors of the wagons are securely fastened, and that persons working in connection with trucks have been duly warned.

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

SHUNTING INSTRUCTIONS—Continued.

SHUNTING, &c., IN STATIONS AND STATION YARDS—Continued.

Shunters must be careful not to move passenger trains or carriages whilst men are on the roof.

(G.A. 8.—5/41. LK1/6406/5.)
The staff concerned should exercise great care in order to prevent damage to goods by rough shunting.

Milk Traffic, etc.—Guards, Shunters and others concerned must take the necessary care to ensure the proper handling of this traffic.

Shunting of Acids in Glass Carboys. Instructions have been issued to the manufacturers in regard to the proper loading, etc., of this traffic.

Nitric Acid, if spilt upon the straw packing of the carboy, may set it on fire, and the fracture of one carboy may lead to the fracture and loss of acid from a large number of others. In addition to this, Nitric and other acids are liable to cause serious injury to persons or damage to merchandise and rolling stock.

Wagons containing Acids in carboys bear distinctive labels, e.g., "Dangerous Goods," "Acids in Glass Shunt with Care," and it is essential that the instructions in regard to shunting should be strictly observed in dealing with the wagons so as to prevent the breakage of the carboys.

The trucks containing acids must be formed as far from the engine as possible and loose shunting of, or against, such trucks is prohibited.

The stations must see that the "Dangerous Goods" and the "Acids in Glass Shunt with care" labels are affixed to both sides of the truck in which Acids are loaded before despatch.

Station Masters and Goods Agents are enjoined to see, as they go about the station yards, that the regulations herein laid down are strictly observed by the staff concerned.

DEFINITION OF THE SEVERAL MODES OF SHUNTING WAGONS.

In order that there may be no misunderstanding as to the meaning of the shunting terms "Double Shunt" and "Fly Shunt" in common use among Shunters, Guards and other staff, the following definitions are to be noted:—

Double Shunt.—This means the propelling of two lots of wagons uncoupled from the engine propelling them, but coupled together in two separate lots from the line of rails on to two different lines of rails, that is, one lot going through one set of points on to one line of rails, and the other lot going through another set of points on to another line of rails. The term "Double Shunt" also applies to the case of an engine propelling wagons when the wagons are turned on to one line of rails and the engine turned on to another line.

Fly Shunt. This means that while an engine is drawing wagons attached to it towards a set of facing points, the wagons are uncoupled from the engine or from each other, and the engine or engine and trucks are run on to one line of rails after which the following wagons are run on to another line of rails.

Double Shunting is strictly prohibited except when done by engines specially for the purpose of shunting, attended by experienced shunters.

Fly Shunting is strictly prohibited unless the circumstances do not permit of the shunt being performed in any other manner, and even then, the operation must be performed by an engine specially used for shunting and attended by an experienced shunter.

ENGINE DRAWBAR HOOKS AND COUPLINGS.

Engine screw connections, when not in use, must always be placed on the hook provided on the buffer beam for the purpose. They must not be allowed to hang down or be thrown back over the engine draw bar hook.

CARRIAGE AND WAGON DRAWBAR HOOKS AND COUPLINGS.

Carriage or Wagon screw connections, when not in use must be placed upon the hook provided on the headstock for that purpose. They must not be allowed to hang down or be thrown back over the drawbar hook.

FASTENING OF WAGON DOORS—page 156.

These instructions to be amended as follows:—

Guards must see that doors of all wagons formed on their trains or attached during the journey are securely fastened. It will not, however, be necessary for the end doors of 15-ton and 12-ton colliery wagons to be fastened when returning empty from docks to local collieries on this Company's system in South Wales, also to the Forest of Dean or to local collieries situate on the L.M.S. Company's Swansea Vale Line, and as far north as Llandovery on the L.M.S. Main Line.

(G.A.3. 12/37. L.K.1/4081,6.)

SHUNTING, ETC., IN STATIONS AND STATION YARDS—Page 156.

The following to be inserted as the third paragraph on page 156—

Steam Rollers and Tractor Engines.—Loose shunting of vehicles loaded with steam rollers or tractor engines, is prohibited—they must be brought to a state of rest whilst coupled to train or shunting engine. (G.A.19—10 48. E.85921.H.)

Reference to the following to appear on page 156.

SHUNTING INTO PRIVATE SIDINGS

In order to avoid the possibility of injury to persons who may be working near lines in private sidings or walking along such sidings or over level crossings, a man should, when practicable, accompany or precede wagons being propelled, and no engine or wagon should be moved until warned, by means of the engine whistle, or otherwise, has been given to such persons. After dusk or during fog or darkness, where practicable, be exhibited on the engine or leading wagon when shunting into private sidings.

(G.A. 7.—3, 40. LK1/5418/6.)

TELEPHONE BOXES IN YARDS AND RUNNING LOOP LINES, ETC.

Arrangements have been made and when repainting becomes necessary, for telephone boxes to be painted with black and white diagonal stripes.

(G.A.12.4/43. LK1/7014/7.)

Reference to the following to be made on page 156 :—

REGULATIONS FOR THE WORKING OF DIESEL ELECTRIC SHUNTING ENGINES OVER RUNNING LINES.

Diesel Electric Shunting Engines working in Yards are normally manned with Driver only on the footplate.

Where such Engines are required to work over the running lines the Driver will be held responsible for obeying fixed signals and the engine must be accompanied by a second person competent to carry out Rule 55 e. Detention of Trains at signals and also The Protection of Running Lines, in the event of an Emergency in accordance with Rules 178 to 181. The second person will, normally, be directed by the Traction Foreman, but where difficulty or delay would occur in so doing the Traffic Department representative in charge must provide a competent person.

In the event of a section being obstructed by a derailment or disabled train a Diesel Electric engine when a steam engine is not available, may be used to clear the section providing it is accompanied by a second person competent to carry out Rules 55 and 178 to 181, subject to conforming to the speed restriction laid down for this class of locomotive. (G.A.23—7 49. LK1 9369 Gen.)

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.
SHUNTING INSTRUCTIONS—Continued.

SHUNTING POLES.

Stations at which goods shunting is done must keep a supply of shunting poles on hand, and these can be obtained by requisition from the General Stores, Sandton. A shunting pole must always be used for coupling and uncoupling wagons whenever it is possible to do so, and poles issued will in every case be fitted with hooks. When any pole becomes worn out, or broken, it must be replaced and the pole must be returned to the General Stores, Sandton, in a new pole, together with the hooks obtained from the station stock. The iron hook must in no case be retained, nor must the staff in any case attempt themselves to fix the iron hooks on the poles. The use of shunting poles for any purpose other than coupling and uncoupling of vehicles is strictly forbidden, and the poles must not be thrown upon the tops of shunted loads.

The shunting pole must be examined as soon as they are received from Sandton, and any which appear to be cross-grained or otherwise defective should be returned to the General Stores, Sandton, immediately, an advice being sent to the Stores Superintendent of the number returned.

IMPROPER USE OF SHUNTING POLES.

The use of shunting poles for coupling or breaking down or wagons or for any purpose other than that of coupling or uncoupling vehicles is strictly forbidden.

Inspectors and others must immediately report to their superior officer any infringement of this regulation which may come under their observation.

USE OF BRAKE-STICKS AND SHUNTING POLES ON WAGONS FITTED WITH EITHER-SIDE BRAKE

Guards, Shunters and others must not use the handles of the either-side brake as levers for putting on or taking off the brakes in the looped handles of the either-side brakes as this will cause the brakes to be put on full power by hand without the need of a brake-stick or shunting pole.

"INSTANTER" PATENT COUPLER.

With the ———

INSTANTER COUPLINGS.

A number of vehicles are fitted with Instanter Couplings, which can be in either of two positions, as shewn in the following diagrams:—

Fig. 1—Short Position.

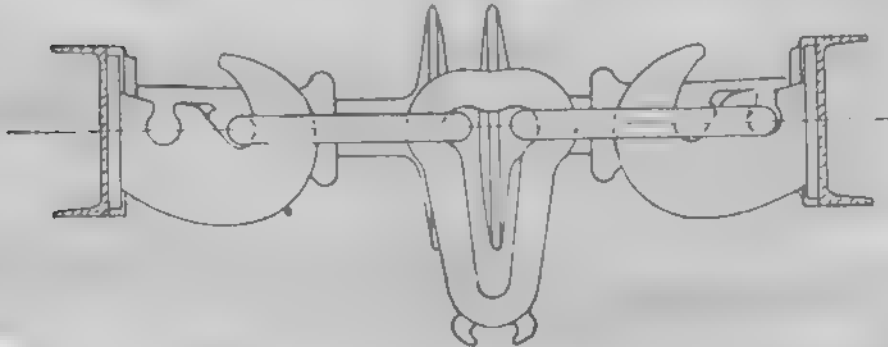
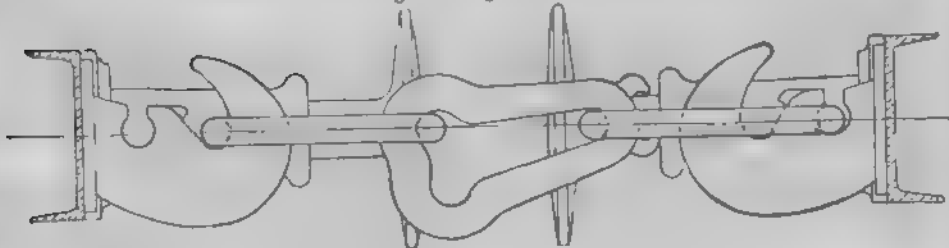


Fig. 2—Long Position.



"INSTANTER" PATENT COUPLER.—Page 158.

The paragraph following figure 2 to be amended to read:—

The coupling can be used for shunting operations as an ordinary loose coupling as shewn in figure 2. When the centre link is required to be placed into the short or close-coupled position the following procedure to be observed.

(GA 31—7/56 LK 1/9003/Gen E.)

Reference
BRAKE STICK
 All b
 of June
 returned
 usual ch

Reference to the following to be made on Page 158 :—

BRAKE STICKS

All brake sticks on hand at stations and in yards must be examined during the first week of June and December and any which shew signs of having become defective must be returned to Swndon Stores. Requisitions for replacements must be submitted through the usual channels.

(G.A.31—7/56—L.K.1/13121/420)

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.
SHUNTING INSTRUCTIONS—Continued.
"Instantan" Patent Coupler—continued



FIG. 2.

(a) The shunter must face the same way as the claw end of the link.

(b) After placing the ferrule end of the shunting pole in the claw end of the centre link from the under side of the buffer rod the shunter with an upward and forward movement (the shunting pole pressing against the inside of the buffer head to obtain the necessary leverage, as shown in Fig. 3) slides the centre link into the short position, see Fig. 1.

This operation can be carried out with one hand only on the shunting pole, and can be performed easily with a little practise.

To uncouple with the link in the short position, the shunter knocks the centre link into the long position, and with the same stroke lifts the end link off the draw-bar hook. This can be done with the shunting pole either above or below the buffer.

To short couple, or uncouple when on a curve, it is necessary for the buffers of the vehicle to be touching or slightly compressed.

To facilitate the working of the coupling a little grease may, if necessary, be used on the inner side of the centre link.

Vehicles fitted with Instantan couplings may be attached to trains as shown below :—

(a) Trains Conveying Passengers.

One vehicle only, fitted with Instantan couplings, may be attached to trains conveying passengers provided it conforms in other respects to coaching stock requirements and subject also to the instructions relative to the conveyance of four-wheeled vehicles on passenger trains. In such circumstances the screw couplings of the adjacent vehicles must be used. This does not modify the authority (where given) to attach at the extreme end to a vehicle not conveying passengers and not fitted with the continuous brake or through pipe.

(b) Coaching Stock Trains Not Conveying Passengers.

Vehicles fitted with Instantan couplings and conforming to coaching stock requirements may be conveyed in trains composed of coaching stock, other than those conveying passengers, but the screw couplings of the vehicles on each side of the Instantan-fitted vehicle must be used. When two or more vehicles fitted with Instantan couplings are marshalled together in such trains, the screw couplings of the vehicles on each side of the Instantan-coupled vehicles must be used and the Instantan coupling, or couplings, in use, must be in the short position.

The instructions respecting the conveyance of four-wheeled vehicles will apply in respect of the running of Instantan-fitted vehicles on empty coaching stock trains.

(c) Freight Trains.

The couplings must be in the long position during shunting operations, and also when used in ordinary freight trains except when the vehicles are conveying cattle when the couplings of the cattle wagons must be in the short position.

(b) Coaching Stock Trains Not Conveying Passengers.

Vehicles fitted with Instanter couplings and conforming to coaching stock requirements may be conveyed in trains composed of coaching stock, other than those conveying passengers, but the screw couplings of the vehicles on each side of the Instanter-fitted vehicle must be used. When two or more vehicles fitted with Instanter couplings are marshalled together in such trains the screw couplings of the vehicles on each side of the Instanter-coupled vehicles must be used and the Instanter coupling, or couplings, in use, must be in the short position.

The instructions respecting the conveyance of four-wheeled vehicles will apply in respect of the running of Instanter-fitted vehicles on empty coaching stock trains.

(c) Freight Trains.

The couplings must be in the long position during shunting operations, and also when used in ordinary freight trains except when the vehicles are conveying cattle when the couplings of the cattle wagons must be in the short position.

Vehicles with Instanter couplings can be used in fitted freight trains in exactly the same way as screw-coupled vehicles, but when used in the fitted portion of the train the screw coupling of the adjacent vehicle or vehicles should be used if possible. If this is not possible the Instanter coupling must be in the short position.

Under no circumstances must an Instanter coupling be used for attaching to vehicles fitted with the Buck-eye coupling, and a screw coupling must be used.

(G.A.27.Op.—1/51. L.K.1/9003 Gen.E)

REGULATIONS FOR THE USE OF CAPSTANS.

1. No person except a duly authorised man must be allowed to work capstans. Members of the staff of the railway must be authorised by proper authority will be liable to dismissal.

2. The person in charge of a capstan must give warning to anyone who may be passing, or at work, in the neighbourhood of the capstan.

3. The person in charge of a capstan must see that the tow rope is clear of all obstructions, and that the hook is properly attached to the tow rope.

4. Except in special cases authorised to the contrary the hook of the capstan rope shall not be placed on the fork of the axle, and when it should be attached to the back of the axle guard before the axle box and gear must be exercised to prevent damage. The hook must not be attached to, or detached from a wagon whilst the capstan is working.

A rope with knots or one which has become stranded or is otherwise defective must not be used.

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS. SHUNTING INSTRUCTIONS—Continued.

Operating Instructions.

(a) Loose chain or rope hydraulic capstans.

Draw the rope or chain tight by hand and wind three coils round the capstan drum. Apply foot treadle or hand lever gradually until the required speed is obtained. It is undesirable to move a load beyond the limits of the capstan or to

4. The numbers of empty or loaded wagons which may be hauled at any one time will depend on the condition of the track, whether straight, curve, or gradient, and must be fixed locally and exhibited at or near the capstan.

5. Wagons must not be started or stopped by means of the ropes or chains on the capstans. Wagon brakes or set blocks only must be used for stopping and starting, and must be taken in drawing wagons towards the capstan that the rope hook does not fly off.

6. The ropes of all types of capstans must be left clear of the engine wheels.

7. Proper care must be taken of the ropes or chains when they are being run uncoupling then, and when they are being used for starting or stopping. The ropes or chains must be added under the wheels of the engine, and must be kept clear of the wheels of the engine and the wheels of the wagons.

8. Under no circumstances must the ropes or chains be used for starting or stopping.

9. Any defect in a capstan must be reported to the Mechanical Department, who must certify that the capstan is in proper working order before it is used again.

TOWING VEHICLES BY MEANS OF ENGINES AND DRAWING VEHICLES BY HORSES, TRACTORS OR CAPSTANS.

Where the towing of vehicles by engines is authorized, it is objectionable to attach the rope or chain to the draw-bar hook or to a axle, and the proper place being to attach the hook on the rope to the towing loop, or to the draw-bar hook, or to the axle of the vehicle, and, when ready to move, the engine must always start gently.

The towing loop on the side bar, however, is only of use if it is strong enough to pull the wagon itself, and, if it be necessary to move more than one vehicle at a time, the hook of the towing engine must be connected to the draw-bar hook, but when it is so connected it must be kept to prevent it becoming entangled in the wheels.

When vehicles are drawn by horses, tractors or capstans the draw-chain or rope should be attached to the tow-loop on the side bar whenever practicable, but, when it is not possible to do this, the chain or rope may be attached to the draw-bar hook, or to the axle, and great care must be exercised when this is done in order to prevent damage. The axle must be examined before, or after, drawing is done during the towing operation, the attention of a Canadian Department Examiner must be at once called to the matter to ensure the defect being rectified before the vehicle again passes into traffic. Not more than one wagon should be towed at a time by the axle guard.

The number of vehicles to be towed at one time must be regulated by the weight of the loads they contain, the gradients, and other circumstances, care being taken that no greater number of vehicles than can be towed with safety are moved at one time.

Shunters and others must not pass in front of moving vehicles for the purpose of hooking or unhooking draw chains or ropes, except in those cases where it is absolutely necessary for them to do so.

(a) Loose chain or rope hydraulic capstans

Draw the rope or chain tight by hand and winch three coils round the capstan drum. Apply foot treadle or hand lever gradually until the required speed is obtained. It is undesirable to surge a capstan rope in endeavouring to move a load beyond the capacity of the capstan or to vary the coils of rope on the capstan head while it is in motion. When a capstan is finished with or left unattended, the power should, whenever possible, be shut off, but in cases where this is not convenient, the push for the treadle, where not a fixture, should be removed to a place of safety. After use, the rope must be taken off and not left on the drum and no capstan must be used to coil slack ropes.

(b) Fixed chain hydraulic capstans.

After pressure has been turned on and hook attached to wagon, apply foot treadle gradually until required speed is obtained. In no test case should a load be moved before wagon to which it is attached reaches capstan. The wire must be taken off the end of each shift or when a snap is shot. When finished with a run must be taken a capstan.

(c) Free rope type electric capstans.

Closes circuit to the capstan motor and the capstan is started. When the capstan is started, the power is cut off and the rope is free to be used. The capstan is started by the foot treadle or hand lever. The capstan is started by the foot treadle or hand lever. The capstan is started by the foot treadle or hand lever.

(d) Fixed wire rope type electric capstans.

Closes circuit breaker controlling electric supply to the capstan. This is hand lever and should be turned to "back" position before starting the capstan. The hand lever should be turned to "forward" position when the capstan is started. The capstan is started by the foot treadle or hand lever. The capstan is started by the foot treadle or hand lever. The capstan is started by the foot treadle or hand lever.

The capstan is started by the foot treadle or hand lever. The capstan is started by the foot treadle or hand lever. The capstan is started by the foot treadle or hand lever. The capstan is started by the foot treadle or hand lever.

After using a capstan the electric power is cut off and the capstan is free to be used. The capstan is started by the foot treadle or hand lever. The capstan is started by the foot treadle or hand lever. The capstan is started by the foot treadle or hand lever.

(e) Fixed wire rope type electric capstans (foot treadle type).

Closes circuit breaker controlling electric supply to the capstan. Place arrow on foot treadle in "neutral" position and release dog lever on capstan top. This will free rope for attachment to wagons.

When ready to haul, lift foot treadle into "forward" position as shown by arrow, reverse a gear lever into "close" position and pressure on. To stop capstan take foot treadle into "reverse" position and pressure on. When finished with a run must be taken a capstan.

NOTE.—No load must be hauled by a fixed wire rope capstan unless at least two turns of rope are round the capstan head.

REGULATIONS FOR THE USE OF CAPSTANS—page 160.

The following to be added as an additional paragraph at the end of Clause (a)—loose chains or rope, hydraulic capstans:—

"Special care must be exercised by the capstan men to see that the safety catches and other safety devices attached to the bedplate of hydraulic turnover capstan engines are properly secured in all cases."

(G A 29 Cp.—5 52 M. & E E. 98890. 45 WT)

9 x 1/2 inch x 10 ft 6 in x 1/2 inch x 1/2 inch x 1/2 inch

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

SHUNTING INSTRUCTIONS—Continued.

GUARDS' HAND LAMPS TO BE LIGHTED THROUGH CERTAIN TUNNELS.

With reference to Rule 129 of the Rule Book, Guards must have their hand lamps lighted during the whole of the time their trains are in the Tunnels on the Metropolitan Railway, or any of the tunnels included on page 120.

LIST OF TUNNELS (2 CHAINS AND OVER IN LENGTH), ON G.W.R. AND ON JOINT LINES.

Name.	No of lines through.	From		To		Length in Yards.
		Miles.	Chains.	Miles.	Chains.	
Reading to Taunton direct.						
Somerton	2	126	5½	127	20½	1073
Reading to Bristol and Creech Junction						
Box	2	99	12	100	78	3912
Middle Hill	2	101	38½	101	47½	...
Sydney Gardens L.	2	106	21	106	27½	...
W.	2	106	28½	106	33	...
Twerton	2	109	5	109	15	...
Salisbury	2	111	7	111	66	...
Foxes Wood No. 1	2	117	8½	116	21	1075
Foxes Wood No. 2	2	117	4	116	8	1064
Flax Bourton	2	123	0½	123	66	116
Creech Junction to Perzance.						
Whitland	2	171	13	173	6½	1000
Kennaway	2	206	14	206	13½	200
Coryton	2	206	52½	206	61	20
Philot	2	206	16½	206	68½	75
Cheltenham	2	206	72	206	75	66
Parson's	2	207	18½	207	42	592
Upton	2	217	6½	217	74½	264
Marley { Up Line	1	227	12	228	32	800
Down Line	1					
Wrangaton	2	231	59	231	61	17
Mutley	2	245	32	245	11½	184
Devonport	2	248	3½	248	42	125
Wivelascombe (Shillingham)	2	254	6½	254	27	452
Brown Queen	2	275	16	275	20	88
Trevorru	2	279	18½	279	14½	565
Polperro	2	287	4½	287	76	581
Buckshead	2	299	10	299	24½	320
Higher Town	2	301	0	301	13½	70
Redruth	2	309	62	309	64	47
City Line.						
Paddington (Suburban)	2	0	0	0	5	110
Suburban	2		At 0'60			117
West London Extension Line.						
Branch No. 1.	2	2	47½	2	51½	92
Main Line.	2	2	3	2	7	10
Victoria Branch.						
Mitre	2	0	8	0	10	48
Reading.						
Reading L.I. (Southern)	2		At 35'42			50
Wyoome Branch.						
Wheatley	1	14	13½	14	37½	524
Newbury and Winchester Branch.						
Winchester	1	24	75½	25	16	441
Devizes Branch (B. and H. Line)						
Devizes	1	85	53	85	62	190
Bristol Harbour.						
Bristol Harbour (Redcliffe)	1 & 1 Sdg.	118	57½	118	71	292

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS
LIST OF TUNNELS (2 CHAINS AND OVER IN LENGTH) *Continued.*

Name.	No. of lines through.	From		To.		Length in Yards.
		Miles.	Chains.	Miles.	Chains.	
Portishead Branch.						
No. 1 Clifton Bridge	1	122	23	122	20 $\frac{1}{2}$	59
No. 2 " "	1	122	52 $\frac{1}{2}$	122	63 $\frac{1}{2}$	232
Sandstone	1	123	77 $\frac{1}{2}$	124	1 $\frac{1}{2}$	88
Pill	1	125	33 $\frac{1}{2}$	125	63 $\frac{1}{2}$	665
Bradford Branch.						
Bradford	2	7	18 $\frac{1}{2}$	7	25 $\frac{1}{2}$	159
Camerton and Limpley Stoke Branch.						
Combe Hay	1	6	74 $\frac{1}{2}$	6	77 $\frac{1}{2}$	66
Cheddar Valley Line.						
Shute Shelf	1	24	77	25	5	180
Castle Cary to Weymouth.						
Evershot	2	149	48 $\frac{1}{2}$	149	62 $\frac{1}{2}$	311
Frampton	2	156	70	157	19 $\frac{1}{2}$	699
Poundbury	2	161	3	161	15	264
Bincombe	2	164	44 $\frac{1}{2}$	165	1 $\frac{1}{2}$	814
Weymouth and Portland Railway						
Rodwell	1	169	42	169	44 $\frac{1}{2}$	51
Malmesbury Branch.						
Malmesbury	1	93	60 $\frac{1}{2}$	93	74 $\frac{1}{2}$	105
Chard Branch.						
Hatch	1	3	72	3	79	154
Barnstaple Branch.						
Bathealton	1	175	144	175	34 $\frac{1}{2}$	447
Venn Cross	1	177	1 $\frac{1}{2}$	177	1 $\frac{1}{2}$	243
Nightcote	1	186	38	186	4 $\frac{1}{2}$	44
Castle Hill	1	199	55 $\frac{1}{2}$	199	50 $\frac{1}{2}$	321
Exeter Railway.						
Perridge	1	4	8	4	46	836
Culver	1	4	75 $\frac{1}{2}$	5	6 $\frac{1}{2}$	253
Torquay Branch.						
Greenway	1	226	9	226	31 $\frac{1}{2}$	495
Kingsbridge Branch.						
Sorley	1	10	29	10	58	638
Launceston Branch.						
Shaugh	1	5	20	5	34	307
Yelverton	1	7	49 $\frac{1}{2}$	7	78 $\frac{1}{2}$	641
Grenofon	1	10	62	10	79	374
Fowey Branch.						
Pinnock	1	284	21	284	74 $\frac{1}{2}$	1173
Newquay Branch.						
Luxulyan (Bridge-)	1	285	44 $\frac{1}{2}$	285	46 $\frac{1}{2}$	50
Coswarth	1	299	23	299	25	44
Goonbarrow Branch.						
Goonbarrow (Stenalees)	1	289	53 $\frac{1}{2}$	289	69	341
Falmouth Branch.						
Sparnick	1	302	68	303	10	491
Perran	1	306	23	306	40	374
Badminton Line.						
Ableton	2	97	34	97	57	506
Chipping Sodbury	2	101	6	103	48	4444
Severn Tunnel Line (B. and S.W.U. Line.						
Patchway New (Up Line)	1	6	56	7	56	1760
Ol. (Down Line)	1	6	68 $\frac{1}{2}$	7	44 $\frac{1}{2}$	1246
Short (Down Line)	1	7	53	7	56	62
Ableton Lane	2	10	50 $\frac{1}{2}$	10	54 $\frac{1}{2}$	97
Severn	2	11	0 $\frac{1}{2}$	15	29 $\frac{1}{2}$	7668
Avonmouth and Filton Line.						
Charlton	2	113	78 $\frac{1}{2}$	114	12 $\frac{1}{2}$	302

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.
LIST OF TUNNELS (2 CHAINS AND OVER IN LENGTH)—Continued.

Name.	No. of lines through.	From		To		Length in Yards.
		Miles.	Chains.	Miles.	Chains.	
Swindon to Severn Tun. Jc. via Gloucester.						
Kemble	2	90	1 $\frac{3}{4}$	90	59 $\frac{1}{2}$	415
Sapperton (Short)	2	94	7 $\frac{1}{2}$	94	60 $\frac{1}{2}$	359
Sapperton (Long)	2	94	69	94	74	1860
Newnham	1	125	8 $\frac{1}{2}$	125	19	232
Severn Tunnel Junction to Fishguard.						
Newport (Old)	2	158	71	159	25	742
Newport (New)	1	158	70	159	25	762
Cockett	2	216	27 $\frac{1}{2}$	216	6	789
Whitland	2	257	1 $\frac{1}{2}$	257	9 $\frac{1}{2}$	184
Spittal	2	274	49 $\frac{1}{2}$	274	51 $\frac{1}{2}$	243
Swansea District Line						
Llanlas	2	1	7 $\frac{1}{2}$	1	11 $\frac{1}{2}$	925
Llanugyfelach	2	4	14 $\frac{1}{2}$	5	12 $\frac{1}{2}$	1553
Penllergaer	2	6	47 $\frac{1}{2}$	6	55 $\frac{1}{2}$	287
Aberystwyth Branch.						
Bryn Teify	1	1	61 $\frac{1}{2}$	1	66	100
Llyn-y-Craig	1	29	25 $\frac{1}{2}$	29	25 $\frac{1}{2}$	86
Carmarthen and Newcastle Emlyn Branch.						
Pencader	1	58	258	23	188	188
All	1	74	200	15	168	168
North Pembrokeshire Branch.						
Castle	1	69	76	270	4	100
Llanelli Line.						
Pontardulas	1	5	15 $\frac{1}{2}$	5	16 $\frac{1}{2}$	88
Pembroke and Tenby Branch.						
Narberth	1	264	16	254	284	273
Pembroke	1	287	5	285	265	460
Vale of Neath.						
Quakers Yard	1	16	9	16	41	763
Pencraedrain	2	28	64	29	85	526
Taff Vale Extension.						
Glyn	2	4	59	4	72	280
Bryn	2	9	46	9	64	398
Newport to Hereford						
Rodhill	1	3	15	3	17 $\frac{1}{2}$	50
Merthyr Branch.						
Merthyr	1	3	37 $\frac{1}{2}$	4	7	2497
Forest of Dean Branch.						
Bullo	1	1	51	1	54	664
Bradley Hill	1	1	60	1	71 $\frac{1}{2}$	249
Blue Rock	1	2	69	2	15	169
Grange Court and Hereford.						
Micheldean (Lea)	1	127	71 $\frac{1}{2}$	127	21 $\frac{1}{2}$	782
Fawley	1	159	32	159	7	540
Ballingham	1	137	28 $\frac{1}{2}$	138	3	1208
Dinedor	1	141	49	141	54 $\frac{1}{2}$	110
Ross & Little Mill.						
Lydbrook	1	4	62 $\frac{1}{2}$	5	11 $\frac{1}{2}$	630
Swynnerton Yat	1	7	25	7	4 $\frac{1}{2}$	434
W	1	13	78	13	19 $\frac{1}{2}$	118
W	1	24	79 $\frac{1}{2}$	5	11	256
Bridgend to Abergwyrf.						
Gymmer	1	10	17 $\frac{1}{2}$	11	10	1591
Gaer Branch.						
Gaer	2	150	47	150	65	403
Porthcawl Branch.						
Nottage	1	8	56	8	58 $\frac{1}{2}$	63
Halls Road L. Section.						
Pencar	1	4	42	4	55	289

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.
LIST OF TUNNELS (2 CHAINS AND OVER IN LENGTH)—*Continued.*

Name,	No. of lines through.	From		To		Length in Yards.
		Miles.	Chains.	Miles.	Chains.	
Coleford Branch.						
Whitecliffe ..	1	4	45	4	38	50
Wye Valley Branch.						
Tidenham ..	1	1	34	2	0	1188
Tintern ..	1	4	22½	4	3¾	182
Didcot to Chester.						
Harbury ..	2	100	41	100	32½	73
Snow Hill ..	2	128	72	12	19	506
Hockley No. 1 ..	4	120	60	120	22½	135
" No. 2 ..	3	120	76	130	1	160
Swan Village ..	2	135	54	135	73	412
Wolverhampton ..	2	141	30	141	47	577
Oakengates ..	2	17	75	18	10½	411
Clirk ..	2	192	34½	192	57	72
Whitehurst (Llangollen Road	2	94	54	94	9½	45
Ruabon Road (Bersham) ..	2	200	42½	200	4½	64
Balderton ..	2	200	48½	2	5½	53
Northgate Street (L.M.S. Railway						
(Double Tunnel) ..	2	213	14	213	24½	216
Windmill Lane (L.M.S. Railway)	4	213	30½	213	35	196
Ruabon and Dolgelley Branch.						
Berwyn ..	1	7	55½	8	6½	682
Llan Idrefel ..	1	24	47½	24	7½	1505
Vale of Glamorgan Line.						
Porthkerry No. 1 ..	2	0	52½	0	77	545
Porthkerry No. 2 ..	2	1	72½	1	76	71
Barry Line.						
Cogan ..	2	2	75	3	5	225
Barry Island ..	1	9	10½	9	23½	280
Cadoxton and Trehafod Branch						
Wenvoe ..	2	3	23½	4	28½	1,808
Pontypridd ..	2	14	17	14	77½	1,323
Fenrhos Branch.						
Walnut Tree ..	2	9	14	9	36	490
Cardiff and Rhymney Line.						
Caerphilly ..	2	6	5½	7	13½	1,941
Cardiff Railway.						
Tongwynlais ..	1	4	12½	4	21	180
Port Talbot to Blaengarw.						
Bryn (Cwmcerwyn) ..	1	5	8	5	53½	1,010
Tonmawr ..	1	5	64½	6	35	1,100
Swansea and Treherbert						
Danygraig ..	2	23	55	23	69	79
Golli ..	2	4	71½	4	79	169
Rhondda ..	1	1	45	3	41½	3,443
Andoversford and Redpost Junction.						
Chedworth ..	1	53	64	54	6½	494
Marlborough ..	2	18	50½	18	79½	647
Moat Lane and Talylllyn Line.						
Marteg ..	1	70	21½	70	38½	373
Rhayader ..	1	74	14½	74	26½	271
Brecon and Dowlais Branch.						
Talylllyn ..	1	3	41½	3	72½	674
Torpantau ..	1	13	52½	14	2½	667
Dovey Junction to Pwllheli.						
No. 1 Aberdovey ..	1	81	11	81	20½	200
No. 2 Aberdovey ..	1	82	7	82	17	219
No. 3 Aberdovey ..	1	83	74½	84	2½	191
No. 4 Aberdovey ..	1	84	14	84	34½	533
Barmouth ..	1	100	7½	100	11	71

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

LIST OF TUNNELS (2 CHAINS AND OVER IN LENGTH)—*Continued.*

Name.	No. of lines through.	From		To		Length in Yards.
		Miles	Chains.	Miles.	Chains.	
Ketley Branch. Horsehay	1	164	30½	164	33	50
Oxford, Worcester, Wolverhampton, via Worcester.						
Camden	2	97	47	98	7½	887
Worcester (Rainbow Hill)	2	120	78½	121	8½	919
Dudley	2	147	36½	147	79½	940
Ashendon and Aynho Railway. Hill (Rushwood)	2	2	29	2	38½	193
Ardley	2	15	13	15	65	1147
Worcester and Hereford. Colwall (Old)	1	140	48½	141	39½	1507
Colwall (New)	1	130	48	131	40½	1389
Ledbury	1	135	15	135	75½	1323
Kidderminster Loop. Bewdley	1	130	65½	137	10½	480
Birmingham and North Warwickshire Line. Wood End (Tanworth)	2	8	62½	8	70½	176
Galton Branch. Galton	2	133	43	133	40	64
Halesowen Branch. Hales Hill	1	137	46	137	53	151
Stourbridge Extension. Old Hill	2	136	30	136	70½	887
Banbury and Cheltenham Branch. Hook Norton	1	92	35½	92	54	418
Chipping Norton	1	97	36	97	67	685
Andoversford	2	101	62½	102	0	384
Cheltenham to Honeybourne. Greet (Winchcombe)	2	12	25½	12	67	693
Hunting Butts	2	19	19½	19	24½	97
Severn Valley Branch. Mount Pleasant	1	136	4	136	9½	123
Bridgnorth	1	150	13	150	38	550
Much Wenlock Branch. Presthope	1	166	59½	166	68½	198
Joint Lines						
G.W. and G.C. Joint. White House	2	13	12½	13	28	348
Saunderton (Up Line)	1	23	30½	23	34½	84
G.W. and L.M.S. Joint. Clifton Extension Line.	2	0	12	0	25	289
.....	2	1	52	2	51	1,35
Severn and Wye Joint Line.	1	17	61½	17	72½	242
.....	1	12	47½	12	70	593
.....	1	5	62	6	5	506
Severn and Hereford Joint Line.	2	27	46	27	52	132
.....	2	42	60½	43	30	1051
Cheltenham and Evesham Joint Line.	2	13	58½	14	3½	565
Cheltenham and Warrington Joint Line.	2	10	6½	10	10½	86
.....	2	11	6½	12	70	1920

PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

CONTROL AND DISTRIBUTION OF ROLLING STOCK.

The control and distribution of the Company's Rolling Stock is in charge of the Rolling Stock section of the Office of the Superintendent of the Line at Paddington, which department is also responsible for the working and distribution of sheets and ropes.

As far as Passenger Train stock is concerned the Superintendent of the Line exercises complete and absolute control, but with regard to freight train vehicles, sheets and ropes, the duty is exercised in conjunction with the Chief Goods Manager.

For Divisional distribution each Divisional Superintendent has a local Rolling Stock section under his direct supervision. These sections are stationed as follows:

Reading	Gloucester	Worcester
Bristol	Newport	Birmingham
Exeter	Cardiff	Chester
Plymouth	Swansea	Lawestry

Station Masters, Goods Agents, &c., must make their applications for carriage stock, wagons, sheets, and ropes to the Rolling Stock section of the division in which their duties are situated. Should the stock ordered not be promptly supplied the requisition must be repeated and followed up with the Divisional Superintendent or District Goods Manager as the case may be.

Each station must forward to the Rolling Stock section of the division by agreed train daily, or by telegraph or telephone, the undermentioned information on the W.G. return:

(a) The number of vehicles, and other vehicles, loaded or empty, on hand at the station, or in the sidings connected with it.

(b) The number of vehicles, and other vehicles, on hand at the station or the following day; and what, if any, can be spared for use elsewhere.

(c) What additional stock, if any, is required.

This information must be sent by train on Form 2,000-A in all cases where it can be forwarded sufficient early to reach the Rolling Stock section by 2.0 p.m., so that it may be dealt with the same afternoon. Where this cannot be done, or where the Rolling Stock section requires it, the information must be sent by telegraph (on Form 450-4 or 450-5) or telephone at 4.0 p.m.

Private Owners' Wagons must not appear on these returns.

Station Masters and Goods Agents must, as far as possible, anticipate their requirements and advise their Rolling Stock section of any extraordinary demands that are likely to be made upon the stock, and if from any unforeseen circumstances a station is short of stock the Rolling Stock section must be immediately advised by telegraph or telephone, in order that arrangements may be made for a supply to be sent.

Station Masters and Goods Agents must see that the Daily Stock Returns are prepared by a competent member of the staff, and the instruction that the return is to be signed by the Officer in charge or his Chief Clerk to be strictly adhered to.

All stock must be properly and fully labelled in accordance with the instructions and telegraph rules. In applying for stock particulars of the date and time required for delivery must be given, and the destination of the traffic whenever possible.

For all Rolling Stock sent to all stations daily by telegraph or otherwise as to the disposal of the stock returned by the station, the Rolling Stock section must be kept informed and replied to by return. A special return must be sent to the Rolling Stock section, or the Office of the Superintendent of the Line at Paddington.

All loaded vehicles must be released as quickly as possible after receipt, and the consignees of station to station traffic must be required to unload the wagons assigned to them as soon as possible after their receipt.

The vehicles, when unloaded, must at once be made available for forwarding traffic, or placed at the disposal of the Rolling Stock section if not required for immediate use.

Every effort must be made to place vehicles in position for unloading immediately on arrival, special care being taken to prevent the unloading sidings being blocked with Private Owners' Goods.

CONTROL AND DISTRIBUTION OF ROLLING STOCK.—Page 166.

The instructions in G.A.9 under this heading to be cancelled and substituted by the following:—

The existing instructions for the Supply and Distribution of Freight Stock are shown in booklets BR.87237 and 87238 "Standard Instructions to Stations and Standard Instructions to District Officers" respectively.

(G.A.30 Op.—9/54 B.).

FREIGHT TRAIN INSTRUCTIONS.

PREPARATION OF GOODS GUARDS JOURNALS.

The attention of Goods Guards is directed to the importance of accurately filling up and promptly rendering the journals as instructed immediately on completion of a journey.
All entries should be made in a clear and distinct manner.

MARSHALLING.

Instructions as to the correct method of marshalling trains running through two or more divisions are contained in the pamphlets issued for the purpose. Local trains should be dealt with in accordance with the instructions issued by the Divisional Superintendents or District Traffic Managers.

The following should be noted:

1. Shunting at local stations by train engines should be reduced to a minimum, and in order to assist in this connection, various trains are shown in the marshalling instructions pamphlet to attach traffic at one shunt. When a train is detained at a local station waiting a margin to proceed, or from any other cause, advantage should be taken of such periods to marshal the train as far as possible in such a way as will assist the work in the marshalling yards.

The work at the stations where trains are timed to call varies considerably, and in many cases it can be seen that the instructions should be followed. It is a rule that advantage will be taken of this to expedite the running.

Station Masters must personally supervise freight train working and make such arrangements as will obviate waste of power.

2. Trains should be formed to wagons for destinations specified in the Marshalling Instructions pamphlet and must be formed in the order shown.

3. Unless authority to the contrary is given by the Divisional Superintendents or Controls concerned the formation of trains must not be varied from that shown in the Marshalling Instructions pamphlet. Where a station stop for traffic purposes it states except where noted to do so. Any other variation from the instructions shown in the pamphlets should be reported to the Divisional Superintendent or District Traffic Manager.

4. The ruling route is that through traffic must be worked by through trains to destination and not short to intermediate stations.

5. Traffic for the longest distances must have preference by through trains.

6. Wagons containing explosives must always be marshalled as near the middle of the train as possible.

CONVEYANCE OF PASSENGER TRAIN STOCK ON FREIGHT TRAINS.

1. ~~Passenger coaches, passenger brake vans and parcel vans must not be conveyed~~

2. Passenger coaches, passenger brake vans and parcel vans must not be conveyed by freight trains of 7 wagons or over. If absolutely necessary for traffic reasons one vehicle only, either a scenery van, 8-wheeled carriage truck or siphon F, G, H, or J may be conveyed.

The following to be substituted for clauses 1, 3 and 4:—

1. Coaching Stock exceeding 65ft. in length may be conveyed on a through freight train, with 'F' or infer or headcode only subject to being formed next in front of the rear Goods Brake Van, with an additional Goods Brake Van or 5-ton open Goods (TUB), wagon or other vehicle 24ft. over headstocks or longer, formed next in front of the coaching stock.

Freight trains conveying such coaching stock must not enter sidings not normally used for such stock, and the stock must not be conveyed on trains passing by routes over which the stock is prohibited except when notified by the Operating Superintendent giving Civil Engineer's Conditions of passage.

This instruction does not apply to stock passing from private building works particulars of which are notified by the Operating Superintendent as out of gauge or exceptional loads with the applicable conditions of passage.

3. If essential for traffic reasons 8-wheel passenger train vehicles when conveyed by freight trains must be formed immediately in front of the brake van except in the case of local trains and pilot trips conveying not more than equal to 25 wagons of Class 1 traffic, when they may be placed in any position on the train.

4. If essential for traffic reasons 4 and 6-wheel passenger train vehicles may be formed in any position on freight trains.

(G.A.30 Op.—9/54 Ex.5299/52)

The following to be added as clause 6:—

6. Empty Parcel Vans Siphons 'F', 'G', 'H' or 'J' must not be conveyed on freight trains except where essential for traffic requirements on Branch Line services as arranged by the local District Officer.

(G.A.30 Op.—9/54 E.1088H (2-C) R.8/-)

SECTION II (c)

SECTION III (a)
CHANDISE

The following
the General Apper

EXISTING
Code Word

A
B
C
D

aw
aw A
aw B
aw C
aw D
aw E
aw F
aw G
aw H

B

A & B

A
B
C & D
E & G

..

A

B

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F

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O

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(1) No train consisting of more than 5 freight vehicles (excluding the brake van, may have any vehicle coupled with the vacuum brake pipe to the engine.

(2) No freight train may have more than 10 such coupled vehicles (including the brake van) behind the vacuum brake vehicles connected with the vacuum pipe to the engine. If this number be exceeded, the train must be run as a non-vacuum train.

(3) Shunting. Vacuum brake vehicles which are connected with the vacuum pipe to the engine are to be kept on the engine during shunting operations in the same way as a shunting truck, where this can be done without delaying trains or disorganising the yard working.

SECTION III (a)—INSTRUCTIONS CONCERNING LOADING, CONVEYANCE, ETC., OF MER-
CHANDISE TRAFFIC.—Page 188.

Existing Code Word	New Code Word	Interpretation
at	Passenger	Passenger Fruit Van, 10ft. wheel base, load 6 tons.
at A	Ventilated	Ventilated Fruit Van.
at B	Insulated	Insulated Banana Van.
at C	Insulated C	Insulated Van, 22 1/2 ft. wheel base load 1 tons
at D	Insulated D	Insulated Van, 22 1/2 ft. wheel base, 12 ft. long over head, 12 ft. wheel base, load 10 tons.
at E	Single	Single Bolster Wagon.
at F	Double	Double Bolster wagon.
at G	Bogie	Bogie Bolster wagon with length over headstocks 45-47ft. 6ins.
at H	Eight-wheel	Eight-wheel bogie timber truck (30 tons) 70ft. long.
at I	Eight-wheel	Eight-wheel bogie timber truck (40 tons), 43ft. long, extreme width 8ft. 8ins
at J	Eight-wheel	Eight-wheel bogie timber truck (30 tons), 43ft. long, extreme width 8ft. 8ins
at K	Double	Double bolster wagon.
at L	Double	Double bolster wagon, 10 tons, 72ft. long.
at M	Double	Double bolster wagon with length over headstocks 30 ft
at N	Cattle	Cattle wagon (not fitted).
at O	Ox	Cattle Wagon. V.B. complete.
at P	Ox	Ventilated Meat Van.
at Q	Meat	Insulated Meat Van
at R	Ins	Covered Goods Van, 12 tons and under.
at S	Van	Covered Goods Van, 12 tons and under. V.B.
at T	Vanfit	Covered Goods Van of over 12 tons capacity
at U	Travan	Covered Goods Van of over 12 tons capacity.
at V	Travan	Twin Bolster wagons.
at W	Twin	Twin Bolster wagon.
at X	Twin	High bed Good wagon over the trucks
at Y	High	High bed Good wagon over the trucks
at Z	High	High bed Good wagon over the trucks
at AA	High	High bed Good wagon over the trucks
at AB	High	High bed Good wagon over the trucks
at AC	High	High bed Good wagon over the trucks
at AD	High	High bed Good wagon over the trucks
at AE	High	High bed Good wagon over the trucks
at AF	High	High bed Good wagon over the trucks
at AG	High	High bed Good wagon over the trucks
at AH	High	High bed Good wagon over the trucks
at AI	High	High bed Good wagon over the trucks
at AJ	High	High bed Good wagon over the trucks
at AK	High	High bed Good wagon over the trucks
at AL	High	High bed Good wagon over the trucks
at AM	High	High bed Good wagon over the trucks
at AN	High	High bed Good wagon over the trucks
at AO	High	High bed Good wagon over the trucks
at AP	High	High bed Good wagon over the trucks
at AQ	High	High bed Good wagon over the trucks
at AR	High	High bed Good wagon over the trucks
at AS	High	High bed Good wagon over the trucks
at AT	High	High bed Good wagon over the trucks
at AU	High	High bed Good wagon over the trucks
at AV	High	High bed Good wagon over the trucks
at AW	High	High bed Good wagon over the trucks
at AX	High	High bed Good wagon over the trucks
at AY	High	High bed Good wagon over the trucks
at AZ	High	High bed Good wagon over the trucks
at BA	High	High bed Good wagon over the trucks
at BB	High	High bed Good wagon over the trucks
at BC	High	High bed Good wagon over the trucks
at BD	High	High bed Good wagon over the trucks
at BE	High	High bed Good wagon over the trucks
at BF	High	High bed Good wagon over the trucks
at BG	High	High bed Good wagon over the trucks
at BH	High	High bed Good wagon over the trucks
at BI	High	High bed Good wagon over the trucks
at BJ	High	High bed Good wagon over the trucks
at BK	High	High bed Good wagon over the trucks
at BL	High	High bed Good wagon over the trucks
at BM	High	High bed Good wagon over the trucks
at BN	High	High bed Good wagon over the trucks
at BO	High	High bed Good wagon over the trucks
at BP	High	High bed Good wagon over the trucks
at BQ	High	High bed Good wagon over the trucks
at BR	High	High bed Good wagon over the trucks
at BS	High	High bed Good wagon over the trucks
at BT	High	High bed Good wagon over the trucks
at BU	High	High bed Good wagon over the trucks
at BV	High	High bed Good wagon over the trucks
at BW	High	High bed Good wagon over the trucks
at BX	High	High bed Good wagon over the trucks
at BY	High	High bed Good wagon over the trucks
at BZ	High	High bed Good wagon over the trucks
at CA	High	High bed Good wagon over the trucks
at CB	High	High bed Good wagon over the trucks
at CC	High	High bed Good wagon over the trucks
at CD	High	High bed Good wagon over the trucks
at CE	High	High bed Good wagon over the trucks
at CF	High	High bed Good wagon over the trucks
at CG	High	High bed Good wagon over the trucks
at CH	High	High bed Good wagon over the trucks
at CI	High	High bed Good wagon over the trucks
at CJ	High	High bed Good wagon over the trucks
at CK	High	High bed Good wagon over the trucks
at CL	High	High bed Good wagon over the trucks
at CM	High	High bed Good wagon over the trucks
at CN	High	High bed Good wagon over the trucks
at CO	High	High bed Good wagon over the trucks
at CP	High	High bed Good wagon over the trucks
at CQ	High	High bed Good wagon over the trucks
at CR	High	High bed Good wagon over the trucks
at CS	High	High bed Good wagon over the trucks
at CT	High	High bed Good wagon over the trucks
at CU	High	High bed Good wagon over the trucks
at CV	High	High bed Good wagon over the trucks
at CW	High	High bed Good wagon over the trucks
at CX	High	High bed Good wagon over the trucks
at CY	High	High bed Good wagon over the trucks
at CZ	High	High bed Good wagon over the trucks
at DA	High	High bed Good wagon over the trucks
at DB	High	High bed Good wagon over the trucks
at DC	High	High bed Good wagon over the trucks
at DD	High	High bed Good wagon over the trucks
at DE	High	High bed Good wagon over the trucks
at DF	High	High bed Good wagon over the trucks
at DG	High	High bed Good wagon over the trucks
at DH	High	High bed Good wagon over the trucks
at DI	High	High bed Good wagon over the trucks
at DJ	High	High bed Good wagon over the trucks
at DK	High	High bed Good wagon over the trucks
at DL	High	High bed Good wagon over the trucks
at DM	High	High bed Good wagon over the trucks
at DN	High	High bed Good wagon over the trucks
at DO	High	High bed Good wagon over the trucks
at DP	High	High bed Good wagon over the trucks
at DQ	High	High bed Good wagon over the trucks
at DR	High	High bed Good wagon over the trucks
at DS	High	High bed Good wagon over the trucks
at DT	High	High bed Good wagon over the trucks
at DU	High	High bed Good wagon over the trucks
at DV		

CONVEYANCE OF PASSENGER TRAIN STOCK ON FREIGHT TRAINS. Page 168.

2. and passenger brake vans should not be conveyed on freight trains without the consent of the Board. The freight trains scheduled in the Marshalling Instructions for the conveyance of coaching stock are excepted from this arrangement."

no further to be tested for Clause 1 :-

more in length over the buffers (except stock passing from private
to the carrier by the Operating Superintendent, as out of grade
at the conditions of passage) must not be conveyed by Freight
(G A 19-10 48 FRANK H.)

COVENANCE OF PASSENGER TRAIN STOCK ON FREIGHT TRAINS.—Page 168

The following to be added as paragraphs 7 and 8:—

- (G A.31—7/56 L.K 1/12693/232)

Reference to the following to be made on page 168 —

DRIVERS' TICKETS.

It is very important that Drivers' Tickets are correctly and fully completed as the information is the basis of Railway Statistics called for by the Transport Act of 1947. There is evidence to show that Column 13 "Description of Trip" is not being accurately compiled in that "loaded" or "empty" freight trains are not being properly described. It is important to remember that if there are one or more loaded wagons on a train that train must be regarded as a "loaded" train.

Guards are responsible for informing Drivers as to the "Description of Trip" (Column 13 of Drivers' Ticket) and the number of wagons and the equivalent load (Column 21 of Drivers' Ticket). It is imperative that care should be taken to see that the Driver is properly informed as to —

- (a) Whether the train is "loaded" or "empty".
- (b) Number of vehicles and equivalent number of wagons at starting point.
- (c) Any alterations to load en route.

(G.A.30 Op —9 54 E)

SCREW COUPLINGS ON VEHICLES FORMED IN FREIGHT TRAINS—page 169.

The following to be added as the first paragraph of these instructions

In the event of two vehicles, one fitted with screw couplings and the other with Instantan couplings coming together the latter should be used for joining up whether formed in the vacuum portion or not, but in the case of two vehicles being together one with screw couplings and the other with ordinary three-link couplings the former should be used for joining up if formed in the vacuum portion, but not otherwise.

(G. A. 12 37 E 72899 H)

Reference to the following to be made on page 169 :—

WORKING OF ENGINEERING DEPARTMENT SLEEPER TRAFFIC.

Trains arranged for conveyance of sleepers, whether loaded in sleeper wagons or crocodiles (also odd wagon loads or small lots), must conform with the following conditions—

Timing .. Not to exceed speed for "ordinary" freight trains, viz. 25 m.p.h.

Loads .. Not to exceed those authorised for "mineral" trains, subject to the following restriction in the case of trains comprising sleeper crocodiles

Group A. engines restricted to 20 loaded crocodiles

B.	"	"	"	25	"	"
"	"	C.	"	"	25	"
"	"	D.	"	"	30	"
"	"	E.	"	"	40	"

* 20 ton brake vans to be provided when these loads are conveyed.

All sleeper carrying vehicles to be calculated in accordance with the table shown in the Service Time Tables headed "Calculation of Special Wagons—Loaded and Empty"

(G. A. 12 37 E 73194 H)

SCREW COUPLINGS ON VEHICLES FORMED IN FREIGHT TRAINS—page 169

SCREW COUPLINGS ON VEHICLES FORMED IN FREIGHT TRAINS—Page 169.

The following to be substituted for the instructions appearing on page 169 and the amp fiction contained in Circular G.A.5 :—

Vehicles working in the vacuum portion of the partly vacuum fitted freight trains, are fitted with either screw couplings or knuckle couplings. The screw couplings, if fitted, must be used.

The screw couplings, however, must not be used in non-vacuum freight trains, nor in the non-vacuum portion of a partly vacuum-fitted freight train unless both vehicles to be coupled are fitted with screw couplings, or difficulty would arise in using the three link couplings owing to difference in length of buffers. If, however, considerable delay would occur in attaching or detaching the brake van if the knuckle coupling were used, the screw coupling of the last vehicle fitted may be used.

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~~In those cases where, in consequence of the difference in the length of the buffers, it is not possible to get the link coupling over the drawbar hook of the screw fitted vehicle, the train may have been considerably compressed and considerable delay in attaching and detaching would result, it should be understood that the rule may be relaxed, provided the screw coupling is well fitted and works freely, so that it is not likely to lift off the drawbar hook on the journey.~~

INSTRUCTIONS FOR WORKING RAILWAY TRAINS

INSTRUCTIONS FOR WORKING BALLAST TRAINS.—Page 169

The instructions under this heading to be cancelled and the following substituted —

INSTRUCTIONS FOR WORKING OF CIVIL ENGINEERING DEPARTMENT BALLAST AND MATERIAL TRAINS.

Additional instructions relating to the working of fully fitted Ballast trains are shown under separate heading "Fully fitted trains with Ballast loaded in Engineering Department Hopper Wagons and Plough or Freight Brake Van from Quarries to unloading sites and return of empty Hoppers".

Arrangements
of engines

The District Operating Superintendent or District Traffic Superintendent from whose District the trains will be run, will arrange for the running, and advise the other District Operating Superintendent or District Traffic Superintendent, as his requirements, the District Engineer must be notified of the requirements of the Engineering Department work at site or sites to enable the Operating Department to arrange relief of trainmen when necessary.

Any emergency arrangements after office hours will be made by the respective Control Offices.

Engines

The District Operating Superintendent will arrange with the District Motive Power Superintendent for the provision of the necessary engine power.

When making application for the running of these trains or engines in connection with relaying, reconstruction of bridges or other Engineering Department work, the District Engineer must advise the District Operating Superintendent or District Traffic Superintendent (giving type of engine or engines if necessary), stating whether during any interval, the engines may be utilised for other purposes, and, whenever possible, name the time when it is expected an engine will be available to return from the site of the work.

Trains
to be
run

When these trains are required to call at places en route to pick up men for unloading the District Operating Superintendent or District Traffic Superintendent must be advised at the time arrangements are made for the running of the train.

P-1

e.g. of the cancellation of
of the joint names

in case with the Locomotive Department for the Engine Power.
before ordering an engine to work a
it should be made as to whether an engine returning to
to run in the direction of the ballast train, and can work

of trains or engines in connection with
Engineering Department work, the
District Superintendent or District Traffic
able) and whether, during any interval, the engines
poses, and, whenever possible, name the time when
be available to return from the site of the work.

passenger train routes should be curtailed as
summer traffic (during Bank Holiday periods,
F. days and Saturdays.

at places en route to pick up men for
the Divisional Superintendent or District Traffic Manager must be advised
at the time arrangements are made for the running of the train.

Running of
Ballast Trains
during Summer
period
Ballast trains
picking up men
for unloading

FREIGHT TRAIN INSTRUCTIONS.

INSTRUCTIONS FOR WORKING BALLAST TRAINS—*Continued.*

Instructions to Guards, and Signalmen as to site for unloading.

The section or station and mileage of the site with a description of the line at which such a train is to be worked must be shown in the Civil Engineer's weekly ballast programmes, Speed and Engineering Notice, or other notice, and the guard of the train must be instructed accordingly. Where it is necessary to unload some or all the material on another site and time will not admit of the Guard being notified in the usual way, information must be given by the Permanent Way Inspector or Ganger to the Signalman at the commencement of the section affected, and he in turn must inform the Guard of the train accordingly.

Ballast not to be deposited on points, rodding wires, connections, or A.T.C. Ramps.

When ballast or other material is being unloaded, great care must be exercised to see that it is not deposited on point rodding wires or connections, or automatic train control ramps.

Wagons put off for repairs

When an empty wagon is put off for repairs it should be labelled by the Guard to the home station, and when a loaded wagon is detached for repairs it is to be labelled to the destination of the train.

Should any loaded wagons be put off for repairs, etc., the Guard must give particulars in writing to the man responsible for unloading at destination in order that he may be aware that the complete train has not arrived, thus obviating the risk of error in the certification of the advice note.

The guard must show on the back of the engine ticket (form No. 2009), Engineering Department service vehicles, wagons put off for repairs, etc., giving the name of station and wagon numbers, and stating if loaded or empty and how labelled.

Guards to examine trains

Guards must examine their trains immediately after unloading or on taking charge, also before proceeding to or leaving a Contractor's temporary road, and in the event of a wagon having been damaged, a report of the circumstances must be sent to the District Operating Superintendent or District Traffic Superintendent.

Reversing on incline

Trains which have to reverse on a steep incline must be provided with a brake van at each end. The provisions of Rule 151 must be observed.

Propelling on incline.

The amplification of Rule 149, exception (vii) as shown on page 20 of the General Appendix to the Rule Book to apply.

Marking of wagons

The marking of Engineering Department wagons is not to be altered without authority from the Civil Engineer.

Guard's Journal

The Guard of each train must send a sectional journal on form B.R.87210 to the Operating Superintendent of each District through which his train passes.

Journals need not be rendered to the Engineering Department.

Engine Ticket

The Guard of each train must prepare engine ticket (form No. 2009), including on the back thereof particulars of the loading of the train, and immediately the last journey for the day is completed, it must be forwarded to the District Engineer in whose District the train originated.

The Guard must also give an engine ticket (form 2009A) to each Driver for the time the Driver was on the train.

Each Driver must obtain from the Guard an engine ticket and attach it to his daily record.

Guards are responsible for rendering an engine ticket to the Driver after the completion of the work, covering the whole of the time the engine has been in traffic. The Driver must communicate with the Guard after completion of the work in order to obtain this engine ticket.

Engines returning home.

When engines which have been used for working loaded trains are not required on the return journey and return home light, the time will be debited to the Engineering Department, but should they be utilised for ordinary traffic working no charge will be made.

Guard to inform Driver formation and number of wagons.

Before starting, the Guard must inform the Driver in charge the total number of wagons on the train, and what proportion of it is composed of vacuum fitted wagons, the brakes of which can be applied from the engine. **VACUUM-FITTED VEHICLES MUST BE FORMED NEXT TO THE ENGINE.**

Vacuum pipes out of use.

On arrival at site when these vehicles are disconnected the Guard must see that any loose vacuum pipes are placed on the stop plugs provided.

Headcodes to be carried.

Engineering Department trains must carry "F" headcodes except in the case of trains fully or partially vacuum fitted which should carry "C," "D" or "E" headcodes respectively.

FREIGHT TRAIN INSTRUCTIONS.

Reference to the following to be made on page 171.

FULLY FITTED TRAINS WITH BALLAST LOADED IN ENGINEERING DEPARTMENT HOPPER WAGONS AND PLOUGH OR FREIGHT BRAKE VAN FROM QUARRIES TO UNLOADING SITES AND RETURN OF EMPTY HOPPERS.

Loaded trains must run under the following conditions:—

- (i) Carry "C" headcode.
- (ii) Maximum speed—50 m.p.h.
- (iii) Engine provided to have not less than 5ft. 8in. diameter coupled wheels.
- (iv) Vacuum brake on all wagons to be operative, also plough or freight brake van to be fitted or piped.
- (v) It will not be necessary to observe incline instructions.
- (vi) Maximum load not to exceed 22 20-ton loaded Engineering Department hopper wagons. On rising gradients the standard loading for Class I traffic shown in the Service Time Tables must not be exceeded. Engineering Department 20-ton hoppers loaded with ballast to be calculated as "Two equals Three 10-ton wagons of Class I traffic."
- (vii) Instantan couplings must be in short position.

Procedure to
be adopted
in arranging
trains

The Civil Engineer will issue a weekly programme to all concerned on Tuesdays showing the requirements for fully fitted trains of ballast to run from the following Monday to Sunday (both days inclusive).

The District Operating Superintendent or District Traffic Superintendent concerned with the despatch of the ballast must confer with the District Engineer and agree departure time of trains, according to the unloading point for all the trains listed and advise other District Operating or Traffic Superintendents concerned by telegram on Wednesdays full details of the trains arranged for the following week.

The District Engineer involved with the unloading of the ballast to consult the District Operating Superintendent or District Traffic Superintendent in whose District the unloading will take place, on Wednesdays and agree at what time this can be accomplished. The District Operating Superintendent or District Traffic Superintendent will order a fresh engine, if this is necessary, to undertake the work of discharging the ballast and bringing back the empties, and arrange departure time for a special train conveying the empty hoppers back to the Quarry.

Train
Schedules.

Standard schedules will be laid down for these trains, where practicable, and must be adhered to. In the case of journeys where a standard schedule cannot be applied, throughout timings must be pre-arranged in the case of loaded trains to arrive at unloading site by 7.30 a.m. unless otherwise specified in the Civil Engineer's weekly programme.

District
Operating
Notices

Details of the loaded and empty trains required to run including timings must be issued in the District Operating or Traffic Superintendent's notices, copies of which must be sent to the Operating Superintendent's T.D. Section.

Engine Power
and trainmen

In general where the transit is within 100 miles of the quarry, an engine and trainmen must be programmed to cover the loaded and empty journeys throughout, although in the case of branch lines it may be necessary to utilise a smaller type of engine from the junction in which case the engine used to convey the ballast from the Quarry, or nearby marshalling yard, must be available for working back the empty hoppers to the Quarry.

When the hopper wagon has been placed in position the plough is lowered to the rails, but not screwed hard down. That the French keys in the centre and right-hand levers of the wagons are then removed and the key of the left-hand lever kept in.

That the hopper wagon next to the engine is discharged first, the door being opened by removing the key from the left-hand lever and so on.

Lubrication

The lubrication, which includes doors, pins and slides of hopper ballast wagons and plough vans will be done by the C. & W. Engineer's Department staff at the loading points.

Closing doors
of hopper
wagons

Should any difficulty be experienced in closing the doors of hopper wagons, force must not be used, but the doors swung back against the stop on the solebar to dislodge any fine ballast from the ledge on the "Z" iron.

(G.A.30 Op.—9/54. E.82672H. (12-C.).

- (f) Before proceeding with the empty train the doors of the hoppers of the wagons are closed and secured with the French keys which are to be properly fixed in the three levers, the keys being inserted in the holes from the left-hand side.

FREIGHT TRAIN INSTRUCTIONS.

In the case of longer distance transits or where it is considered guaranteed arrivals at unloading sites within reasonable limits are uncertain, it is desirable that ballast should be despatched from Quarries as soon as possible after loading has been completed, and be stabled at a convenient point near to unloading site. The Operating Superintendent will diagram engine and trainmen to work to stabling point and back to the Quarry.

A fresh engine and trainmen will be diagrammed to carry out the unloading of the ballast unless the District Engineer has made other arrangements.

Proper arrangements must be made in order to ensure that engemen and guards are available to work the loaded and empty trains at the recognised changing points en route.

return of
empty hoppers
to Quarries

Owing to limited supply of hopper wagons for ballast traffic, it is important no delay occurs in returning these wagons, when empty, to Quarries in order that the programmed supplies of ballast may be maintained, and movement must be undertaken by special trains.

empty hopper
wagons

Empty trains may run under "C" headcode freight train conditions not exceeding 50/20 ton hoppers. Engineering Department hopper wagons being calculated on the basis of 20/21 ton wagons shown in the Service Time Tables.

WORKING OF HOPPER BALLAST WAGONS AND PLOUGH.

and to
be stand
ing of
hopper trains
working of
plough to be
examined
Guard

Whenever practicable, a loaded hopper ballast train must be worked by a Guard who from previous instruction and experience, is qualified to take charge.

The working of the hopper ballast plough for spreading the ballast is to be undertaken by the Guard, under direction of the Permanent Way Inspector or Sub Inspector.

where ploughs
not to
be used
to be
examined
Guard

The hopper ballast plough is not to be used in station yards.

Hopper wagons are only to be loaded with material as authorised by the Civil Engineer and the plough must not be employed for spreading material other than crushed slag or stone, gravel and ashes suitable for top ballast.

to be
examined
Guard

When required to be unloaded on lines already opened for traffic, the Permanent Way Inspector or the district will give instructions where the ballast is to be discharged, but the Guard of the train and the Ganger of the length must satisfy themselves that there are no fittings, catch points, guard rails, signal wires, locking gear rods, detonator machines, automatic train control ramps, level crossings, or other works with which the plough can possibly come in contact on the lengths of line to be ballasted.

to be
examined
Guard

The Guard will be held solely responsible for the proper working of the train and for satisfying himself that:—

- Hopper wagons and plough vans are in good order and examined at the usual examining points en route.
- When not engaged in unloading, the plough is in its normal position by being raised to the fullest extent and the two keys inserted and secured by padlock.
- When starting on a journey with a train which has to be unloaded in a tunnel, the plough is in the right direction, also that the hoppers are arranged so that all the levers are on the side of the train, and that where unloading has to be performed in a double line tunnel, all levers face the 6-ft. way.
- Upon arrival of the train at the place where it is intended to unload, and after it has been placed in position the plough is lowered to the rails, but not screwed hard down. That the French keys in the centre and right-hand levers of the wagons are then removed and the key of the left-hand lever kept in.

That the hopper wagon next to the engine is discharged first, the door being opened by removing the key from the left-hand lever, and the engine started at a speed of not more than four miles per hour. Before the whole of the contents of the first wagon to be discharged are run out, that the hopper door of the next wagon to it is lowered, and so on through the train.

The Permanent Way Inspector or Ganger will assist in the discharge of the train, one being on one side of the train and the Guard on the other.

- When the contents of the whole train have been spread, and before resuming the journey, the plough is cleaned with the scrapers which are kept in the plough van, then raised to the fullest extent and secured by means of the keys and padlock.
- Before proceeding with the empty train the doors of the hoppers of the wagons are closed and secured with the French keys which are to be properly fixed in the three levers, the keys being inserted in the holes from the left-hand side.

FREIGHT TRAIN INSTRUCTIONS.

INSTRUCTIONS FOR WORKING BALLAST TRAINS—Continued.

ALL TRAINS WITH THE EXCEPTION OF THOSE CARRYING 'C' HEADCODE, MUST COMPLY WITH THE INCLINE INSTRUCTIONS AS SHOWN ON PAGES 178 AND 80 OF THE GENERAL APPENDIX TO THE RULE BOOK, MUST STOP AT STOP BOARDS AND PIN DOWN BRAKES AS REQUIRED IN ACCORDANCE WITH THE "GENERAL INSTRUCTIONS FOR WORKING INCLINES."

It is of importance that close co-operation should exist between the District Operating Superintendents and District Engineers so that Engineering Department trains shall be scheduled at a time when they will cause least interference with the ordinary booked train services. This is especially necessary where the trains have to work on, or pass over, single lines.

On main passenger train routes the running should be curtailed as far as possible throughout the period of summer traffic (during Bank Holiday periods, etc.) especially on Mondays, Fridays and Saturdays.

(G.A.30 Op.—9/54 E.8267 H(12-C.).

and the regulations will apply if this being done

light or ballast trains, which are partly composed of vacuum fitted stock, not be coupled to trains not fitted with the vacuum brake except in an emergency when the vacuum brake must be cut out from the engine and both trains work as ordinary vacuum fitted trains.

Ballast Guards are responsible for rendering a list of the work to the Driver at the completion of the work, covering the time taken to do this work. Drivers of ballast trains must co-operate with the Traffic Department in order to obtain the ballast to be used and return it to the day

It is of importance that close co-operation should exist between the Traffic and Engineering Departments, so that ballast trains shall be scheduled at a time when they will cause least interference with the ordinary booked train services. This is especially necessary where the trains have to work on, or pass over, single lines.

Co-operation between Traffic and Engineering Departments.

WORKING OF HOPPER BALLAST WAGONS AND PLOUGH.

Whenever practicable, a loaded hopper ballast train must be worked by a Guard from previous instructions and experience, is qualified to take charge.

The working of the hopper ballast plough for spreading the ballast is only to be taken by the Guard and each van is to contain a notice to this effect.

The hopper ballast plough is not to be used in station yards, nor on any but the hopper road, leaves on the outside.

Hopper wagons are only to be loaded with material suitable for top ballast, and must not be employed for spreading material other than crushed slag or coarse gravel and ashes suitable for top ballast.

When required to be introduced on lines already opened for traffic, the Permanent Supt. of the district will give instructions where the ballast is to be deposited but the Guard of the train and the Gangers of the length must satisfy themselves that there are no fittings, cat h. pants, guard rails, signal wires, locking points, detonator machines, automatic train control, drains, level crossings, or works with which the plough can possibly come in contact on the lengths of the train to be ballasted.

Guard will be held solely responsible for the proper working of the train and for seeing himself that

(1) Hopper wagons and plough vans are in good order and examined at the usual examining points en route.

(2) When not engaged in unloading, the plough is in its normal position by being raised to the fullest extent, and the two keys inserted and secured by the lock.

(3) Before the hopper wagons are taken to be loaded, the French keys are firmly fixed in the three levers, the keys being inserted in the holes from the left hand side.

Guard to understand working of hopper train

Working of plough to be undertaken by Guard.

Where ploughs are not to be used.

Materials carried by hopper trains.

Discharging ballast from hopper trains.

Responsibilities of Guard in charge of hopper train

FREIGHT TRAIN INSTRUCTIONS.

WORKING OF HOPPER BALLAST WAGONS AND PLOUGH *Continued.*

(d) When starting on a journey with a train which has to be unloaded in a tunnel, the plough is in the right direction, also that the hoppers are arranged so that all the levers are on one side of the train, and that where the unloading has to be performed in a double line tunnel, all the levers face the 6-ft. way.

(e) Upon arrival of the train at the place where it is intended to unload, and after it has been placed in position, the plough is lowered to the rails, but *not screwed hard down*. That the French keys in the centre and right-hand levers of the wagons are then removed and the key of the left-hand lever kept in. That the hopper wagon next to the engine is discharged first, the door being opened by removing the key from the left-hand lever, and the engine started at a speed of not more than four miles per hour. Before the whole of the contents of the first wagon to be discharged are run out, that the hopper door of the next wagon to it is lowered, and so on through the train.

The Permanent Way Inspector or Ganger will assist in the discharge of the train, one being on one side of the train and the Guard on the other.

(f) When the contents of the whole train have been spread, and before resuming the journey, the plough is cleaned with the scrapers which are kept in the plough van, then raised to the fullest extent and secured by means of the keys and padlock.

(g) Before proceeding with the empty train the doors of the hoppers of the wagons are closed and secured with the French keys.

Lubrication.

The lubrication, which includes doors, pins and slides of hopper ballast wagons and plough vans will be done by the Chief Mechanical Engineer's Department at the loading points.

Closing doors of hopper wagons

Should any difficulty be experienced in closing the doors of hopper wagons, force must not be used but the ploughing back against the stop of the door to dislodge any fine ballast from the ledge on the "Z" iron which may be resting there.

BALLAST TRAINS WORKING IN SECTION AND BACKING.

Where it is necessary for a ballast train which has to perform work in a block section, to move back a short distance in the wrong direction for the purpose of passing the train, or for any other object, such movements in the wrong direction must be done with great care, especially on sections of the line where gradients exist and where the engine is at the rear end. The Guard must be held responsible for seeing that the train is not run back into a siding or into the tunnel, some other competent person in the van ready to stop the train when the necessary movement is being made. The Guard must also be held responsible for seeing that the train is not run back into a siding or into the tunnel, some other competent person in the van ready to stop the train when the necessary movement is being made. The Guard must also be held responsible for seeing that the train is not run back into a siding or into the tunnel, some other competent person in the van ready to stop the train when the necessary movement is being made.

Under no circumstances must a ballast train set back in the wrong direction to plough out ballast in tunnels.

Attention is directed to the instructions set out on page 263 in regard to securing catch points before a train is run over them in a facing direction.

WORKING OF VACUUM AND PARTLY VACUUM FITTED FREIGHT TRAINS.

8 Freight Trains carrying "D" Headlamps.

Freight trains composed partly of vacuum brake fitted vehicles booked to carry "D" headlamps and run at the times set out in the Service Time Tables for such trains, which are based on average speeds of 40 to 45 m.p.h., and a maximum under suitable conditions of 55 m.p.h., must be formed and worked in accordance with the following regulations:—

(1) All the vehicles formed in a partly vacuum-braked freight train carrying "C" headlamp must be fitted with OIL AXLE-BOXES. *Note. On certain trains oil axle-boxes packed with horse hair or waste are prohibited.* See opposite.

(2) The MAXIMUM number of vehicles which may be formed on a partly vacuum fitted freight train is 70, excluding the brake van.

G 26

SECURING

The attention of the person in transit and

The French keys in the centre and right-hand levers of the wagons are then removed and the key of the left-hand lever kept in. That the hopper wagon next to the engine is discharged first, the door being opened by removing the key from the left-hand lever, and the engine started at a speed of not more than four miles per hour. Before the whole of the contents of the first wagon to be discharged are run out, that the hopper door of the next wagon to it is lowered, and so on through the train.

Similarly, the end is dropped

The pins at the top or bottom

it is the responsibility of the person in transit.

Diagram



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WORKING OF VACUUM AND PARTLY VACUUM FITTED FREIGHT TRAINS

-page 172.

The following to precede Clause (A) of the existing instructions:—

Freight Trains carrying "C" Headcodes

The working conditions applicable to "C" Headcode Freight Trains are:—

Maximum load equal 50 wagons (10-ton carrying capacity) of Class 3 traffic
Exclusive of brake van.

2. Maximum speed 40 miles per hour

3. Normal vacuum reading to be not more than 26 inches and not less than 23 inches

4. ~~Freight goods train~~ must be provided with 8 inches on the goods brake van gauge

4. May run 160 miles with at intermediate examination

6. The vacuum brake to be operative on not fewer than 75 per cent. of the total of the train excluding the goods brake van

They will not be required to stop at Stop Boards.

Where assistance is necessary this to be performed under the same conditions as to ordinary freight train except that the bank engine vacuum pipe must be in the train, when the assisting engine is coupled in the rear

Special "Z" stop at Pining for Severn Tunnel examination (not Carriage and Department examination) to be made as in the case of other freight trains.
Arrangement to apply at Severn Tunnel Junction in respect to Up Freight Trains.

Wagons conveyed must all be vacuum fitted or piped and fitted with oil axle
packed with horsehair or waste

Case "A" Freight Train carrying "D" Headcodes to be relettered " (B)." 31
B) Freight Trains carrying "E" Headcodes to be relettered " (C)."

SECURING OF BOTTOM DOORS—HOPPER WAGONS, Etc.

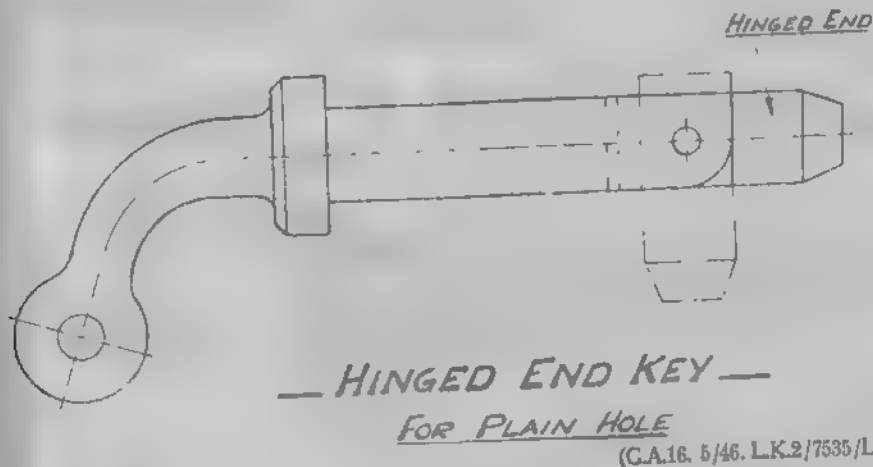
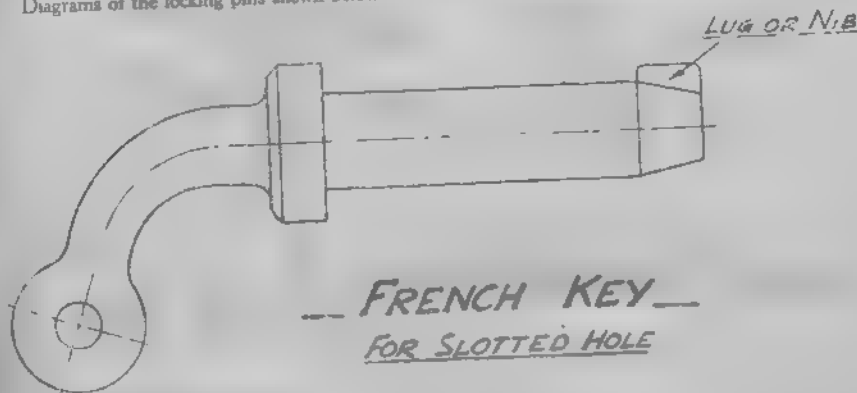
The attention of all concerned is drawn to the importance of the French keys or locking pins which secure the bottom doors of Hopper wagons, etc. being in the proper position to prevent the doors opening in transit and depositing some, or all of the load upon the track.

The French keys, with a small rut which registers with a slot in the plate through which the key is passed, must be positioned so that the keys require to be turned through an angle (up to a full half-turn in some cases) before they can be withdrawn. Inspection should easily show if there is any likelihood of the key working out because it has not been turned round to the "secure" position, i.e. the nib and slot should be out of line whilst the vehicle is in traffic.

Similarly, locking pins, with a hinged end, should be seen to be passed right through the hole so that the end is dropped into the "secure" position.

The pins or keys are sometimes behind the solebar and not easily visible without looking over or under the top or bottom of the solebar, but in all cases these bottom doors are locked with a locking pin and though it is the responsibility of the loader to see that the pins are properly secured at the time of loading, it is the duty of the guards and examiners to see that the pins have not worked round to the "withdraw" position during transit.

Diagrams of the locking pins shown below.



(G.A.16. 5/46. LK2/7535/L)

Note. On certain trains oil gate-boxes packed with horse hair or waste are prohibited. These trains are marked with a ⊗ in the service timetables, which signifies that in addition to the restriction regarding axle-loads packed with horse hair or waste, the trains travel faster than the average speeds of 40-45 miles per hour and do not travel more than 115 miles without examination.

(G.A.16. 5/46. E.18/100 M.H.)

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WORKING OF VACUUM AND PARTLY VACUUM FITTED FREIGHT TRAINS—Page 173

The following to be inserted immediately after note "Z":—

Note. For the purpose of giving effect to this instruction the minimum number of wagons to be coupled with the vacuum pipe to the train engine to be based on the equivalent load for Class 3 traffic, e.g., should a load of 44 wagons of Classes 2 and 3 traffic be equal to 60 wagons of Class 3 traffic the minimum number of wagons to be coupled with the vacuum pipe to the engine to be 20 and not 15.

(G.A.30 Op.—9/54. E.84669H. (2-C.)).

WORKING OF VACUUM AND PARTLY FITTED FREIGHT TRAINS. page 172.

Class 1, of the instructions under heading "(B) Freight Trains carrying ~~1~~ ² ~~3~~ ⁴ ~~5~~ ⁶ ~~7~~ ⁸ ~~9~~ ¹⁰ ~~11~~ ¹² ~~13~~ ¹⁴ ~~15~~ ¹⁶ ~~17~~ ¹⁸ ~~19~~ ²⁰ ~~21~~ ²² ~~23~~ ²⁴ ~~25~~ ²⁶ ~~27~~ ²⁸ ~~29~~ ³⁰ ~~31~~ ³² ~~33~~ ³⁴ ~~35~~ ³⁶ ~~37~~ ³⁸ ~~39~~ ⁴⁰ ~~41~~ ⁴² ~~43~~ ⁴⁴ ~~45~~ ⁴⁶ ~~47~~ ⁴⁸ ~~49~~ ⁵⁰ ~~51~~ ⁵² ~~53~~ ⁵⁴ ~~55~~ ⁵⁶ ~~57~~ ⁵⁸ ~~59~~ ⁶⁰ ~~61~~ ⁶² ~~63~~ ⁶⁴ ~~65~~ ⁶⁶ ~~67~~ ⁶⁸ ~~69~~ ⁷⁰ ~~71~~ ⁷² ~~73~~ ⁷⁴ ~~75~~ ⁷⁶ ~~77~~ ⁷⁸ ~~79~~ ⁸⁰ ~~81~~ ⁸² ~~83~~ ⁸⁴ ~~85~~ ⁸⁶ ~~87~~ ⁸⁸ ~~89~~ ⁹⁰ ~~91~~ ⁹² ~~93~~ ⁹⁴ ~~95~~ ⁹⁶ ~~97~~ ⁹⁸ ~~99~~ ¹⁰⁰ ~~101~~ ¹⁰² ~~103~~ ¹⁰⁴ ~~105~~ ¹⁰⁶ ~~107~~ ¹⁰⁸ ~~109~~ ¹¹⁰ ~~111~~ ¹¹² ~~113~~ ¹¹⁴ ~~115~~ ¹¹⁶ ~~117~~ ¹¹⁸ ~~119~~ ¹²⁰ ~~121~~ ¹²² ~~123~~ ¹²⁴ ~~125~~ ¹²⁶ ~~127~~ ¹²⁸ ~~129~~ ¹³⁰ ~~131~~ ¹³² ~~133~~ ¹³⁴ ~~135~~ ¹³⁶ ~~137~~ ¹³⁸ ~~139~~ ¹⁴⁰ ~~141~~ ¹⁴² ~~143~~ ¹⁴⁴ ~~145~~ ¹⁴⁶ ~~147~~ ¹⁴⁸ ~~149~~ ¹⁵⁰ ~~151~~ ¹⁵² ~~153~~ ¹⁵⁴ ~~155~~ ¹⁵⁶ ~~157~~ ¹⁵⁸ ~~159~~ ¹⁶⁰ ~~161~~ ¹⁶² ~~163~~ ¹⁶⁴ ~~165~~ ¹⁶⁶ ~~167~~ ¹⁶⁸ ~~169~~ ¹⁷⁰ ~~171~~ ¹⁷² ~~173~~ ¹⁷⁴ ~~175~~ ¹⁷⁶ ~~177~~ ¹⁷⁸ ~~179~~ ¹⁸⁰ ~~181~~ ¹⁸² ~~183~~ ¹⁸⁴ ~~185~~ ¹⁸⁶ ~~187~~ ¹⁸⁸ ~~189~~ ¹⁹⁰ ~~191~~ ¹⁹² ~~193~~ ¹⁹⁴ ~~195~~ ¹⁹⁶ ~~197~~ ¹⁹⁸ ~~199~~ ²⁰⁰ ~~201~~ ²⁰² ~~203~~ ²⁰⁴ ~~205~~ ²⁰⁶ ~~207~~ ²⁰⁸ ~~209~~ ²¹⁰ ~~211~~ ²¹² ~~213~~ ²¹⁴ ~~215~~ ²¹⁶ ~~217~~ ²¹⁸ ~~219~~ ²²⁰ ~~221~~ ²²² ~~223~~ ²²⁴ ~~225~~ ²²⁶ ~~227~~ ²²⁸ ~~229~~ ²³⁰ ~~231~~ ²³² ~~233~~ ²³⁴ ~~235~~ ²³⁶ ~~237~~ ²³⁸ ~~239~~ ²⁴⁰ ~~241~~ ²⁴² ~~243~~ ²⁴⁴ ~~245~~ ²⁴⁶ ~~247~~ ²⁴⁸ ~~249~~ ²⁵⁰ ~~251~~ ²⁵² ~~253~~ ²⁵⁴ ~~255~~ ²⁵⁶ ~~257~~ ²⁵⁸ ~~259~~ ²⁶⁰ ~~261~~ ²⁶² ~~263~~ ²⁶⁴ ~~265~~ ²⁶⁶ ~~267~~ ²⁶⁸ ~~269~~ ²⁷⁰ ~~271~~ ²⁷² ~~273~~ ²⁷⁴ ~~275~~ ²⁷⁶ ~~277~~ ²⁷⁸ ~~279~~ ²⁸⁰ ~~281~~ ²⁸² ~~283~~ ²⁸⁴ ~~285~~ ²⁸⁶ ~~287~~ ²⁸⁸ ~~289~~ ²⁹⁰ ~~291~~ ²⁹² ~~293~~ ²⁹⁴ ~~295~~ ²⁹⁶ ~~297~~ ²⁹⁸ ~~299~~ ³⁰⁰ ~~301~~ ³⁰² ~~303~~ ³⁰⁴ ~~305~~ ³⁰⁶ ~~307~~ ³⁰⁸ ~~309~~ ³¹⁰ ~~311~~ ³¹² ~~313~~ ³¹⁴ ~~315~~ ³¹⁶ ~~317~~ ³¹⁸ ~~319~~ ³²⁰ ~~321~~ ³²² ~~323~~ ³²⁴ ~~325~~ ³²⁶ ~~327~~ ³²⁸ ~~329~~ ³³⁰ ~~331~~ ³³² ~~333~~ ³³⁴ ~~335~~ ³³⁶ ~~337~~ ³³⁸ ~~339~~ ³⁴⁰ ~~341~~ ³⁴² ~~343~~ ³⁴⁴ ~~345~~ ³⁴⁶ ~~347~~ ³⁴⁸ ~~349~~ ³⁵⁰ ~~351~~ ³⁵² ~~353~~ ³⁵⁴ ~~355~~ ³⁵⁶ ~~357~~ ³⁵⁸ ~~359~~ ³⁶⁰ ~~361~~ ³⁶² ~~363~~ ³⁶⁴ ~~365~~ ³⁶⁶ ~~367~~ ³⁶⁸ ~~369~~ ³⁷⁰ ~~371~~ ³⁷² ~~373~~ ³⁷⁴ ~~375~~ ³⁷⁶ ~~377~~ ³⁷⁸ ~~379~~ ³⁸⁰ ~~381~~ ³⁸² ~~383~~ ³⁸⁴ ~~385~~ ³⁸⁶ ~~387~~ ³⁸⁸ ~~389~~ ³⁹⁰ ~~391~~ ³⁹² ~~393~~ ³⁹⁴ ~~395~~ ³⁹⁶ ~~397~~ ³⁹⁸ ~~399~~ ⁴⁰⁰ ~~401~~ ⁴⁰² ~~403~~ ⁴⁰⁴ ~~405~~ ⁴⁰⁶ ~~407~~ ⁴⁰⁸ ~~409~~ ⁴¹⁰ ~~411~~ ⁴¹² ~~413~~ ⁴¹⁴ ~~415~~ ⁴¹⁶ ~~417~~ ⁴¹⁸ ~~419~~ ⁴²⁰ ~~421~~ ⁴²² ~~423~~ ⁴²⁴ ~~425~~ ⁴²⁶ ~~427~~ ⁴²⁸ ~~429~~ ⁴³⁰ ~~431~~ ⁴³² ~~433~~ ⁴³⁴ ~~435~~ ⁴³⁶ ~~437~~ ⁴³⁸ ~~439~~ ⁴⁴⁰ ~~441~~ ⁴⁴² ~~443~~ ⁴⁴⁴ ~~445~~ ⁴⁴⁶ ~~447~~ ⁴⁴⁸ ~~449~~ ⁴⁵⁰ ~~451~~ ⁴⁵² ~~453~~ ⁴⁵⁴ ~~455~~ ⁴⁵⁶ ~~457~~ ⁴⁵⁸ ~~459~~ ⁴⁶⁰ ~~461~~ ⁴⁶² ~~463~~ ⁴⁶⁴ ~~465~~ <

() An order of credit to the order of the holder of the bill was recorded for in the note below must be calculated with the value of the bill to the order of the holder of the bill.

N , the relation t , be an n -ary r -ary t -ary

The following table is inserted at the end of Article 1 of Chapter (1)

Note. The minimum number of vehicles included in the fitted portion of a "B" head of train
the number of vehicles included in the fitted portion must not be fewer than one vehicle (with a minimum of
1) or more than 100% of that train number vehicles on the train.

$$(GA \rightarrow i^* \pm E, \text{ and } G\{H\})$$

1. In the event of the MINIMUM proportion of 100,000 valid MINIMUM DOLLAR votes

FREIGHT TRAIN INSTRUCTIONS.

WORKING OF VACUUM AND PARTLY FITTED FREIGHT TRAINS -continued.

(3) The Minimum and Maximum proportions of vacuum brake fitted vehicles required to be connected with the vacuum pipe to the train engine, if the train is to run at its booked speed, except in cases where a different proportion is specially authorised, are given below

No. of vehicles on train excluding Brake Van	No. of Vacuum-braked vehicles required to be coupled with the vacuum pipe to the train engine.		No. of vehicles on train (excluding Brake Van).	No. of Vacuum-braked vehicles required to be coupled with the vacuum pipe to the train engine.	
	Minimum.	Maximum.		Minimum.	Maximum.
Not exceeding			Not exceeding		
5	5		40 wagons	13	20
10	6		42 "	14	21
15	7	See Note Z below.	44 "	15	22
20	8		46 "	15	23
25	9		48 "	16	24
30	10		50 "	17	25
35	11		52 "	17	26
40	12		54 "	18	27
45	13		56 "	19	28
50	14		58 "	20	29
55	15		60 "	21	30
60	16		62 "	21	31
65	17		64 "	22	32
70	18		66 "	22	33
75	19		68 "	23	34
80	20		70 "	24	35

2. ... coupled with vacuum pipe to the train

... coupled to the train engine exceed 35.

... head lamps having less than the

... accordance with Clause (3), or any such train

... vacuum brake-fitted vehicles so as to reduce the number

... the vacuum pipes to the engine below the minimum required, must

... driver properly to control the train with the brake

... as the case may be

... at a station at which the number of vacuum

... the Guard must inform the Driver of the actual number of

... vacuum brake to the engine, and the Driver must not start until he

has received this information.

B Freight Trains carrying "E" Headlamps.

... headlamps may run at a maximum speed of 35 miles per hour subject

... following regulations:—

... vacuum brake fitted vehicles must be coupled with the vacuum pipe to the train engine in accordance with the following table:—

No. of Vehicles on Train (excluding Brake Van).	No. of Loaded Vacuum-Braked Vehicles Required.
Not exceeding 40	Not less than 4
45	5
50	6 and not more than
55	7 half the
60	8 total.
65	9
70	11

In the event of the MINIMUM proportion of loaded vacuum-braked vehicles not being made to the engine, the train should run at the point-to-point times for

... provided the load does not exceed 70 wagons of Class 3 traffic

... must run at the point-to-point times as set out in the Service ... and no vacuum-brake fitted vehicles must be coupled with the vacuum pipe to the engine.

... for RELEASE AXLE-BOX vehicles to be formed in trains of this class, the following conditions must be observed:—

i.) The weight of the load must not exceed ONE-HALF of the registered carrying capacity of the wagon.

ii.) The load must be evenly distributed.

iii.) Examiners must give special attention to grease axle-boxes at examining stations.

FREIGHT TRAIN INSTRUCTIONS.

General.

"The foregoing instructions apply to Railway Companies' vehicles. ~~Privately~~ ^{less} owned Requisitioned wagons fitted with oil axle boxes and loaded with Class 2 traffic (or empty) may be covered by instructions of Freight Trains." ^{oil}

"The term 'vehicle' must be understood to mean a four-wheeled vehicle."

(G.A. 10. 3/42. E.78000/15.H.)

(b) No partly vacuum fitted freight train may have more than 10 loose coupled vehicles (including brake van) behind the vacuum braked vehicles connected with the vacuum pipe to the engine. If this number be exceeded, the train must be run as a non-vacuum train.

(c) The Driver must apply the vacuum brake gradually so as to avoid damage to the train and to the contents of the wagons.

WORKING OF SPECIAL FREIGHT TRAINS.

The working of special freight trains including stops for examination (in accordance with the regulations) will be arranged by the Controls, in Divisions where such exist, and in the absence of Control by the person deputing by the Divisional Superintendent or District Traffic Manager.

Controllers or others responsible, must forward all necessary advice in accordance with the instructions relating to "Freight train advice," including particulars of head-lamps, and point to point allowances applicable.

Special freight trains must not be run without the consent of all Controls or Divisions through whose area the train has to pass, and it is the responsibility of the Controller to secure a reasonable path and utilising power to the best advantage.

In the event of a train being short of the arranged destination, the Controller, or person, making the arrangement, must advise all concerned accordingly.

As long notice as possible must be given the Locomotive Department when power is required for special trains.

WORKING OF FREIGHT TRAINS.—Page 174.

The instructions under the above heading to be cancelled and the following substituted:—

EXAMINATION OF FREIGHT TRAINS.

Unless otherwise specially agreed between the Operating Superintendent and the Carriage & Wagon Engineer, freight trains must not run more than the following distances shown under the various headcodes without stopping for Carriage and Wagon examination.

Class of Freight Train	Maximum Distance in Miles, train is allowed to run without full Carriage & Wagon Examination	Type of Axle Box
"C"	160	Oil Axle Box
"D"	125	Oil Axle Box.
"E"	125	Oil Axle Box.
"E"	85	Grease Axle Box
"F" and below.	85	Oil and Grease Axle Box. Wagons (loaded or empty).
"F"	125	Oil or Grease Axle Box empty wagons only*

* Full train loads of empty wagons capable of being run under 'C', 'D' or 'E' headcode conditions to be so arranged.

(G.A.30 Op.—9/54. E.81019. H(7-B).)

and utilising man and engine power to the best advantage—

In the event of a train being short of the arranged destination, the Controller, or person making the arrangement, must advise all concerned accordingly.

As long notice as possible, minimum three hours must be given to the Locomotive Department when power is required for special trains.

(G.A.19—10 48 E.81019/H. (7—B))

make arrangements for its return in accordance with the following instructions:—

1. Unbalanced engines, other than those referred to in these instructions as 'engines of seasonal traffic specials,' must not be detained at a "foreign" depot for a longer period than 10 hours, unless there is a reliable prospect of a suitable service being found for them in the homeward direction, when they may be retained for a period not exceeding 12 hours.
2. Engines must be despatched from a foreign depot in the direction of the home station only, and must normally be worked to the home depot by the most direct route.

Note.—If a train is a booked "D" headlamp train, and the only reason it is reduced to a "D" headlamp train is because there is an insufficient number of vacuum fitted vehicles connected with the vacuum pipe to the train engine to conform to "D" headlamp conditions, it can run as scheduled so far as examination is concerned, i.e. up to 115 miles between examinations.

Controllers or others responsible must forward all necessary advice in respect to the running and loading of both booked and special freight trains. It is the responsibility of the Controller in the originating Division to agree with all other interested Controllers the point or points at which special freight trains must be examined.

Special freight trains must not be run without the consent of all Controllers through whose area the train has to pass, so that every opportunity may be taken of securing the best practicable path and utilizing man and engine power to the best advantage.

In the event of trains terminating short of their assigned destination, the Controller, or person making the arrangement, must advise all concerned accordingly.

As long notice as possible, minimum three hours, must be given to the Locomotive Department when power is required for special trains.

(G.A.19—10 48. E 81019/H. (7—B)

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The instructions on page 174 under heading **PROVISION OF ENGINE CONDUCTORS** to be deleted (see page 141)

(G.A. 28. Op.—4 5

FREIGHT TRAIN RUNNING TIMES.

Whenever the maximum load exceeds that provided for the class of vehicle or traffic conveyed in a Freight train is essential to allow the train to be running being maintained, the Driver should be instructed to run his train to the appropriate standard point timing to suit the altered circumstances.

REGULATIONS TO BE OBSERVED IN RETURNING OR BALANCING ENGINES OF SPECIAL, CANCELLED OR DIVERTED TRAINS.

(A) Engines of Special Trains.

When an engine belonging to another station becomes available for which there is no immediate return service, the Locomotive Depot concerned must advise the Traffic Department or Control and make arrangements for its return in accordance with the following instructions.

1. Unbalanced engines, other than those referred to in these instructions as "engines of seasonal traffic specials," must not be detained at a "foreign" depot for a longer period than 10 hours, unless there is a reliable prospect of a suitable service being found for them in the homeward direction, when they may be retained for a period not exceeding 12 hours.
2. Engines must be despatched from a foreign depot in the direction of the home station only, and must normally be worked to the home depot by the most direct route.

WORKING OF EX-PRIVATELY OWNED WAGONS ON FREIGHT TRAINS.

The conditions under which ex-privately owned wagons are to be conveyed on freight trains are given below :—

Wagons fitted with Oil Axle-boxes.

Class of Traffic.

Head Codes.

*Class 1 ... } "F" or inferior.
Class 2 ... }

Wagons fitted with Grease Axle-boxes.

Class of Traffic.

Headcodes.

Class 1 ... } Inferior to "F."
Class 2 ... }
Class 3 ... } "F" or inferior.
Empty ... }

(G.A.26 Op.—5/50.)

Following additional instruction to be inserted on page 174 immediately preceding the instructions head "Working of Special Freight Trains":

HEADLAMPS OF SPECIAL TRAINS CONSISTING OF EMPTY RAILWAY-OWNED STOCK.

The following instructions must be observed in respect of headlamps to be carried by special trains of empty railway company's wagon

(1) **D** Headlamps to be carried
General A

ly when the instructions on pages 172,

E ...

ly when the instructions in the General
laid with. The number of wagons coupled
over than one with (with a minimum of) or more than

of ... stock or suitable wagons cannot be complied with, the special trains must be run at ordinary speed

(G.A. 18. 11/47. E.81563H.)

GA26

Reference to the following to be made on page 174:—

RUNNING OF SPECIAL FREIGHT TRAINS.

When an inter-District Special freight train is required (other than those pre-arranged by ... means) the request to be passed by the originating District Control to Headquarters, with details of headcode, load to be conveyed and time the ... The Headquarters Control to make the necessary arrangements ... Controls including points at which Carriage and Wagon examination is to be carried out and agree or otherwise with the District Control making the application whether the Special train can run.

... as possible, minimum 4 hours, must be given to the Motive Power Department ... to be made in the provision of power. A serial number will be given by ... freight trains agreed and this number to be quoted in all ... or telegraphic, sent in connection with the train concerned.

(G.A.30 Op.—9/54. E.81019 H(7-B.)).

Wagons fitted with Oil Axle-boxes.

Class of Traffic.

Head Codes.

*C 155 1

C 155 2 ..

C 155 3 .

Empty

} "F" or inferior

< All classes of freight trains except those
, marked *

• The maximum number of ex private, owned or former railway company owned wagons of Class 1 traffic permitted on "F" head code trains is ten, and they must be fitted with oil axle-boxes
(G A.27 Op — 1 51 E 78000 15.W.)

FREIGHT TRAIN INSTRUCTIONS.

REGULATIONS TO BE OBSERVED IN RETURNING OR BALANCING ENGINES OF SPECIAL, CANCELLED OR DIVERTED TRAINS—*Continued.*

3. Unbalanced engines must not be ordered away with trains the first portion of the journey towards the home depot, and the remainder away from it, unless arrangements are made for the unbalanced engine to be taken off the train at a suitable point *en route*.
4. Unbalanced engines, when once despatched on a special train for a point short of the home depot, or taken off a train in accordance with Clause 3, must not be detained at such intermediate point, but must be worked to the home depot with the least possible delay.
5. Unbalanced engines must not be worked on regular services unless and until arrangements have been made between the Locomotive Depots concerned to balance the engine working satisfactorily.

(B) Engines of seasonal traffic trains.

1. Engines which have worked seasonal traffic and ocean specials are excepted from the operation of Clause 1 of the foregoing instructions relating to the retention of unbalanced power for the purpose of finding a return loading. Such engines must be returned to their home depots in accordance with the special instructions appearing in the working notice relating to the particular traffic, or, where no notice is issued, with the least possible delay.
2. Engines to which the foregoing clause applies are those which have worked seasonal traffic, viz., flower, fruit, broom, potato and other vegetable specials, and ocean specials (passenger or mails).

(C) Engines of cancelled trains.

1. When a regular freight train is cancelled, the Control or Yard Master must confer with the destination Control and ascertain whether—
(a) When the train is a "home" engine belonging to the home station, the engine is to be sent light to the opposite end to return with the balancing train, or
(b) When the train is a "foreign" engine, the "foreign" engine must be retained to work another train, or
(c) The engine is to be sent to the Locomotive Department.
2. If consent is obtained for the home engine or the "foreign" engine, as the case may be, to be used for a service other than scheduled in the direction of the balancing point, the engine must be arrived at as to how such engine is to work forward to its balancing point and particularly as to the time required there.

(D) Engines of diverted trains.

1. In the event of a regular long distance freight train being loaded at its starting point to a destination other than booked, or diverted intermediate, the engine being required at the booked destination to work a balancing train, the Control arranging the diversion, after consulting with the Locomotive Depot, will be responsible for advising in ample time all Controls and Locomotive Depots concerned the arrangements which are necessary to maintain the scheduled balance of engines.
2. If a train is diverted away from its booked destination, the engine should be dealt with at the nearest junction as indicated below—
 - i. Train diverted to a distant station, i.e. more than 35 miles beyond junction point. Fresh engine to be provided to work train forward and return as ordered. Train engine to run to booked destination to work balancing train.
 - ii. Train diverted to a near station, i.e. not more than 35 miles beyond junction point. Train engine to work through to destination of train, returning immediately to junction point and then be sent to booked destination for balancing train.
3. If the train is ordered to a destination short of that booked, the receiving Control and Locomotive Depot will be responsible for sending the engine forward to its booked destination for the balancing train.

RESTRICTIONS ON RUNNING OF WAGONS LOADED WITH BOILERS, GIRDERS, OR OTHER HEAVY TRAFFIC.

1. Great Western Company's oil box wagons of the following types, also other Company's wagons of the similar types fitted with the Railway Clearing House, 1823, or subsequent standard oil-axis boxes):— **GANE A, MACAW J, AERO PROPELLER WAGON**
 Pollens, Crocodiles (except Crocodile L), Rectanks, Loriot, Loriot A, B, C, D, E, L, M, and R, Loriot W when fitted with removable bulging struts, Hydras, Serpents (except Serpent D), Crane Motors (except N, 4199), Beavers, Totems, Open C, Macaw A, B, C, D, E, F, G, and H, Gane.

except as stated in Clauses 5 and 6 when loaded with boilers, girders, or other heavy traffic, must not travel on freight trains carrying superior to "F" headcodes.

The following to be inserted as Clause 1A:—

Engines must not be employed on partially fitted Vacuum trains which are timed in excess of 40 miles per hour, or on fully fitted Vacuum trains more than 115 miles without examination. On partially fitted Vacuum trains which are timed in excess of 40 miles per hour or run over 115 miles without examination "Serpents" may be employed on such trains, but "Serpents" 409 and 410 must not be employed on partially fitted Vacuum trains of any description.

C.A. 37 F.C. 2.5

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RESTRICTIONS ON THE RUNNING OF WAGONS, ETC.—page 176.

Clause 1 to be amended to read:

Great Western and other Companies' motor axle-box special wagons of types shown in Clauses 1 and 2 must not be conveyed on any freight train carrying superior than "E" headlamps, except the following in temporary use:

Warflat

~~GR 23~~ GR 23

(G.A. 18, 11, 47, E.78000, 1, H.)

3 Empty Great Western and empty "foreign" Companies' oil axle-box special wagons of the type enumerated in Clauses 1 and 2 must not travel on any freight train carrying superior than "E" headlamps

Following to be inserted as clause 5A—

Consignments of any description loaded on home wagons must not be conveyed by freight train, when the length of load exceeds 100 ft. if the wagon upon which it is loaded or when the weight of load exceeds 10 tons.

(G.A. 5.—2.39. E.76927 H.)

examining station being near the 25 mile limit, the Chief Mechanic or must accompany the load and make a special examination of the wagon and load after travelling 25 miles.

The following to be inserted between clauses 3 and 4

NOTE:—Clauses 1, 2 and 3 apply also to Road Rail and demountable tanks loaded on wagons
specally constructed for the conveyance of liquids in these cases the star or stars be painted
on the rail tank. In no case must a Road-Rail or demountable tank containing Class
"A" or other dangerous liquids be mounted on a rail chassis bearing three stars.

(G.A. 5.—2'39. LK1 4944 14)

Tanks for the conveyance of highly inflammable liquids (Class A) are painted
aluminum colour with a bright red band 6 inches wide running round the ends, the
red band is stopped short on the sides of the tank to provide space for the name of the
owner or commodity. These tanks must be dealt with in accordance with Rule 240

FREIGHT TRAIN INSTRUCTIONS. CONVEYANCE OF TANK WAGONS.

(f) Private Owners' tank wagons having one star painted on each side of each tank, former G.W.R. tank wagons and Messrs. Alsopps (Beer) tank cars, (loaded or empty), also Road Rail and Demountable tanks (loaded or empty), loaded on wagons specially constructed for their conveyance with one star painted on the rail chassis only, may be conveyed on trains scheduled to carry "E" or inferior to "E" head codes, irrespective of the class of traffic the tanks contain, as indicated on the wagon label.

(G.A.27.Op.—I, 51. E.76159/5.W.)

on each side of each tank also Road Rail and Demountable Tanks (loaded or empty) loaded on wagons specially constructed for their conveyance with two stars painted on the rail chassis only, may be run in partially vacuum freight trains scheduled to carry "D" head codes except those marked "A" in the Service Table (I, 51). Tanks to be marshalled at the rear end of the vacuum coupled portion of the train and must be the last vacuum coupled vehicle.

3 Private Owners' and Railway Companies fully vacuum brake fitted six wheeled tank wagons (full or empty) carrying non-inflammable liquids and having three stars painted on each side of each tank, also Road Rail and Demountable Tanks (loaded or empty) loaded on wagons specially constructed for their conveyance with three stars painted on the rail chassis only, may be run in partially vacuum freight trains scheduled to carry "D" head codes (except those marked "A" in the Service Table) also in passenger trains which do not exceed a speed of 10 m.p.h. at any point on the journey. Tanks to be marshalled in the vacuum portion of the train and must be the last vacuum coupled vehicle. IN NO CASE MUST A ROAD RAIL OR DEMOUNTABLE TANK CONTAINING CLASS "A" OR OTHER DANGEROUS LIQUIDS BE MOUNTED ON A RAIL CHASSIS BEARING THREE STARS.

4 Road Rail Demountable Tanks (loaded or empty) loaded on wagons specially constructed for their conveyance with three stars painted on the rail chassis may be run in partially vacuum fitted freight trains scheduled to carry "D" head codes and marked "A" provided they are marshalled either—

(a) next behind the train engine coupled to the engine and its neighboring vehicle by means of screw coupling with buffers slightly compressed.

(b) at the rear end of the train coupled to the last vehicle in the vacuum portion of the train and provided that it forms part of the vacuum coupled portion of the train. In the latter case the train must also be a vacuum train.

5 ROAD RAIL OR DEMOUNTABLE TANK CONTAINING CLASS "A" OR OTHER DANGEROUS LIQUIDS MOUNTED ON A RAIL CHASSIS BEARING THREE STARS.

6 Road Rail or Demountable Tanks (loaded or empty) must only be conveyed on trains scheduled to carry "D" head codes (Tank wagons for Class "A" inflammable liquids, dangerous liquids or compressed gases to be marshalled as near the middle of the train as possible).

When no freight train service is available, tank wagons for inflammable liquids and highly inflammable liquids (loaded or empty) may be conveyed by Mixed trains. When such wagons are so conveyed by Mixed trains they must be marshalled next inside the rear brake van and in all cases at least one ordinary wagon must be placed between any vehicle containing passengers and the tank wagons. In no case must wagons conveying readily combustible traffic, such as hay or straw, be placed next to the tank wagons.

The foregoing instruction modifies Clause 16 of Rule 24 in so far as the conveyance of these tank wagons is concerned.

(G.A.16. 5/46. E.76159/5. W.) IS.

7 Road Rail or Demountable Tanks (loaded or empty) may be conveyed on trains scheduled to carry "D" head codes (except those marked "A" in the Service Table) also in passenger trains which do not exceed a speed of 10 m.p.h. at any point on the journey. Tanks to be marshalled in the vacuum portion of the train and must be the last vacuum coupled vehicle. IN NO CASE MUST A ROAD RAIL OR DEMOUNTABLE TANK CONTAINING CLASS "A" OR OTHER DANGEROUS LIQUIDS BE MOUNTED ON A RAIL CHASSIS BEARING THREE STARS.

(a) The overhang must not exceed 4 feet beyond the end of the wagon.

(b) The total weight of the wagon load must not exceed one half the carrying capacity of the vehicle.

The overhanging traffic to be well secured and the wagon next to the overhanging end must always be loaded and of such height as to form a suitable match.

8 Low sided wagons carrying one or two planks must not be used for loading or unloading traffic with overhanging ends on partly fitted "D" or "E" head code freight trains.

"D" or "E" WORKING OF GOODS BRAKE VANS

1 Booked "D" and "E" head code trains must, as far as possible, be worked by Vans entered with the letter "S" and every effort must be made to keep these vans in balanced working to ensure adequate vans working "Express" Freight trains. At certain Depots vans are set aside for specific working and are branded R.C. Restricted Users, these vans must not be allowed to work other than on the local services for which they are appointed.

2 Station Masters, Yard Masters and Yard Inspectors are responsible for revealing on the train report to the Chief or Districtal Officer accurate particulars of Brake Vans on hand in accordance with instructions to enable a satisfactory distribution to be made.

3 When it is necessary for Freight trains to be stabled short of destination i.e. at an Cutting Refuge siding or Loop arrangements must be made by Control Staff or other responsible Officer, for the disposal of the brake vans, in order to avoid any unnecessary delays thereto.

WEIGHT OF TRAFFIC IN OWNERS' TANK WAGONS.

22
27
The tare for conveyance and the tare is readily available; it is obtained by deducting the painted tare from the gross weight shown on the outward and return journals. When returned to the sender the sender must declare the weight and the carriage charges must be paid thereon.

The wagons must be subject to the allowances for expansion be fully loaded unless fitted with expansion valves, allowing them to be used for variable loads from half-ton, but must not be loaded beyond the registered weight carrying capacity. Tank wagons, when fully loaded empty, will not be subject to expansion unless all doors and outlets are securely closed with tight.

Tank-Tank Wagons. Both tanks must be equally loaded when the wagon is in motion.

CONVEYANCE OF OVERHANGING LOADS ON PARTLY VACUUM-FITTED FREIGHT TRAINS.

D
The Rules of the Railway Service for the conveyance of loads, including the following, apply to all freight trains, whether they be loaded or empty, and to all partly vacuum-fitted freight trains, with the exception of those fitted with vacuum brakes, 40 inches or more in diameter, which must be examined at examination, also by **E** ~~Headbooks~~ **Headbooks** and the following regulations.

The overhanging load must extend to the rear end of the wagon.

The total weight of the wagon load must not exceed one half the carrying capacity of the

The overhanging traffic to be well secured and the wagon next to the overhanging end to be well secured and of such height as to form a suitable matron.

The overhanging traffic must not be used for loading or unloading traffic.

on partly fitted **D** or **E** ~~Headbooks~~ freight trains.

G A26

To Shops other than Swindon.

When vans are labelled to Shops other than Swindon, the equipment must be retained in the Vans, which must be locked.

The Traffic Department Staff at the Station or Yard adjacent to such Shops must, before handing over the vans to the Chief Mechanical Engineer's Department, remove the :—

Set of side lamps
Tail Lamp
Oil Can.
Bucket.

Shunting Pole.
Hand truck
Face Shovel

The Traffic Department Staff must leave in the Vans the :—

Brake Stick.
Wheel Sprags.
Short Drawbar and Block.

The equipment removed must be placed in store to replenish missing equipment as the vans ex Shops are put into traffic.

The Chief Mechanical Engineer's Department will be responsible for ensuring that the Brake Stick, Wheel Sprags, Short Drawbar and Block are in the Vans before being put into traffic.

(G A 13. 9 43 E 44407 H.)

4. Doors of Goods Brake Vans must be kept locked on the G W System when not in use.

Goods brake van doors are fitted with rim locks. Appointed Guards may, as a rule, retain possession of van door keys when resting out when called out or absent from duty through illness or any other exceptional cause; the keys must be left at the depot or other appointed place. A number of spare keys for use by emergency men acting as Goods Guards are kept at the Depots and will be handed to these Guards as required, who must return them to the Depots from which they are obtained.

5. The Standard equipment of each van is as shown below :—

Set of side lamps.	*Hand Brush.
Tail lamp.	*Fire shovel.
Chain	*Bucket
Shunting Pole.	* If any of these articles is
Sprags (not less than two).	missing this does not render
Brake Stick.	the van useless for service
Short Drawbar and Block.	

These articles must be retained in the Van and the set of Side Lamps and Tail Lamp when not being used must be placed inside the Van on the brackets provided. Special care must be taken to keep the Van door locked when the Van is not used for train working. In no circumstances must the equipment be removed from the Vans except for replacement from Stores. Neither must additional articles be kept in the Brake Vans.

The locker provided with the Vans for the storage of ARP equipment must be used for the ticket, shovel handle, etc., and Guards must lock these articles up in this receptacle when not in use.

6. Guards are responsible for the cleanliness and correct equipment of their vans. When leaving the Vans at the termination of train they must place the side and tail lamps on the brackets inside the Van and see that the other equipment is placed inside and that the door is locked; they must report in the book provided for the purpose particulars of any missing equipment or any matter requiring attention. They should keep their own property in the lockers provided in the Guards' room or cabins or deposit it with the Yard Inspector.

Note.—In the event of a Guard being authorised to leave a train on a Goods Line at a terminal point before the train is disposed of the person in charge must set in the side and tail lamps are deposited inside the van immediately the train is cleared from the Goods Line and the door locked.

7. When Brake Vans are ordered to be sent from one Yard or Station to another, the equipment must be sent with the van and the van must be locked without delay. Vans so sent must be labelled to the particular station to which they are to be sent on train and so.

8. Station Masters, Yard Masters and Vans in Charge must arrange for the inspection of Goods Brake Vans at least once a week to check the cleanliness and equipment, including the supply of sprags, and to report to the Divisional Superintendent any irregularity.

9. Guards must not paint over the bottom panes of the windows of Goods Brake Vans, thereby obstructing the look-out; neither must the construction of the vans be tampered with in any way.

10. Guards must, when leaving a brake van, satisfy themselves that there is sufficient sand and oil for the next journey and must report to person in charge of yard any deficiency. Guards must, before starting, test sanding appliances to ensure satisfactory working.

11. Guards must show on journals particulars of vans working "light".

12. In the event of a brake van being marked off, or stopped for repairs, the Guard or the examiner (if marked off in the Yard) will be responsible for advising the Traffic Inspector, or person in charge, in writing of the fact. When vans have to be sent to C & W shops for repairs the vehicle must be locked up with all the equipment so that the van can be put into traffic immediately after repairs have been completed. The Yard Master or Station Master must also advise the Divisional Superintendent or District Traffic Manager the number of each van marked off, the shop sent to for repairs and the date.

13. When it is necessary to send Brake Vans to Shops for repairs the following instructions must apply :—

To Swindon Shops.

The standard equipment must not be removed from the Vans before being sent to Swindon Shops, but must be locked inside the Van which must be labelled. Upon arrival at Swindon Works the equipment will be removed from the Vans and transferred to the General Stores. When the Vans have been repaired arrangements will be made to re-equip the Vans in the Swindon shops from the General Stores.

GENERAL INSTRUCTIONS FOR WORKING INCLINES.

Ascending Inclines.

1. The Assisting Engine must be placed as under

Freight or Mineral Trains. Behind the rear van, except in certain cases where the varying nature of the gradient necessitates it being placed in front of the Train Engine. As a general rule the Assisting Engine must not be coupled to the rear van, unless there be special circumstances which render it desirable. An exception to the general principle laid down above must be shown in the Appendix to the Service Rules, Rules or in Special Instructions issued by the Divisional Superintendents or District Traffic Manager.

On those sections of the line where a time interval is given for Freight and Mineral trains to be assisted in the rear, the Assisting Engine must, during Foggy Weather or Falling Snow be coupled to the rear of the train in every case. The Bank Engine must also be coupled when there is a failure of the Bank engine, or when worked by time interval.

2. Whenever it is necessary for a Freight or Mineral train to have the assistance of an Assisting Engine up a rising gradient the train must be stopped and it must be ascertained that the Assisting Engine may come to the tail of the train in readiness to assist it. On no account must an Assisting Engine come to the rear of any train whilst the train is in motion.

3. The signal to start must be given by the Guard to the Driver of the engine in the rear, and when there are two Guards this must be done by the rear Guard after he has exchanged signals with the Guard in front. When the Driver of the engine in rear has received the Guard's signal to start, he must call the attention of the Driver in the front of the train by giving two "crows" which must be acknowledged by repetition from the front engine after the Driver of that engine has satisfied himself that the necessary fixed signal has been lowered, and until these "crows" have been given and acknowledged, neither the train engine nor the assisting engine must move forward.

GOVERNMENT OF INDIA.

Divisional Superintendent's Office,
P. O. (P.). G. 7.

Ref:- A1/23,000

10th. July, 1941.

Dear Sir,

STORING OF GOODS IN BARRACKS.

With reference to the instructions under the above heading on pages 177 and 178 (amended in C. A. 1, of the General Appendix to the Rule Book, the following addition should be added to clause 17 of the revised instructions:-

"Not more than one bucketful of sand should be placed in the sand boxes when the supply is exhausted".

Please bring this instruction to the notice of all Guards and others concerned.

Yours truly,
for R.C. Pole.

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Descending Inclines.

Clause 15 amended to read:—

15 All ballast and freight trains, including those consisting partly of vacuum stock but excluding those which are fully vacuum fitted, must stop at the foot of inclines as well as at the top.

(G A 30 Op —9 54. E 82672 H (12-C.))

BRAKING VACUUM-FITTED AND PARTLY VACUUM-FITTED FREIGHT TRAINS DOWN INCLINES.—Page 181.

Clause 5 amended to read:—

5. These Instructions do not apply to Ballast Trains partly composed of vacuum fitted stock which must work under the Incline Instructions

(G.A.30 Op.—9/54. E.8267 H.(12-C.)).

RAILWAYS COMPANIES' WAGONS STOPPED FOR REPAIRS. —Page 181.

Paragraph 1 to be amended to read:—

The Carriage and Wagon Examiner must inform the Traffic Department Inspector, Guard, or Shunter where loaded wagons are marked off, and arrangements must be made immediately by the Traffic Department to berth the trucks in a Cripple Yard. The Carriage and Wagon Examiner must inform the Traffic Department Inspector, Guard, or Shunter of freely loaded wagons coming by Class 3 traffic are marked off when arrangements will then be made by the Traffic Department to connect them with the Goods Department by telephone or other means to permit of their being forwarded to the forwarding and destination stations without delay. In cases where such wagons are marked off at night, the Traffic Department to transmit particulars immediately the Goods Office is open.

(G.A. 1337 C.G.M.—W 14191.)

Reference to the following to be made on page 181:

WAGONS WITH BROKEN DRAW-BARS.

When a loaded wagon is found to have a broken draw-bar, the Carriage and Wagon Examiner must inform the Traffic Department Inspector, Guard, or Shunter where loaded wagons are marked off, and arrangements must be made immediately by the Traffic Department to berth the trucks in a Cripple Yard. The Carriage and Wagon Examiner must inform the Traffic Department Inspector, Guard, or Shunter of freely loaded wagons coming by Class 3 traffic are marked off when arrangements will then be made by the Traffic Department to connect them with the Goods Department by telephone or other means to permit of their being forwarded to the forwarding and destination stations without delay. In cases where such wagons are marked off at night, the Traffic Department to transmit particulars immediately the Goods Office is open.

(G.A. 18.11/47. LK1/8788/Gen. 4.)

FREIGHT TRAIN INSTRUCTIONS.

BRAKING VACUUM-FITTED AND PARTLY VACUUM-FITTED FREIGHT TRAINS DOWN INCLINES—*Continued.*

When the number of loaded vacuum braked wagons connected to the engine of a "D" headlamp train is equal to the number required in the case of a partly vacuum fitted "C" headlamp freight train, Clause 2 of these instructions will apply.

When the proportion of vacuum-braked vehicles connected with the engine of a "D" headlamp train is less than that required in the case of a partly vacuum-fitted "C" headlamp freight train, Clause 3 of these instructions must be carried out.

~~These instructions do not apply to Ballast Trains, which must work under the Incline Instructions, whether formed completely or partly of vacuum-fitted stock.~~

WORKING INCLINES WITH A ROPE ON THE BALANCE SYSTEM.

In order to minimise the risk and danger of trucks running away owing to a broken link or draw-bar hook they must be doubly connected, whenever practicable, when ascending and descending inclines worked by means of a rope on the balance system.

RAILWAY COMPANIES' WAGONS STOPPED FOR REPAIRS.

When a wagon is stopped for repairs at a station where there is no Wagon Examiner appointed by the Chief Mechanical Engineer's Department, an advice of such stoppage must be immediately forwarded to the representative of that department at the nearest examining station. If the forwarding station is a sub-station, it must be advised by telegram and, should it bear the name of the line, the particulars of the stoppage must be advised immediately particulars have been received from the nearest station to the point of the stoppage, and to carry out the repairs.

RAILWAY COMPANIES' WAGONS STOPPED FOR REPAIRS—Page 181.

The existing instructions to be canceled and the following substituted:—

RAILWAY-OWNED AND EX-P.O. WAGONS STOPPED FOR REPAIRS.

The following instructions must be carried out in connection with all Railway-owned and ex-P.O. Wagons stopped for repairs:—

1. The Traffic and Wagon Examiner must inform the Traffic Department Inspector, Guard and the loaded wagon is marked off and arrangements must be made immediately by the Traffic Department to berth it in a Cripple Siding. Arrangements will then be made by the Traffic Department to communicate quickly with the Goods Department by telephone or other means to advise the Goods Office of the stoppage and the forwarding and destination stations as far as possible. In cases where such a wagon is marked off during the night, the Traffic Department to transmit particulars immediately the Goods Office is open.

2. The Traffic and Wagon Examiner must place the red "Not to Go" card over the traffic label on the side of the wagon but not to completely obscure the traffic label. If the traffic label is obscured the red card must be placed immediately by the side of it. The Traffic and Wagon Examiner must mark off for repair, particulars of the stoppage must be entered in the Traffic and Wagon Examiner's Book and must be forwarded to the Station Master or Goods Agent immediately to enable the required advices to be sent promptly.

3. The Traffic and Wagon Examiner must be issued to forwarding and receiving stations by the Traffic Department by the receipt of the particulars in the "949" Form 2826 must be used for this purpose and must in all cases contain the following particulars:—

- (a) Name of Station or Depot.
- (b) Name and number of wagon.
- (c) Forwarding and destination station.
- (d) Traffic.
- (e) Cons' notes.

to enable the receiving station to be notified of the stoppage of the wagon for repairs. The wagon stoppage or repairs must be issued to forwarding and receiving stations by the sending station immediately on receipt of the particulars in the "949" form. The Department Form 2826 must be used for this purpose and for the following particulars:—

- (a) Name of Station or Depot.
- (b) Name and number of wagon.
- (c) Forwarding and destination station.
- (d) Traffic.
- (e) Consignees.
- (f) Label date.

The forwarding station or Personable Station advises of the stoppage must also be forwarded to receiving stations by telegram or telephone, the appropriate "2826" form being attached.

At stations where there is no Wagon Examiner appointed, the forwarding station must advise of such stoppage must be immediately forwarded to the nearest station where a Wagon Examiner is stationed. The forwarding station must also advise the nearest examining station of the stoppage of the wagon, and the forwarding station must obtain the material necessary for the repair of the wagon.

The forwarding station must advise the receiving station after the necessary repair has been carried out, with the "2826" form, and the forwarding station must advise the receiving station that the wagon is fit to continue its journey.

When a wagon is found to be unfit to travel during transit to be unfit to travel and the necessary repairs to the vehicle can be carried out without causing greater delay than would otherwise occur, owing to transshipment of the load, the vehicle should be repaired while under load. The local Goods and Cattle and Wagon Departments must remain in close contact to avoid the transshipment of the goods being loaded by delay. In other circumstances the load must be transhipped, a record of the occurrence being kept at the transshipment station (at large stations in a separate book), and, in labeling the wagon with the load is transhipped, such transshipping station must state on the label the name of the original forwarding station, the date on which the disabled wagon left there and its name and number.

The transshipping station on the day of the occurrence must send a further advice on Form 2826 to the sending and receiving stations of the consequent transshipment.

Reference to the following to be made on page 182:—

REGULATIONS AS TO PRIVATE OWNERS' WAGONS.

1. The Owners of all new or rebuilt vehicles intended to work upon the Western Region must, before they are brought into use, communicate with the Carriage & Wagon Engineer, Swindon, so that he may inspect them without unreasonable delay, and if built, or rebuilt, in accordance with the Railway Clearing House Standard Specification and addenda thereto, register plates, as described in the Specification, shall be forthwith affixed to each.

2. The name and address of the Owner or Lessee, the wagon number, and the tare, shall be painted conspicuously on both sides of the wagon; the maximum load must also be clearly indicated on both sides of the wagon.

When wagons are let on hire the Lessee will, for the purpose of these Regulations, be regarded as the Owner.

Provided that if the hire is for a term of not less than three months, the name and address of the Lessee shall be painted or exhibited on a board or plate on both sides of the wagon, and that when the hire is for less than three months, the name and address of the Lessee shall either be so painted, or exhibited on a card (other than the wagon label), on both sides of the wagon.

3. The Owners or Lessees, as the case may be, shall keep their wagons in good working condition, and shall have them properly lubricated and examined and put into good repair before being tendered to the Western Region for transit.

4. The British Transport Commission may remove the register plates from any wagon if wheels, axles, or any other materials of less dimensions or strength than those provided for by the Railway Clearing House Standard Specification are afterwards substituted in contravention of the conditions of the said Specification.

5. If any defect shall be observed, which for the proper and safe working it is necessary to repair before the vehicles are allowed to proceed further, the British Transport Commission may, with the consent of the Owners, make such repairs, and charge them with all expenses incurred in effecting the same.

6. In accordance with the rules made by the Board of Trade under the provisions of the Railway Employment (Interchangeable Wagons) Act, 1900, when it is necessary in the ordinary course of business that any vehicle be sent to any destination or consignee shall be placed upon any railway wagon, such label or direction shall be placed on both sides of such wagon, and no Private Owner's Wagon will be accepted for carriage by the Western Region unless so labelled or directed on both sides.

All wagon Owners or representatives of wagon Companies and their repairers, when labelling defective wagons at stations and depots shall clearly set forth on the labels the station or siding from which the wagons are required to travel, and hand in a proper Consignment Note or written forwarding instructions.

The British Transport Commission agrees, without prejudice, to dispense with a Consignment Note or written forwarding instructions in the case of wagons painted with a yellow star in accordance with the provisions of the Commuted Empty Haulage Scheme, labelled to Repair Works.

Note—Not applicable to wagons working in Scotland

7. Wagons sent for the purpose of repair, are required to be shunted into and out of Sidings belonging to the British Transport Commission and on into and out of premises in the occupation of Private Owners, at a charge of 2s. 8d. per wagon will be made for such services, except where a higher charge is specified in the Consignment Note, in which case such higher charge shall be the maximum charge under these regulations.

Siding rent shall be chargeable to the owner, or his agent, in respect of standing room for any wagon detained at a siding for repairs at the rate of 1s. 5d. per wagon per day, which will be calculated from the date of arrival of three days, exclusive of the date of the advice note, Sundays and Bank Holidays, and the wagon is placed at the disposal of the Owner or Repairer, and to terminate when the wagon is labelled for despatch after repair.

The foregoing regulations do not apply when the owners are parties to the Commuted Shunting and Siding Rent Charge Scheme in which case the wagons are plated or stencilled "C.C."

8. Any authority or agent of the British Transport Commission may detain any wagon which may appear to him to be in need of repair, and has been put into proper repair and passed by an authorised person of the Western Region Carriage & Wagon Engineer's Department.

9. The British Transport Commission will not be responsible for any damage to Private Owners' Wagons left unprotected in an imperfect state by the Owners, nor for any injury that may occur to wagon repairers, who will be required to execute an indemnity before they are allowed to work on British Transport Commission's premises.

10. Private Owners' Wagons running over the British Transport Commission lines must not, apart from a reasonable description of the contents of the wagon, be used for advertising purposes, but the British Transport Commission will not object to a description (to be approved by them) of a product of the Owner's manufacture being painted thereon.

11. Nothing contained in these Regulations shall prejudice or affect any legal liability to each other of the actual Owners or Lessees of Wagons and the British Transport Commission.

(G.A.30 Op.—9/54. E.86585 H.10-F.).

any case of a wagon stopped on a siding belonging to the British Transport Commission for repairs to be carried

the Commuted Charge for repair or stencilled "C.C." charges, as per General Rules, and at the destination.

Instructions re R.O. Wagons on Page 182
Deleted. Jan 23

FREIGHT TRAIN INSTRUCTIONS.

REGULATIONS AS TO PRIVATE OWNERS' WAGONS *Continued.*

9. The Company will not be responsible for any damage to Private Owners' Wagons left unprotected in an imperfect state by the Owners, nor for any injury that may occur to wagon repairers, who will be required to execute an indemnity before they are allowed to work on the Company's premises.

10. Private Owners' Wagons running over the Railway Company's lines must not, apart from a reasonable description of the contents of the wagon, be used for advertising purposes, but the Railway Company will not object to a description (to be approved by them) of a product of the Owner's manufacture being painted thereon.

11. *Nothing contained in these Regulations shall prejudice or affect any legal liability to each other of the actual Owners or Lessees of Wagons and the Railway Companies.*

PRIVATE OWNERS' WAGONS STOPPED FOR REPAIRS.

PRIVATE OWNERS' WAGONS STOPPED FOR REPAIRS—Page 183.

The existing instructions under this heading is to be deleted and the following substituted:—

PRIVATELY OWNED WAGONS STOPPED FOR REPAIRS.

When privately owned wagons must be stopped for repairs at a station with privately owned wagons stopped for repairs:—

The Wagon Examiner and Wagon Examiner must inform the Traffic Department Inspector, Guard and the Traffic Department to berth the trucks in a Cripple Siding.

The Wagon Examiner must place the red "Not to Go" card over the traffic label. If the traffic label is not present, the red card must be placed immediately by the side of it.

At a station where there is no Wagon Examiner appointed, the necessary repairs must be immediately carried out by the Traffic Department staff at the respective examining station.

When necessary repairs have been carried out, the Traffic Department staff must ensure that the wagons are available for use.

When a wagon is stopped for repairs particulars must be entered in the "1949 Book" which is maintained by the Traffic Department.

When a wagon is stopped for repairs, the necessary advice to be sent to the Traffic Department must be taken by a private owner or repairer to the Traffic Department.

When a wagon is stopped for repairs, the necessary advice to be sent to the Traffic Department must be taken by a private owner or repairer to the Traffic Department.

When a wagon is stopped for repairs, the necessary advice to be sent to the Traffic Department must be taken by a private owner or repairer to the Traffic Department.

FREIGHT TRAIN INSTRUCTIONS.

REGULATIONS AS TO PRIVATE OWNERS' WAGONS—*Continued.*

9. The Company will not be responsible for any damage to Private Owners' Wagons left unpro

6. Immediately upon receipt of the particulars in the "949" Book the Goods Agent or Station Master must, in the absence of any special instructions to the contrary, despatch an advice upon Form 2827 as follows:—

- (a) To the freighter upon whose account the wagon is running.
- (b) To the owner of the wagon as indicated by the iron plate affixed to the vehicle, in cases where the freighter and owner are not one and the same.

Where special instructions are given upon the wagon as to the firm to whom the advice is to be addressed, such instructions must be carried out, or if the owners or freighters have issued any standing order in writing as to the repair of their wagons such orders must be complied with.

At the same time an advice upon Form 2826 must be despatched to:—

- (a) Receiving station.
- (b) Forwarding station where known.

7. The advice issued on Form 2827 must in all cases contain the following particulars:—

- (a) Name of station or depot.
- (b) Name of owner or lessee of wagon.
- (c) Wagon Number.
- (d) Name of destination station.
- (e) Nature of defect as far as possible.
- (f) Full particulars of any material required.

The advice issued to forwarding and receiving stations must in all cases contain the following particulars in accordance with Form 2826:—

- (a) Name of station or depot.
- (b) Name of owner or lessee of wagon.
- (c) Wagon number.
- (d) Forwarding and destination stations.
- (e) Nature of defect as far as possible.

8. When a wagon containing a through load is found during transit to be unfit to travel and Carriage and Wagon Department men with suitable material are available at or near the spot and the necessary repairs to the vehicle can be effected without causing greater delay than would otherwise occur owing to transhipment, the repairs should be carried out under load. The local Goods and Carriage Department men should be in contact to avoid the transit of the Goods being unduly delayed. In all other circumstances the load must be transhipped, record of the occurrence being kept at the transhipping station (at large stations in a separate book) and in affixing the wagon into which the load is transhipped such transhipping station must be entered in the name of the original forwarding station, the date on which the disabled wagon left and its name and number.

The transhipping station, on the day of the transhipment, must send a further advice on Form 2826 to the sending and receiving stations of the transhipment.

Wagons stopped with hot boxes or requiring other light repairs which are not of a nature to prevent the wagon being used for traffic must be marked with a yellow star, and the Goods Agent or Station Master must send a remark to the effect that the repairs are in hand.

10. Wagons ordered from one point to another for the purpose of repair must not be sent to the repair works without forwarding order specifying the route it is desired the wagon should follow, and the Goods Agent or Station Master must be responsible for the wagon being painted with a yellow star as referred to in Clause 11.

Wagons sent forward from a Western Region Station without a Western Region forwarding order must be sent to the forwarding station and the route shown on the Goods Agent's order must be followed. Wagons marked with a yellow star which indicate that they come within the scope of the Commuted Charge Scheme which may when labelled to Repair Works, be worked forward to the Repair Works without Railway Executive labels being affixed.

Wagons marked with a yellow star must not be sent from another Region at an exchange junction unless the labels indicate the name of the station from which it was forwarded.

13. Guards must not take forward from a station, junction or siding, Private Owners' wagons bearing labels which do not comply with Clauses 11 and 12 hereof.

At stations or sidings, payment of any charges due in accordance with the Railway General Classification in the General Classification must be obtained before the wagon is sent forward. In cases where the senders have an authorised ledger account or the wagon is sent forward to the Commuted Charge for empty haulage arrangement, in which case the vehicles bear a yellow five-pointed star, six inches in diameter.

15. A charge for shunting of 1s. 8d. per wagon must be made in every case of a wagon stopped for repairs, whether required to be shunted into a private siding or into a siding belonging to the Executive. The charge of 1s. 8d. to cover the shunting both into the siding for repairs to be carried out, and out of the siding after the repairs have been completed.

The foregoing charges do not apply when the owners are parties to the Commuted Charge Scheme for Shunting and Siding Rent arrangement, in which case the wagons are plated or stencilled "C.C."

Wagons conveyed from one station to another upon which haulage charges as per General Classification Scale are entered by invoice will not be subject to a charge for shunting at the receiving station.

FREIGHT TRAIN INSTRUCTIONS.

PRIVATE OWNERS' WAGONS STOPPED FOR REPAIRS—Continued.

9. A sharp look-out must be kept for wagons labelled empty to collieries for loading but inter-empted en route and labelled to other points for repairs, in order to ensure the instructions contained in Clauses 1, 5, 7, and 8 being strictly observed.

10. A charge for shunting of 1/- per wagon must be made in every case of a wagon stopped for repairs, whether it is shunted into a private siding or into a siding belonging to the Company. The charge of 1/- for the shunting both into the siding for repairs to be carried out, and out of the siding after the repairs have been completed.

The foregoing charges do not apply when the owners are parties to the Commuted Charge for Shunting and Siding Rent arrangement, in which case the wagons are plated or stencilled "C.C."

If after the expiration of five days a wagon has been taken by a private owner or repairer to undertake repairs to the wagon, the Examiner must re-enter particulars in the "949" book of the wagon stopped to mark the entry "Second advice" or "Third advice," as the case may be.

Wagon owners will be liable for the charges for shunting at the destination station, in addition to the charges for shunting at the destination station.

Charges of wagons other than those included in the commuted scheme for shunting and siding

11. In cases of wagons other than those included in the commuted scheme for shunting and siding rent clause (5), the charges for repairs, shunting, or siding of a wagon, shall be other than a siding charge to wagon owners, if the wagon is shunted at the rate of 10/- per wagon per day must be charged after the expiration of three days (exclusive of the date of advice note, Sundays and Bank Holidays).

Example: A wagon stopped and advice despatched on Monday. The three free days will be Tuesday, Wednesday and Thursday. Repairs will commence on Friday and will continue until the date on which repairs are completed, irrespective of where the wagon has been standing, less Sundays and Bank Holidays, and accounts must be made up on this basis.

12. In cases where the Executive are unable to place wagons in their usual or people sidings or in sidings where the needful protection can be afforded to men engaged on repairs, owners have been informed that the Executive is prepared to consider on its merits any cases where it can be proved that the detention was not due to the fault of the owner or repairer of the wagon, and therefore, to safeguard the Executive's interests, the following information be recorded:

1. The actual date the wagon is placed in position for repairs.
2. If the wagon cannot be placed on the date of stopping in the usual siding, or any other siding where there is no danger of being damaged, or protection on the advice to Owners and repairers should be made that a further advice will be issued when the wagon is in a position where the repairs can be safely carried out, and the number of the members of the Firms' or Traders' wagons already stopped, both standing in and out of position.
3. As the property of Private Owners' Wagons, the number of the wagon is, the number of the wagon, the number of the wagon, the number of the wagon, and the number of the wagon.
4. A record of the wagon to be placed in such a position that it can be repaired must be recorded both as to date and time of the application.

8. Siding Rent is also to be charged on Private Owners' wagons waiting access to a wagon repairs, private siding or a siding rented by a repairer from the Executive. Owners and repairers must be advised on arrival, and the next three days will be the free period.

(G.A.23—7/49. E.86585 H.)

LOADING OF PRIVATE OWNERS' WAGONS AFTER GENERAL REPAIRS.
LABELLING OF NEWLY LIFTED WAGONS—Page 184.

The existing instructions to be cancelled and the following substituted:

WAGONS LABELLED TO PASS ON SLOW TRAINS.

When wagons have received attention for hot axle boxes en route they should be labelled with the following Executive "Not to run on Fast Trains" label (No. 521) and the authority of this label they may only work to destinations or points as the case may be on trains timed to run at a slow speed and which do not run more than 25 miles between stops. Such wagons must not be returned to normal traffic until they have received a proper repair.

(G.A.23—7/49. E.86274 H. (2-C.))

RAILWAY COMPANIES' OR PRIVATE OWNERS' WAGONS LABELLED FOR REPAIRS.

Private Owners' wagons bearing green "Repairs" labels must not be worked on Vacuum or Partial Vacuum fitted freight trains, but Railway Companies' wagons may be worked on any freight train irrespective of the main power applied, except as follows:

Wagons bearing the Green Label endorsed "Not to run on Fast Trains" must be restricted to trains carrying "J" or "K" headlamps.

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WAGONS LABELLED TO PASS ON SLOW TRAINS.—Page 184.

The instructions under this heading to be cancelled.

(G.A. 30 Op.—9/54 E.86585H.)

RAILWAY COMPANIES' OR PRIVATE OWNERS' WAGONS LABELLED FOR REPAIRS.—Page 184.

The instructions under this heading to be cancelled and the following substituted —

WAGONS LABELLED FOR REPAIRS.

Wagons with oil axleboxes and bearing green "FOR REPAIR" labels, indicating defects in axleboxes, axleguards, wheels and axles may be conveyed by trains running under "D" or inferior headcodes.

Wagons with grease axleboxes and bearing green "FOR REPAIR" labels, indicating defects in axleboxes, axleguards, wheels and axles may be conveyed by trains running under "F" or inferior headcode conditions.

Wagons bearing green "FOR REPAIR" labels indicating a defective axlebox, axleguard, wheel or axle, the nature of which does not call for the wagon to be loaded upon another vehicle, may be conveyed by trains running under "H" or inferior Headcode conditions.

(G.A.30 Op.—9/54. E.86585H.)

Following instruction to be inserted on page 184:—

PRIVATE OWNERS' WAGON REPAIRERS MAKING USE OF "REPAIR WHEN EMPTY" CARDS.

When attaching a "Green Card" to a loaded wagon giving instructions for the vehicle to a Private Wagon Repairer after discharge, the label should be tacked to the wagon about six inches of transit label, except in the case of all steel wagons when they should be attached to the holder.

G.A. 30 Op.—9/54. RCHM 107

FREIGHT TRAIN INSTRUCTIONS.

LABELS FOR VEHICLES WITH DEFECTIVE BRAKES.

It is sometimes found that owing to wagon brakes being defective or requiring adjustment, the hand brake lever when pulled from the rest and released from the grasp falls to the bottom of the rack or guide.

Shunters and others discovering such a defect must replace the lever in the rest and secure it to the guide, so as to prevent any attempt being afterwards made to operate the brake.

Should the vehicle be empty at the time it must be detained and the attention of the Station Master, Inspector, or any other responsible person in charge called to it, so that the Carriage and Wagon Examiner at the nearest point may be advised.

In the event of a defect of this kind being discovered on a loaded wagon in transit, in addition to securing the lever, the guard, on arrival at the next examining station must report the matter to the Examiner who will if possible have the brake repaired. Necessary attention will be taken for repairs with a special train, provided for the purpose, or repaired on the following "Defective Brake," as shown below:—

Words "Defective Brake" printed in Red.

	Railway.
<h2 style="margin: 0;">FOR REPAIRS</h2>	
From _____	<div style="transform: rotate(-30deg); font-size: 2em; font-weight: bold; color: red;">DEFECTIVE BRAKE.</div>
To _____	
Date _____	
Defects _____	
<p>Any unauthorised person removing this Card from the vehicle to which it has been attached will render himself liable to criminal prosecution.</p>	

When a vehicle is found to be defective, the guard must immediately inform the driver, after reading their destination card, and the vehicle must not be moved until the brake has been put into good working order.

IRON FITTINGS OR APPLIANCES FALLING FROM ENGINES, CARRIAGES OR WAGONS.

Couplings, brakework and other fittings on freighters' wagons break off in the ordinary course of travelling, and are not returned for repairs in all cases with the wagons.

When the fittings become detached while the trucks are running, they cannot of course in all cases be returned to the place where they were detached.

When being shunted at Stations or at any place where the fittings can be put into the hands of the Guards and Shunters or other persons concerned to see that such portions of the trucks are properly secured, so that they may be sent to the proper Stations for repairs.

In the case of a coupling, or of ironwork of any kind falling from a loaded private wagon, the guard must take possession of the broken parts and hand them over to the first examining station, at the first examining station, as if put on top of the load they are liable to be lost while the trucks are running or being unloaded.

EMPTY PRIVATE WAGONS.

EMPTY PRIVATE WAGONS, bearing neither label nor painted instructions for disposal, must be held and application made to the owners for forwarding orders, care being taken to see that payment of the haulage charges due is obtained in accordance with the regulations in the General Railway Classification, unless the owners are parties to the commuted charge for empty haulage arrangement, in which case, the vehicles bear a yellow five pointed star 6 inches in diameter.

COUPLING WAGONS WITH DRAW-BAR HOOKS OF VARYING HEIGHTS.

Goods Guards, Shunters and others concerned should, as far as they can consistently with reasonable despatch, adopt the principle of placing the coupling of the lower hook over the upper hook when coupling wagons having hooks of varying heights.

FREIGHT TRAIN INSTRUCTIONS

COUPLINGS NOT PROPERLY DROPPED INTO DRAW-BAR HOOKS.

When a plug wig is Go to guards and Shunters must see that in all cases the coupling link drops properly into the hook, and not allow it to ride on the "Gedge" slot.

WAGON DRAWGEAR BREAKING.

It is a fact that a coupling may be caused by the train being started with the brake of a heavy brake van hard on, thus causing a severe snatch. Enginemen should exercise every care in starting their trains in order to avoid undue snatching of the couplings and drawgear, and when their trains are started to go they should be careful to use off the hand brake before the engine picks up the full load.

Drivers sometimes apply the steam brake to check trains when the hand brake would be sufficient for the purpose. Drivers should be careful not to use the steam brake unnecessarily, nor (except in emergencies) in such a manner as to cause a violent jerk on the train.

It is the duty of the Guard of any train in which heavily loaded vehicles, such as "Crocodiles," "Mudlarks," etc., are carried, to see that the drawgear is in good order and the position in the train.

In the event of a train being loaded without any portion of the drawgear breaking, the Guard, when required to do so, should be careful to see that the drawgear of both ends in connection with the train is in good order. In the event of a drawgear being found to be in need of repair, the Guard should be careful to see that the drawgear of the vehicles may, if necessary, be labelled for repairs, which can be effected on arrival at destination.

INSTRUCTIONS TO GUARDS OF TRAINS, LEFT "DEAD" EN ROUTE.

In the event of a train being left dead in a siding or place not attached to a station except the guard will be responsible for seeing that the train is left in the control of any cattle or other animals that are on board, that any animals may be kept for working it forward promptly.

WITH MAIN RULES OF COMBINED

TRANSIT OF STEAM AND HAND TRAVELLING CRANES.

Steam and hand travelling cranes should be carried in the best possible place, as far as possible, on slow freight trains, and then subject to the following conditions:

1. A travelling crane weighing 30 tons may be attached to the rear of any freight train, next to the brake van, and then only provided no other vehicle conveying an exceptionally heavy consignment is also attached.

2. A travelling crane weighing 30 tons may be attached to the rear of any freight train, next to the brake van, and then only provided no other vehicle conveying an exceptionally heavy consignment is also attached.

3. A travelling crane weighing 30 tons may be attached to the rear of any freight train, next to the brake van, and then only provided no other vehicle conveying an exceptionally heavy consignment is also attached.

4. A travelling crane weighing 30 tons may be attached to the rear of any freight train, next to the brake van, and then only provided no other vehicle conveying an exceptionally heavy consignment is also attached.

5. A travelling crane weighing 30 tons may be attached to the rear of any freight train, next to the brake van, and then only provided no other vehicle conveying an exceptionally heavy consignment is also attached.

6. A travelling crane weighing 30 tons may be attached to the rear of any freight train, next to the brake van, and then only provided no other vehicle conveying an exceptionally heavy consignment is also attached.

7. A travelling crane weighing 30 tons may be attached to the rear of any freight train, next to the brake van, and then only provided no other vehicle conveying an exceptionally heavy consignment is also attached.

8. A travelling crane weighing 30 tons may be attached to the rear of any freight train, next to the brake van, and then only provided no other vehicle conveying an exceptionally heavy consignment is also attached.

9. A travelling crane weighing 30 tons may be attached to the rear of any freight train, next to the brake van, and then only provided no other vehicle conveying an exceptionally heavy consignment is also attached.

10. A travelling crane weighing 30 tons may be attached to the rear of any freight train, next to the brake van, and then only provided no other vehicle conveying an exceptionally heavy consignment is also attached.

TRANSIT OF STEAM AND HAND TRAVELLING CRANES—Page 186.

The following to be added as clause (c) —

A special train conveying a 45 ton steam crane complete with match truck and Stokes bogies, which are piped, when travelling within the Western Operating area to or from the site of Engineering Department Operations, may carry "B" headcode provided the Crew's van is also piped and the other vehicles comprising the train are fully vacuum fitted. Such trains must not exceed a speed of 45 m.p.h. at any point.

G.A 30 Op—9 54 LK1 12204 417. EX.3509/53.).

SECTION III.

GENERAL INSTRUCTIONS AFFECTING THE LOADING AND CONVEYANCE OF MERCHANDISE TRAFFIC, ALSO LIVE STOCK BY PASSENGER AND FREIGHT TRAINS.

(a) Loading, conveyance	PAGES 184 to 186
(b) Loading, conveyance of live stock by passenger and freight trains	... 248 to 258

SECTION III. (a).

INSTRUCTIONS CONCERNING LOADING, CONVEYANCE, ETC., OF
MERCHANDISE TRAFFIC.

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INDIVIDUALS --

* Serpents conveying containers need not be labelled with "Examine Load" label, provided special attention is given to the securing of the containers to the rail vehicles.

(G A 8-5 41 E 81567 H.)

LOADING, ETC., OF MERCHANDISE TRAFFIC.

SELECTION OF SUITABLE WAGONS.

Suitable wagons should be selected for the convenience of freight traffic according to the nature of the consignment to be carried. It is the responsibility of the person in charge to see that the risk of loads shifting during transit will be minimised.

CARE IN LOADING.

Goods Foremen, Loaders, Shunters and Guards must take the utmost care to see that the contents of all wagons are evenly distributed, safely and properly loaded and secured where necessary. Doors must be correctly secured and no wagon loaded beyond its registered capacity.

Wagons tendered by another company must comply with the foregoing conditions before going forward.

Improper loading or overloading must be reported at once by the person in charge to the District Goods Manager.

EXAMINATION OF LOADS.

Guards must strictly comply with the instructions in Part IV of the Rule Book which lay down the absolute need for careful examination of loads before starting the journey. Loads received at junctions with other companies must be examined by the Goods Foremen, Inspectors, Foremen, Shunters and Guards. It is the responsibility of the person in charge to see that the way is replaced on the junction, and to make sure that the loading is done in a safe and secure manner proper condition.

Guards should not refuse to take wagons loaded for other companies unless the loading is not in accordance with the Regulations. The person in charge of the wagon must be satisfied that the loading is correct and that the goods are properly secured. If the person in charge is not satisfied, he should report the matter to the District Goods Manager, who will then be responsible for the proper inspection.

ARRANGEMENTS FOR LOADING AND DESPATCH OF SPECIAL CONSIGNMENTS.

Wagons of the types enumerated below must before despatch be labelled with "Examine Load" labels as per illustration appended.

"Bulls."	"Crocodiles."	"Rectanks."
"Cattle."	"Scorpents."	"Scorpions."
"Cows."	"Moles."	"Hydras."
"Totems."	"Beavers."	

Particular wagons of the "Maxi" and "Cape" type which are built up with iron plates, or consist of exceptional character, must be labelled with the "Special Load" label. The person in charge of the wagon must be satisfied that the loading is correct and that the goods are properly secured. If the person in charge is not satisfied, he should report the matter to the District Goods Manager, who will then be responsible for the proper inspection.

The person in charge of the Loading Staff have authority to refuse to take special loads.

66043. G.P.

Great Western Railway Company.

EXAMINE LOAD

WEIGHT T	C	Q	Lbs
NOT TO GO FORWARD FROM LOADING POINT UNTIL INSPECTED AND FOUND TO BE PROPERLY LOADED AND SECURED.		DATE _____	19 ____
FROM _____		TO _____	
Via _____		This load must be examined during transit and at all exchange points.	
EXAMINED BY— DATE _____		SHEETS IN or ON Wagon Total No. _____	
		Owner & No. of Wagon _____	
		Consignee _____	

3

SHUNT WITH CARE

200 000 Est. 617 4/31. S.

"Examine Load," "Shunt with Care" and Figure "3" appear on the label in red print.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

ARRANGEMENTS FOR LOADING AND DESPATCH OF SPECIAL
CONSIGNMENTS—Continued.

The Exhibit I and label must be signed in the space provided for signature, by a member of the committee in the case of the Engineering Department, or by the chief engineer, in the case of the Locomotive, Stores, or Road Transport Departments. This applies to the case of goods department and property, and for experimental work, headed on behalf of the

The signifying of the following is that the Engineering Department will be undertaken by the District Goods Manager and Motor Vehicle Inspectors. In the cases where one of these is unable for the purpose, the other will be required to communicate with the District Goods Manager, who will then refer the matter to the Chief Engineer and certify the special duties to the District Goods Manager. In the latter event the Engineering Department will be responsible for the special duties at the point of examination stipulated by the District Goods Manager.

84. The respective responsibilities of the various departments for the safe loading and despatch of

Appendixes 2 and 3 of the Control System Procedures are properly labeled in accordance with these requirements. An illustration of the labeling of the components is shown in the label. They are not labeled in accordance with the requirements of the label.

[illegible]

ARTICLES EXCEEDING THE PUBLISHED GAUGE DIMENSIONS

The District Managers will advise the Chief of Consuls Manager of any "out of sight" consignments of goods. The Chief of Consuls Manager will furnish the Superintendent of the Line with a list of the consignments of goods, and the Superintendent of the Line will, in turn, furnish the District Manager with a list of the consignments of goods. After the foregoing has been done, the District Manager will advise the Superintendent of the Line and the District Manager of the consignments of goods, and the Superintendent of the Line will, in turn, advise the District Manager of the consignments of goods.

[illegible]

For the purpose of the above information, the Division Engineer must advise the Division Engineer.

[illegible]

et al. Later he was sent to the Trial D. where he was put in a cell in the following form:

stock on the adjoining line, between the following signal boxes:—

The Divisional Superintendent District Traffic Managers must make such arrangements as will convey coaching stock on the adjoining line, especially in regard to the passing of Freight trains

may travel.

[illegible]

The loads of S.A.L. specials may be made up with through traffic only to the maximum single engine

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The District Goods Manager must also advise the other Companies concerned of the dispatch and the Divisional Superintendents or District Traffic Managers must advise the Divisional Engineer concerned only when this action is stipulated in the conditions of passage by the use of the Code word 'DELOD' "

(G.A. 8.—5/41. E.56423.X.)

"The special examination of the Loads by Chief Mechanical Engineer's Inspectors at the exchange station may be dispensed with in the case of out of gauge consignments arising on other Companies' Lines."

ARTICLES EXCEEDING THE PUBLISHED GAUGE DIMENSIONS page 190.

The following to be added after the eighth paragraph on page 191:

"Should a Traffic Inspector not be readily available, and in order to obviate delay to traffic, the Divisional Superintendent or District Traffic Manager may arrange, for a competent man to undertake the duty.

"In the case of Loads not exceeding 10 ft. in width but which through exceptional length or other peculiarity, stringent conditions of passage, etc., in the opinion of the Divisional Superintendent or District Traffic Manager, require the services of a Traffic Inspector, such to be provided by the Divisional Superintendent or District Traffic Manager. Should a Traffic Inspector not be readily available, arrangements to be made for a competent man to undertake the duty."

(G.A. 7.—3/40. O.M.11949.)

LOADING, ETC., OF MERCHANDISE TRAFFIC

ARTICLES EXCEEDING THE PUBLISHED GAUGE DIMENSIONS—Continued.

All wagons used for the conveyance of exceptionally heavy machinery or any other article exceptionally heavy or lengthy must be provided with front and rear brake van. If it is found that exceptional loads are conveyed by train carrying other traffic, a special train must be provided by a Swindon District, the Chief Mechanical Engineer will stipulate with the District that the load must be marshaled at the front or rear of the train, and if in front a second brake van should be provided next to the engine. (Such loads must be carefully watched when crossing special tracks on a Saturday morning out-of-gauge traffic must be booked for examination in accordance with what is shewn below. If there is no local Examiner available the Swindon District or a company representative out-of-gauge traffic will be responsible for the examination of the wagons.

In the event of ordinary traffic being conveyed out of gauge special trains on a Saturday, such will be examined in the ordinary way at points where an Examiner is on duty, but in cases where no Examiner is on duty special arrangements must be made for examination by the District and the Loco. Superintendents.

Orders issued in Pillen's wagons loaded must be conveyed by special train with no other traffic attached and one of the Chief Mechanical Engineers' Inspectors from Swindon must accompany the train to destination or point of exchange with other Company.

Loads which exceed the width of the truck on the 6-foot side must not be conveyed through any section where the width of the truck is less than 6 feet 6 inches. The load must be placed on each side of the truck over which the rails will be crossed with care so as not to damage the rails, until proper enquiries and arrangements for working have been made.

A Chief Mechanical Engineer's Inspector must always examine and, where considered necessary, travel with out-of-gauge consignments up to 10 feet 6 inches. A Chief Mechanical Engineer's Inspector for a Swindon District always examines loads exceeding 10 feet 6 inches and loads with first consignments.

This must be done with consignments arriving on the W. Line and those received from other Companies' Lines.

In all cases care must be exercised to ensure that the load is not placed on a single line or single line working purposes unless this can be safely done.

Load Inspectors must always be present at loading and unloading points to ensure that the load is placed on the safe working gauge and that the load is not placed on a single line or single line working purposes unless this can be safely done. Special instructions issued are carried out.

Loads are to be placed on the safe working gauge and that the load is not placed on a single line or single line working purposes unless this can be safely done. It is important that loads of all descriptions be as evenly distributed as the circumstances allow.

In no circumstances must any load be placed on a single line or single line working purposes unless this can be safely done. It is important that loads of all descriptions be as evenly distributed as the circumstances allow. The Chief Mechanical Engineer's Department.

If a load is to be conveyed on a single line or single line working purposes, the load must be placed on the safe working gauge and that the load is not placed on a single line or single line working purposes unless this can be safely done. The District Superintendent will see that the load is placed on the safe working gauge and that the load is not placed on a single line or single line working purposes unless this can be safely done.

This instruction is to be read in connection with the instructions issued by the District Superintendent. The District Superintendent will see that the load is placed on the safe working gauge and that the load is not placed on a single line or single line working purposes unless this can be safely done.

It is essential that the greatest care be exercised at loading points in connection with exceptional and out of gauge loads to ensure that the dimensions do not exceed those for which passage authority has been given. (G.A. 10. 3.42. E 82 (6, II)).

LOADING OF ROUND TIMBER.

LOADS EXCEEDING 8 FEET for G.W. Stations and all other Companies' Stations or the equivalent are to be considered as exceptional dimensions, and special authority for their conveyance must be obtained from the Chief Goods Manager before being sent forward. Consignments exceeding 8 feet must not be conveyed by Freight trains carrying "D E F" or "H" head codes.

Note. The equivalent length of a load is arrived at by multiplying the longest overhang measured from the headstock of the wagon by two and adding the distance over the headstock of the vehicle.

X Denotes points at which load is to be lifted or to be lowered. (G.A. 12. 4.13. C.G.M. W.T./X. 68878.)

⊕ Denotes points at which chains or ropes are fastened loosely round load only.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF ROUND TIMBER—*Continued.*

INSTRUCTIONS FOR LOADING.

1. LOADS EXCEEDING 20 FEET AND UP TO 30 FEET IN LENGTH. The timber must be loaded on 20-ton Rail and Timber Trucks (Macaw H) when available, with end bolsters placed in inner brackets as shown on Diagram 1, and care taken to see that the butts overlap the end bolsters by at least one foot six inches. When Macaw H are not available Twin Timber Trucks (Mite) may be used as an alternative—see Diagram 2. The butts should, as a rule, be reversed, but where there is a difficulty in doing this, they may be loaded at one end. In either case the load must be securely chained at the bolsters and all stanchions used.

Diagram 1.

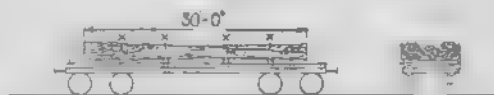
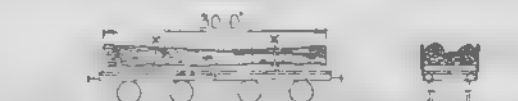


Diagram 2.



2. LOADS EXCEEDING 30 FEET AND UP TO 40 FEET IN LENGTH. The timber must be loaded on Macaws B, D or E with end bolsters placed in inner brackets as shown in Diagram 3, and care taken to see that the butts overlap the end bolsters by at least one foot six inches with butts reversed on a Macaw H or loaded on a truck connected to a twin timber truck and entirely free of 20-ton Diagram 4. When Macaw B, D or E are not available Twin Timber Trucks (Mites) with match truck at one end to act as under runner only, may be used as an alternative, but the match truck must be entirely free of the load; where load exceeds 30 feet a match truck without bolster should be used—see Diagram 5. In the case of Macaw H and Twin Timber Trucks (Mites), the butts must be loaded at one end of the load and must be secured and loaded securely chained at the bolsters of Macaw H or Mites only with all stanchions in position. When Twin Timber Trucks (Mites) are used, the smaller end of load should be secured with a separate chain or rope when necessary to prevent spreading, but this chain or rope must not be secured in any way to the match truck.

Diagram 3.

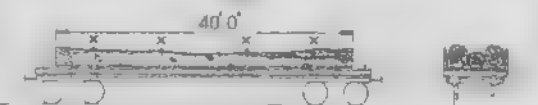


Diagram 4.

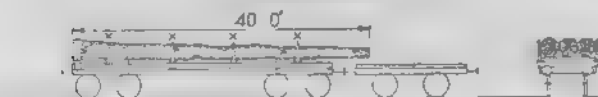
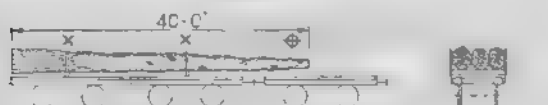


Diagram 5.



LOADS EXCEEDING 40 FEET must not be conveyed on Twin Timber Trucks (Mites).

3. LOADS EXCEEDING 40 FEET AND UP TO 45 FEET IN LENGTH. The timber must be loaded with butts reversed on Macaws B, D or E to Diagram 6, or on 20-ton Rail and Timber Truck (Macaw H) with match truck at one end to act as under runner only, as shown on Diagram 7. When the method shown on Diagram 7 is adopted, the butts must be loaded at one end of Macaw H and load securely chained at the bolsters of this wagon, with all stanchions in position. Where load exceeds 43 feet in length, a match truck without bolster should be used. The smaller end of load should be bound round with a separate chain or rope, when necessary, to prevent spreading, but this chain or rope must not be secured in any way to the match truck.

LOADING OF ROUND TIMBER.

INSTRUCTION 5. LOADS EXCEEDING 54 FEET AND UP TO 60 FEET IN LENGTH—page 193.

The following to be added at the end of the paragraph:

Macaw J may also be used but match truck will not be required.

(G.A. 7.—3/40. C.G.M.—W.T./X.14231.)

INSTRUCTION 6. LOADS EXCEEDING 60 FEET AND UP TO 70 FEET IN LENGTH—page 193.

The following to be added at the end of the instruction:

Macaw J may also be used, but match truck will be required at one end only for lengths exceeding 62 ft 6 inches. The small end of load only to overhang, as in Diagram 9, and the end bolster at that end should only be placed in the outer brackets, when the load exceeds 64 ft. in length.

(G.A. 7.—3/40. C.G.M.—W.T./X.14231.)

INSTRUCTION 7. LOADS EXCEEDING 70 FEET AND UP TO 80 FEET IN LENGTH—page 193.

The following to be added at the end of the instruction:

Macaw J may also be used and the foregoing conditions are to be observed.

(G.A. 7.—3/40. C.G.M.—W.T./X.14231.)

LOADING, ETC., OF MERCHANDISE TRAFFIC

LOADING OF ROUND TIMBER—Continued.

Diagram 6.

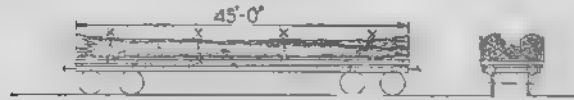
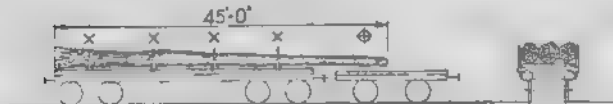
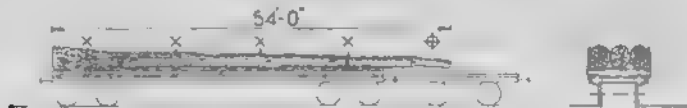


Diagram 7.



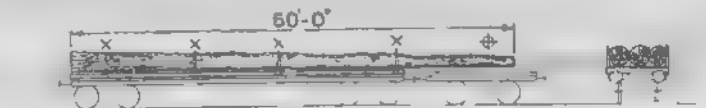
4. LOADS EXCEEDING 45 FEET AND UP TO 54 FEET IN LENGTH. The timber must be loaded on Macaws B, D or E, with a match truck without bolster, as shown in Diagram 8. Care must be taken, when a match truck with bolster is used, to ensure that load cars bolster when bolsters are compressed. The small end of load should be bound round with a separate chain or rope, where necessary, to prevent spreading, but this chain or rope must not be secured in any way to the match truck.

Diagram 8.



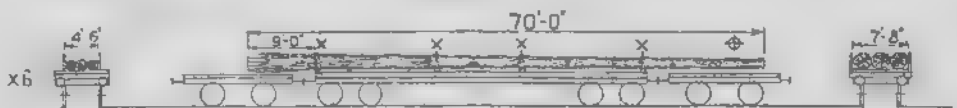
5. LOADS EXCEEDING 54 FEET AND UP TO 60 FEET IN LENGTH. The timber must be loaded on Macaws B, D or E, with a bolster at one end placed in outer brackets, as shown in Diagram 9. A match truck without bolster, used at one end, to act as under runner or γ . The small end of load must be bound round with separate chain or rope, to prevent spreading, but this chain or rope must not be secured in any way to the match truck.

Diagram 9.



6. LOADS EXCEEDING 60 FEET AND UP TO 70 FEET IN LENGTH. The timber must be loaded on Macaws B, D or E, with a length of more than 4 feet at butt end of load, with end bolsters placed in outer brackets, as shown in Diagram 10. A match truck with bolster used at both ends, as shown in Diagram 10. The width of load must be confined to 4 feet 6 inches at small end, and when overhang over bolsters is 15 feet or over, see Diagram 10b. A separate chain or rope should be bound round small end of load, to prevent spreading, but this chain or rope must not be secured in any way to match truck.

Diagram 10

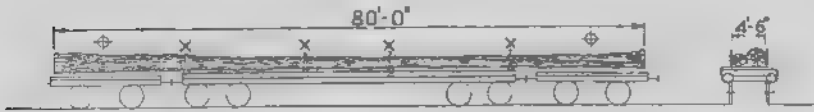


7. LOADS EXCEEDING 70 FEET AND UP TO 80 FEET IN LENGTH. The timber must be loaded with butts reversed on Macaws B, D or E, with a match truck (without bolster) at each end, and the width of load confined to 4 feet 6 inches, see Diagram 11. A separate chain or rope should be bound round both ends of load, to prevent spreading, but these chains or ropes must not be secured in any way to the match trucks.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF ROUND TIMBER—Continued.

Diagram 11.



In loading sets of big timber it is important that the bottom tier should rest upon the bolsters and that, when the sets are wider than the bolsters, the tiers should be loaded in a staggered fashion, i.e. approximately, four in the lower tier, then in the second tier, and so on. In no case must any trees be allowed to rest on the top of the stanchions, and care must be taken that butts of the timber are so placed on the bolsters as to ensure a clearance over sides and ends of wagons when passing over curves.

In all cases where the ends of timber extend to the full width of the bolsters, the D Shakes, where fitted, must be turned round the stanchions, and in every case careful inspection of the trees should be made before loading is commenced, so that the best trees may be placed in the middle of the sets, to allow the maximum amount of play in rounding curves.

Before loading is commenced, the D Shakes attached to the trucks must be fitted, and the chains must be fitted to the stanchions to a large extent dependent upon them, and in addition the ends of the timber must be secured by the use of bolsters, the most useful as possible, be carefully examined, the fitting parts by the Station Masters or Goods Agents as the case may be.

When the timber is loaded, the D Shakes must be greased underneath in the centre of the trucks and the points upon which they have to run, to give them free play. This duty rests with the Chief Mechanical Engineer's Department, but at stations where there are no Greasers or where the Greasers are not in the Department, the greasing of the D Shakes must be carried out by the Goods Agent, the Department Staff who perform the loading. The Inspector, or Foreman, or other person superintending the loading will be held responsible for seeing that the bolsters are properly greased or oiled before the wagons are loaded.

In loading timber on branch Lines it is desirable that the butts should as far as possible be placed so as to lead on to the Main Line.

Trucks loaded with Timber must be formed as near the end of the train as circumstances will admit.

The ends must be examined by the Guards when taken on, and also by Guards and Shunters at each stopping place, where the chains should be adjusted if necessary.

LOADING OF BAULK TIMBER.

~~BAULK TIMBER EXCEEDING 80 FEET IN LENGTH FOR G.W. STATIONS AND 60 FEET FOR STATIONS ON OTHER COMPANIES' LINES MUST NOT BE ACCEPTED FOR CONVEYANCE WITHOUT THE AUTHORITY OF THE CHIEF GOODS MANAGER.~~

X Denotes points at which load is chained to bolster or to wagon.

~~Consignments exceeding 70 feet in length must not be conveyed on freight trains carrying "C," "D," "E" or "F" Handamps.~~

Loads of Baulk Timber must overlap the Bolsters upon which they are carried by at least 1 foot 6 inches.

Baulk timber exceeding 60 feet in length must not be accepted for conveyance over the undermentioned sections of the Great Western Railway without the authority of the Chief Goods Manager.

- Barry and Bridgend and Cotty Junction.
- Bassaleg to Caerphilly and Duffryn Isaf.
- Blaenan Festiniog Branch, between Maentwrog Road and Blaenau Festiniog.
- Bristol Port and Pier Lane.
- Burry Port and Gwendraeth.
- Coleford Branch.
- Cornwall Minerals Lines
 - Lostwithiel to Fowey
 - Fowey to St. Blazey
 - Par to Newquay
- Goonbarrow Branch.
- Bugle to Carbis W. Port
- Burngallow to St. Dennis Junction
- Treamble Branch.
- Rotew Branch
- Culm Valley Line (Tiverton Junction to Hemjock).
- Bardisley Branch.

LOADING OF TELEGRAPH POLES—page 134

The following additional instructions to be added —

Telegraph Poles exceeding 40 ft in length must be loaded tip to butt on Bobol "C" or Borais "B" or "D" with the end bolsters placed as near the headstocks as possible, and the load securely chained at the bolsters.

Poles up to 60 ft in length may be loaded with the overhang at one end, but in such cases the load must not exceed three quarters of the written capacity of the wagon.

Poles exceeding 60 ft in length must be loaded centrally on the wagon, and when the overhang at each end exceeds 1 ft the width of the load must not exceed 1 ft 1 in.

Borais "F" or "C" may also be used if available, but the overhang should not exceed 1 ft and if at one end of the wagon only, the load must not exceed three quarters of the written capacity of the wagon.

Loads exceeding 80 ft for C.W. stations and 60 ft for other Companies' stations or the equivalent with the roof are to be considered as of exceptional dimensions, and special authority for their conveyance must be obtained from the Chief Clerk. Maximum length of overhangs exceeding 4 ft. must not be used on freight trains carrying **D, E, F or H** loads **codes GH26**

Note. The equivalent length of a load is arrived at by multiplying the longest overhang measured from the headstock of the wagon by two and adding the distance over headstock of the vehicle.

Other Companies' Bobols "A", "B", "C" and "D" which are now in Common User may also be loaded subject to the arrangement of bolsters of these vehicles, allowing the foregoing instructions to be complied with in principle.

(G.A 16 3 46. C.G.M. W.T. X66806)

LOADING OF TELEGRAPH POLES.

Methods of carrying Telegraph Poles tip to butt loading.

Telegraph poles should be loaded as self contained load on bolster wagons as shewn in Diagram 12.

If suitable wagons for this purpose are not available, telegraph poles may be loaded on single bolster wagons as shewn in Diagram 13.

Care should be taken to ensure that the poles are securely chained together to prevent any disintegration of the load during transit.

Diagram 12.

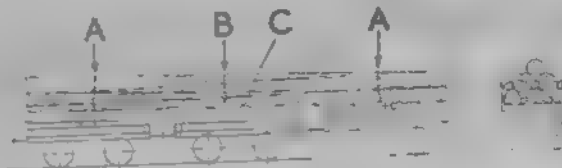


Self contained load.

May be loaded to full width of Bolster

Where shackles are not provided on bolsters, the load to be chained to the securing rings on the wagon frame as close as possible to the bolsters.

Diagram 13.



Three Wagon Set.

May be loaded to full width of Bolster.

Load to be removed from Centre Wagon and load chained free of wagon.

Load to not exceed capacity of two wagons

A—Load chained to Bolsters, stanchions in position, Shackles outside Stanchions when, owing to load extending to full width of Bolsters, they (the Shackles) cannot conveniently be placed inside the Stanchions.

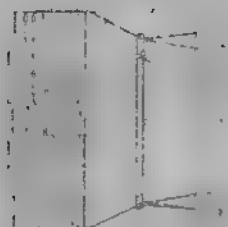
B—Load chained free of wagon.

C—Bolster to be retained.

(G.A. 13 9 43 C.G.M.—W.T. X 66806.)

Load to be arrived at by multiplying the longest overhang measured at the end of the load by the number of wagons in the set.

(G.A. 12 4 43 C.G.M.—W.T. X 68878.)



ROPE MUST BE USED TO SECURE THE LOAD AS INDICATED

NOTES -

LENGTH OF LOAD MUST IN NO CASE EXCEED LENGTH OF WAGON BODY BY MORE THAN 8'0"

TABLES FOR LOADS OVERHANGING ONE END

MAX. WT. OF LOAD AT ONE END	MAX. WT. OF LOAD AT OTHER END	MAX. WT. OF LOAD AT CENTER	MAX. WT. OF LOAD AT EACH END
1000	1000	1000	1000
1500	1500	1500	1500
2000	2000	2000	2000
2500	2500	2500	2500
3000	3000	3000	3000
3500	3500	3500	3500
4000	4000	4000	4000
4500	4500	4500	4500
5000	5000	5000	5000
5500	5500	5500	5500
6000	6000	6000	6000
6500	6500	6500	6500
7000	7000	7000	7000
7500	7500	7500	7500
8000	8000	8000	8000
8500	8500	8500	8500
9000	9000	9000	9000
9500	9500	9500	9500
10000	10000	10000	10000

DIAGRAM "X"

MAX. LOADS OVERHANGING ONE END
 1. MAX. LOAD AT ONE END 4000 LBS.
 2. MAX. LOAD AT OTHER END 4000 LBS.
 3. MAX. LOAD AT CENTER 4000 LBS.
 4. MAX. LOAD AT EACH END 4000 LBS.
 5. MAX. LOAD AT EACH END 4000 LBS.
 6. MAX. LOAD AT EACH END 4000 LBS.
 7. MAX. LOAD AT EACH END 4000 LBS.

LOADING EQUIPMENT EACH END



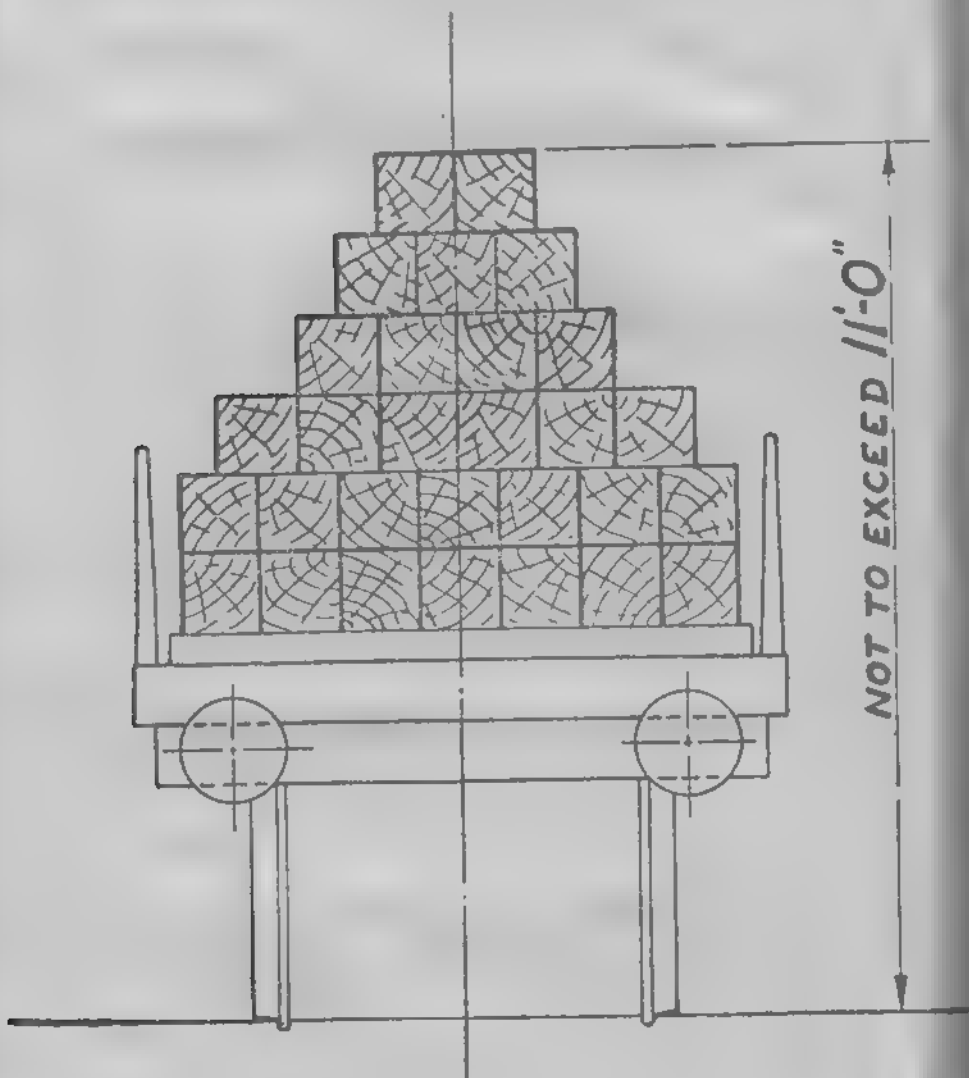
TABLES FOR LOGS OF \sin , \cos , \tan , \cot , \sec , \csc

MAY 11 CAPT 8 OIL TANKS OF
 75000 LBS EACH, 1000 LBS EACH ETC
 ON RAILING BOTH ENDS OF MA 100
 8, 10, 15 TON WAGONS.
 (C.C.H. W/1X 72180)

The following to precede the last paragraph on page 193 headed "Timber 20 ft. to 30 ft. (inclusive) in length":

TIMBER FOR BOLSTER WAGONS.

When baulk timber is loaded on Bolster wagons in the manner indicated on diagrams 1 to 9 on pages 196 and 197 of the General Appendix, and the load does not exceed the length of the carrying wagon, the load may extend to the full width of the bolsters between the stanchions providing the base of the top full width tier is below the top of the stanchions. When above the stanchions the timber must be recessed, pyramid fashion, to a height not exceeding 11 ft. from rail, as shewn in the following diagram:



The loads to be built up as far as possible, with the largest baulks at the base.

Loads exceeding the length of the wagon must not be built up higher than the stanchion and when loaded as shewn on Diagram 9 must not exceed 4 ft. 6 ins. wide.

(G.A. 18, 11/47, C.G.M.WT/X1/14353.

G. W. R.

Divl. Supt's Office,
Bristol. G.W.7.

2nd August, 1947.

Ref:- A1/52944.

Dear Sir,

Securing of imported timber through
South Wales Ports.

In connection with the heavy importation of timber through South Wales Ports loaded in accordance with diagram 'X', Supplement G.A. 15 of the General Appendix to the Rule Book and having regard to the acute shortage of ropes now being experienced, it has been agreed by the Chief Goods' Manager, in conjunction with the Chief Mechanical Engineer, to dispense with the rope round the middle of the wagon (illustrated in diagram 'X') as a special case and until such time as the rope position improves, this relaxation to apply to traffic destined for G.W. destinations only, subject to such loads not passing through the Severn Tunnel.

It must be stressed that the roping illustrated in the General Appendix mentioned in the foregoing, must be strictly adhered to when the rope position again becomes normal.

Please note and advise all concerned.

Yours truly,

For R. G. Pol. f.b.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF BAULK TIMBER—*Continued.*

Eastern Valleys Line
 Newport Dock Street to Trevelth Junction.
 Trevelth Junction to Blaenavon and local lines.
 Trevelth Junction to Abersychan and Talywain and local lines.
 Ely Valley Line -
 Llantrisant to Penygraig.
 Mwyndy Junction to Brofiskin.
 Forest of Dean Branch, from Bullo Junction to Drybrook.
 Garw Branch (Brynmenyn to Blaengarw).
 Gwendraeth Valley.
 Halesowen Line.
 Kerry Branch.
 Lambourn Valley Line.
 Liskeard and Looe Branch.
 Llanelly and Mynydd Mawr.
 Llynvi Valley Branch (Tondy to Abergwynfi).
 Nantmawr Branch.
 New Radnor Branch.
 Onmore Valley Branch (Tondy to Nantymoel).
 Plymouth (Millbay) Dock Lines.
 Portland Branch.
 Princetown Branch.
 Quaker's Yard and Merthyr (ex R.R. Section).
 Rhydyar Junction and Morlan Junction.
 Severn and Wye Joint Line
 Tanat Valley.
 Tondy to Porthcawl.
 Totnes Quay Branch.
 Van Branch.
 Watlington Branch.
 West Cornwall Mineral Lines - Portreath Branch
 North Crofty Branch. North Roskear Branch
 Tresavean Branch. Hayle Wharves.
 Western Valleys Line—
 Newport (Dock Street) to Aberbeeg. Aberbeeg to Nantyglo.
 Aberbeeg to Ebbw Vale.
 Risca to Nine Mile Point Junction.
 Hall's Tramroad, Upper and Lower Sections.
 Cwmearn Branch.
 Llanhilleth Junction to Cwmlln Junction
 Abertillery Junction to Cwmllery.
 Weymouth Quay Line.

INSTRUCTIONS FOR LOADING.

TIMBER OF LENGTHS NOT EXCEEDING THE INSIDE LENGTH OF OPEN WAGONS. This timber must be loaded flat in ordinary open goods wagons. When the load extends above the wagon it must be secured with the sides and ends of the vehicle to prevent them sliding. The loads must be firmly secured by ropes in all such cases.

TIMBER NOT EXCEEDING 22 FEET IN LENGTH.

With overhang at one end on v. Loads not exceeding 22 ft in length with weights may be carried in ordinary 8, 10 or 12 ton open goods wagons, not exceeding 3 ft. without end doors. The loads must overhang the trailing ends of the wagons and be firmly secured by ropes as shown on Diagram "X", except in cases where the overhang does not exceed 1 ft., when the load must be secured as shown on Diagram 4, page 207.

With overhangs at both ends. Timber in lengths not exceeding 22 ft with overhangs at both ends may be carried in ordinary 8, 10 or 12 ton open goods wagons, not exceeding 3 ft. without end doors. Care must be taken to secure the load over each end of the wagon and to secure load as shown on Diagram "Y". A suitable match truck must be used at each end of each loaded wagon.

(G.A. 7.—3/40. C.G.M.—W.T/X.14231.)

clusive) in

on pages
 wagon, the
 the top full
 recessed

on pages
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 the top full
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on pages
 wagon, the
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X1/14353.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF BULKY TIMBER—Continued.

bolsters placed in inner brackets as shown on Diagram (1). When Macaws H are not available, Twin Rail Trucks (Mite) may be used as an alternative, see Diagram (2). The loads must be firmly secured by means of the chains attached to the bolsters of the vehicles.

Diagram 1.

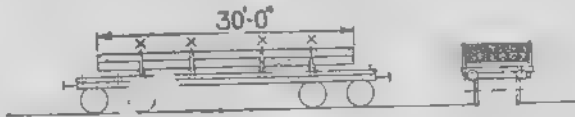


Diagram 2.



Timber in mixed loads, not exceeding 30 feet in length, may be carried in 25-foot deal wagons (Open C).

TIMBER MORE THAN 30 FEET AND NOT EXCEEDING 35 FEET IN LENGTH. The timber must be loaded on 20-ton bogie rail trucks or trucks Macaws H when they are available, see Diagram 3. When Macaws H are not available, Bogie Rail Wagons (Macaws B, D or F) to be used, with the end bolsters placed in the inner brackets as indicated in Diagram 4, and in taking it see that load overlaps end bolsters by at least 1 foot in length. In the case of Macaws B, D or F, not being available, standard Twin Timber Trucks may be used, and the timber, according to length, loaded as indicated in Diagram 5. Loads to be firmly secured by the chains provided on the vehicles.

Diagram 3.

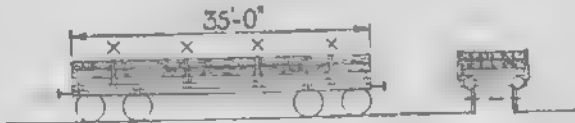


Diagram 4.

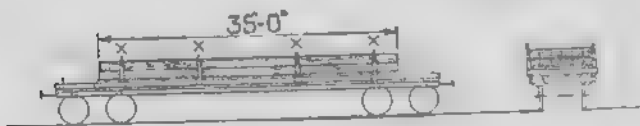
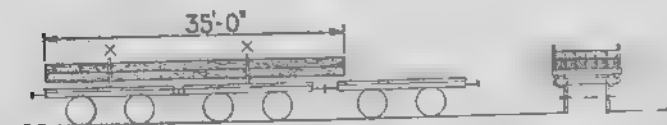
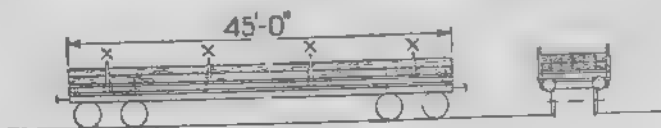


Diagram 5.



TIMBER MORE THAN 35 FEET AND NOT EXCEEDING 45 FEET IN LENGTH. The timber must be loaded on Bogie Rail and Timber Trucks Macaws B, D or L—see Diagram 6. For loads up to 40 feet in length, the end bolsters must be placed in inner brackets as indicated on Diagram 4. Loads to be firmly secured by the chains provided on the vehicles.

Diagram 6.



TWO ENDS TO BE USED TO
 USE THE TECHNIQUE
 STOPS TO IN JUMPED CIRCULAR



LENGTH OF LOAD MUST IN NO CASE EXCEED L₁
 OF WAGON BODY BY MORE THAN 8' 0"
 (SEE NOTE 3 FOR END CROSS WAGONS)

② LOADED FLAT ON OTHER SIDE

END DOOR WAGONS MAY BE USED PROVIDED THE END
 DOORS ARE PROPERLY SECURED THE OVERHANGING
 PLATE MUST REST ON THE DOOR HINGE BAR & NOT
 EXCEED 2' 1" NOR THE MAXIMUM LOAD 2 TONS

④ OVERHANG TO BE AT TRAILING END WHERE POSSIBLE

RIFES MUST BE USED TO SECURE THE OVERHANGING
 END OF THE LOAD AS INDICATED & THE ADDITIONAL
 ROPE SHOWN AT THE CENTRE OF THE WAGON IS TO BE
 JOEL WHEN A LUMBER IS ABOVE RAKE OF WAGON AT THE

NOT TO OVERHANG WAGON BULKERS THAN 50"

PAUL

ANGLES, BARS ETC, NOT LOADED TO FULL WIDTH
 OF WAGON, WHICH MAY BE CONVEYED IN ORDINARY
 8, 10 & 13 TON WAGONS. (G.A. 15 C.G.M.-WTA. 22780)

TABLES FOR LOADS OVERHANG

MAXIMUM OVERHANG (SOLID LOAD)	8 TON WAGON		10 TON WAGON		13 TON WAGON		OVERHANGS WITH 8 TON WAGON 10 TON WAGONS NOT FEEDING SOLID STEEL IN OF LOAD TO BE SOLD FOR
	1	2	3	4	5	6	
1 FT	5	1	1	1	1	1	1
2 FT	1	1	1	1	1	1	1
3 FT	1	1	1	1	1	1	1
4 FT	3	1	1	1	1	1	1
5 FT	2	2	2	2	2	2	1
6 FT	2	1	1	1	1	1	1

DIAGRAM Z

TIMBER MORE THAN 50 FEET AND NOT EXCEEDING 70 FEET IN LENGTH
—page 197.

Amplified to read:

The timber must be loaded centrally on Bogie Rul and Timber Trucks (Macaws B, D or E) with end bolsters placed in outer brackets, and a match truck used at each end is under runners only. When loads exceed 60 feet in length match trucks without bolsters must be used (see Diagrams). Macaw J may also be used. Lengths not exceeding 50 feet to be loaded centrally with the bolster over each bogie placed in the bolster guides provided 1 foot from bogie centres towards centre of wagon. Lengths exceeding 50 feet but not exceeding 60 feet to be loaded centrally with bolsters over each bogie placed in the bolster guides provided at the bogie centres. Lengths exceeding 60 feet but not exceeding 70 feet to be loaded as in Diagram 7 with bolsters arranged centrally at each end. Lengths 60 feet to 70 feet to be loaded as in Diagram 7 with end bolster placed in outer bolsters at overhanging end of lower wagon match truck at that end. Loads must be firmly secured by the chains provided on the bogie vehicles.

(G.A. 7.—3/40. C.G.M.—W.T/X.14231)

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF BULK TIMBER—Continued.

The method of loading bulk timber more than 30 feet and not exceeding 45 feet in length refers only to traffic arising at stations on the Great Western Railway.

Other Companies do not adopt this method, the practice being to use three single timber trucks loaded separately with the middle bolster removed and in order to avoid difficulty at junctions with other Railways it has been agreed that this traffic may be accepted at such junctions when so loaded, provided that the width of the load does not exceed 3 feet 6 inches; the trucks are short coupled, the loads are secured on the end bolsters only and are satisfactory in all other respects.

TIMBER MORE THAN 45 FEET AND NOT EXCEEDING 50 FEET IN LENGTH. The timber must be loaded on Bogie Platform Timber Trucks Classes B, D or F with a matching truck at one end to act as under-runner only—see Diagram 7.

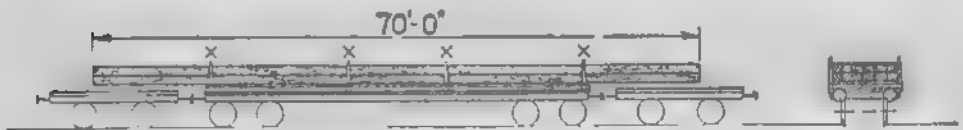
Loads must be firmly secured by the chains provided on the bogie vehicles.

Diagram 7.



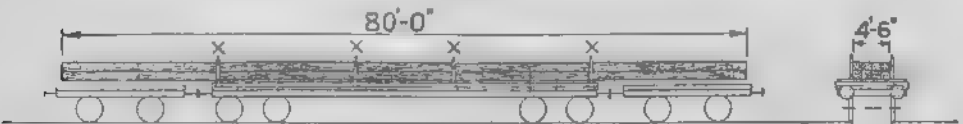
TIMBER MORE THAN 50 FEET AND NOT EXCEEDING 70 FEET IN LENGTH. The timber must be loaded on Bogie Platform Timber Trucks Classes B, D or F with a matching truck at one end to act as under-runner only—see Diagram 8.

Diagram 8.



TIMBER MORE THAN 70 FEET AND NOT EXCEEDING 80 FEET IN LENGTH.—The timber must be loaded on Bogie Platform Timber Trucks Classes B, D or F with end bolsters placed in outer stanchion sockets of trucks without bolsters used at each end as shown on Diagram 9. The load must be secured by chains attached to the bolsters by placing the shackles in the inner stanchion sockets provided in the bolsters of the vehicles.

Diagram 9.



Before loading is commenced the chains and "D" shackles attached to the trucks must be thoroughly examined as the safety of the loads to a large extent is dependent upon them, and, in addition to the inspection of the loads by the Timber Loaders, they must, as far as possible, be carefully examined at the starting points by the Station Masters or Goods Agents, as the case may be. When Two Timber Trucks Mates are used, the bolsters must be greased underneath in the centre before loading, and also the quadrants upon which they move, in order to give them free play. This duty rests with the Chief Mechanical Engineers Department, but at stations where there are no Greasers or Examiners belonging to that Department, the greasing (or oiling) of the bolsters must be carried out by men of the Goods or Traffic Departments who perform the loading, and the Inspector, Foreman or other person superintending the loading will be held responsible for seeing that the bolsters are properly greased or oiled before the wagons are loaded.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF PITWOOD.

The following instructions shew how imported pitwood must be loaded at the Ports. Instructions relating to the loading of pitwood at stations other than Ports appear on page 22 after diagram No. 12. Guards must satisfy themselves that the wagons of pitwood are properly loaded and safe to travel before they are attached to trains.

(G.A. 18. 11/47. C.G.M.WT/X.1/12770.)

DEEP-SIDED WAGONS SHOULD BE USED FOR THIS TRAFFIC AS FAR AS PRACTICABLE. ONE, TWO OR THREE PLANK WAGONS, OR WAGONS WITH SIDES LESS THAN TWO FEET FOUR INCHES HIGH, MUST NOT BE USED.

PITWOOD INTENDED TO TRAVEL VIA THE SEVERN TUNNEL MUST BE SECURELY ROPED IN ALL CASES.

PITWOOD WITH BARK ON.

Diagrams Nos. 1, 2 and 3, shewing the mode in which Pitwood is to be loaded in Five or Seven-Plank Wagons.

When five or seven plank wagons with tip doors are used for pitwood in 9 ft. lengths it must be loaded crosswise or on end, as shown in the diagrams. In each side, as per diagram No. 1, where a five plank wagon is used, it is possible, with the ends not exceeding 6 ft. 6 ins. in length, to place crosswise or on end eight lengths of 6 ft. 6 ins. may be loaded as shown in diagram No. 2, with two standing upright, each length of pitwood in each side of wagon, or may be loaded on end throughout the wagon.

Diagram No. 1.



When five or seven-plank wagons with tip-end doors are loaded with 9 ft. lengths, as shewn in diagram No. 1, the fixed end of wagon may be filled with 6 ft. 6 in. lengths, crosswise or on end, but at the tip door end the 6 ft. 6 in. lengths must be loaded in the upright position and the Loaders must see that the tip door is properly fastened before the wagon is loaded. Lengths of 6 ft. 6 ins. are to be loaded as shewn in diagram No. 2. Lengths of 4 ft. 6 ins. may also be loaded as shewn in diagram No. 2, provided that the uprights or stanchions are comprised of 6 ft. 6 in. lengths.

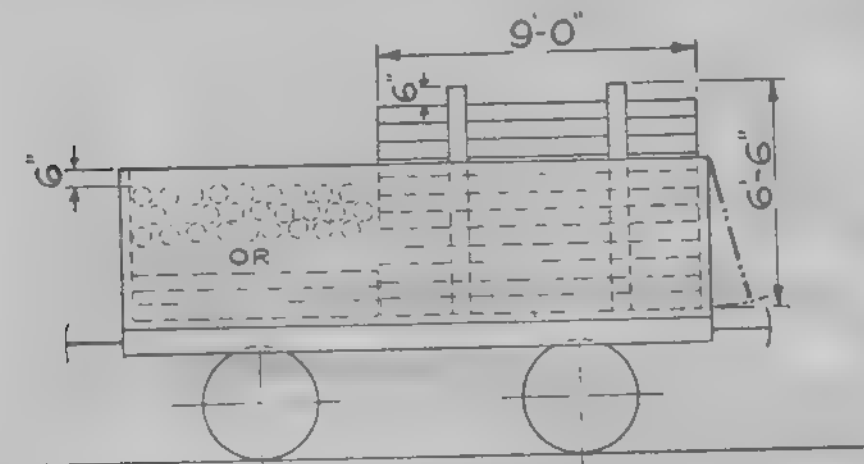
LOADING OF PITWOOD AT PORTS—Page 198.

The following to be inserted as a fourth paragraph on page 198 :—

PITWOOD FROM SOUTH WALES PORTS TO LOCAL COLLIERIES.

Pitwood may be loaded in accordance with the following diagram, the traffic being restricted to service from South Wales Ports to local Collieries.

Diagram "A"



This method of loading must conform to the following instructions :—

1. Not less than 7-plank ex P.O. wagons to be employed.
2. The longitudinal timbers to be loaded at the "end door" of the vehicle and the space at the fixed end filled with timbers loaded athwart or gunshot.
3. Timber to be kept 6 in below the tops of the stanchions and below the sides of the wagon at the fixed end

(G.A. 9-10-48 C.S.—W.T. XI, 44660.)

A large, dark, rectangular metal box, possibly a safe or a heavy-duty container, with a handle on the left side and a latch on the right side. The box is mounted on a metal frame with four legs. The image is very dark and grainy.

The highest point of the loads, as per diagrams Nos. 1 and 2, must be at least 10 ins. below the top of the side stanchions.

Diagram No. 3.



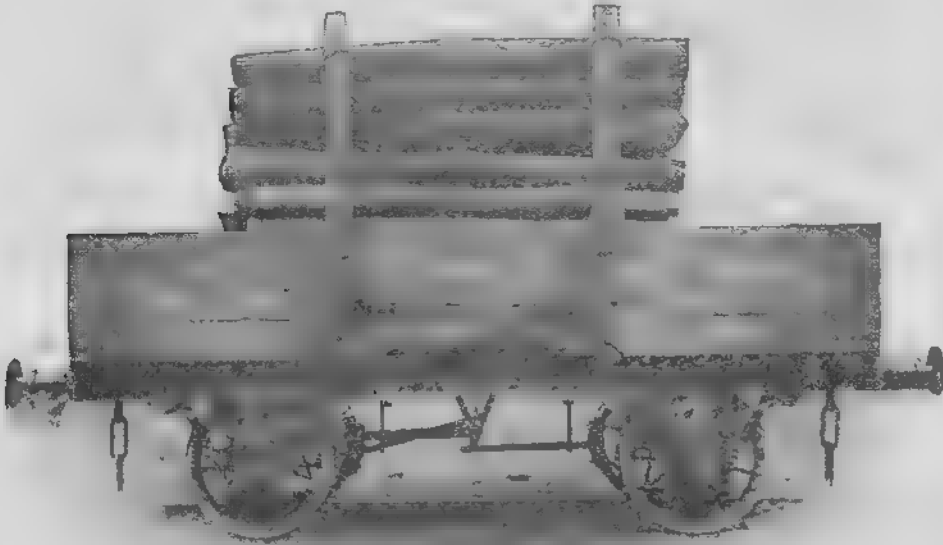
Pitwood loaded direct into trucks from ship by means of slings, may be loaded as per diagram No. 3, care being taken that sufficient pieces are placed crosswise in the centre of the wagon, from the bottom to well tighten the load.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF PITWOOD—*Continued.*

Diagrams Nos. 4 and 5 showing the mode when Four-plank Wagons are used.

Diagram No. 4.



When four-plank wagons with fixed ends are used for pitwood in 9 ft. lengths the method of loading as per Diagram No. 4 is to be used. Lengths of 6 ft. 6 ins. are to be loaded as shown in diagram No. 5, but in both cases the loads must be roped with at least one turn at each end of the loads, as per Diagrams Nos. 4 and 5, must be at least 6 ft. below the top of the wagon.

When Pitwood in lengths of 9 ft. and 6 ft. 6 ins. as shown in Diagram No. 4 is loaded together in a Truck which has a tip end, the shorter loads must be loaded at the fixed end of Wagon only and not across the Truck at the tip end and must be kept below the sides of the Truck.

Diagram No. 5.



Pitwood loaded crosswise must be kept well below the sides of the Wagon in all cases.

For a
W. A.
W. A.

100

1999, 2000, 2001, 2002, 2003, 2004, 2005, 2006, 2007, 2008, 2009, 2010, 2011, 2012, 2013, 2014, 2015, 2016, 2017, 2018, 2019, 2020, 2021, 2022, 2023, 2024, 2025, 2026, 2027, 2028, 2029, 2030, 2031, 2032, 2033, 2034, 2035, 2036, 2037, 2038, 2039, 2040, 2041, 2042, 2043, 2044, 2045, 2046, 2047, 2048, 2049, 2050, 2051, 2052, 2053, 2054, 2055, 2056, 2057, 2058, 2059, 2060, 2061, 2062, 2063, 2064, 2065, 2066, 2067, 2068, 2069, 2070, 2071, 2072, 2073, 2074, 2075, 2076, 2077, 2078, 2079, 2080, 2081, 2082, 2083, 2084, 2085, 2086, 2087, 2088, 2089, 2090, 2091, 2092, 2093, 2094, 2095, 2096, 2097, 2098, 2099, 2100, 2101, 2102, 2103, 2104, 2105, 2106, 2107, 2108, 2109, 2110, 2111, 2112, 2113, 2114, 2115, 2116, 2117, 2118, 2119, 2120, 2121, 2122, 2123, 2124, 2125, 2126, 2127, 2128, 2129, 2130, 2131, 2132, 2133, 2134, 2135, 2136, 2137, 2138, 2139, 2140, 2141, 2142, 2143, 2144, 2145, 2146, 2147, 2148, 2149, 2150, 2151, 2152, 2153, 2154, 2155, 2156, 2157, 2158, 2159, 2160, 2161, 2162, 2163, 2164, 2165, 2166, 2167, 2168, 2169, 2170, 2171, 2172, 2173, 2174, 2175, 2176, 2177, 2178, 2179, 2180, 2181, 2182, 2183, 2184, 2185, 2186, 2187, 2188, 2189, 2190, 2191, 2192, 2193, 2194, 2195, 2196, 2197, 2198, 2199, 2200, 2201, 2202, 2203, 2204, 2205, 2206, 2207, 2208, 2209, 2210, 2211, 2212, 2213, 2214, 2215, 2216, 2217, 2218, 2219, 2220, 2221, 2222, 2223, 2224, 2225, 2226, 2227, 2228, 2229, 2230, 2231, 2232, 2233, 2234, 2235, 2236, 2237, 2238, 2239, 2240, 2241, 2242, 2243, 2244, 2245, 2246, 2247, 2248, 2249, 2250, 2251, 2252, 2253, 2254, 2255, 2256, 2257, 2258, 2259, 2260, 2261, 2262, 2263, 2264, 2265, 2266, 2267, 2268, 2269, 2270, 2271, 2272, 2273, 2274, 2275, 2276, 2277, 2278, 2279, 2280, 2281, 2282, 2283, 2284, 2285, 2286, 2287, 2288, 2289, 2290, 2291, 2292, 2293, 2294, 2295, 2296, 2297, 2298, 2299, 2300, 2301, 2302, 2303, 2304, 2305, 2306, 2307, 2308, 2309, 2310, 2311, 2312, 2313, 2314, 2315, 2316, 2317, 2318, 2319, 2320, 2321, 2322, 2323, 2324, 2325, 2326, 2327, 2328, 2329, 2330, 2331, 2332, 2333, 2334, 2335, 2336, 2337, 2338, 2339, 2340, 2341, 2342, 2343, 2344, 2345, 2346, 2347, 2348, 2349, 2350, 2351, 2352, 2353, 2354, 2355, 2356, 2357, 2358, 2359, 2360, 2361, 2362, 2363, 2364, 2365, 2366, 2367, 2368, 2369, 2370, 2371, 2372, 2373, 2374, 2375, 2376, 2377, 2378, 2379, 2380, 2381, 2382, 2383, 2384, 2385, 2386, 2387, 2388, 2389, 2390, 2391, 2392, 2393, 2394, 2395, 2396, 2397, 2398, 2399, 2400, 2401, 2402, 2403, 2404, 2405, 2406, 2407, 2408, 2409, 2410, 2411, 2412, 2413, 2414, 2415, 2416, 2417, 2418, 2419, 2420, 2421, 2422, 2423, 2424, 2425, 2426, 2427, 2428, 2429, 2430, 2431, 2432, 2433, 2434, 2435, 2436, 2437, 2438, 2439, 2440, 2441, 2442, 2443, 2444, 2445, 2446, 2447, 2448, 2449, 2450, 2451, 2452, 2453, 2454, 2455, 2456, 2457, 2458, 2459, 2460, 2461, 2462, 2463, 2464, 2465, 2466, 2467, 2468, 2469, 2470, 2471, 2472, 2473, 2474, 2475, 2476, 2477, 2478, 2479, 2480, 2481, 2482, 2483, 2484, 2485, 2486, 2487, 2488, 2489, 2490, 2491, 2492, 2493, 2494, 2495, 2496, 2497, 2498, 2499, 2500, 2501, 2502, 2503, 2504, 2505, 2506, 2507, 2508, 2509, 2510, 2511, 2512, 2513, 2514, 2515, 2516, 2517, 2518, 2519, 2520, 2521, 2522, 2523, 2524, 2525, 2526, 2527, 2528, 2529, 2530, 2531, 2532, 2533, 2534, 2535, 2536, 2537, 2538, 2539, 2540, 2541, 2542, 2543, 2544, 2545, 2546, 2547, 2548, 2549, 2550, 2551, 2552, 2553, 2554, 2555, 2556, 2557, 2558, 2559, 2560, 2561, 2562, 2563, 2564, 2565, 2566, 2567, 2568, 2569, 2570, 2571, 2572, 2573, 2574, 2575, 2576, 2577, 2578, 2579, 2580, 2581, 2582, 2583, 2584, 2585, 2586, 2587, 2588, 2589, 2590, 2591, 2592, 2593, 2594, 2595, 2596, 2597, 2598, 2599, 2600, 2601, 2602, 2603, 2604, 2605, 2606, 2607, 2608, 2609, 2610, 2611, 2612, 2613, 2614, 2615, 2616, 2617, 2618, 2619, 2620, 2621, 2622, 2623, 2624, 2625, 2626, 2627, 2628, 2629, 2630, 2631, 2632, 2633, 2634, 2635, 2636, 2637, 2638, 2639, 2640, 2641, 2642, 2643, 2644, 2645, 2646, 2647, 2648, 2649, 2650, 2651, 2652, 2653, 2654, 2655, 2656, 2657, 2658, 2659, 2660, 2661, 2662, 2663, 2664, 2665, 2666, 2667, 2668, 2669, 2670, 2671, 2672, 2673, 2674, 2675, 2676, 2677, 2678, 2679, 2680, 26

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF PITWOOD—*Continued.*

PITWOOD WITH THE BARK STRIPPED OFF, i.e. PIT PROPS.

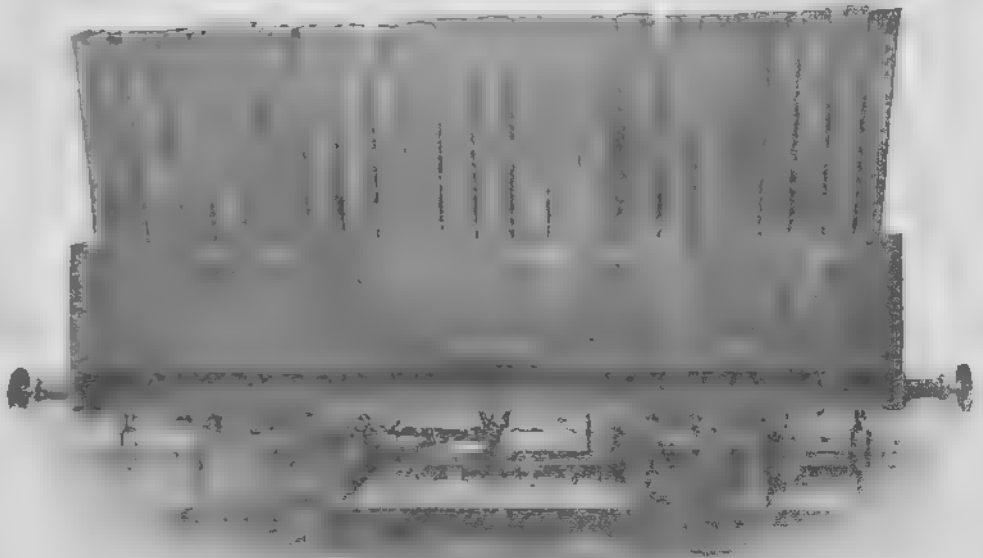
Pitwood with the bark stripped off must not be loaded as shown in Diagrams Nos. 1, 2, 4 or 5, nor above the sides of Wagons unless it is loaded as shown in Diagrams Nos. 3, 6, 7 or 8. NO RAIL WAGONS WITH SIDES LESS THAN FOUR PLANKS HIGH AND SEVEN-PLANK WAGONS SHOULD BE USED WHEN AVAILABLE.

When loading, as per Diagram No. 6, pitprops exceeding 6 ft. 6 ins. in length, must be placed on end like stanchions, close together at the ends of the Wagon, the load being made up by pieces placed either lengthwise or across the truck.

With a mixed load of pieces, 9 ft. and 6 ft. 6 ins. in length, the uprights must be of 6 ft. 6 ins. props, the 9 ft. pieces being placed in the spaces between the 6 ft. 6 ins. props. The 9 ft. pieces should be filled with lengths of 6 ft. 6 ins. pieces, the 6 ft. 6 ins. pieces being placed on top of the 9 ft. pieces.

When loaded in accordance with Diagrams Nos. 7 and 8, the traffic must be securely roped, AND PITPROPS MUST BE PLACED AT THE

Diagram No. 6.



LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF PITWOOD *Continued.*

Diagram No. 7.

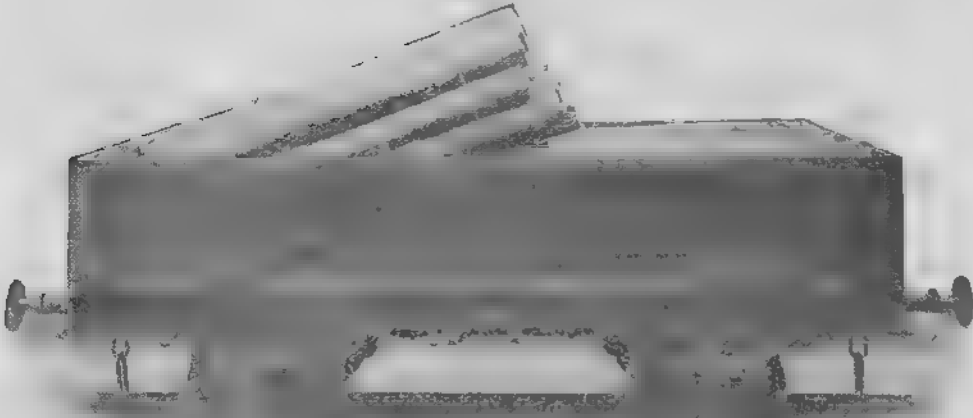
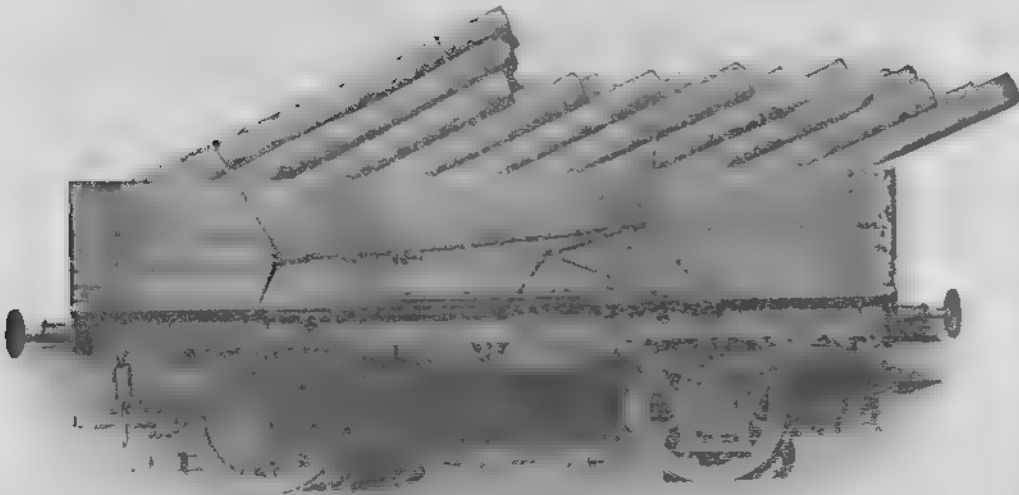


Diagram No. 8.



LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF PITWOOD—*Continued.*

PITWOOD IN 20-TON MINERAL WAGONS.

When pitwood is loaded in 20-ton mineral wagons, 6' 6" lengths must be loaded as shewn in diagram No. 10 or No. 11, 9' 0" lengths in accordance with diagram No. 9, and 6' 6" and 9' 0" lengths in accordance with diagram No. 12.

When loaded as shewn in diagrams No. 11 and 12 sufficient pieces must be loaded on the floor of the wagons at the ends to catch the load, care being taken to see that the end doors are properly secured. 6' 6" lengths to be used as side stanchions.

Diagram No. 9.



Diagram No. 10.



LOADING, ETC., OF MERCHANDISE TRAFFIC.

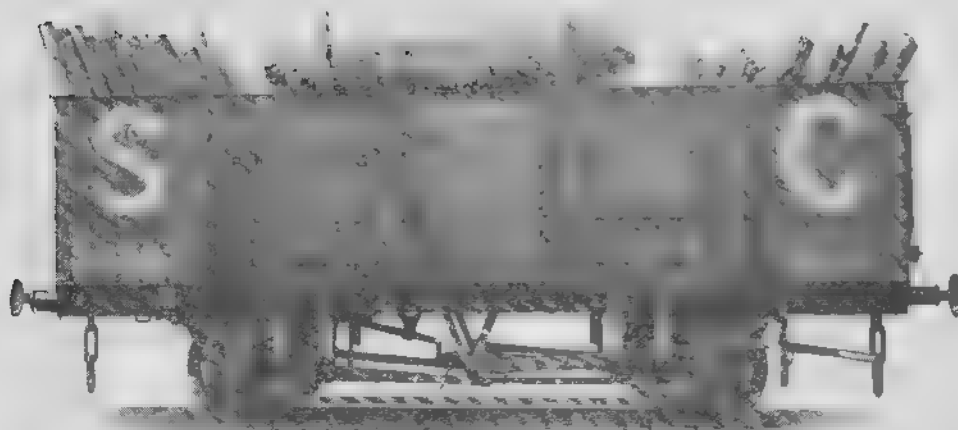
LOADING OF PITWOOD—*Continued.*

PITWOOD IN 20-TON MIXFRAL WAGONS—*Continued.*

Diagram No. 11.



Diagram No. 12.



LOADS MUST NOT EXCEED THE CARRYING CAPACITY OF THE WAGONS.

Reference to the following to be made on page 204:

LOADING OF PITWOOD AT STATIONS OTHER THAN PORTS.

General Instructions.

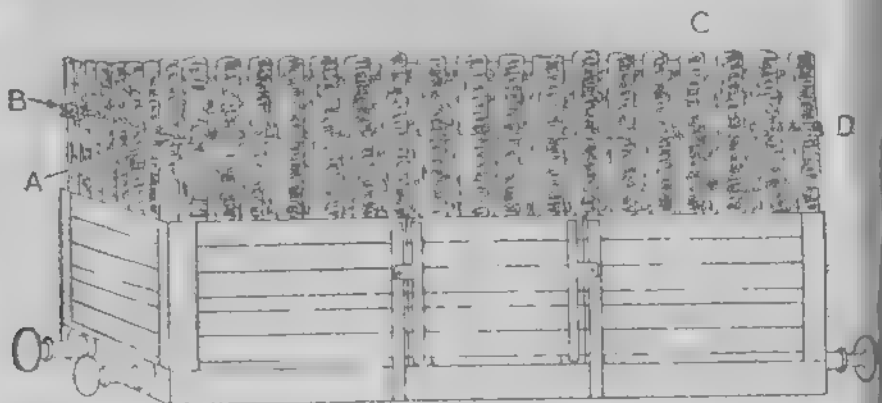
Loads must be tightly and securely roped to prevent any movement in transit. This is especially necessary in the case of loads shown in diagrams 13, 14, 15 and 16 below, where it is essential to prevent the load spreading.

Pitwood, with bark stripped off, must not be loaded above the sides of wagons, except as shown in diagram 13, 15 or 16 below.

- (a) Wagons with whole drop sides are not to be used for this traffic.
- (b) Wagons of less than 4-plank type are not to be used for this traffic.
- (c) Wagons with end doors must not be loaded in the manner shown on diagrams 14, 15 and 16.
- (d) End-door wagons may be loaded in the manner shown on diagrams 13 and 17.
- (e) Where horse hooks are not fitted on wagons in the positions shown on the diagrams, ropes may be secured to any convenient place on the wagon underframe.

The traffic differs in shape, length and girth and the following diagrams are supplied as guides to good and safe loading:

DIAGRAM No. 13. 4 ft. 0 in. to 6 ft. 6 in. lengths.

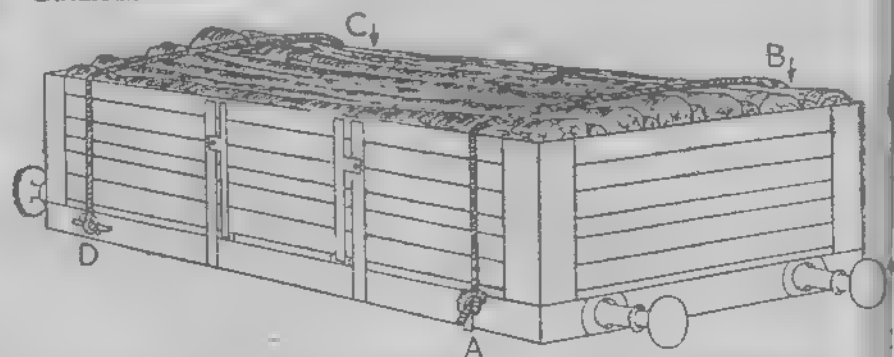


No spaces should be left between the upright pieces and the space inside those should be tightly packed. The inner layers must be at least six inches below the surrounding upright pieces.

Diagram No. 13 has been drawn to allow of the inner layers being seen but in practice no spaces should be allowed between the upright pieces.

Roping. Turn rope round two uprights at "A", "B", "C", and "D", both ends of rope being tied at "A".

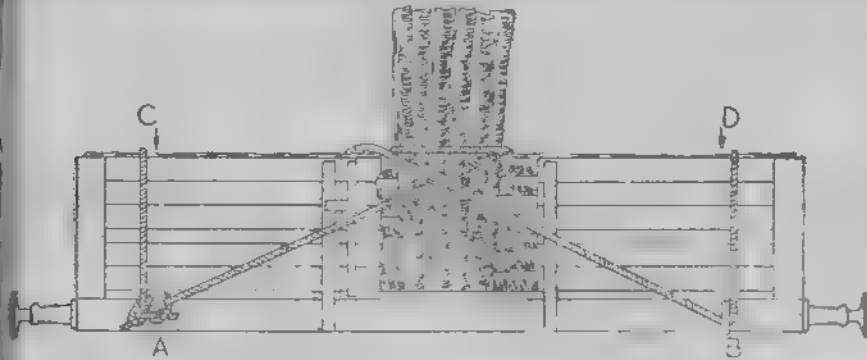
DIAGRAM No. 17.



The butts should be alternated to secure rigidity, and to obtain tight roping, the centre pieces may be slightly arched above the rave of the wagon.

Roping. Secure rope at "A", over to hook at "B", along solebar to hook at "C", across load to "D" and secure.

DIAGRAM No. 14. Alternative method for 6 ft. 6 in. lengths.

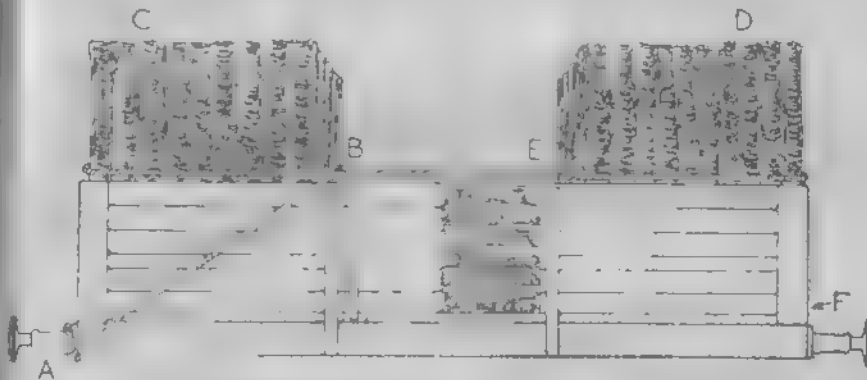


The space left in the centre between the pieces loaded lengthwise must be tightly packed.

Roping. Start at horse-hook "A", cross to "D", back to "B", over to "C" and to "A".

The wagon flap must be put up and secured before roping.

DIAGRAM No. 15. Alternative method for 6 ft. 6 in. lengths.

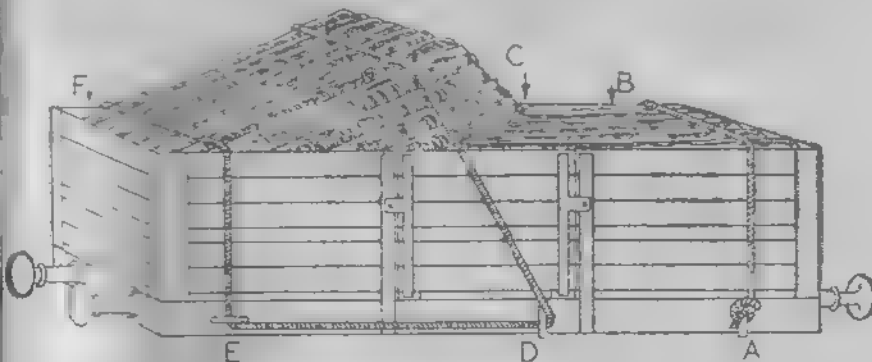


The pieces placed lengthwise must be loaded up to the rave and the spaces left at the ends packed tightly.

Roping. Secure rope at "A", to "B", encircle uprights "C", across to "E", encircle uprights "D" and to "F" (opposite side to "A").

The wagon flap must be put up and secured before roping.

DIAGRAM No. 16. 7 ft. to 9 ft. lengths, also 6 ft. to 9 ft. lengths mixed.



Pieces must be kept below the rave of the truck.

Roping. Secure rope at "A", over load to hook at "B", to wagon flap guard at "C", over load to "D", across to "E", over load and tie on horse-hook at "F".

LOADING, ETC., OF MERCHANDISE TRAFFIC.

**LOADING OF DEALS, BOARDS, BATTENS, SCANTLINGS
AND SIMILAR TRAFFIC.**

The following Instructions must be observed:—

Diagram No. 1. (See Instruction No. 1.)

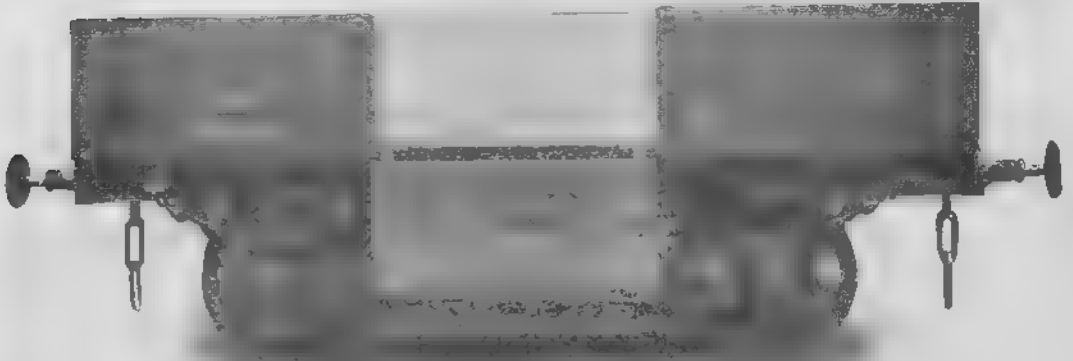


Diagram No. 2. (See Instruction No. 1.)



LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF DEALS, BATTENS, ETC.—*Continued.*

1. Lengths that are less than the inside length of the wagon may be loaded in open wagons, with sides of not less than three planks, as shown in Diagram No. 1, the timber being kept below the sides of the Wagons, or, as shown in Diagrams Nos. 2 and 3, Wagons with sides of not less than four planks being used.

When loaded as shown in Diagram No. 2, the timber must be of such a length as to permit of pieces being used as stanchions round the sides and ends of the wagon. These pieces must not exceed 6 feet 6 inches in length and must be placed end to end, close together along the ends and as far as possible round the sides of the wagon and must rest on the floor of the wagon.

The timber inside the stanchions must be kept 6 inches below the tops of the stanchions. When sufficient upright pieces cannot be obtained to extend entirely along the sides, as well as the ends, of the wagon, the load must be secured by upright pieces placed close together at each end of the wagon, and by not less than ten upright pieces along each side, the load being firmly secured by ropes. Tip end door wagons must not be used for loads, as per Diagram No. 2.

When loaded in accordance with Diagram No. 3, two or more stout stanchions, not exceeding 6 feet long and of uniform size, must be used on each side of the load, and must extend upwards from the floor of the wagon. The timber must be kept 6 inches below the tops of the stanchions, and must be secured by passing in ropes or wires, the ends of the stanchions, and tightly across the load from stanchion to stanchion, so as to secure the load in the centre, the top of the load being rounded off for the purpose of obtaining great security from the roping. The wheels of the wagon must be filled with timber.

Diagram No. 3. (See Instructions Nos. 1, 2 and 3.)



LOADING OF DEALS, BOARDS, BATTENS, SCANTLINGS AND SIMILAR TRAFFIC.—Pages 205 and 210.

Clause 1.—The following to be added as the fifth paragraph on page 206 :—

Clause 1.—The following to be added to the said regulations:—

When the sawn planks and boards may be loaded 18 inches above the rails of a wagon or over the top of the wagon, provided the ends of the wagon are completely latched by the coupling, and the height of the load does not exceed 5 feet high, with 4 upright standards of similar height secured to the wagon at each end and each pair of standards is being secured by a chain or rope to the ends of the wagon at the ends and sides of the wagon, and the height of the load does not exceed 18 inches above the top of the shortest standard.

of the shortest standard
 wagon is held rigidly in position.

End-doors are properly secured before loading is commenced

Note.—Alternative method of loading from that shown in diagrams 2 and 3.

(G.A.13. 9/43. Op.Com.Min.3950.)

The following additional instructions to be inserted on page 206 :—

LOADING OF SLEEPERS (UNCHAIRD).

papers to be loaded end to end and flat on the floor of an open goods wagon, and tied up to the level of the wagon rave.

When a quantity of sleepers should be loaded flat and tiered at one end of the car, the end of the pile and at the opposite end sleepers may be loaded in the door or on a wagon with the sleeper leaning against the ends of those loaded flat, and at an angle sufficient to form a compact load.

This method is similar to that used for the loading of Pitwood, and is illustrated on page 202, diagram 7.

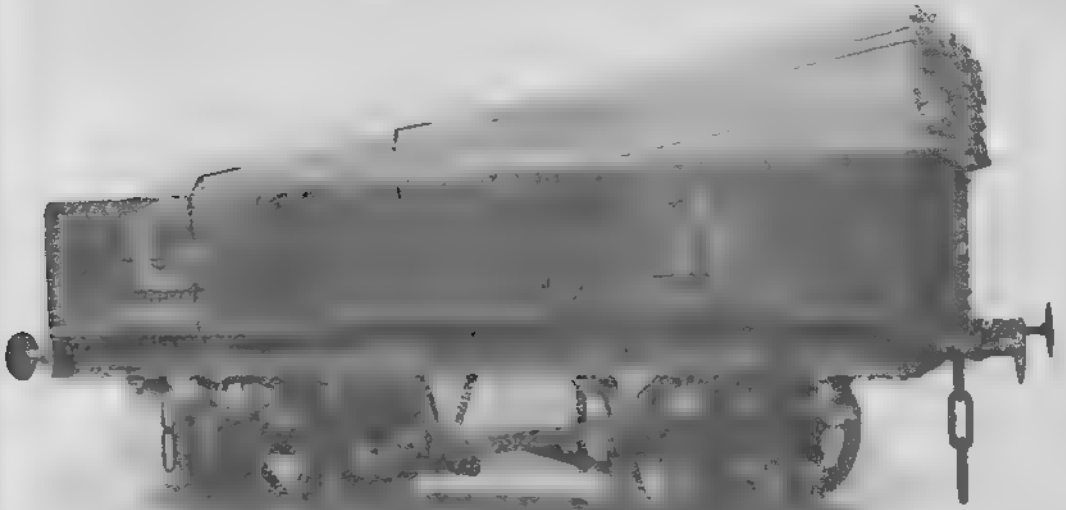
When flat-bottomed wagons are used, the sleepers loaded in the slanting position must be placed at the fixed end.

(G.A.13. 9/43. C.G.M.—W.T./X. 76389.)

LOADING, ETC., OF MERCHANDISE TRAFFIC.

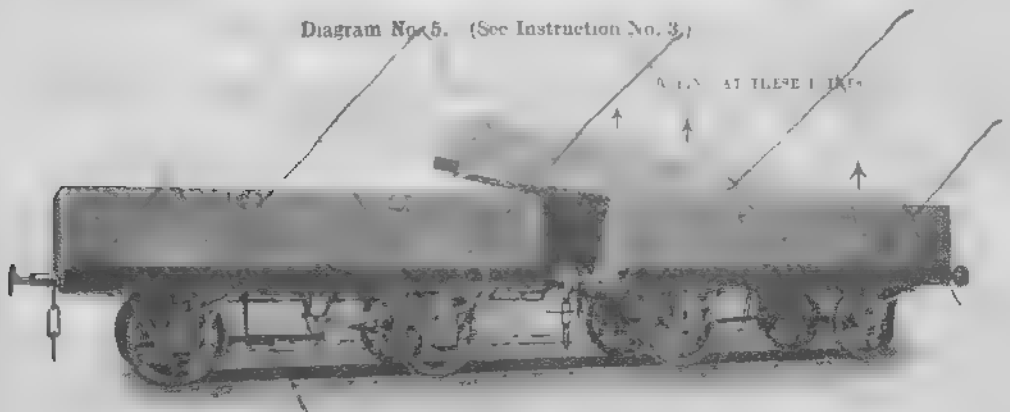
LOADING OF DEALS, BATTENS, ETC.—Continued.

Diagram No. 4. (See Instruction No. 2.)



2. Lengths exceeding length of wagon by not more than one foot may be loaded and roped as shown in Diagram No. 4. The ends of the object must be secured with weight in accordance Diagram X

Diagram No. 5. (See Instruction No. 3.)



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LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF DEALS, BATTENS, ETC.—Continued.

Diagram No. 6. (See Instruction No. 3.)

**3A** Lengths exceeding length of wagon by more than one foot but not exceeding 24 feet 6 inches :

Not exceeding 6 tons in weight must be loaded and secured in accordance with Diagrams Nos. 3, 6 or 7, and the following instructions, on a Double Bolster Timber Truck (Macaw A).

Exceeding 6 tons but not exceeding 10 tons must be loaded and secured in accordance with Diagrams Nos. 3, 6 or 7, and the following instructions, on a Double Bolster Timber Truck (Macaw A).

Exceeding 10 tons but not exceeding 14 tons must be loaded and secured in accordance with Diagrams Nos. 6 and 7, and the following instructions, on a Double Bolster Timber Truck (Macaw H) (see Diagram No. 8), or on a Double Bolster Timber Truck (Macaw A).

Exceeding 14 tons but not exceeding 20 tons must be loaded and secured in accordance with Diagrams Nos. 7 and 8, and the following instructions.

Exceeding 20 tons but not exceeding 30 tons must be loaded and secured in accordance with Diagram No. 8.

When the method of loading as shown in Diagram No. 3 is adopted, Check Wagons must be used as required; the well of the carrying Wagon must be covered with timber of lengths less than inside length of wagon; two or more stout stanchions not exceeding 6 feet long must be used on each side of the load and must extend upwards from the floor of the Wagon; the Timber must be kept 6 inches below the top of the stanchions, and must be secured by passing one rope, or more if necessary, round the stanchions, and tightly across the load from stanchion to stanchion, also across the load in the centre, the top of the load being rounded off for the purpose of obtaining greater security from the roping.

When the method of loading as shown in Diagram No. 5 is adopted and there are not more than half a dozen pieces over 6 feet but not exceeding 12 feet in length, some of which may be in the end, provided they are so mixed with other pieces of timber as to ensure safe transit. The average length of the load must not, however, exceed 21 feet, and an additional rope must be bound round the overhanging end of load. The low end of the load shown in Diagram No. 5 must be kept 6 inches below the top of the wagon.

Dated 6/4/4

The following to be inserted as Instruction No. 3 on page 208.

3. Lengths which overhang the end of wagon by 1 foot or more. These may be carried in cars 8, 1, or 2 on open goods wagons not exceeding 4 ft 3 inches in height and with 11 end doors, provided the weights and applicable overhangs given in the table of Diagram "X" are adhered to and the load secured as shewn on Diagram "X". Alternative Double Reister Trucks (Macaw A, Twin Rail and Timber Trucks (Mite) or 14 ton Open Wagons (Open C) may be used for loads not less than 2 ft 6 in length. The loads to be roped or chained in accordance with Diagrams No. 6 page 208, and No. 7 page 209.

(G.A. 7.—3 40. C.G.M.—W.T/X.14231.)

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF DEALS, BATONS, ETC. *Continued*

ds on Twin Timber Tr . must be composed of lengths of not less than 20 feet resting on
and must l . ned at the Bolsters, a binding chain or rope being fastened
Diagram No 7. In the case of thin boards liable to sag,
must be 21 feet or more. A load requiring the width of the bolsters
only, must be secured in the same way, the stanchions being
bolsters. Single Timber Trucks must not be used for such loads.

Diagram No. 7. (See Instruction A



Diagram No. 8. (See Instruction No. 3.)



INSTRUCTION No. 4. LENGTHS EXCEEDING 24 FEET 6 INCHES, ETC. -
page 209.

To be amended to read:

Lengths exceeding the maximum laid down in table of Diagram "X" must be loaded on Twin Rail and Timber Trucks (Mite), 20 ton Bogie Rail and Timber Trucks (Macaws H) or Bogie Rail and Timber Trucks (Macaws B, D or E) with match trucks as required

The overall length of double load as shown in Diagram No. 8 must not exceed 48 feet for Bore Rail and Timber Trucks (Macaws B, D or E) or 36 feet for 20 ton R. and Timber Trucks (Macaw H) without the use of check wagons. The Tops of loads as per Diagrams 3, 6, 7 and 8 should be rounded off to make the roping more effective.

(G.A. 7. 3/40. C.G.M. -W.T.X.14231.)

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF DEALS, BATTENS, ETC. -Continued.

GENERAL INSTRUCTIONS TO BE OBSERVED.

All load which overhang the ends of Wagons must, when necessary, be firmly secured by roping, or other suitable means, before being despatched.

When twin timber trucks ("Mites") are used the bolsters must be greased underneath in the centre before loading, and also the quadrants upon which they move, in order to give them free play. Greasers or Examiners belonging to that Department, the greasing (or oiling) of the bolsters must be carried out by the Goods or Traffic Department staff who perform the loading. The Inspector, Foreman, or other person superintending the loading will be held responsible for seeing that the bolsters are properly greased or oiled before the wagons are loaded.

Loose chains must be only wound round the load and the ends fastened together as shown in the Diagrams, and not secured to the trucks in any way.

Before loading is commenced the "D" shackles and chains attached to the Wagons must, as far as possible, be kept in the position in which they are shown in the Diagrams attached to the end of the wagon.

494 ~~When loading is commenced the "D" shackles and chains attached to the Wagons must, as far as possible, be kept in the position in which they are shown in the Diagrams attached to the end of the wagon.~~

LOADS MUST NOT EXCEED THE MAXIMUM CARRYING CAPACITY OF WAGON.

LOADING AND SECURING CRATEWOOD, CRATE HEADS AND CRATE RODS.

LOADING

1. This class of traffic should not be loaded into wagons with less than "four plank" sides.
2. Six good poles, not exceeding 6 ft. 6 ins. in height, should be fixed as stanchions, three on each side of the wagon.
3. If a load is to be loaded into a wagon, the load should be placed on the wagon as closely as possible, and tight with the stanchions.
4. If the load is to be loaded into a wagon, the load should be placed on the wagon as closely as possible, and tight with the stanchions.

ROPING

5. Two ropes should be used to secure each load.
6. The first rope should be attached to the leading end of the load, and passed over the stanchion on the off side.
7. This will leave a spare end of rope which must be brought tightly down and passed through the ropeshook and secured.
8. The load must then be completed as described in paragraph 4; and the spare end of the rope at the trailing end of the wagon.
9. The second rope should then be passed twice over the load, commencing at the leading end of the load, and passing over the stanchion on the off side. The second rope will in this way be roped over the load three times altogether.

LOADING, ETC., OF MERCHANDISE TRAFFIC

LOADING OF CHANNEL, BULB AND ANGLE IRON, TINPLATE BARS, IRON AND STEEL BARS, BILLETS, PLATES, RAILS, GIRDERS AND OTHER SIMILAR TRAFFIC, ALSO SHEARINGS, SCRAP, ETC.—Pages 211 and 212.

The following to be substituted for the instructions issued for the guidance of staff in loading and securing the above descriptions of traffic:—

All consignments (or any part of a mixed consignment) of traffic of these classes which are capable of lying on the floor and entirely inside the body of the wagon, must be so loaded for transit, but end-door wagons must not be used for the conveyance of heavy flat steel plates. Light steel plates may be loaded in end-door wagons provided timber is secured to the floor of the wagon to prevent end-wise movement of the load towards the end door. Care must be taken to see that the end-door is fastened securely.

Bundles of black plates, tin plates and similar traffics of such length as can be accommodated in the bed of the wagon may be carried in end-door wagons provided that each bundle is securely fastened together, and a strip of packing is secured to the floor between the load and the end door of the wagon. Care must be taken to see that the end-door is securely fastened.

Railway Companies' and Private Owners' end door wagons with one end-door only may be used for overhanging loads not exceeding 2' 6" in weight provided the overhang does not exceed 2ft. One end of the load must be supported on the floor at the fixed end of the wagon and the other end of the load carried on the hinge bar of the end-door. Care must be taken to see that the end-door is securely fastened.

Wide plates must not be loaded in Open Goods Wagons with the plates resting against, on top, or overhanging the side of the vehicle.

Plates loaded on flat topped wagons without the use of stanchions must be chained laterally as well as longitudinally to avoid movement of the plates, even though this may involve a slight increase in the width of the load due to the binding chains.

Where it is not possible to load traffic between the stanchions, application should be made to the Chief Goods Manager for specially fitted wagons, full dimensions of the load being given when the application is made.

Wagons with rounded ends must not under any circumstances be used for the conveyance of overhanging traffic.

In selecting check wagons care must be taken to see that the sides of the check wagons are lower than the sides of the wagon carrying the overhanging load, and in no circumstances is a wagon with sides higher than the carrying truck to be used to check an overhanging load.

Instructions have been given at certain works for the loading of steel bars, varying in length from 12ft. to 24ft. on check wagons with double doors in the manner shown on diagram No. 1, but the practice is not to be extended except by authority of the District Goods Manager or District Traffic Manager.

(G.A.12.41 C.G.M. W.I. X. 6224)

The third lift to be placed overhanging the same end as the first.

All three lifts must be placed close together in the middle of the wagon, and must not consist of more than about 20 bars each.

After loading, the top six bars from the first and third lifts are to be pulled over towards, the sides of the truck, making practically five rows of bars loaded in each vehicle.

Diagram No. 1.



LOADING OF WOODEN SOLEBAR WAGONS WITH TINPLATE BARS, IRON AND STEEL BARS, CHANNEL, ANGLE, AND BULB IRON, BILLETS, PLATES, RAILS, GIRDERS, ROUND TIMBER, BAULK TIMBER, PITWOOD, DEALS, BATTENS, BOARDS, SCANTLINGS, &c.

Railway Companies' wagons with wooden solebars may carry and load up to and beyond 75 per cent of the truck's carrying capacity, when the goods are loaded one end only provided the exchange does not exceed 2 ft 6 ins. If it when loaded with timber, with the exception of round timber, as shown in Diagram No. 1 they may be loaded up to full carrying capacity.

1. LENGTHS THAT CAN BE ACCOMMODATED IN THE BEDS OF OPEN WAGONS must be equally distributed over the truck and the type of carriage in which it is loaded. Lengths of

2. LENGTHS NOT EXCEEDING LENGTH OF WAGON BY MORE THAN 1 FOOT. When the bars cannot be laid on the ground in the wagon they must be measured in 12, 16 or 20 foot lengths up to the nearest 16 feet as shown in Diagram No. 1. In the case of 12 foot bars they must be placed in the wagon as shown in Diagram No. 1 so as to leave a sleeping accommodation.

NOTE. Lengths of 12, 16, and 20 feet are preferred inside the length of wagon as they are most convenient. A small amount of wagon by more than 1 foot. Short bars must be packed with and between the feet of the wagon.

BARs, FLAT OR TINPLATE, exceeding 12 ft length of wagon but not exceeding outside length of wagon, may be loaded in the wagon as shown in Diagram No. 2. See also Diagram No. 3 on next page.

The first lift of bars to be placed slightly overhanging the trailing end of the wagon.

The second lift to be placed at the other end of the wagon, leaving the other end.

The third lift to be placed between the second and the first.

All three lifts must be placed close together in the middle of the wagon, and must not consist of more than a total of 10 bars each.

After loading, the top six bars from the first and third lifts are to be pulled over towards the sides of the truck, making the ready access of bars loaded in each vehicle.

LOADING OF CHANNEL, BULB AND ANGLE IRON, TINPLATE BARS, IRON AND STEEL BARS, BILLETS, PLATES, RAILS, GIRDERS AND OTHER SIMILAR TRAFFIC, ALSO SHEARINGS, SCRAP, ETC. page 211.

The following to be added at the end of the first paragraph of the instructions issued for the guidance of staff in loading and securing the above descriptions of traffic:

End door mineral wagons with steel floor are not to be used for the conveyance of this traffic.

(G.A. 18. 11/47. C.G.M.-WT/X.1/39032.)

Lengths that can be accommodated in the beds of open wagons must be equally distributed over the floor and the top of load must be kept at least 1 foot below the eave of the wagon. The load must not be loaded diagonally across the floor as this produces uneven loading at the corners of the wagons. In those cases where the use of end door wagons is authorised the doors must be properly secured.

(G.A. 15-12/44 C.G.M.-W.T./X.76333)

The load must not be placed diagonally across the floor as this produces uneven loading at the corners of the wagon.

(G.A. 12. 4/48. C.G.M.-W.T./X. 65759.)

2. LENGTHS OVERHANGING END OF WAGON BY NOT MORE THAN 1 FOOT may be loaded in ordinary 1, 2 or 3 ton open coal wagons not exceeding 10 feet 6 inches in depth with weights as shown in table of Diagram "X" which is overhanging one end only. In loading flitches one half must be placed on each end side of wagon as shown in Diagram No. 3, to form a sloped station then

Weights must be placed on the floor of the wagon.

(G.A. 7.-3/40. C.G.M.-W.T/X.14231.)

BARS, FLAT OR TINPLATE--page 212.

The following paragraph to be added at the foot of page 212 before Diagram No. 1.

The following method of loading lengths overhanging each end of the wagon by not more than 1 foot, when handled by crane, shown on Diagram 2, page 213, with weights in accordance with table of Diagram "Y", is authorised at certain works, but must not be extended except by authority of the Chief Goods Manager. Instructions for loading remain unaltered.

(G.A. 7.-3/40. C.G.M.-W.T/X.14231.)

The following to be added to paragraph 3 of the instructions relating to "Lengths exceeding outside length of wagon by more than 1 ft., but not exceeding 21 ft.":

Wagons deeper than 3 ft. 3 in. inside must not be used for conveying overhanging loads of metal traffic, when loaded in accordance with Diagrams 1, 2, 3, and 4."

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF CHANNEL IRON, ETC.—*Continued.*

Diagram No. 2.



**INSTRUCTION 3. LENGTHS EXCEEDING OUTSIDE LENGTH OF WAGON
BY MORE THAN 1 FOOT BUT NOT EXCEEDING 21 FEET** page 213.

The following to be substituted for the existing instruction:

Loads overhanging one or both ends of the wagon by more than 1 foot, but with overhangs not exceeding those laid down in the tables of Diagrams "X" or "Y", may be carried in ordinary 8, 10 or 12 ton open goods wagons not exceeding 3 feet 6 inch (five planks deep provided the goods be distributed evenly and the overhangs are observed. Loads must be properly secured by ropes as shown in Diagrams "X" and "Y", also Diagrams on page 211. Check wagons to be used where necessary.

(G.A. 7.—3/40. C.G.M.—W.T/X.14231.)

Diagram No. 3.



LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF CHANNEL IRON, ETC. *Continued*

Diagram No. 6.



5. LENGTHS EXCEEDING 21 FEET BUT NOT EXCEEDING 36 FEET AND NOT EXCEEDING 20 TONS IN WEIGHT. If the length of the beam exceeds 21 feet but does not exceed 36 feet and the weight does not exceed 20 tons, the beam may be carried on a 20-ton Bogie Rail Wagon (Macaw H) if available, or alternatively on a Macaw B, D or E. The beam must be carried on the check wagons and the following instructions must be observed:

The beam must be carried on the check wagons and must be supported by two vertical posts on the wagon's deck. The beam must be supported by the inner stanchion sockets of the bolsters. The beam must be supported by the inner stanchion sockets of the bolsters. The beam must be supported by the inner stanchion sockets of the bolsters.

Lengths liable to sag on to the carrying or check wagons must not be carried on Twin Timber Trucks but on a 20-ton Bogie Rail Wagon (Macaw H) if available, or alternatively on a Macaw B, D or E.

Diagram No. 7.



6. LENGTHS EXCEEDING 21 FEET BUT NOT EXCEEDING 46 FEET AND EXCEEDING 20 TONS BUT NOT EXCEEDING 30 TONS. If the length of the beam exceeds 21 feet but does not exceed 46 feet and the weight exceeds 20 tons but does not exceed 30 tons, the beam must be carried on a 20-ton Bogie Rail Wagon (Macaw H) if available, or alternatively on a Macaw B, D or E. The beam must be carried on the check wagons and the following instructions must be observed:

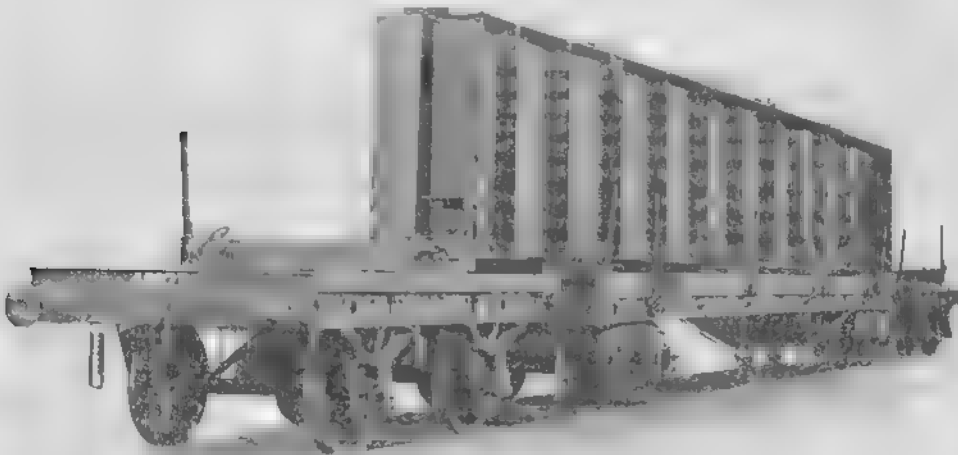
LOADING, ETC., OF MERCHANDISE TRAFFIC.
LOADING OF CHANNEL IRON, ETC. -Continued.

Diagram No. 8.



7. LENGTHS EXCEEDING 36 FEET BUT NOT EXCEEDING 49 FEET AND NOT EXCEEDING 30 TONS IN WEIGHT must be conveyed on Bogie Rail and Timber Trucks (Macaws B, D or E), as shown in Diagram No. 9, with check wagons where necessary. These lengths can be loaded to the full width of the bolster between stanchions.

Diagram No. 9.

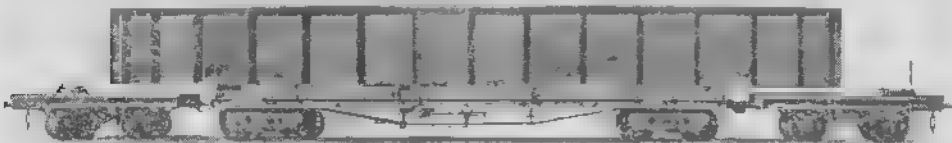


8. LENGTHS EXCEEDING 49 FEET BUT NOT EXCEEDING 70 FEET may be loaded on a Bogie Rail and Timber Truck (Macaw B, D or E), as shown in Diagram No. 10, check wagons being used as required.

The load must be placed centrally on the bolsters and must not exceed 30 tons in weight nor 5 feet in width when load exceeds 62 feet 6 inches in length, and check wagons without bolsters must be used.

LENGTHS EXCEEDING 62 FEET 3 INCHES BUT NOT EXCEEDING 70 FEET may be loaded on a 30 ton Bogie Rail and Timber Truck (Macaw C), but not to any other Company's line without the authority of the Chief Goods Manager being first obtained.

Diagram No. 10

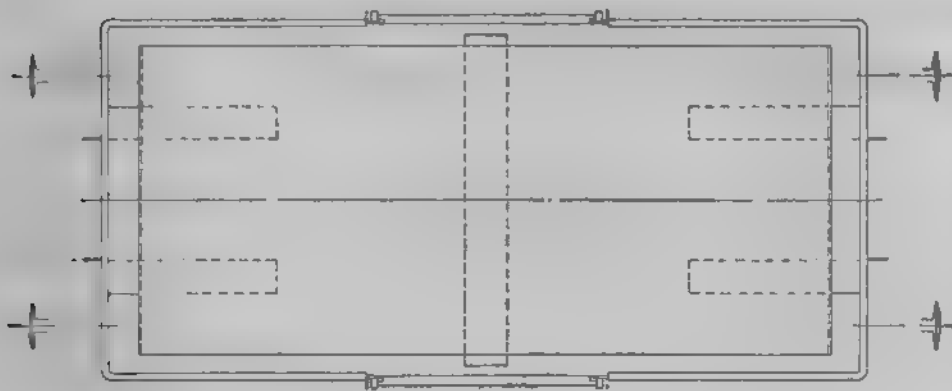
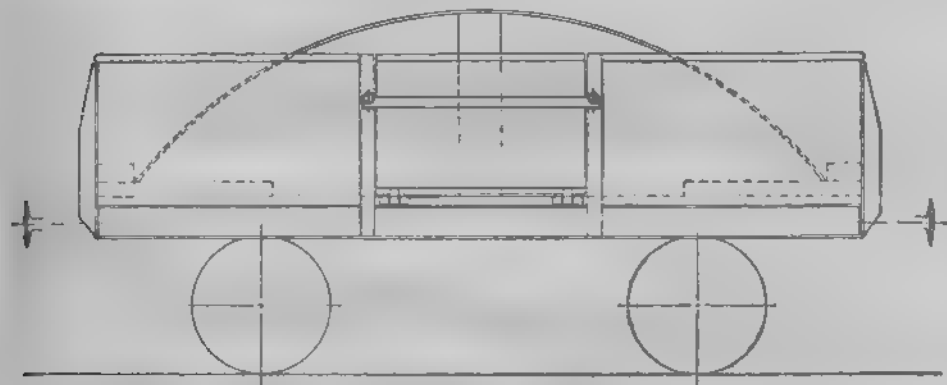


For loading of traffic over 46 feet in length which is flexible and liable to sag, see Special Instructions, pages 218 and 220.

The plates can also be loaded, self contained, curve downwards, upon double bolster wagons or bogie bolster wagons. There must be a minimum overhang of one foot over the outside edge of both bolsters upon which the load rests. Picking to be secured on top of the bolsters, if necessary, and the securing chains should encircle the load and be inside the stanchions and bolsters.

10. Loading of Curved Plates—continued

Diagram No. 12



11. LOADING OF SHEARINGS, SCRAP, ETC.—This clause to be re-numbered 11.

(G.A.25—1 50. RE Stand. C.S. WTX 1 46431.)

LOADING OF CHANNEL IRON, ETC.—Continued

LOADING OF CHANNEL IRON, ETC.—Continued

9. **General Instructions** All loads which overhang the ends of wagons must, when necessary, be secured by chains or other effective means, before being despatched.

of varying lengths a foundation must first be made with the greater lengths on the pieces being loaded upon them. If loaded against iron is apt to slip, therefore, a. Large Plates, or similar Traffic, close together, a piece of wood or old sacking must be the binding chains and the load, and also between the Girders, Plates, etc., when to prevent slipping during transit.

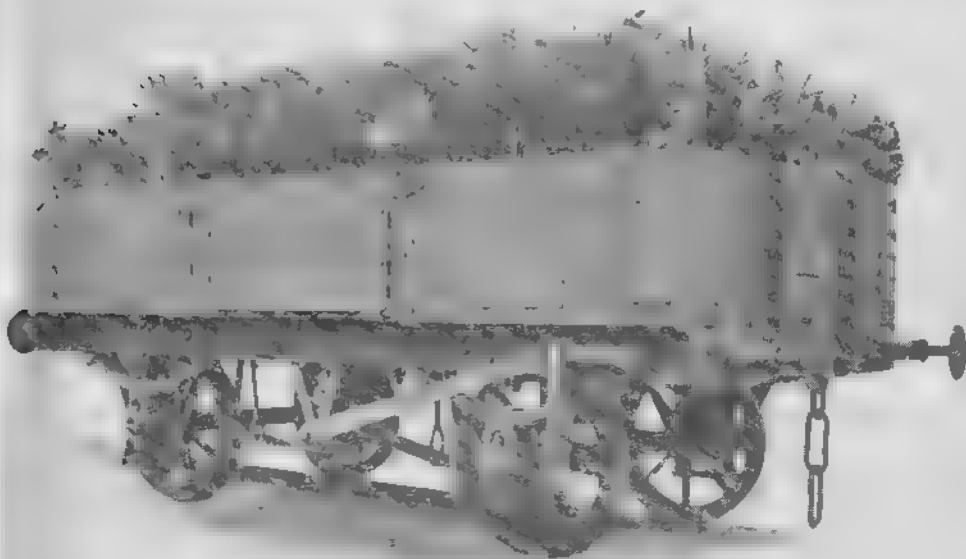
Timber Trucks & Motors, and all the parts must be greased thoroughly before use, and also the quadrants upon which they move, in order to give them free play. The Motors should be greased at all stations where there are men to do so, and the Drivers should be assigned to the Motors to take care of them. Drivers should be instructed to grease the Motors before starting. The Inspector should put out a card in the newspapers, for all the drivers, to be greased or oiled before the wagons are loaded.

These chains must be wound round the load and the ends fastened together, as shewn in the

Before loading is common. It is, however, said that it is not at all to be regarded as a rule, but that, as a rule, the safety of the load is to a large extent dependent

[illegible]

Diagram No. 11.



10. **Loading of Shearings, Scrap, etc.** When it is necessary to load bundles of Shearings, Scrap, etc. on the sides of the Wagon, the bundles shall be stacked and placed in and under the sides and ends of Wagon in such a manner that such bundles may be stanching over which the bundles loaded in the interior of the side. All loose or over-laying pieces must be removed if the rails are to go forward. Wagons with sides of five or seven planks must be used for this traffic when available.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF TRAFFIC OVER 46 FEET IN LENGTH WHICH IS FLEXIBLE AND LIABLE TO SAG, SUCH AS POLES, BARS, TUBES, RAILS, &c.

KEY

Length.	Weight not exceeding. Ton	Class of Wagon.	Instruction.	Diagram.
Exceeding 46 feet but not exceeding 50 feet.	30	Macaw B with match truck at one end.	1	A
Exceeding 50 feet but not exceeding 58 feet.	30	Macaw B with match trucks at each end.	2	B
Exceeding 58 feet but not exceeding 64 feet.	30	Macaw B with match trucks at each end (match trucks to have no bolsters)	3	C
Exceeding 62 feet 6 inches but not exceeding 70 feet.	30	Macaw C	4	D
Exceeding 80 feet but not exceeding 85 feet.	30	Two Macaws H with outer bolsters moved to extreme ends of pair of wagons.	5	E
Exceeding 85 feet but not exceeding 70 feet.	35	Two Macaws H with bolsters in position over bogie centres.	6	F
Rails 90 feet in length	30	Two Macaws B	7	G

THE FOLLOWING INSTRUCTIONS AND DIAGRAMS ARE ISSUED FOR THE GUIDANCE OF THE STAFF IN LOADING AND SECURING THE ABOVE DESCRIPTIONS OF TRAFFIC AND MUST BE STRICTLY OBSERVED.

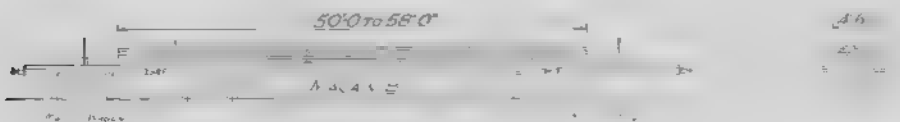
1. Lengths exceeding 46 feet and not exceeding 50 feet may be loaded and lashed at one end on Macaw B and one match truck (with or without bolster) used (see Diagram A). The bolster over bogie centre of Macaw B at end where load is overhanging, to be placed in the bolster guides provided at the state of bridge wagon to prevent the load from overhanging beyond the end of the wagon as far as possible. Load to be secured to bogie wagon at each bolster and, in addition, loose chains or ropes to be used to hold the load down over the bogie centres, according to the nature of the load. Width of load not to exceed 4 feet 6 inches.

Diagram A.



2. Lengths exceeding 50 feet and not exceeding 58 feet may be loaded and lashed on Macaw B with equal overhang at each end and two match trucks (with or without bolsters) used (see Diagram B). The load is to be secured to the match trucks at each end and to the Macaw B at each end of bogie wagon. Load to be secured to bogie wagon at each bolster and, in addition, loose chains or ropes to be used to hold the load down over the bogie centres, according to the nature of the load. Width of load not to exceed 4 feet 6 inches.

Diagram B.



[illegible]

THE UNIVERSITY OF CHICAGO

**LOADING OF TRAFFIC OVER 46 FEET IN LENGTH WHICH
IS FLEXIBLE AND LIABLE TO SAG, SUCH AS POLES,
BARS, TUBES, RAILS, ETC.**

KEY—page 218.

The following additions to be made to the Class of Wagon column:

Items 1 and 2 add: "or Macaw J without match truck"

Item 3 add: "or Macaw J with match truck at one end for lengths exceeding
62 feet 6 inches".

(G.A. 7.—3/40. C.G.M.—W.T/X.14231.)

OF THE
MUST BE

The following to be added after the second sentence:

Macaw J, loaded centrally, may also be used without match truck. The bolsters
over the bogie centres at each end of Macaw J to be placed in the bolster guides provided
4 feet from bogie centres towards centre of wagon.

(G.A. 7.—3/40. C.G.M.—W.T/X.14231.)

**INSTRUCTION 2. LENGTHS EXCEEDING 50 FEET AND NOT EXCEEDING
58 FEET—page 218.**

The following to be added after the second sentence:

Macaw J, loaded centrally, may also be used without match truck. The bolsters
over the bogie centres at each end of Macaw J to be placed in the bolster guides provided
4 feet from bogie centres towards centre of wagon.

(G.A. 7.—3/40. C.G.M.—W.T/X.14231.)

The following to be added after the first sentence:

Macaw J may also be used. Lengths up to 62 feet 6 inches to be loaded centrally, with bolsters in positions over bogie centres of Macaw J, and no match trucks. Lengths over 62 feet 6 inches and not exceeding 64 feet to be loaded as in Diagram "A", page 218, with end bolster placed in outer brackets at overhanging end of load only, with match truck at that end.

(G.A. 7—3/40. C.G.M.—W.T/X.14231.)

The following to be added to Instruction 3 —

Forty 60 ft. rails from the Port Talbot Steelworks to this Company's order may be loaded on Ganes with Match Trucks at each end (Match Trucks to have no bolsters) to an extreme width of 5 ft.

Similar loads on Macaws B, D and E, will still be confined to 35 rails giving a width less than 4 ft 6 in

(G.A. 7—3/40. C.G.M.—W.T/X.14231.)

LOADING OF TRAFFIC OVER 46 FEET IN LENGTH, ETC.

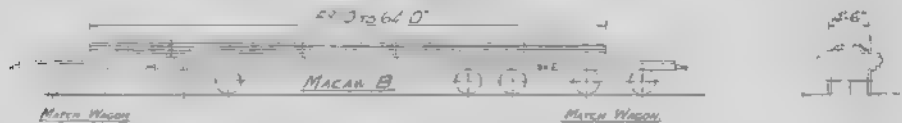
With reference to the instruction contained in the General Instructions No. 29 in connection with the conveyance of lengths exceeding 58 feet and not exceeding 64 feet, there has been a difficulty in the Swansea Division in connection with the loading of such lengths on to the Chief Marshal Locomotive has agreed as a temporary measure only, to permit 60 feet 6 inch rails being loaded on a Macaw "B" with match trucks fitted with bolsters, but stipulates that the rails must be loaded in such a manner as to ensure that the clearances between the ends of rails and bolsters of match trucks (with buffer faces in contact) are not less than 2 feet. Care is to be taken to see that the load is efficiently secured and arrangements are made by the Goods Department to ensure that the load is covered and for the same to be watched by the Locomotive Department to ensure the Marshalling Yard in the Swansea Division.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF TRAFFIC OVER 46 FEET IN LENGTH, ETC. —Continued.

3. Lengths exceeding 58 feet and not exceeding 64 feet to be loaded in the same manner as 50 feet to 58 feet lengths, except that match trucks without bolsters must be used (see Diagram C). Load to be secured to bogie wagon at each bolster and, in addition, loose chains or ropes to be wound round load to prevent spreading where considered necessary, according to the nature of the load. Width of load not to exceed 4 feet 6 inches.

Diagram C.



4. Lengths exceeding 62 feet 6 inches and not exceeding 70 feet may be loaded in the same manner (see Diagram D), but not to another Company's line without the authority of the Chief Goods Manager being first obtained. End bolsters to be placed in the bolster guides provided over the bogie centres. Load to be secured to bogie wagon at each bolster and, in addition, loose chains or ropes to be wound round load to prevent spreading where considered necessary, according to the nature of the load. Wagon may be loaded to full width of bolsters between the stanchions.

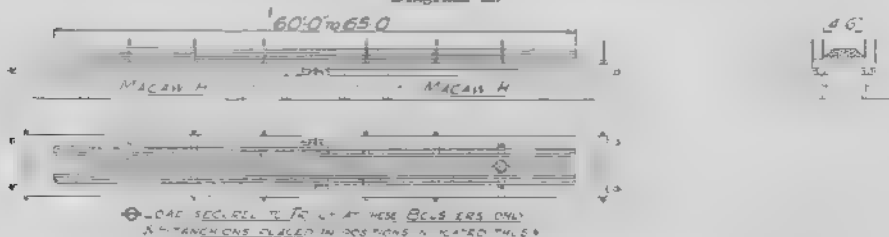
Diagram D.



5. Lengths exceeding 60 feet and not exceeding 65 feet may be loaded in the same manner (see Diagram E), coupled together with a special short three-link coupling. Bolsters at the ends of the pair of wagons must be placed in the bolster guides provided over the bogie centres. Load secured to bogie wagons at bolsters marked ⊕ on Diagram E, and, in addition, loose chains or ropes to be wound round load to prevent spreading where considered necessary, according to the nature of the load. The chains at securing bolsters ⊕ must be passed round the load. Suitable steel plates to act as skids to be fixed to tops of all bolsters except those marked ⊕ and top surfaces of plates well greased to ensure free movement of load when negotiating curves. Securing bolsters marked ⊕ not to be greased but packed up with timber approximately 1/4-inch thicker than the steel skid plates on the other bolsters.

Not to negotiate curves of less than 3-chain radius.
Width of load not to exceed 4 feet 6 inches.
Load to carry not to exceed 30 tons.

Diagram E.



6. Lengths exceeding 65 feet and not exceeding 70 feet may be loaded in the same manner (see Diagram F), coupled together with a special short three-link coupling, in the same manner as 60 feet to 65 feet lengths, except that the bolsters at ends of the pair of Macaws H must be placed in the bolster guides provided over the bogie centres. Load secured to bogie wagons at bolsters marked ⊕ on Diagram F, and, in addition, loose chains or ropes to be wound round load to prevent spreading where considered necessary, according to the nature of the load. The chains at securing bolsters must be passed round the load. Suitable steel plates to act as skids to be fixed to tops of all bolsters except those marked ⊕ and top surfaces of plates greased to ensure free movement of load when negotiating curves. Securing bolsters marked ⊕ not to be greased but packed up with timber approximately 1/4-inch thicker than the steel skid plates on the other bolsters.

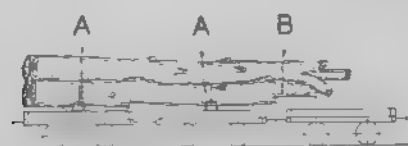
LOADING OF SINGLE BOLSTER WAGONS. EMERGENCY LOADING FOR WAR PERIOD ONLY. -

Page 220.

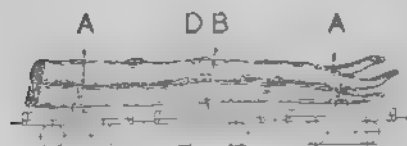
The following circuit diagram (see Fig. 1) is removed, this is illustrated by drawing an arrow from D'' on the diagram to the end node carrying with it

(C 15-12 1) CGM W.S. X.609)

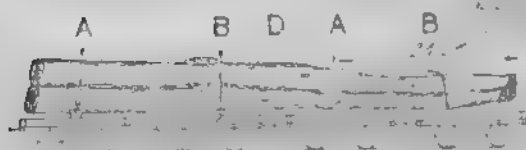
LOADING OF SINGLE BOLSTER WAGONS EMERGENCY METHODS FOR WAR PERIOD ONLY



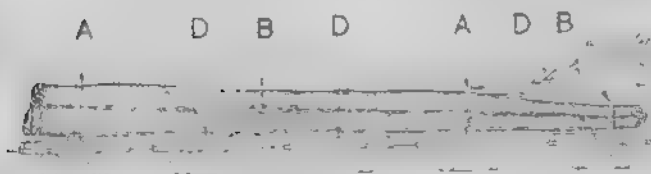
— 2 WAGON SET —



— 2 WAGON SET —



— 4 WAGON SET —



— 5 WAGON SET —

CARRYING WAGONS MUST NOT BE LOADED IN EXCESS
OF THEIR REGISTERED CARRYING CAPACITY

METHOD OF CARRYING RIGID ROUND TIMBER



5

-2 WAGON SET

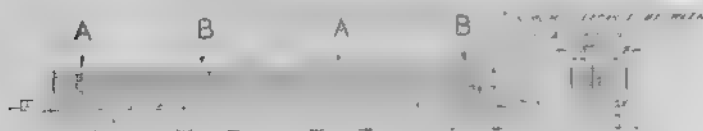
CARRYING WAGONS MUST NOT BE LOADED IN EXCESS OF THEIR REGISTERED CARRYING CAPACITY



6

-3 WAGON SET-

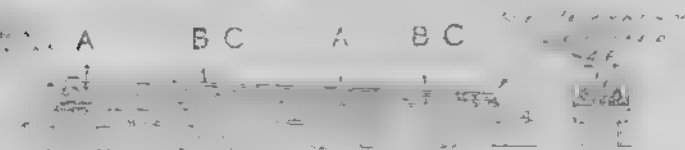
TOTAL WEIGHT OF LOAD NOT TO EXCEED CARRYING CAPACITY OF TWO WAGONS



7

-4 WAGON SET-

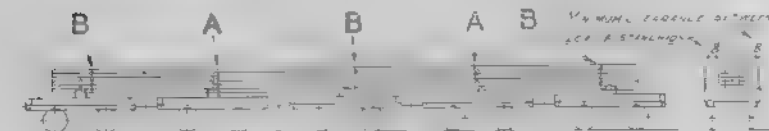
TOTAL WEIGHT OF LOAD NOT TO EXCEED TWO THIRDS OF THE CARRYING CAPACITY OF THE FOUR WAGONS



8

-4 WAGON SET-

TOTAL WEIGHT OF LOAD NOT TO EXCEED TWO THIRDS OF THE CARRYING CAPACITY OF THE FOUR WAGONS.



9

-5 WAGON SET

TOTAL WEIGHT OF LOAD NOT TO EXCEED TWO THIRDS OF THE CARRYING CAPACITY OF THE FIVE WAGONS

METHOD OF CARRYING FLEXIBLE LOADS SUCH AS RAILS, ROLLED SECTIONS, POLES, PLATES, ETC.
(excluding Telegraph Poles) GAR

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prevent load

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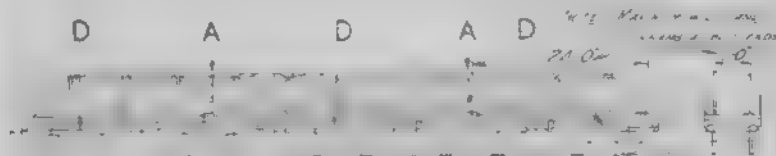
— 3 WAGON SET —

10



— 4 WAGON SET —

11



— 5 WAGON SET —

12

CARRYING WAGONS MUST NOT BE LOADED IN EXCESS OF THE:

METHOD OF CARRYING RIGID LOADS SUCH AS GIRDERS,
LARGE SQUARED TIMBER, ETC

A. LOADS TO BE CARRIED BETWEEN STANCHIONS, AS FOR LOADS SHOWN ON 3 WAGON SETS
OUTSIDE STANCHION.

B. LOAD CHAINED FREE OF WAGON

C. BOLSTERS TO BE RETAINED

D. BOLSTERS TO BE REMOVED

NOTE. LOADS EXCEEDING 60 FEET IN LENGTH TO BE TREATED AS SPECIAL LOADS

THE DIMENSIONS ON ALL DIAGRAMS HAVE BEEN BASED ON WAGONS 15 FEET LONG OVER
HEADSTOCKS ON A MINIMUM CURVE OF 60 MINS

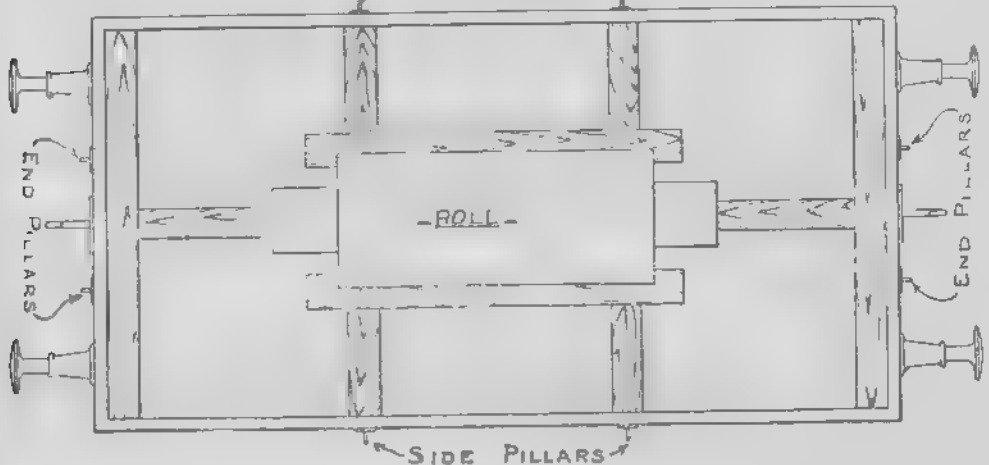
ALL LOADS SHOULD OVERHANG THE CARRYING BOLSTERS BY AT LEAST 2 FEET 6 IN.

THE MAXIMUM POSSIBLE MOVEMENT BETWEEN ANY TWO COUPLED WAGONS TO BE 1 FOOT, 0 IN.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING, SECURING AND CONVEYANCE OF IRON OR STEEL ROLLS.**Rolls One Ton and Under Three Tons in Weight.**

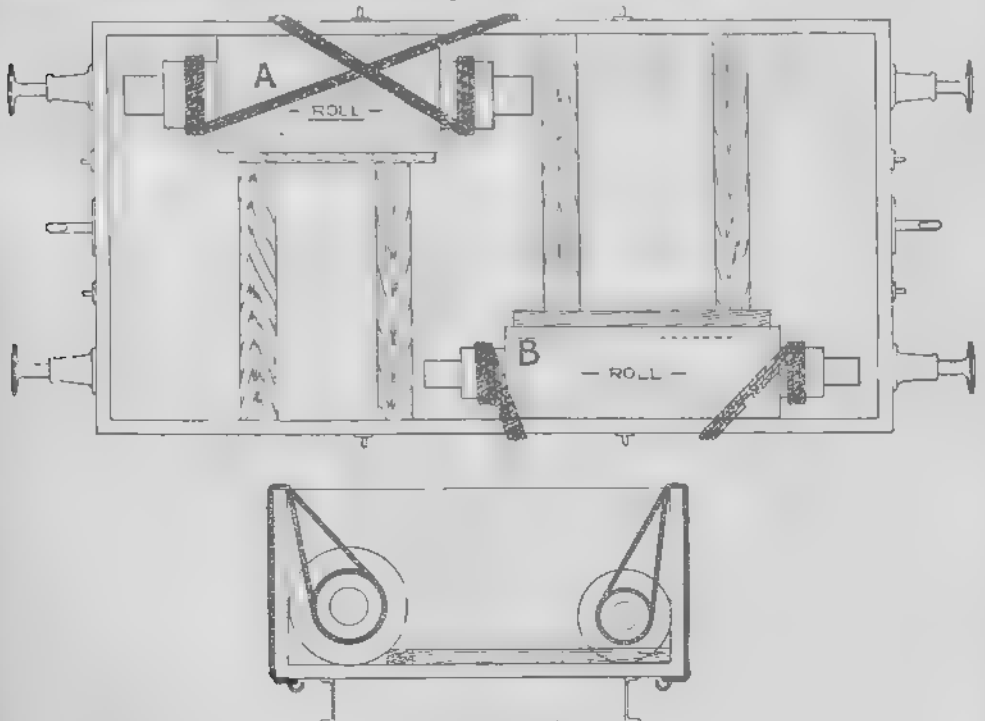
Rolls one ton and under three tons in weight, if carried singly, must be loaded in the centre of the wagon, as indicated in Diagram No. 1.

Diagram No. 1.
SIDE PILLARS

Scotches of suitable timber not less than 7 inches by 3 inches must be placed on each side of the roll, and struts be firmly fixed between the scotches and side pillars of the wagon, as indicated in the Diagram. A second set of scotches of the same length and height to reach from the end timber to the neck of the roll must be used between this and the roll as indicated to prevent longitudinal movement.

Timber packing must be firmly nailed to the floor of the wagon.

Rolls not exceeding two tons in weight forwarded in pairs must be loaded in opposite corners of the wagon as indicated in Diagram No. 2 but this method must not be adopted when there is a greater contact with the floor of the wagon of the respective rolls.

Diagram No. 2.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING, SECURING AND CONVEYANCE OF IRON OR STEEL ROLLS *Continued.*

One roll must be placed at each end of the wagon on opposite sides with a scotch of suitable timber not less than 7 inches by 3 inches, with struts firmly fixed between the scotch and side of the wagon, as indicated in the Diagram, the timber packing being firmly nailed to the floor of the wagon. Ropes must be placed round the neck of each roll at either end and firmly secured by means of the rope fasteners provided for the purpose.

Heavy Rolls, e.g. any Roll Weighing Three Tons or Over.

NOTIFICATION BY SENDERS.—Senders are required to notify the Station Agent when they are about to forward rolls for the purpose of traffic so that special arrangements may be made for the conveyance of the load by the Company.

Rolls from Three Tons to Nine Tons in Weight.

Wagons permanently fitted with packing for the loading of rolls are available, and these must, whenever possible, be used for the conveyance of heavy rolls up to nine tons in weight. Under no circumstances must the box bolsters or any part of the packing with which the wagons are equipped be removed from the vehicles.

Application for these wagons must be made to the Divisional Superintendent or District Traffic Manager.

In the absence of the specially fitted wagons, ordinary 10 ton Open Goods Wagons with sides not more than two feet four inches in height must in all cases be used, and the following method of loading be adopted:—

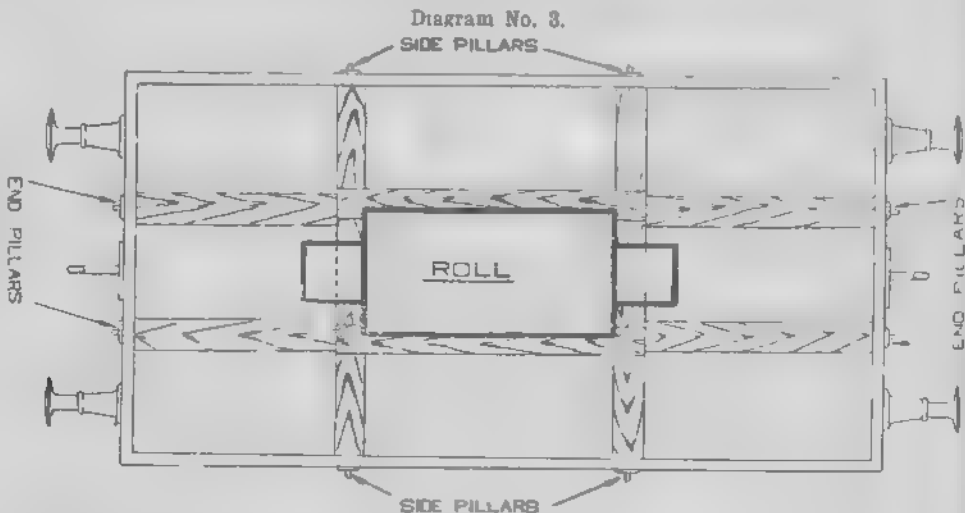
The rolls must be placed in position with the necks resting on baulks of timber of such dimensions as will assure the barrel of the roll clearing the floor of the wagon.

The baulks must extend across the wagon and be held in position by four struts of suitable timber (two at each end) firmly fixed between the baniks and the end pillars of the wagon to prevent longitudinal movement.

To prevent lateral movement a longitudinal tie of suitable timber must be placed at each side of the roll and flush with it and be firmly secured to the baulks, and the baulks must be sufficiently grooved in the centre for the neck of the roll to rest in.

The rolls, when loaded, must have the necks packed with wedges if the groove space is too large for the neck of the roll to fit tightly therein.

The method of packing is illustrated in Diagram No. 3.

**Rolls Exceeding Nine Tons in Weight.**

Rolls exceeding nine tons in weight must not be loaded in ordinary Open Goods Wagons, but on specially constructed vehicles under the supervision of an Inspector from the Chief Mechanical Engineer's Department.

Particulars of such rolls to be forwarded must be promptly submitted to the District Goods Manager, who will make arrangements for the supply of the special wagons and the attendance of an Inspector.

Broken Rolls.

These must be embedded in sand or ashes to a depth of not less than three fourths of the diameter of the roll, except in cases where a specially constructed wagon, or wagons, with special fittings are used.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF RAILWAY CARRIAGE AND WAGON WHEELS.

The following instructions must be observed in the loading of railway carriage and wagon wheels. —

Wheels when mounted on their own axle to be loaded with axles lengthwise to the wagon with outside tires or wheels pressed toward the end of the wagon in accordance with methods shown on diagrams 1, 2, 3 and 4. The practice of loading wheels crosswise on top of the axles of other wheels which are loaded lengthwise to be discontinued.

2. The most suitable wagons available must be selected for the conveyance of this traffic, but in no case must wagon having sides with a height inside of less than 24 inches be used.

3. The whole of the wheels must be properly scotched in order to prevent rolling or shifting during transit.

4. Port on Masters and Goods Agents must see that these regulations are strictly carried out and that they are complied with by private Wagon Companies and others as well as by the Company's own staff.

Note. Instances have occurred of carriage wagon and engine wheels and axles being received in perfect condition but being loaded in low sided flat wagons without being properly secured. Such traffic must not be accepted in low sided wagons unless the wheels are properly scotched and securely roped to prevent movement in transit.

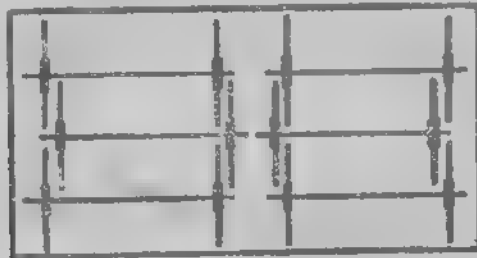


DIAGRAM 1



Instructions are given for Carriage and Wagon and Engine Wheels and Axles being received are to be loaded in low sided flat wagons without being properly secured. Such traffic must not be accepted in low sided wagons unless the wheels are properly scotched and securely roped to prevent movement in transit.

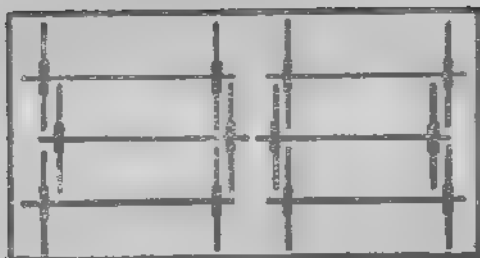


DIAGRAM 1

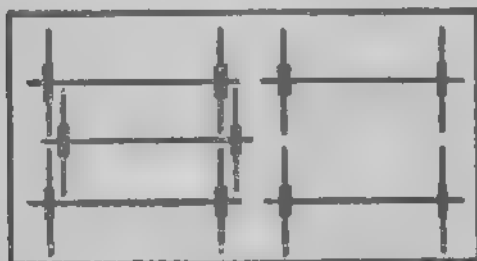


DIAGRAM 2

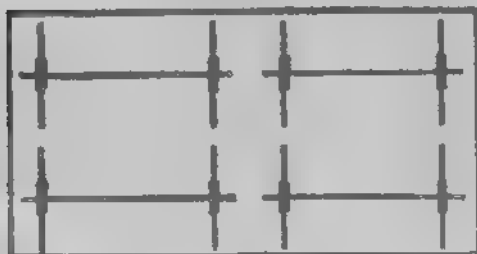


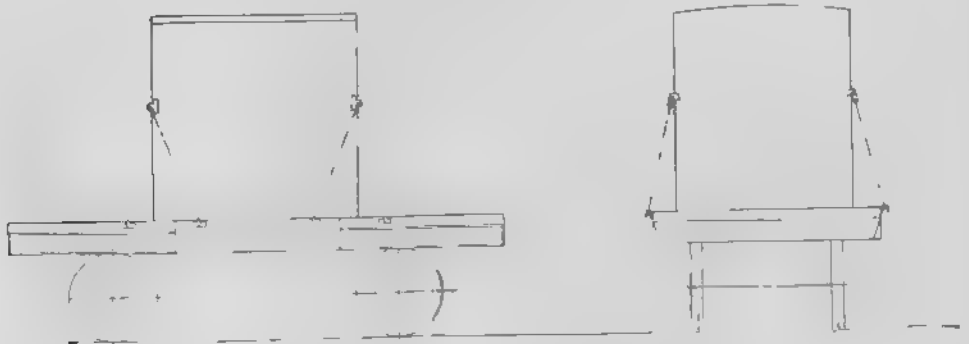
DIAGRAM 3



DIAGRAM 4

LOADING, ETC., OF MERCHANDISE TRAFFIC.
LOADING AND SECURING OF ENGINES, ETC.—Continued.

Diagram C.



METHOD OF LOADING 1 SMALL CONTAINER
4 BINDING CHAINS IN USE

Diagram D.

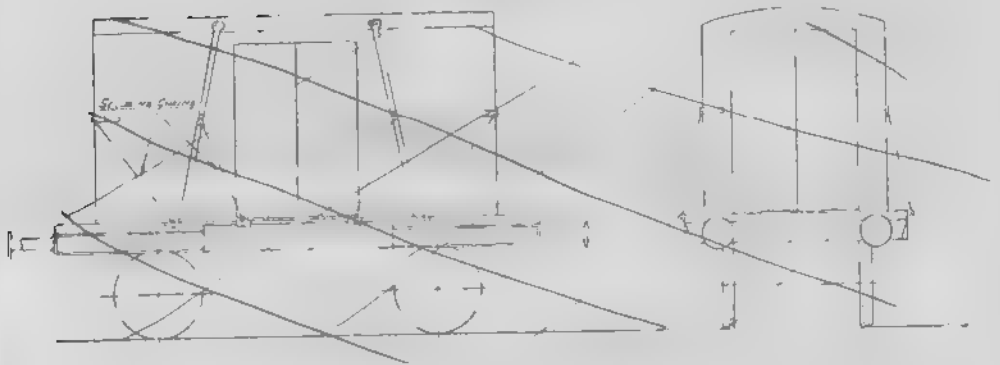
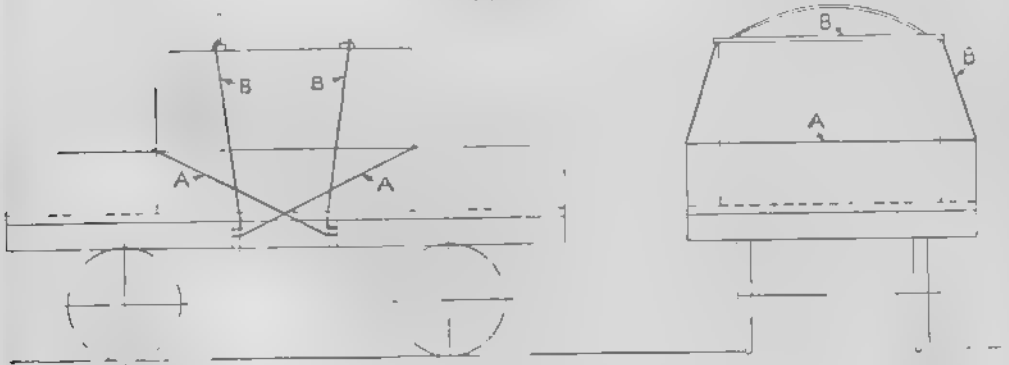


Diagram E



METHOD OF LOADING "C" & "D" TYPE OPEN CONTAINERS

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING AND SECURING OF ENGINES, ETC., *Continued*

To facilitate unloading at Receiving Stations, Lift Vans and Covered Containers must be loaded, where possible, in container wagons, unless it is definitely known that destination stations can, without difficulty, unload from wagons of other types.

Small Open Containers must not be loaded upon wagons without sides and ends, and where possible wagons with bodies not less than three planks deep should be used.

The majority of Great Western covered containers are provided with holding down loops, rope brackets or rings, and these must be used in preference to roping over the roof. The ropes must be placed through the brackets both front and rear, and then braced at the ends of the containers, in order to prevent movement of the containers and damage to the roof canvas.

Other Companies' instructions governing the loading of containers do not in all cases coincide with those of the Great Western Co. In such instances it will be the duty of the Guards, Yard Staffs, Examiners, etc., to satisfy themselves that the vehicle is safe to travel.

Furniture vans, Showmen's vans, and similar road vehicles on their own wheels, whether loaded or empty, must have the wheels fastened to the trucks by ropes, straps or chains, and, in addition, the leading end of the van bodies must be secured to each side of the railway trucks by ropes passed over the cross-bar underneath the van bodies (generally known as the body bolster bar), the ropes being afterwards tightly braced. Particular care is necessary in dealing with Furniture Vans and other vans on springs to comply with the maximum load gauge, as when travelling empty the height of such vans is greater than when travelling loaded. There is no necessity for the wheels of Furniture vans, etc., to be scotched when loaded on Carriage Track.

Roof boards of all Furniture Vans must be firmly fixed on their hinges, and the pins at either end used to keep the boards secure in their proper position.

The staff engaged in loading or unloading Lift Vans must satisfy themselves that all lifting eyes, binding straps and ropes are in a satisfactory condition.

The provisions of Rule 158 must also be complied with. These regulations also apply to similar traffic received from other Companies' Lines.

LOADING OF STEAM ROLLERS AND TRACTION ENGINES.

The following instructions must be observed:—

Loading.

- 10-ton "Loriot K" to carry rollers and engines not exceeding 6 tons.
- 12-ton "Loriot" to carry rollers and engines not exceeding 8 tons.
- 15-ton "Loriot" to carry rollers and engines not exceeding 10 tons.
- 20-ton "Loriot" to carry rollers and engines not exceeding 16 tons.
- 25-ton "Loriot" to carry rollers and engines not exceeding 18 tons.
- 30-ton "Re-tanks" to carry rollers and engines exceeding 10 tons.

Subject to examination by the Chief Mechanical Engineer's Inspector before dispatch, rollers and engines not exceeding 12 tons in weight may be loaded on the following 15-ton "Loriot":—

- "Loriot D" Nos. 42138 to 42153.
- "Loriot E" Nos. 42154 to 42164, 42202 to 42217.

NOTE.—The wheel base of the roller or engine must not exceed the length of the well of the vehicle upon which it is loaded.

In the event of the standard load gauge dimensions being exceeded, full particulars must be furnished to the District Goods Manager, in accordance with the standing instructions.

Securing Load.

Scotches must be fixed at the front and rear of the back wheels. The front wheels should not be scotched fore and aft, but should be substantially packed with 10 in. by 5 in. sleepers at the sides only.

The method of securing with binding chains, illustrated below, must be employed.

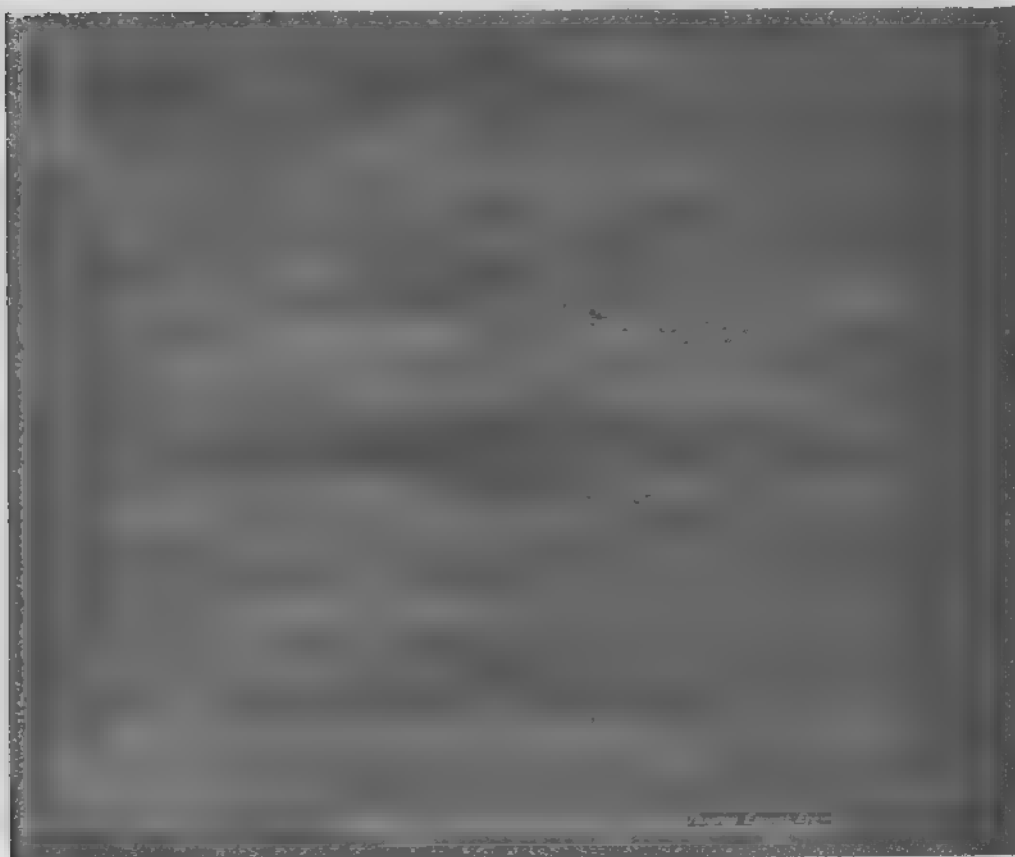
The brakes of the rollers must be put on tightly before transit.

Securing Railway Vehicle.

Before unloading or loading a steam road roller or traction engine, the Foreman or person in charge must see that the brakes of the railway vehicle are securely applied and that scotches are placed behind all wheels of the vehicle.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

OLD TYPE ROLLERS, ALSO TRACTION ENGINES.



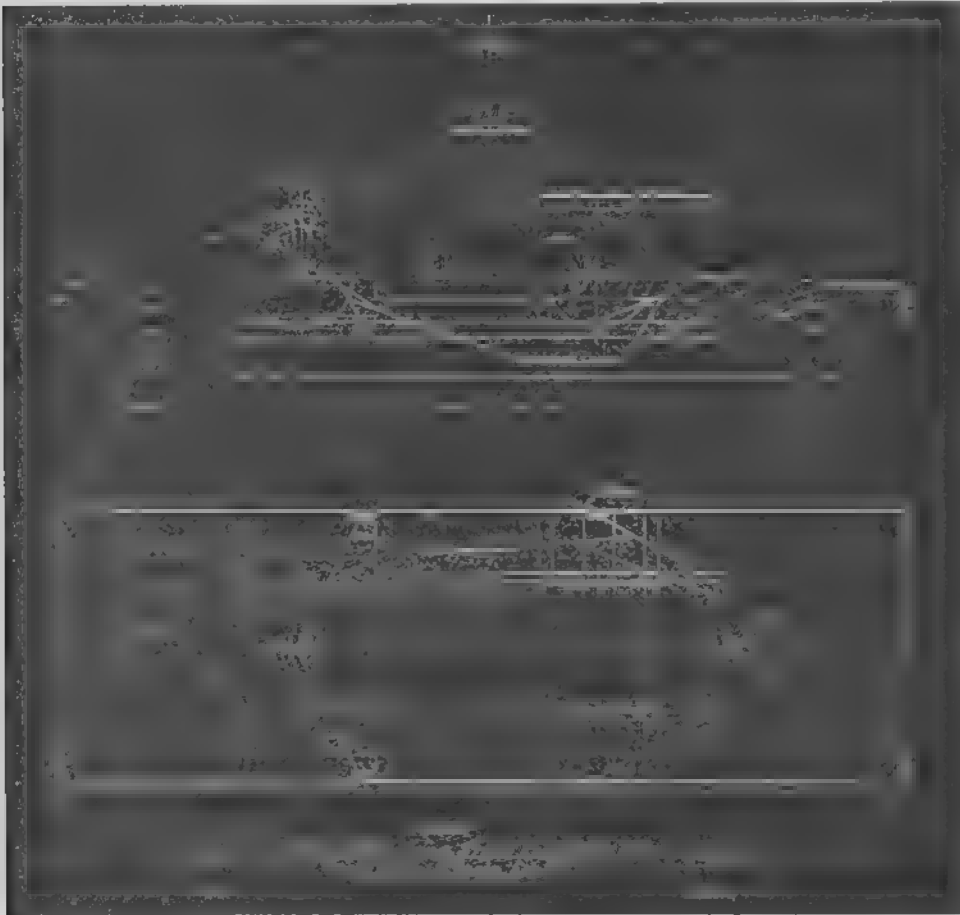
On no account may the chains be placed or bound over the front forks.

Driving or fly wheels must be thoroughly secured by ropes and specially examined to ensure that they are securely keyed on the shaft. If the fly wheel is the solid type and contains no holes through which a rope may be passed, strips of timber should be placed across it, one strip on each side, these to be securely roped or bolted together and lashed back to the body of the machine.

(G.A.3. 12/37. E.75868/H)

LOADING, ETC., OF MERCHANDISE TRAFFIC

NEW TYPE ROLLERS, WITH BOTH ROLLERS THE SAME SIZE, AND CLOSED
AT ENDS.



LOADING, ETC., OF MERCHANDISE TRAFFIC

LOADING AND SECURING OF WINES AND SPIRITS IN PIPES, BUTTS, PUNCHEONS,
AND SMALLER CASKS, ALSO BEER AND OIL, ETC., IN CASKS OR BARRELS.

Wines and Spirits in Pipes, Butts and Puncheons must be loaded on the barge lengthwise in the wagon on rope rings of sufficient circumference and depth to permit of the casks being well nested and prevent contact with the floor of the truck, the pipes, &c., being securely roped to the wagon in such a way as completely to encircle each cask at both ends - see Diagrams Nos. 1 and 2. The use of straw rings must be discontinued.

Diagram No. 1.



Diagram No. 2.



LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF WINES AND SPIRITS, ETC. *Continued.*

Wines and Spirits in Hogsheads and Smaller Sized Casks must be loaded on end and secured by suitable means within the space they occupy in the wagon if other traffic is not available to fill the vehicle.

Trucks with sides less than 21 inches high must not be used.

The use of wagons with single lever, or ratchet, or headstock brakes is prohibited for Wines and Spirits in Pipes, etc.

Beer. Casks must be loaded on end and secured by suitable means within the space they occupy in the wagon if other traffic is not available to fill the vehicle.

Petroleum and other Mineral Oils. Casks must be loaded on end and secured by suitable means within the space they occupy in the wagon if other traffic is not available to fill the vehicle.

Pipes, Butts, etc. Casks must be loaded on end and secured by suitable means within the space they occupy in the wagon if other traffic is not available to fill the vehicle.

The bungs of all Pipes, Butts, Casks, etc., loaded on the bilge must be placed uppermost.

N.B.—All casks having a worn or bulged appearance must be loaded on the bilge.

Scotchies. Bags must be loaded on end and secured by suitable means within the space they occupy in the wagon if other traffic is not available to fill the vehicle. Wads of twisted straw must be jammed firmly under the casks.

Scotchies must be loaded on end and secured by suitable means within the space they occupy in the wagon if other traffic is not available to fill the vehicle.

LOADING OF GRAIN, FLOUR AND OTHER SIMILAR TRAFFIC

All traffic must be loaded on end and secured by suitable means within the space they occupy in the wagon if other traffic is not available to fill the vehicle. The following instructions and diagrams must be adhered to:

Wagons having sides 31 and 39 inches in height, i.e. 7 and 5 plank Wagons, must be loaded with Grain, Flour and similar heavy traffic in accordance with Diagrams 1 and 2. Light traffic such as Bran, Sharps, Dried Grains, Pollards, Offals, etc., may be loaded in these Wagons in accordance with Diagrams 3, 4, 6, 7, 9 and 10.

The method of loading to be adopted when Wagons which have sides of 28 inches in height, i.e. 4 plank Wagons, must be loaded in accordance with Diagram No. 5.

When Wagons with sides of 21 inches, i.e. 3 plank Wagons are used, the sacks are to be loaded as shown in Diagrams Nos. 6 and 7. If it be found impracticable to load in accordance with Diagrams Nos. 6 and 7, the sacks may be loaded as shown in Diagram No. 8.

Wagons having sides less than 21 inches in height, i.e. 1 plank Wagons must be loaded in accordance with Diagrams Nos. 9 and 10.

Whenever a Truck of Grain, Flour, etc., is not loaded in accordance with these instructions, the Agent at the Receiving Station must report the fact to his District Goods Manager.

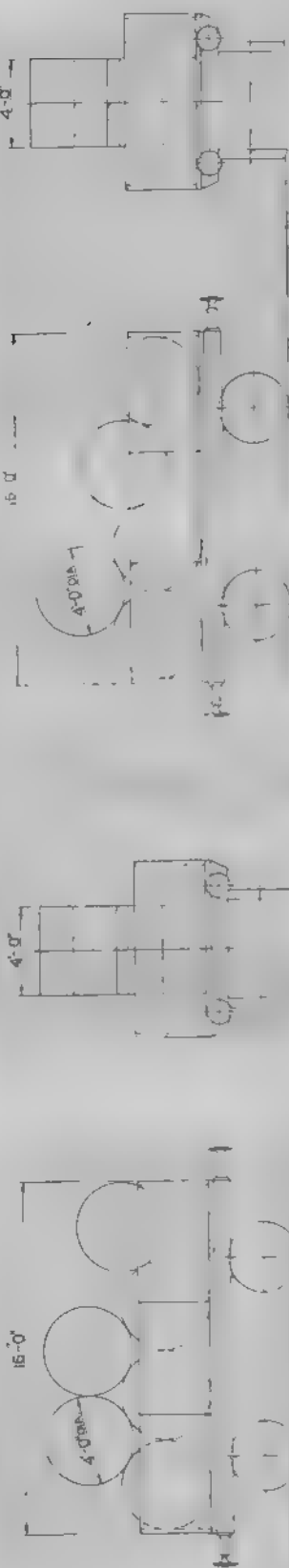
In the case of Diagrams Nos. 2, 5 or 8, where top loading upon sacks on end is permitted with Trucks having sides 39, 24 or 21 inches high, the top load must be placed towards the centre of the Truck, AND THE SACKS MUST NOT EXTEND FURTHER THAN COVERING THE THIRD ROW OF SACKS FROM EACH END.

—TIERCES OF TOBACCO LOADED IN CHAIN WAGON—
—TIERCES TO BE PROPERLY SHEETED AND ROPED—

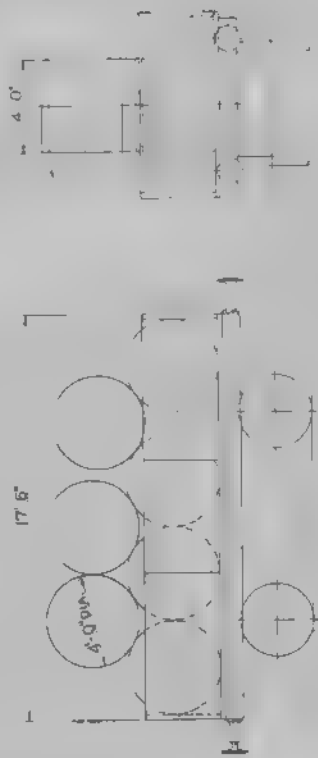
THE FOLLOWING TO BE INSERTED OPPOSITE PAGE 2302-

—TIERCES OF TOBACCO LOADED IN OPEN WAGONS—

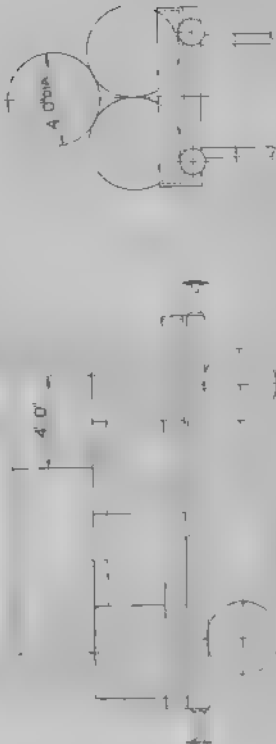
—TIERCES TO BE PROPERLY SHEETED AND ROPED.—



—5 TIERCES OF TOBACCO LOADED IN 16'-0" OPEN GOODS WAGON—



—6 TIERCES OF TOBACCO LOADED IN 17'-0" OPEN GOODS WAGON—



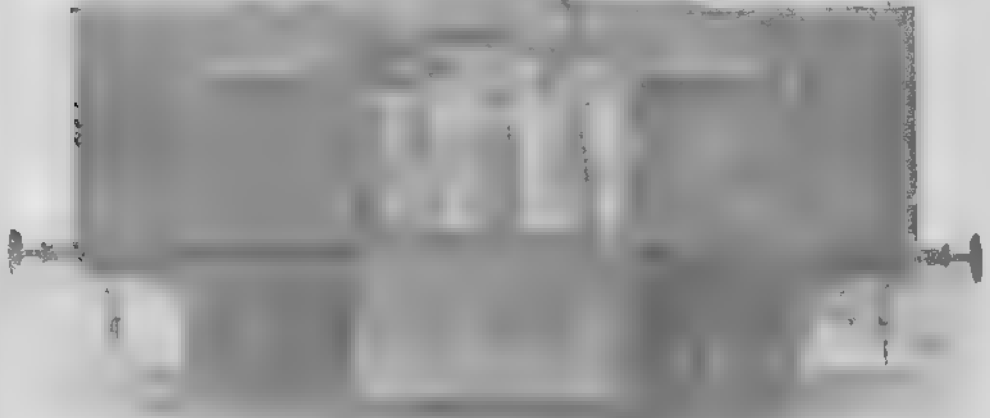
—7 TIERCES OF TOBACCO LOADED IN 18'-0" OPEN GOODS WAGON—

LOADING, ETC., OF MERCHANDISE TRAFFIC

LOADING OF GRAIN, FLOUR, ETC.—*Continued.*

Diagram No. 1 and 2 showing the method of loading sacks of Grain, Flour, &c., should be loaded when seven or five plank wagons are used.

Diagram No. 1.



Additional sacks may be placed lengthwise in the centre of the load.

Diagram No. 2.



The number of sacks which may be loaded in a 7 or 5 plank Wagon varies according to the description of traffic. BUT IN NO CIRCUMSTANCES MUST SUCH A NUMBER BE PLACED IN THE WAGON AS WILL BRING THE LOAD BEYOND THE CARRYING CAPACITY.

LOADING, ETC., OF MERCHANDISE TRAFFIC:

LOADING OF GRAIN, FLOUR, ETC.—*Continued.*

Diagrams Nos. 3, 4 and 5 showing the mode when four plank wagons are used.

Diagram No. 3.

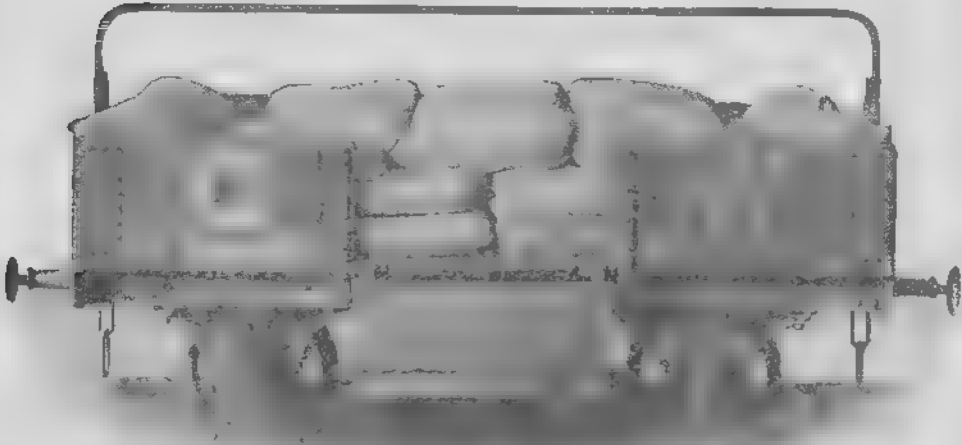
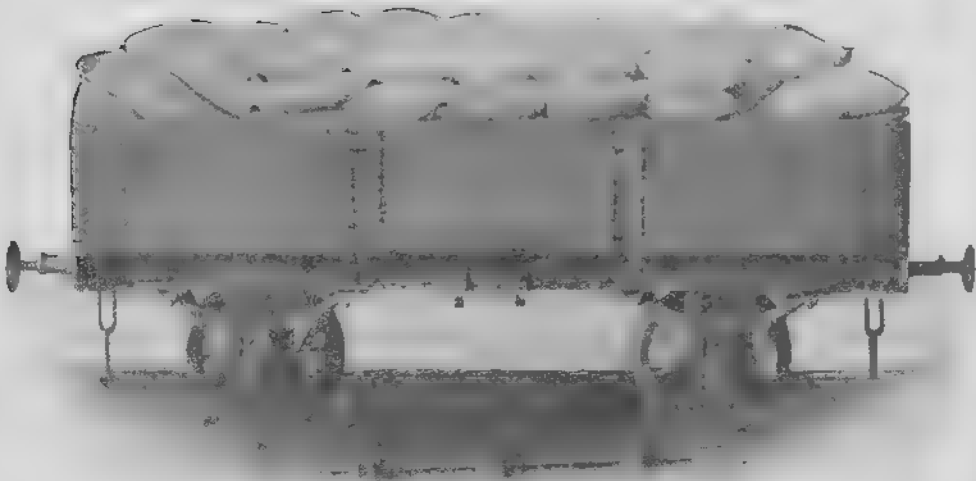


Diagram No. 4.



Two layers of sacks on their flat must be placed over the entire floor, and the third and subsequent tiers stowed in the same way as the first and subsequent tiers in Diagrams 3 and 4. The sides of the wagon must always be several inches above the level of the second layer of sacks in order to give the necessary cant inwards to the third tier.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF GRAIN, FLOUR, ETC. *Continued.*

Diagram No. 5.



The top of the load must be built as tightly as possible with sacks on end upon which may be placed a top load of NOT MORE THAN 14 SACKS.

Diagrams Nos. 6, 7 and 8 showing the mode when three plank wagons are used.

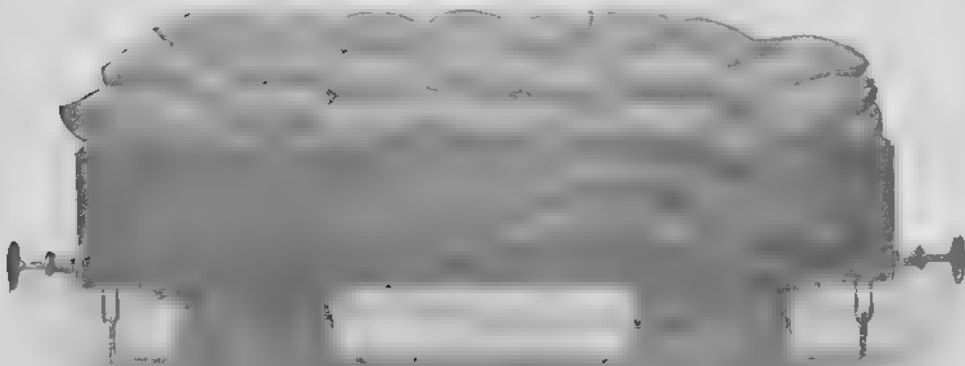
Diagram No. 6.



LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF GRAIN, FLOUR, ETC. *Continued.*

Diagram No. 7.



The entire floor of the Truck is to be first stowed with a layer of sacks on their flat, the second and subsequent layer being added above the first layer of sacks, and so on.

Diagram No. 8.



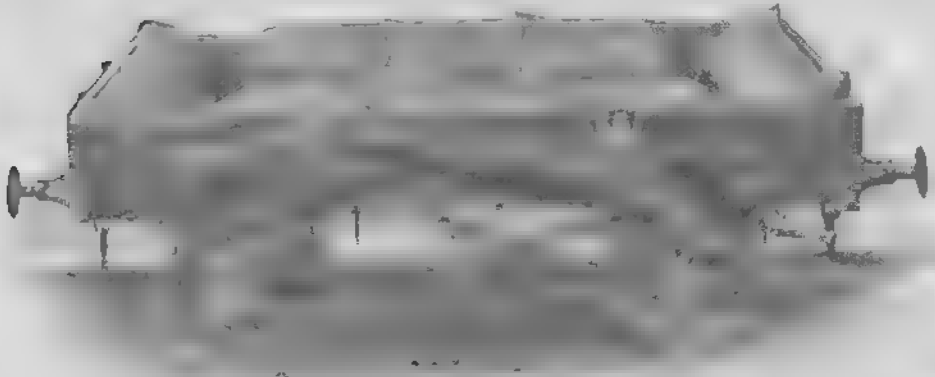
The body of the Truck must be filled as high as possible with sacks on end, upon which may be placed a top load of NOT MORE THAN 14 SACKS.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF GRAIN, FLOUR, ETC. —Continued.

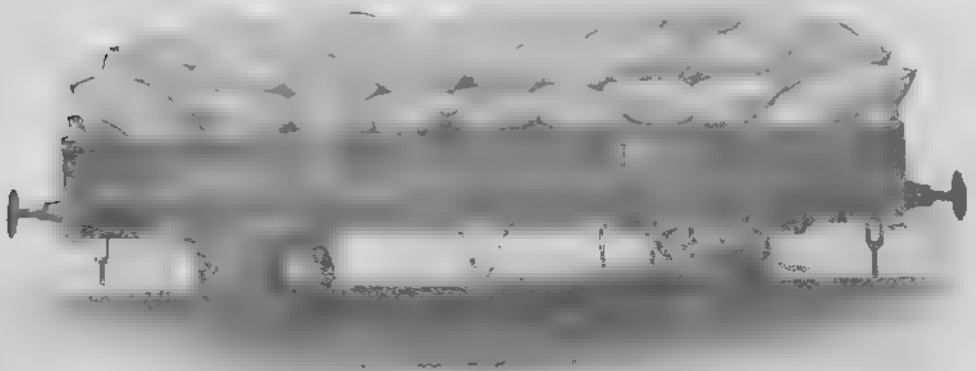
Diagrams Nos. 9 and 10 showing the mode when one plank wagon is used

Diagram No. 9.



In the first tier (i.e. on the floor of the Truck) the sacks are loaded against the sides and ends only, leaving a space of 2 ft. between the sides of the sacks and the sides of the wagon, and a space of 1 ft. between the ends of the sacks and the ends of the wagon. The second tier is loaded with the sacks placed lengthwise across the wagon, the ends of the sacks resting against the sides of the wagon, and the ends of the sacks resting against the ends of the wagon and the ends of the first tier.

Diagram No. 10.



In the second and third tiers, the sacks at the end of the Wagon are placed lengthwise; and those in the middle of the load are placed across the Wagon, and the fourth tier consists of sacks placed across the Wagon in the middle of the load.

In these diagrams, the sacks are shown as being loaded against the sides of the Wagon, as in Diagram 9, and the sacks are shown as being loaded against the ends of the Wagon and the ends of the first tier, as in Diagram 10.

NOTE.—The diagrams are intended to show the number of sacks to be placed in the trucks, and the order in which they are to be placed, for the guidance of loaders.

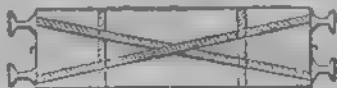
LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF HAY AND STRAW.

The existing instructions under this heading to be cancelled and the following substituted.

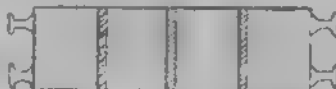
LOADING OF HAND-PRESSED HAY AND STRAW.

1. Hay and Straw in hand pressed trusses must always be sheeted and roped—as many sheets and ropes to be used as may be requisite in order to effectually cover and secure the load.
2. In loading it is desirable to somewhat reduce the width of each successive layer of trusses placed in a truck, thus throwing the center of gravity into the middle of the load, and obviating the possibility of outside trusses becoming displaced. The trusses or bales should also interlock each other as far as practicable.
3. In loading Hay, additional security is given to the load by tying together the end trusses at the various layers with string or haybands.
4. The roping of the traffic must be performed as follows:
Hay—ropes to be placed over the sheets, twice across the load and twice diagonally thus:



and to be drawn as tightly as possible and firmly secured. For each diagonally two ropes must be used to avoid passing the rope from under to over the load across the end of the wagon and making contact with the drawhook.

Straw—ropes to be placed over the sheets three times across the loads only thus:



and to be drawn as tightly as possible and firmly secured. Straw has a tendency to settle down in the truck after loading and when for any reason the load sometimes remain on hand at the forwarding station, the ropes should be tightened before the loads leave.

5. Trucks with "iron" floors must not be used for the conveyance of Hay and Straw when this can be avoided, but, if it is absolutely necessary to use them, no portion of the load must project beyond or rest upon the ends of the trucks.

6. Each truck of Hay and Straw must be carefully examined, prior to despatch, by a competent person at the sending station, and it must not be sent forward unless it is securely loaded, sheeted and roped. Guards and also Yard Inspectors, Shunter and others concerned at Loading Stations must examine trucks of Hay and Straw in transit, and see that a not securely loaded, sheeted and roped are put off for a re-stment.

7. Hay and Straw received in other Companies' lines, and not loaded and roped strictly in accordance with the above diagrams, may be allowed to travel if securely loaded and roped

in accordance with the above diagrams, may be allowed to travel if securely loaded and roped.

LOADING OF ESPARTO IN BALES, AND SIMILAR TRAFFIC.

Trucks containing bales of Esparto, Moss Litter, Pulp or Hemp, and traffic of a similar character, loaded higher than the sides of trucks, must be roped. Station Masters and Goods Agents must use discretion as to sheeting during dry weather as a protection against fire.

LOADING OF RAGS OR WASTE PAPER.

Trucks containing Rags or Waste Paper in Bags, Bales or Bundles must be sheeted and securely roped over the sheets. The class of truck best adapted for the conveyance of the traffic must be used, and under no circumstances is a larger complement to be loaded upon a truck than it is capable of conveying without risk of displacement while running, or during shunting operations.

DEFECTIVE FLOORS OF RAILWAY COMPANIES' OR PRIVATE OWNERS' WAGONS.

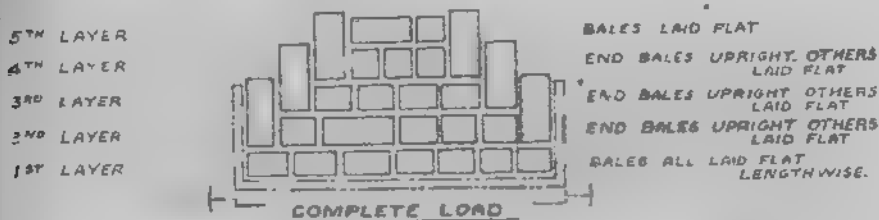
Careful examination must be made of floors of all wagons selected for the loading of Steel Billets, Pig Iron or similar traffic, whether at Stations or in private sidings. The attention of the Chief Mechanical Engineer's Department should be called to any wagon the floor of which may be found to be in defective condition, and steps taken to prevent it being loaded or sent away empty, unless to a repairing depot, until the defects have been attended to.

GOODS DAMAGED BY PETROLEUM, CREOSOTE, &c.

Grain, Flour, or other traffic likely to sustain damage must not be loaded in trucks which bear evidence of having been used for Petroleum, Paraffin, Creosoted Goods, or other similar traffic.

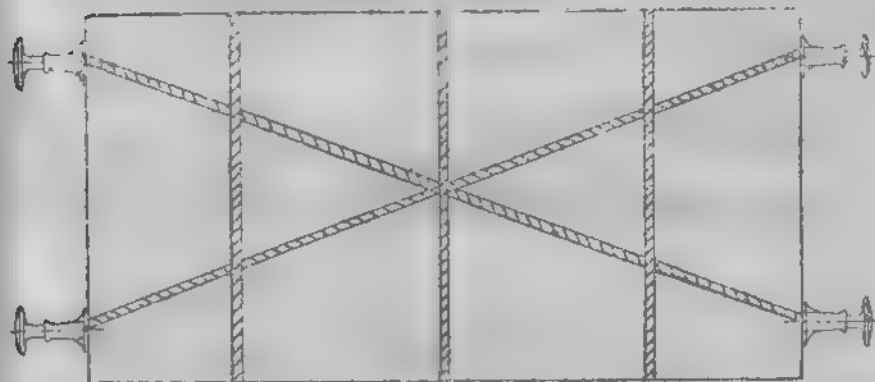
LOADING OF MACHINE-PRESSED HAY AND STRAW.

1. Machine-pressed baled Hay and Straw in top and or more high-sided common user wagons to be loaded in accordance with the following diagram:



2. This traffic must be sheeted and roped in accordance with the following diagram

METHOD OF ROPING.



Ropes to be placed over the sheets, twice drawn over from and round buffer to buffers of wagon and then three times across from side to side over load over diagonal ropes. Ropes to be drawn together at each point of security—firm roping essential.

For roping diagonally, two ropes must be used to avoid passing the rope from buffer to buffer across the end of the wagon and making contact with the drawhook.

3. Wagons with rounded ends must not be used for the conveyance of Hay and Straw when this can be avoided, but it is absolutely necessary to use them, special care must be taken to ensure that diagonal ropes are secured in such a manner that they cannot become loose during transit.

4. Each truck of Hay and Straw must be carefully examined prior to despatch by a competent person at the sending station, and it must not be sent for transit unless securely loaded, sheeted and roped. Guards, and also Yard Inspectors, Shunters and other persons concerned at Junction Stations must examine trucks of Hay and Straw in transit, and see that any not securely loaded, sheeted and roped are put off for adjustment.

5. Hay and Straw received from other Companies' lines, although not loaded and roped strictly in accordance with the above diagrams may be allowed to travel if securely loaded and roped.

(GA. 18. 11/47. C.G.M.WT/X. 81581.)

LOADING OF DRUMS.

Drums, loaded or otherwise, must not be loaded with more than one third of their diameter or one third of their length loaded upright above the sides of any wagon and those on the top layer which are next to the end of the wagon must be loaded longitudinally so that there is no risk of the drums rolling over the end of the wagon.

G.A.16. 5/46. C.G.M. W.T./X. 89722)

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LOADING, ETC., OF MERCHANDISE TRAFFIC.

PACKAGES CONTAINING POISONOUS GOODS.

Packages containing Poisonous Goods must not be accepted for transit if found to be leaking or otherwise in a loose or bad condition.

In the event of leakage occurring before or during the journey they must be immediately removed together with any articles that may have been damaged by the leakage to a place where they will not interfere with other traffic, and the circumstances must be promptly reported to the District Goods Manager.

Goods of a poisonous nature, such as a Wolf Killer Sheep Dip &c., must not be loaded in trucks containing other goods in bulk, unless placed in one of the special vehicles provided for the purpose, and kept on a platform in positions where they will not interfere with other traffic.

CONVEYANCE OF ETHYL FLUID THE ANGLO-AMERICAN OIL COMPANY.

Ethyl Fluid in drums with screwed metal bungs and marked "E.O.C." may be accepted for conveyance, provided the drums bear the Dangerous Goods and Poisonous Chemicals "A" label and the Classification of Dangerous Goods by Merchandise Traffic, and a full note at bottom of the contents, and the following notice:—

IMPORTANT NOTICE.

ETHYL FLUID—POISONOUS.

Do not let the contents of this package come into contact with your hands, body or clothes. If it does so by accident, deal with it at once as follows:—

Remove any clothes affected, and do not wear them again until they have been thoroughly washed with soap and water.

Wash the hands or affected part of the body at once with Petrol or Kerosene Oil, then with soap and water.

The loading regulations applicable to traffic bearing the special "A" classification must be strictly carried out and care exercised in handling the traffic.

Ethyl Fluid is a highly inflammable liquid. It will ignite when mixed with petrol, and before being mixed with the petrol.

LOADING OF EMPTY MINERAL OIL CASKS, DRUMS, CARBOYS, &c.

Empty Petroleum or other Mineral Oil Cask, or any other Cask or Package when fully charged or otherwise, may be taken to have a final, must not be loaded in a box wagon, and any violation of this order must be reported at once to the District Goods Manager.

WAGONS LOADED WITH EXPLOSIVES OR DANGEROUS GOODS.

Traffic is allowed upon arrival at the evening station, and be kept in a separate yard from other Wagon traffic, and if the condition of the wagon is such that it is not suitable for traffic, the District Goods Manager must be notified. No traffic of this kind is to be carried in any of the Company's sidings or warehouses.

CONVEYANCE OF RETURNED EMPTIES (AS PROVIDED IN THE YELLOW-COLOURED PAMPHLET ISSUED WITH THE GENERAL RAILWAY CLASSIFICATION WHICH HAVE CONTAINED INFLAMMABLE LIQUIDS, &c., OR DANGEROUS, CORROSIVE OR POISONOUS CHEMICALS.

Returned empty Carboys, Casks, Drums, or other containers, containing dangerous, corrosive, or poisonous materials, must not be accepted for conveyance unless they are sent by rail to the District Goods Manager. Only return empties, as above, must not be accepted for conveyance unless they are sent by rail to the District Goods Manager. Where it is so provided in the General Railway Classification, these empties when taken to the Company must, in addition, be accompanied by the special consent note provided for such traffic.

CONVEYANCE OF GAS PURIFYING REFUSE, ETC.

A number of hopper vehicles have been allocated for the conveyance of Gas Purifying Refuse, Green Gas, and other traffic of a similarly objectionable nature. Should an emergency arise, an application for the conveyance of such traffic for the supply of these wagons should be made to the Chief Goods Manager.

Ordinary Traffic Wagons must not be used without authority.

TRAVELLING GAS TANKS.

Rough shunting of travelling gas tanks causes the tank to sift on the bolsters and the joints of the fittings to become sprung, resulting in leakages. All concerned are cautioned to use more than ordinary care in handling these tanks, as the consequences of gas escaping and becoming ignited might be very serious.

Under no circumstances whatever may gas tanks be worked through the Severn Tunnel, neither Under no circumstances may gas tanks be sent on trains carrying "A" headlamps.

(G.A. 10. 3/42. LK1/6185/2.)

LOADING, ETC., OF MERCHANDISE TRAFFIC.

EXAMINATION AND SECURITY OF LIDS OF TANK WAGONS CONTAINING ACIDS

To obviate injury and damage to clothing by the splashing of acid from Tank Wagons, the lids must be properly secured.

Before Tank Wagons are removed from the siding, the lids must be properly fastened by any means available. If any leakage is observed, the lids must be secured by Guards, Shunters, or others, they must direct the attention of the senders to the fact, and the lids may be made secure before the wagons are moved.

REMOVAL OF LITTER, &c., FROM TRAFFIC WAGONS.

All Railway Companies' wagons, after unloading, must be properly swept out, or otherwise cleaned, as may be necessary. All wagons (whether Railway Companies' or Private Owners') must, after unloading, be cleared of straw or other ignitable matter.

Numerous cases of fire, resulting in damage to the Company's stock, have been reported, the outbreaks undoubtedly being due to hay or straw sweepings, or material used for packing damageable goods, such as pipes, &c., being left in the bottom of the vehicles after unloading.

Before wagons are despatched in which straw or other similar packing is used, Station Masters and others concerned must see that all unnecessary packing on top of the contents of the wagon is removed.

SHEETING OF LOADED WAGONS OF PIPES, &c., PACKED WITH IGNITIBLE MATERIAL.

Cases of fire have been reported in respect of open Wagons, loaded with such traffic as pipes, &c., where heather and similar packing has been used, and the vehicles have been despatched un-sheeted. It is therefore imperative that all such wagons, if packed with material of an ignitable character,

GUNPOWDER VAN BOOTS AND LOCKS AND KEYS.

The Company's Gunpowder Vans are equipped with a pair of Gunpowder Boots, which must always, when not in use, be suspended from the hook provided for the purpose on the doors of the van, and the boots are to be returned in the van.

Should a pair of boots become separated from the van to which it is allotted, it must be immediately sent to the Stores Superintendent, Swindon, and an advice also sent to the Chief Goods Manager, Paddington.

The whole of the Gunpowder Vans are fitted with the standard lock in use by all Railway Companies, and the keys are to be sent to the nearest Goods Manager to send the keys of Gunpowder Vans to the receiving station.

At the stations where Gunpowder, &c., is regularly dealt with keys are provided, which will lock or unlock any Gunpowder Vans received (either G.W. or vans belonging to other Companies), and additional keys are in possession of the District Goods Managers. In the event of a consignment of explosives arriving at a station not provided with a key the District Goods Manager should be telegraphed at once.

Gunpowder Vans found only to have defective locks, and not in need of any other repairs, must not be sent to a repair depot. An application for a new lock should be made to the nearest of the under-mentioned Carriage and Wagon Depots, and the lock should be sent to the nearest of the under-mentioned stations to substitute a new one:

Old Oak Common.	Exeter.	Gloucester.
Reading.	Newport.	Hereford.
Bristol.	Cardiff.	Worcester.
Swindon.	Barry.	Wolverhampton.
Newton Abbot.	Cathays Yard.	Oswestry.
	North.	

DISCHARGING PETROLEUM FROM TANK CARS AT OIL AND SPIRIT DEPOTS.

Foremen and Shunters must avoid moving the Tank Cars while being discharged, and instructions have been given to the Oil Companies that a conspicuous red flag must be hoisted in a prominent position over the rail tank car to indicate that discharging is in progress. An Oil Company's representative must also be in attendance when discharging, to see that the pipe is disconnected and removed from the Tank Car.

(G.A. 18. 11/47. L.K. 1/8696/16.) Oil

Company's representative in charge must be required to first REMOVE THE DISCHARGE PIPE and give his assurance that the Tank Car can be moved with safety.

Filling and emptying of Tank Cars must be performed only during daylight.

TRANSIT OF PADLOCK KEYS.

Padlock keys sent to destination stations for unlocking covered vans containing general merchandise and ~~keys~~ for return to originating points must be sent forward in a sealed envelope by the "Booked Value" arrangements.

TRANSIT OF PADLOCKS.

Padlocks sent from one station to another should be locked together hasp to hasp and forwarded by the "Booked Value" arrangements using the special tie on label No. 2670-S which has been issued for this specific purpose.

Padlocks are not subject to common user arrangements and must be returned to the originating point without delay.

In no circumstances must locks and keys be returned together in one package.

(G.A.16, 5/46, F.2, 84270 P.)

SECURING OF CHAINS AND ROPES ON TIMBER, ETC., WAGONS—Page 237

The following to be substituted for the 1st paragraph of the instructions appearing under this heading :—

Wagons received from other Regions at junctions must not be refilled, neither should they be delayed on the grounds that the above instructions are not complied with, provided the chains, ropes or other appliances are so fastened as to render them safe to travel.

(G A 23—7 49. L.K.1/7549 Gen.)

Private Owners' wagons must not be used for overhanging traffic except in cases where Colliery wagons are required to be back loaded from Port to Colliery, when pit props may be conveyed

HANDLING OF WET PELT, HIDE AND SKIN TRAFFIC, &c.

HANDLING OF WET PELT, HIDE AND SKIN TRAFFIC, &c.

form of blood poisoning, unless the hands are protected.
To prevent this, gloves should be worn by all staff who are in contact with the blood of patients.
Aprons and gloves should be worn by all staff who are in contact with the blood of patients.
Managers and must be used by the staff handling such fluid
After use, gloves should be disposed of in a place where they are accessible to
to members of the staff requiring to use them.

[illegible]

(G.A.16, 5, 46, C.G.M., W.F. X, 1 106451)

but must be properly placed in the stanchion sockets.

[illegible]

There is but one solution to this problem: the use of a "load factor" which is always a fraction greater than unity. This factor is determined by the following equation:

Private Owners and Railway Companies' tip end wagons may be used for the conveyance of bars, plates, etc., in accordance with the instructions on page 211.

(G.A.12. 4/43. C.G.M.—W.T./X. 66224.)

other purpose. The Government has a right to use the information for any purpose, and any person who discloses the information to a third party may be liable for a civil penalty. The Government has a right to use the information for any purpose, and any person who discloses the information to a third party may be liable for a civil penalty.

1. Each article or package shall (except as hereinafter provided) bear:

- (a) The Consignee's full name and address in legible and durable characters, or
- (b) A legible and durable distinguishing mark with a label on, (the label and principal) placed on the outside the name of the station or place of destination, and the name and address of the Consignee.
- (c) In the case of a bill of lading, the name of the Consignee, together with the name of the station or place of destination, and the full name and address of the person to whose order the article or package is sent.
- (d) The name of the port or dock of shipment, and the name of the ship or shipping agent.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

ADDRESSING OF GOODS—Continued.

2. Where a consignment consisting of more than ten articles or packages of the same or of a similar description of merchandise is forwarded to the same consignee the following provisions may be adopted:—

Number of articles or packages, 11 to 100:—

Not less than one article or package in every five shall be (a) addressed in accordance with Clause 1, and (b) marked to show the total number of articles or packages forming the consignment, provided that a minimum number of ten articles or packages shall be addressed and marked as aforesaid in each consignment.

Over 100:—

Not less than one article or package in every ten shall be (a) addressed in accordance with Clause 1, and (b) marked to show the total number of articles or packages forming the consignment, provided that a minimum number of twenty articles or packages shall be addressed and marked as aforesaid in each consignment.

Provided that where it is not possible for the Trader to indicate the total number of packages forming a consignment to be despatched by him, each part of the consignment when delivered to the consignee must be addressed in accordance with this regulation as if the same were a separate consignment.

3. Subject to Clause 2, Metal Bars, Rods, Tubes, Plates, Sheets, Forgings, Castings, Chains and any other similar merchandise shall have the addressing particulars as provided in Clause 1 conspicuously shown in legible and durable characters:—

(a) On wooden, metal or other durable tallies fastened to the merchandise by wire or

(b) Painted, stencilled or otherwise legibly and durably specified on the merchandise.

Provided that Bars, Rods, Tubes and other articles which do not afford a suitable surface for painting or stencilling shall be securely bound into bundles convenient for handling, by wire, rope or other material to which the Company has given its approval in writing, and that such bundles shall have attached thereto tallies as provided by this condition.

4. Hides, Skins, Pelts or other merchandise carried loose shall have labels, or wooden, metal or other suitable tallies, affixed, and addressed in accordance with Clause 1 or 2 hereof.

5. Every label, tally, address or mark shall be securely fastened or affixed to the article or package.

6. All old or conflicting labels or addresses shall be removed or entirely obliterated before the article is tendered for carriage.

7. These regulations shall not apply to:—

(a) Returned empties, when legibly branded with the Owner's name and address.

(b) Merchandise for which the exclusive use of a wagon is provided by the Company.

(c) Articles identical in all respects, or packages of uniform description and size containing not less than two tons and upwards from one sender to one station or place of destination.

(d) Export and import merchandise conveyed in through trucks direct to ship, and vice versa.

(e) Merchandise forwarded to Ireland or the continent of Europe, which is carried subject to special addressing regulations.

8. The Railway Companies do not undertake to label consignments on behalf of Traders except in the following cases:—

(a) Goods stored with Railway Companies where it is impracticable for the Trader to arrange the necessary labelling.

(b) "Third Party" Transactions, where goods are collected by the Railway Companies from a Trader, and the original Consignor has no knowledge of the ultimate Consignee or destination.

(c) Goods sent to "Order" at destination station, where the Company is requested to remove the address labels and attach new labels giving the name and address of the ultimate Consignee.

(d) Goods stored in Public Warehouse, or Dock Warehouse, or on Quay, where the Railway Company holds the Bill of Lading for clearance.

9. When the labelling is performed by the Railway Company the following charges must be made for the service (except where other charges already apply):—

When label is supplied and addressed by Company } 1d per label Minimum 3d for each
When blank labels provided by the Trader are } day's forwardings.
addressed by Company

When address is written on } 1d per package Minimum 3d for each
goods by Company. } day's forwardings.

When address labels supplied by Traders are } 1d per label Minimum 1d for each
attached to goods. } day's forwardings.

In the event of the charges being paid by various Traders, the respective minima are to be maintained for each service.

Fractions of a penny must be charged as a penny when they amount to a half-penny or upwards, and fractions of less than a halfpenny must be dropped.

STATION AND PICK-UP TRUCKS.

Station Trucks.

The Yard staff at Junctions or Marshalling Yards are responsible for ensuring that all station trucks which have been received and are available to go forward are despatched by their scheduled train.

Guards must show on their journals when station trucks usually conveyed by such services are absent from the train.

Articles which from their number, weight or awkward nature are likely to cause delay to the train on which the station truck is conveyed must not be loaded on to station trucks, but be sent forward by other means, i.e., by being sent to some transfer point or in a direct wagon.

Care must be taken when loading station trucks that the goods are placed in accordance with the loading diagram provided for each station truck as to the exact order of unloading at the various stations on the journey. Guards are responsible for seeing that at intermediate stations goods are loaded in the proper wagons and are put out at the proper stations. In taking out the goods at intermediate stations the Guards must be careful to see that as far as possible they are securely placed in a safe position where they are not liable to fall or be damaged by wet or other means.

The number of packages for each destination loaded in station trucks must be entered on the station truck labels by the station staff in the wagon and this information must be added to as necessary at intermediate stations.

Guards must record in their journals any case where they find a truck is habitually running with a light load or which is not properly loaded, and this information must be added to as necessary at intermediate stations. The Divisional Superintendents must advise the District Goods Managers concerned of all cases reported on the journals of the Guards.

Pick-Up Trucks.

Guards must record on their journals any instances where pick-up wagons usually conveyed by certain services have not worked on these trains. The Divisional Superintendents must advise the District Goods Managers concerned of any such cases.

Invoices and Labels.

Except in certain cases (with which each Guard should make himself acquainted) where arrangements exist for Invoices to be despatched by Passenger Train, the Guards must see that they have Invoices for all the Wagons in their trains, and must report every instance in which this rule is departed from. They must use great care to prevent their being overcarried, or given out at any point other than the proper one. The Invoices should accompany the wagons.

Guards must see that Station and Pick-up Trucks which are in their trains are appropriately labelled.
(G.A. 5.-2/39, C.G.M.-W.T.25073.)

In order to avoid delay in transit, all wagon labels should be made out in block letters in *Thick Blacklead Pen*, and not in ink or copying-ink pencil, which become indecipherable in wet weather.

(G.A. 10. 3/42. E.78905/1.G.).

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LOADING, ETC., OF MERCHANDISE TRAFFIC.

ADDRESSING OF GOODS—Continued.

10. WHERE FOR PURPOSES OF TRADE SECRECY THE COMPANY IS REQUESTED TO HAVE THE ADDRESS LABELS REMOVED AT DESTINATION STATION, ARRANGEMENTS MUST BE MADE FOR THIS TO BE DONE BEFORE DELIVERY IS EFFECTED, AND CARE MUST BE TAKEN THAT THE POINT OF ORIGIN IS NOT IMPROPERLY DIVULGED.

11. ~~Labels for all goods sent by Railways Companies must be made out in block letters in Thick Blacklead Pen, and not in ink or copying-ink pencil, which become indecipherable in wet weather.~~

LABELLING OF WAGONS.

1. Wagon Labels.

Wagon labels except those containing Mineral traffic in train loads for one destination must be labelled on each side.

~~In order to avoid delay in transit all wagon labels should be made out in block letters in Thick Blacklead Pen, and not in ink or copying-ink pencil, which become indecipherable in wet weather.~~

2. Full Routes must be shown on Labels for Wagons going to other Companies' Lines.

Wagon labels must show the full route of the wagon, and the name of the destination must also be shown —

- (1) Junction of exchange between G.W.R. and intermediate Company;
- (2) Junction or Junctions (if any) between intermediate Companies; and
- (3) Junction between intermediate and receiving Companies

in accordance with the instructions in the Merchandise Route Book.

3. Labels from Wagons received Loaded to be examined and kept

Labels from Wagons received Loaded to be examined and kept with the invoices for the goods, and filed for reference.

Labels from Wagons received Loaded to be examined and kept with the invoices for the goods, and filed for reference.

LABELLING OF DEFECTIVE PRIVATELY OWNED WAGONS.

The following instructions must be followed by the Traffic Department in dealing with defective privately owned wagons.

1. When a defective privately owned wagon is received at a station, the Traffic Department must be notified at once by the Station Master.

2. The wagon must be moved to a siding or to a place where it can be repaired, and the Traffic Department must be notified at once by the Station Master.

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8. The wagon must be moved to a siding or to a place where it can be repaired, and the Traffic Department must be notified at once by the Station Master.

INVOICES AND STATION TRUCK AND PICK-UP LABELS.

Extraordinary instructions must be given to the Traffic Department where arrangements are made for the transport of goods by Station Trucks and Pick-up Trucks.

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LOADING, ETC., OF MERCHANDISE TRAFFIC.

COVERED VANS FOR STATION TRUCK OR PICK-UP TRUCK TRAFFIC.

Covered vans must, whenever practicable, be used.

SHEETS.

The Company make and maintain their own Wagon, Cartage and Station Sheets. The principal part of this work is performed at Worcester, and the Sheet Works Manager who has charge of the Making and Repairing of Sheets is at that Station.

The attention of all concerned is directed to the following Regulations:—

SHEETING OF TRAFFIC.

1. Every opportunity must be taken to examine sheets before they are utilised, and any discovered not waterproof must not be used to cover load but must be sent to the shops for attention.

2. Care must be taken, in sheeting wagons, that the loads do not project in such a way as to render the Sheets liable to be damaged on the journey.

3. When two Sheets are used to cover a load (and this must only be done when absolutely necessary) the Sheet placed on the end of the load nearest the Engine must overlap the other Sheet. The name of the owner and numbers of such Sheets, as well as of those placed under the loads, must be shown on both the wagon labels.

4. Goods must be so loaded as to ensure water draining off the sheet properly. A large number of wagons are fitted with the Sheet Supporter, and where such wagons are used the appliance must, whenever practicable, be placed in an upright position and the sheet carefully spread over it in order that the following sheeting may be entirely prevented. (See instructions on page 244 relating to the sheeting of wagons fitted with sheet-supporters.)

5. Every opportunity must be taken in sheeting Goods (particularly bulky traffic, such as Wool, Hay, Straw, Esparto, Hops, Hemp, Flax, &c.), to ensure protection against inclement weather or fire.

6. Sheets must be properly secured by all their strings to the Sheet fasteners specially provided for the purpose on the wagons, and not to the buffers, brake handles, springs, guard plates, vacuum release valve cords, or drawbars. In the event of any of the Sheet ties being missing, or at all for use they must be replaced by new ones, supplies of which can be ordered from the Stores Department.

Sheet ties must not be fastened to crossbars or catches of tip end doors of wagons.

CARE OF SHEETS AT STATIONS.

7. Before proceeding to unload any sheeted wagon, the sheet or sheets covering it must be taken off, folded so that the number or numbers shall be visible, and removed to a place of safety, and, where practicable, under cover, but in all cases clear of rail or road traffic.

Sheets must not be stored in wagons, and any surplus for which no disposal instructions are held must be sent to the Sheet Shops.

Care must be taken to see that sheets are not:—

- (i) Run over by road vehicles, wagons or engines.
- (ii) Dragged along the permanent way or road way.
- (iii) Left in such a position when a wagon is partially uncovered as to render them liable to damage if the truck is moved.
- (iv) Cut or torn by goods with sharp corners, or edges or projections when being placed over a load, during transit, or when the traffic is being uncovered at destination.
- (v) Tied to buffer rods or castings, brake handles, springs or drawbars.
- (vi) Used as packing.
- (vii) Sent to Private Sidings, Works, Collieries, Brickyards, &c., in otherwise empty wagons when not required for outward traffic.
- (viii) Allowed to remain in Private Sidings, Works, Collieries, Brickyards, &c., where they may be misused and damaged.
- (ix) Burnt by Acids or other Chemicals.
- (x) Misappropriated, or misused in any way.
- (xi) Damaged as a result of hollow sheeting owing to the weight of water which may accumulate in the hollows bringing the sheets into contact with cases, &c. which have sharp corners or edges or projections.

8. When traffic is dealt with in the open, and it is necessary to use a Sheet to protect it pending the completion of the wagon (or loading) operations, the Sheet must invariably be so secured to the wagon as to prevent it falling or being blown over the wagon side and trailing in the ground. Neglect in this particular must not occur, or serious loss will be entailed upon the Company.

9. Wagon Sheets are supplied solely for the purpose of covering goods handed to the Company for transit, and they must not be allowed to be used by the Traders for their own purposes, nor must they, except when absolutely necessary, and then only for a limited period, be used to cover goods in godown warehouses or open wharves at the Stations. If it is necessary to have Sheets on hand for this purpose, application is to be made to the District Goods Manager for "Station Sheets."

LOADING, ETC., OF MERCHANDISE TRAFFIC.

SHEETS—Continued.

SHEETS SENT OFF THE COMPANY'S PREMISES.

10. The numbers of sheets covering traffic sent into Sidings, Works and Collieries situate off the Company's line must be recorded and taken by the person appointed to look after the sidings to ensure the sheets being returned in due course, and in good condition. All cases of damage to and loss of sheets in Sidings, Works &c. must be reported, and the Sidings, Works, &c., must be visited frequently and regularly with the view to seeing that the Company's sheets are properly treated in every respect.

11. All sheets, whether actually damaged or not, should be sent periodically to the Sheet Shops to be overhauled.

The red figures on the Sheets indicate the date on which they are due back at the Sheet Shops for examination, and a special look-out must be kept to ensure that out-of-date sheets are not used for traffic purposes but are sent to the Sheet Shops immediately.

SHEETS DAMAGED OR LOST.

12. If a sheet be damaged during transit, the Guard, on arriving at the Station where the truck covered by the damaged sheet is put off, must call the attention of the Station Master or Goods Agent or other responsible person to the damage, informing him of the circumstances under which it was sustained. He must also give particulars of the occurrence upon his journal, so that extracts from such reports can be sent by the Divisional Superintendents to the Sheet Works Manager, Worcester. Serious notice will be taken of any failure on the part of Guards to carry out these Instructions.

13. The receipt of a sheet in a damaged condition must be immediately reported by the Station Master or Goods Agent at the upwards station to the Sheet Works Manager at Worcester, and also to the sending station. The report must state the train by which the sheet was received, the number of the wagon, the Guard's number, and, when practicable, his explanation of the damage. In the event of a sheet being destroyed or lost, the Station Master or Goods Agent must be strictly watched to ensure that a report is made for such destruction or loss. Any Servant of the Company detected in wilfully damaging a Sheet will be severely dealt with.

14. When Sheets, which are rendered unfit for use otherwise than by ordinary wear and tear, are sent for repairs, the invoices issued must, when possible, state when, where and how the damage occurred. The Station Masters or Goods Agents at the Stations where the Sheet Depots are situated must report to the Sheet Superintendent every case that comes under notice in which a sheet is not carried out.

COLLECTION OF SHEETS FROM EMPTY WAGONS, &c., AND DISPOSAL OF SPARE SHEETS.

15. Sheets passing in otherwise empty wagons, or received at stations in wagons en route to Wagon Repairing Depots, must be collected and disposed of in accordance with the Divisional Superintendent's or District Traffic Manager's Instructions. The "Sheet Trucks" running on various parts of the line must be fully utilised. On Branch lines, served only by mixed trains, sheets may be placed in the Guard's van if no open truck is available. Sheets used to cover Live Stock or Cattle Trucks must be promptly removed when the trucks are released.

16. Sheets not required for immediate use must be shewn as spare upon the daily return to the Divisional Superintendent's or District Traffic Manager's Department, and disposed of in accordance with his instructions. Sheets must not be kept on hand in anticipation of traffic. When sheets are loaded up in wagons and sent for local distribution in accordance with instructions, the total number of sheets in the truck must be shewn on the wagon labels.

DISPOSAL OF SPARE AND DEFECTIVE SHEETS TO SHEET SHOPS.

17. Spare sheets other than those required for distribution (i.e. sheets on hand not ordered elsewhere) by the Divisional Superintendent or District Traffic Manager the same day as they are shewn as spare, together with any defective sheets must be forwarded to the various Sheet Shops, indicated hereunder in the authorised daily Sheet Trucks or by other means specially provided for:—

No. 1 Area for Saltney Sheet Shop.

From stations north of a line drawn from Bryn Teify through Llandilo, Torpantau, Craven Arms, Tenbury Wells, Stourport, Brettell Lane, Lye, Henley-in-Arden to Lapworth (inclusive).

No. 2 Area for Worcester Sheet Shop.

From stations south of No. 1 Area and east of a line drawn from Prestegyn through Hay, Pontilas and down the Golden Valley line to Little Mill Junction, all inclusive, and north of and including the main line from Paddington embracing all London Depots to Llanwrnall via Didcot, Swindon and Gloucester but excluding the section between Wantage Road and Swindon.

No. 3 Area for Cathays Sheet Shop.

From stations in South Wales, i.e. south of No. 1 Area and west of No. 2 Area.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

SHEETS—Continued.

No. 4 Area for Bridgwater Sheet Shop.

From stations south of No. 2 Area and east of the River Severn.

18. Covered Goods Vans, except where specially authorised, must not be used for loading sheets, and all open wagons containing sheets sent from point to point for use, repairs, or any other purpose, must be sheeted.

NEW AND REDRESSED SHEETS.

19. New and redressed sheets must be opened immediately on receipt at stations and used without delay.

Observe to the lackiness of the sheet dressing serious damage done by action of this sheet before

STATION SHEETS.

20. Station Sheets must not be sent away from the station to which they belong nor be used for any other purpose than that for which they are supplied. Station Masters or Traffic Agents will be held personally responsible for the issue and maintenance of these sheets, which must be sent to the Worcester Sheet Depot for overhauling at least once each year.

SHEETING OF WAGONS FITTED WITH SHEET SUPPORTERS.

The following instructions must be observed:

(1) Each sheet must be provided with its full complement of ties. Care as the ties are passing they must be placed by the ones supplied which can be obtained from the Stores Department in the ordinary way.

(2) Each sheet must be placed entirely on the wagon and the sheet ties secured to the fasteners provided on the truck.

WAGONS AND SHEETS INFESTED BY WEEVIL.

Wagons and sheets infested by weevils must be sent to the Carriage and Wagon Works, Swindon where a sheet of orders for the labor is given and a fee is charged for the service. The order to be endorsed "W. VILA FOR CLEANING" and the advice of a sheet to be sent to the Chief Mechanical Engineer, Swindon.

Stations infested by weevils known to be infested with weevils must be sent to the "WEEVILY TRAFFIC" and the sheets must be sent to the Carriage & Wagon Works, SWINDON, FOR CLEANING.

ROPE SCOTCHES

Supplies of rope scotches are allocated to certain stations in each District.

Each scotch bears a round ferrule stamped with the letters "G.W.K.," number, and the name of the station to which it belongs.

Stations receiving these scotches must see that they are properly returned to the forwarding point and properly labelled.

When a large number of scotches is being returned to one station, it will suffice if one in five is labelled.

Stations to which these articles are periodically allocated must check their stock once a month to ensure that they have their full complement.

The number of any missing scotches must be immediately reported to the District Goods Manager concerned.

Any scotch branded for another station must be immediately returned to the station to which they belong, except where otherwise authorised.

Any scotch which is found to be without a ferrule must be reported and placed in the Sheet Department, Worcester.

CARTAGE ROPES FOR ROAD VEHICLES.

Ropes for use on Road Vehicles are supplied to the Stations by the Sheet Department at Worcester and must be ordered through the Stores requisition.

When application is made for new cartage ropes the dimensions with ferrules must be sent to the Sheet Depot, Worcester, and a note made on the order.

WEIGHING OF TRACTION ENGINES AND OTHER HEAVY TRAFFIC.

Traction Engines and other traffic which cannot be weighed at forwarding point must be sent to the nearest station en route, having a weighbridge of sufficient capacity, with strict instructions to weigh and advise, and it will be the duty of the person performing the weighing to see that the wagon is not sent forward if overloaded, or otherwise unfit to travel.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

REGULATIONS FOR THE CONVEYANCE OF PRIVATELY-OWNED LOCOMOTIVES RUNNING ON THEIR OWN WHEELS OVER G.W. OR OTHER COMPANIES' LINES page 245

The instructions under the above heading to be deleted and the following substituted —

REGULATIONS IN REGARD TO THE ACCEPTANCE AND CONVEYANCE OF PRIVATELY OWNED LOCOMOTIVES AND TRAVELLING CRANES RUNNING ON THEIR OWN WHEELS.

The following regulations must be observed by all concerned in the conveyance of this traffic. It should be understood that in these instructions the term privately-owned locomotive embraces privately-owned travelling cranes on their own wheels.

1. Examination.

(a) On receipt of the application, the Goods Agent or Station Master who deals with the Senders in regard to the transit of locomotives must communicate with his appropriate District Officer, who will arrange for an examination to be made by the District Motive Power Superintendent: as much time as possible must be allowed for this examination. It must be clearly notified whether the locomotive is to be worked "dead" or under its own power.

(b) The District Motive Power Superintendent will arrange for the necessary examination to be carried out, and as soon as this is completed, for an advice by telephone or telegram to be given to the appropriate District Officer as to whether the locomotive is fit or unfit to travel on its own wheels. After the examination has been carried out, a report form—Diagram and Certificate of Examination of Privately-owned Locomotives running on own Wheels—shall be filled in by the examiner and countersigned by a responsible member of the District Motive Power Superintendent's staff and sent to the appropriate District Officer.

(c) Before a steam locomotive is allowed to run under its own power, the Goods Agent or Station Master must inform the Superintendent whether the examination made by the Railways does not include the firebox and boiler.

(d) Before a locomotive is allowed to run "dead", the coupling rods, connecting rods and valve rods and electric straps, where fitted, should be secured, the pistons secured to prevent movement and, if the material removed is of a suitable size to be conveyed on the locomotive, it must, when, be properly secured, otherwise it will be necessary for a wagon to be attached to carry this material.

2. Specification.

Privately-owned locomotives must comply with the following:—

(i) The minimum clearance above rail level of any part of the locomotive, either between the wheels on outside and wheels must not be less than the clearances shown in the table below confined to a width of 3 ft. 6 in. on either side of the centre of track, except that the clearance may be down to rail level over a width of 2½ in. on either side of centre of each rail, as shown in sketch depicted on the Diagram and Certificate of Examination.

Any excess over this width of 3 ft. 6 in. to be within the loading gauges of the lines over which the locomotive is required to travel.

Minimum Underclearance	Between
Six inches	Centre Line of Track and 1 ft. 4 in. either side.
Five "	1 ft. 4 in. and 2 ft. 1½ in. either side.
Four "	2 ft. 1½ in. and 2 ft. 2½ in. either side.
Six "	2 ft. 8½ in. and 3 ft. 6 in. either side.

EXCEPTIONS.

Loftus & Whitby Line (N.E. Region)

Nine inches 2 ft. 8½ in. and 3 ft. 6 in. either side.

Manchester and Bury Line and Tottington Branch (L.M. Region)

Six inches 2 ft. 8½ in. and 3 ft. 2 in. either side.

Ten " 3 ft. 2 in. and 3 ft. 6 in. either side.

(ii) The height of centre of buffers above rails should be not more than 3 ft. 6 in. nor less than 3 ft. 4 in. unless the examiner considers the buffer faces are sufficiently large to prevent locking.

"Dead" buffered locomotives may be accepted for conveyance over all Regions; such locomotives must be marshalled between spring-buffered vehicles.

The height of the centre of drawgear above rails should be not more than 3 ft. 6 in. nor less than 3 ft. 2 in.

(iii) The diameter of the wheels forming the rigid wheel base should not be less than 2 ft. 6 in. In the case of a two-wheel or a four-wheel bogie in addition to at least four coupled wheels, the minimum diameter allowed for bogie wheels to be 2 ft.

(iv) The minimum thickness of tyres to be as shown below:—

For axle loads under 15 tons	1½ in. on tread
For axle loads 15 tons and under 18 tons	1¾ in. on tread
For axle loads 18 tons and above	1½ in. on tread

In the case of axle loads under 10 tons, and the tyres formed solid with the rims of the wheels the minimum thickness on the tread may be 1 in.

Axle loads should be supplied by the Owner; if details are not available, an estimate is to be made by the examiner.

(v) The rigid wheelbase of any privately-owned locomotive to be conveyed by Freight train to be not less than 5 ft.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

REGULATIONS FOR THE CONVEYANCE OF PRIVATELY OWNED LOCOMOTIVES RUNNING ON THEIR OWN WHEELS OVER G.W. OR OTHER COMPANIES' LINES.

1. When a privately owned locomotive running on its own wheels is to be transported by District Goods Manager or District Traffic Manager will communicate with the District Traffic Superintendent, who will arrange for a competent mechanic to examine the locomotive, noting the following details:—

(a) Maximum width and height of engine (including work overhauls at the corners and height of the boiler) to be noted.

(b) Centre of drawbar.

(c) Centre of cylinders at front end.

(d) Bottom of buffer beam.

(e) Platform of engine.

(f) Dimensions of buffer faces and whether "Spring" or "Dead."

(g) Minimum clearance above rail level between wheels and outside wheels.

(On the Great Western Railway the minimum clearance above rail level need only be six inches. See diagram below)

(h) Distance between axles.

(i) Wheel arrangement.

(j) Diameter of wheels.

Whether fitted with ashpan and thickness of tyres and flanges between the outsides of the tyres and flanges.

Whether fitted with ashpan.

Whether provided with an efficient hand brake.



THE MINIMUM CLEARANCE ABOVE RAIL LEVEL TO BE 6', BOTH OUTSIDE AND INSIDE THE RAILS FOR A DISTANCE OF 3'-6" EACH SIDE OF THE CENTRE LINE EXCEPT FOR A ON EITHER SIDE OF THE RAIL CENTRES.

If the locomotive is to be transported over other Companies' lines, these details must be filled in on the diagram provided for the purpose. The Company on whose line the journey is commenced must make the necessary arrangements with the other Companies concerned for the acceptance of the engine, and if they are not satisfied with the diagrams, they must give an assurance that the engine complies with the Regulations. Should the engine not so comply with the Regulations, the necessary arrangements for off-gauge loads must be followed, and, before the engine is allowed to go to work, arrangements must be made for the protection of the through traffic.

2. Providing the locomotive is satisfactory and the engine complies with the following, it may be transported on its own wheels.

(a) The height of the buffer faces above rail level should not be more than 3 ft. 6 ins. nor less than 3 ft. 6 ins. and the buffer face is sufficiently large to prevent locking of the wheels. The height of the drawbar above rail level should not be more than 3 ft. 6 ins. nor less than 3 ft. 6 ins.

(On the Great Western Railway a dead end engine is formed in a train for transit, it must be coupled between a spring buffer and a spring buffer, and the train brake van by means of emergency release springs.

(b) The diameter of the wheels forming the rigid wheel base to be not less than 2 ft. 6 ins.

In cases where the engine has a two-wheeled or four-wheeled bogie, in addition to at least four driving wheels, the minimum diameter for bogie wheels to be 2 feet.

LOADING, ETC., OF MERCHANDISE TRAFFIC..

PRIVATELY OWNED LOCOMOTIVES—Continued.

(c) The minimum thicknesses of tyres for engines with the following axle loads to be as shown below:—

Axle Loads under 15 tons	1½ inches.
" " 15 tons and under 18 tons ..	1½ "
" " 18 tons and above	1½ "

In the case of an engine having axle loads under 10 tons, and the tyres formed solid with the rims of the wheels, the minimum thickness on the tread may be 1 inch.

(d) Rigid wheel-base to be not less than 5 ft. (See also Clause (e).)

(e) An engine having a rigid wheel-base of not less than 4 ft. 9 ins. may be run light in steam or hauled by special engine. An engine with a rigid wheel-base of less than 4 ft. 9 ins. must be made up.

3. If run in freight trains, an engine weighing 30 tons and over must be marshalled next the train engine; any engine below this weight must be marshalled next within the rear brake van.

4. Each engine forwarded "dead" must be accompanied by a competent man in charge, and in cases where not provided by the owner, a Railway Company's Fireman, or other competent person, appointed by the Locomotive Running Department, must travel with it. The man accompanying the engine must travel on its foot plate.

(a) Each engine forwarded in steam must be accompanied by a competent man to be provided by the Owner. A Railway Company's Driver and Fireman must travel with the engine and the Private Owner's man must travel with the engine.

(b) An engine in steam must not be accepted for conveyance unless it is fitted with an ash pan, and an efficient hand brake, which must be in good working order.

If the hand brake of an engine to be forwarded "dead" is defective, suitable labels bearing in large letters "DEFECTIVE BRAKE" must be attached to either side of the engine near the foot-plate, and the engine must be coupled to a wagon fitted with "either-side" brakes by means of emergency screw couplings. It must not be detached from this wagon during its transit.

7. Private Owner's engines, if conveyed in freight trains on their own wheels, must be formed only in services carrying "J" or "K" headlamps, and which have a booked sectional speed not exceeding 20 miles per hour and do not travel more than 25 miles without stopping.

8. Should the axle load exceed 14 tons, the Engineer for the respective Railway, or Railways, over which the engine will pass must be consulted.

9. Before despatch, an engine, whether to be run "dead" or in steam, must first be examined by a representative of the Chief Mechanical Engineer's Department as to its fitness to run. The Owner must be given to understand that this examination does not include the firebox and boiler of the engine, and when an engine is to run in steam, a certificate must be obtained from the Owner, stating that the engine is in good condition and fit to be worked in steam, and indemnifying the Railway Company against any accident or injury which may occur. This will be arranged by the District Traffic Manager, or District Goods Manager, who will, where necessary, pass the certificate on to the Divisional Superintendent. The Divisional Superintendent, District Traffic Manager, or other officer concerned, will obtain from the Chief Mechanical Engineer's Department a certificate as to fitness to run, and will give the necessary instructions to the Station Masters, Signalmen and all concerned in the working of the traffic.

(vi) A private locomotive may be run light in steam if the axle load does not exceed 4 ft. 9 ins.

(vii) Steam locomotives.

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(vi) A privately-owned locomotive having a wheelbase not less than 4 ft. 9 in. may be run light under own power or hauled by special engine. If the rigid wheelbase is less than 4 ft. 9 in. the locomotive must be loaded up.

(vii) Steam locomotives without ashpans must not be allowed to run in steam.

3. Acceptance and Conveyance.

(a) **Locomotive within Gauge and Axle Weights not excessive.** The appropriate District Officer, on receipt of Diagram and Certificate of Examination (B.R. 87262), satisfactorily completed in all respects and being satisfied, through prescribed Regional channels, that the locomotive is suitable from a loading gauge aspect and axle load requirements to travel to its destination, must make the necessary arrangements for despatch through the Operating Department, advising Goods Agent or Station Master accordingly.

In cases of interchange between Regions, the appropriate District Officer must make the necessary arrangements for the acceptance of the locomotive with the other Regions concerned, through the prescribed Regional channels.

(b) **Locomotive Out of Gauge and or Axle Weights excessive.**—Should a locomotive not conform in all respects with the loading gauge and axle weight requirements, or present any other features not herein provided for, the procedure in respect of out-of-gauge loads must be followed, and before the locomotive is allowed to go forward, agreements must be obtained from all concerned in the throughout transit, through the prescribed Regional channels.

(c) **General.**—Privately-owned locomotives must not be allowed to work under their own power over lines maintained by the Railway Executive without at least one axle load of 10 tons or over unless specially authorised by the appropriate Regional Headquarters.

Any number of privately-owned locomotives "dead" on own wheels may be conveyed by one freight train up to the loading capacity available, provided axle weights produced are not excessive having regard to the route to be travelled and that instructions are complied with respecting clearances, etc., as shewn on the Diagram and Certificate of Examination of Privately-owned Locomotives running on own Wheels (B.R. 87262). All other cases must be referred to the Civil Engineer for consideration.

A privately-owned locomotive "dead" on its own wheels must not travel at a speed exceeding 25 m.p.h. at any point and must stop for examination purposes at least once every 25 miles.

A locomotive weighing 30 tons and over must be marshalled next the train engine, and if more than one such locomotive is attached to a train they must be separated by at least two ordinary wagons (more if the Engineer requires them).

A locomotive weighing below 30 tons must be marshalled next within the rear brake-van but not more than three such locomotives are to be grouped together; or if required by the Civil Engineer they must be separated by a stipulated number of ordinary wagons.

4. Accompanying and Indemnity.

(a) Each privately-owned locomotive forwarded "dead" must be accompanied by a competent caretaker, and in case such a man is not provided by the sender or owner, arrangements must be made by the appropriate District Officer with the District Motive Power Superintendent to provide a Fireman or other competent person to travel on the footplate.

The person in charge of the locomotive, whether provided by the owner or the Railways, must travel on the footplate of the locomotive and not in any other part of the train. If the destination is on another Region, the Railway caretaker will travel to the junction at which transfer is to be effected, unless otherwise arranged, and the appropriate District Officer must arrange in advance with the other Regions concerned to provide caretakers over their respective Regions, in every case possible. The caretaker should be provided by the sender or owner. (See (c) below regarding indemnity.)

(b) Each privately-owned locomotive forwarded under its own power must be accompanied by a Railway Driver and Fireman provided by the District Motive Power Superintendent to take charge through to destination, or if for another Region, to the junction at which it will be transferred, unless otherwise arranged. A competent man provided by senders or owners must accompany each locomotive forwarded under its own power and travel on the footplate. (See (c) below regarding indemnity.)

(c) Indemnity Form (R.C.H. 60040), properly completed, must be obtained in respect of all journeys of locomotives travelling under own power and also caretakers in charge of locomotives travelling under own power or "dead".

(d) A sender's or owner's man travelling with a locomotive must have a pass issued to him through to destination; this must be obtained from the appropriate District Officer by the most expeditious means. On the return journey the man must pay his fare.

(G A.29.Op.—5, 52. LK1/10310 (Gen. E).)

SECTION III. (b).

INSTRUCTIONS CONCERNING LOADING, CONVEYANCE, ETC., OF LIVE STOCK BY PASSENGER AND FREIGHT TRAINS.

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CONVEYANCE OF LIVE STOCK.

LOADING AND UNLOADING HORSES, &c., CONVEYED BY PASSENGER TRAIN.

1. Before accepting a horse, pony, or other animal for conveyance by rail the animal must be carefully inspected to see whether it is suffering from injury which might render it dangerous. If whatever be observed, the attention of the driver, conductor, or other person in charge of the train must be drawn to it, and a note of the injury or defect must be made upon the way-bill.

2. The Traffic Staff at Stations where horse-boxes, both the G. & N.W. and A.G.W. are received, or loaded, must see that the fittings, etc., are intact, and any deficiencies reported at once.

Before any horse-box is used it must be thoroughly examined as to all particulars properly secured. No box should be loaded unless the whole of the fittings, ropes and accessories are in a proper working order.

Bits and other fittings removed from horse-boxes for purposes of loading or other animals, must be replaced in position, and when any boxes are received without their fittings, the Traffic Staff must be informed to the loading Station to enable the parties to be replaced. Horses and ponies must be placed in the *coupe*, but hung in position in the stalls.

3. No horse, etc., must be accepted for conveyance unless provided with a halter or bridle, and with a label attached giving name of consignee and destination station.

4. In tying up horses, or other animals, the length of the upper part of the rope must be carefully regulated so that 2 1/2 of the horse's neck is secured to the ring. The rope should not be fastened so tight as to prevent the animal from breathing, and the length of the rope or rope will be from 1 1/2 to 2 1/2 ft. according to the height of the horse or other animal, but in no cases should it exceed eighteen inches.

5. The head-stalls, which are adjustable both in the nose and neck straps must be made to fit the head of the animal. When properly fitted, such as the head-stall or the nose and neck straps must be reduced to such a size as to avoid the head-stalls being slipped.

6. With a view to assist those engaged in fastening horses, etc., in a box, the following diagram illustrates the proper method of securing the ropes to the rings:—



Short End.

End attached to Leather Head Collar.

CONVEYANCE OF LIVE STOCK.

LOADING AND UNLOADING HORSES, ETC., BY PASSENGER TRAIN *Continued.*

In tying this knot the end of the rope should be taken in the right hand, and passed through the ring downwards, and brought out at the left hand side of that part of the rope attached to the Head Stall, then carried over the rope towards the right, and passed again through the ring downwards, and brought out between the ring and the rope loop. The end rope must be tied again to complete the knot.

7. Horses fully harnessed should not be boxed except at the special request of the sender, when a remark must be made on the Live Stock Ticket and initialed by the sender. Harness in all other cases must be removed before the animal is loaded.

8. Loose harness must be loaded in the *coupé* of the box, and a remark made on the horse ticket.

9. If a horse tendered for conveyance is considered too large to travel safely in one stall, it must not be loaded until the attention of the owner, consignor, or owner's agent or representative has been called to the fact and his instructions taken, and if two stalls are ordered the sender must endorse the live stock ticket accordingly.

10. If a horse is found to be restive from nervousness or any other cause, and there is a doubt of its travelling safely, the attention of the Station Master or other responsible person must be drawn to it, and he must use his discretion as to whether it is advisable to forward the animal or not.

11. In loading animals preference should, whenever possible, be given to boxes which will travel with their head-stall in the direction of the engine.

12. In ~~view~~ of any ~~unoccupied~~ *charge* of a horse box must, in all cases, be kept closed on the journey.

13. The conditions of the live stock ticket must in all cases be signed and the rate at which the animal is to be charged must be specified in full, i.e. "Company's" or "Owner's" Risk.

[illegible]

These boxes, but with hinges providing two shutters should be fully gassed before commencing the filling process. In the event of no gas fitted boxes being available, gas traps, trimme l and filled, must be provided.

[illegible]

16. In the event of an animal injured itself in transit, the Station Master where the injury is discovered must immediately make him a Certificate of Injury, and record it, stating the origin of the injury, the best means for disposing of the animal, and in case of an animal shipped for sale, or getting ready to be shipped, the necessary information must be obtained, whether any injury is observable or not. The head stall and ropes must be retained when either of these have to be cut or have been broken, and the animal must be kept in the box, and that he be properly examined by the Divisional Superintendent or District Traffic Manager. The manner in which the head stall is buckled should be retained, and the animal must be kept in the box, and that he be properly examined by the Divisional Superintendent or District Traffic Manager. The manner in which the head stall is buckled should be retained, and the animal must be kept in the box, and that he be properly examined by the Divisional Superintendent or District Traffic Manager. Every case must be reported, on Form No. 1874, to the Divisional Superintendent or District Traffic Manager, giving date, number of box, particulars of accident, nature of injury, and where the animal can be seen. If the case is very serious a local Veterinary Surgeon must be called in to examine the animal, and an advice (by wire) in the case of an animal being despatched to the nearest Railway Station in order that the Consignor and Consignee can mutually agree as to the treatment of the animal, and may be present, or represent it, at any post-mortem examination which may be required, and should be carried out by the Veterinary Surgeon who may examine the animal.

17. The staff at the destination Station must search the *coupé* and locker of every horse box immediately on arrival, and any property other than the Company's must be removed. Should any article be found in the box of a travelling passenger it must be dealt with as lost property.

18. On arrival of a loaded box at its destination, steps must be taken to see that any animal that may be in the box is in proper condition before the flap of the box is let down.

19. Immediately a horse is unloaded the box must be properly cleansed and disinfected, and the *coupés* must be swept out and the windows cleaned.

20. Horse box stall partitions when removed for the purpose of loose loading must be replaced by the receiving Station after unloading the traffic, excepting on those occasions when a special event, such as an Agricultural Show or Fair, is being held.

CONVEYANCE OF LIVE STOCK.

LOADING AND UNLOADING HORSES, ETC., BY PASSENGER TRAIN *Continued.*

21. If there is an animal in the rear or platform side stall, the head stall of such animal must be released by a man reaching into the stall from the *coupé* before the flap of box is let down, and the horse must then be held by the bridle or halter until the doors have been fully opened, when the animal must be carefully led from the box.

22. When an animal is loaded in the centre or off side stall it is necessary, before opening the partitions, that the head stall should be released and a man stationed at the animal's head to hold it until the partitions have been fully opened.

23. Should by any chance a horse be found in a box without its owner's bridle or halter, the ropes of the Company's head-stall must be released and the animal can then be led from the box by the head-stall and taken to a stable or shed before removing the head-stall and putting on the bridle. In that case a report of the circumstances must be made to the Station Master and care must be taken to see that the head-stall is replaced in the box.

24. In attaching or detaching horse boxes from a train, special care must be taken to prevent any animals which may be loaded therein being shaken or startled, and this instruction must be particularly observed at Junctions and Stations in transferring horse boxes from one train to another and in shunting other vehicles on to the train.

25. Whenever loaded horse boxes have to be shunted or moved by an engine, the Engine Driver must be informed that the vehicles are loaded, and warned to be careful in starting and stopping in order to avoid any sudden jerk.

TRANSIT OF LIVESTOCK TRAFFIC FEEDING AND WATERING.

Attention is directed to the instructions in regard to live stock in transit contained in General Manager's Circular No. L.S.D. 1747, September, 19

The following instructions must be observed:—

1. It is essential that cattle traffic should receive expeditious transit, and the staff are requested to co-operate in effecting this. The loading station must ascertain the train which will afford the best throughout service to destination, or junction with other Company, and when opportunity presents itself, intimate to Senders that it is to their interest to have the animals at the station in time for this train. In this connection consideration should be given as to whether it will be necessary to feed and water en route.
2. It is important that live stock labels show the time of loading and also when and where the watering and feeding takes place on the journey.
3. ~~General Manager's Circular No. L.S.D. 1747, September, 19~~
by their trains which have been, or will be, in transit longer than the periods within which the various descriptions of live stock are watered and fed to the next station where a position to water, will be reached.
4. If the hours within which animals must be watered after commencement of journey, or after feeding and watering will, or is likely to, elapse before animals can again be conveniently fed and watered, having regard to services available, attention must be given before sending forward from intermediate junctions, or recognised feeding stations.

Delete clause 5 and substitute the following:—

6. Guards must record on their journals particulars of all cattle conveyed. The information must be shown in the general remarks, etc., column and include:—

1. Truck Nos.
2. Forwarding station
3. Destination.

DISEASES OF ANIMALS ACTS. All concerned must pay particular attention to the complete Circular issued, from time to time, by the General Manager, and also to the supplementary Circulars of the Chief Goods Manager. They must carefully check the regulations respecting movements of animals which affects their stations and, in the event of any order applying to a station not included in the list of stations shown in the Circulars, a notification must be given to the Chief Goods Manager at once; if necessary by telegram. Immediately on receipt of a Circular from the Chief Goods Manager, announcing a revocation of a Regulation, or the issue of a new one, the General Manager's Circular must be amended accordingly.

Delete clause 5 and substitute the following:—

A telegraphic or telephonic advice must be despatched by the sending station to the originating Control giving number of wagons, destination and route, description of animals especially disclosing if milking cows, etc., and if attention is required in this connection enter time of loading and service by which being despatched. The originating Control to include these particulars in all train advices, and the information to be included in any advices sent by subsequent Controls or Yards. Where attention is given in transit information to be included in the advices. Where train advices are sent from Marshalling Yards etc. to points other than or in addition to originating Control, full details to also be included in these advices.

(G A 27 Op.—151 E 78511 H)

Cattle are usually fed and watered at a regular interval and the quantity shown on wagon tickets by the forwarding firm is the basis from which to calculate when animals are next due for attention.

At certain times, however, the animals may be fed and watered and the load increased or decreased at the discretion of the forwarding firm. It is clearly understood that the time of feeding and watering shown is the one on which calculations of subsequent attention must be based and not the loading time.

(G.A. 12, 4/43, C.G.M.—W.T. 64311.)

CONVEYANCE OF LIVE STOCK.

LOADING, FEEDING, AND WATERING OF LIVESTOCK, MILKING OF DAIRY COWS, ETC.

In dealing with Live Stock, such as horses, cattle, sheep, pigs and goats, care and patience must be shown at all times, but also in the treatment during transit, such as animals with yards, must be treated with care to avoid fright or injury, and consequently suffering on the part of the animals.

If, upon arrival at a station, an animal is found to be injured, the agent must be at once informed. If the animal is found to be injured beyond recovery, it must be properly slaughtered.

In the case of animals arriving during the night, the person in charge of the station is responsible for seeing that no unnecessary suffering takes place.

When Live Stock by freight train is to be conveyed long distances, or from any cause is detained at an intermediate or receiving station, such stock must, where necessary, be supplied with water as well as food. The following are the periods within which various descriptions of animals must be watered are as follows:—

Horses, Asses and Mules	..	..	24 hours
Cattle	..	..	27 " 24 or 24 hours if destination can be reached Q 11b
Sheep and Lambs	..	..	36 "
Pigs	..	..	27 " reached

The following instructions are given by the Railway Companies to feed animals after they have been in transit for a period of 24 hours. The Practice of Animals Act of 1911, however, now makes it incumbent on the companies to supply food to animals suffering from lack of food in transit, and as such suffering animals are in the hands of the companies, it is their duty to provide food for them in the same way as they are watered. This practice must be continued.

The Railway Companies are responsible for seeing that the instructions are adhered to at the expense of the owners.

When loading or unloading for conveyance, the sender or his representative must be requested to take care of the animals so that they are not subjected to any suffering pain or discomfort, and should be asked to make arrangements to meet the requirements.

When loading or unloading, animals are loaded in transit if it appears they are suffering any pain or discomfort.

Animals should not be loaded with other cattle, but put behind them.

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When loading or unloading, animals are loaded in transit if it appears they are suffering any pain or discomfort, and should be asked to make arrangements to meet the requirements.

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Live Stock labels, including those for Horses, should show the time of loading, and also when and where the watering and (or) feeding takes place on the journey.

SUPPLY OF FOOD AND WATER TO ANIMALS IN TRANSIT

Animals in transit are liable to be loaded, unloaded or detained during the journey, and such provision of water to the satisfaction of the animals is required. The following are the periods within which various descriptions of animals must be watered are as follows:—

The water supply for animals in transit shall be supplied to any such animal by the company in charge of the train, or if any person in charge thereof.

The periods within which various descriptions of animals must be watered are as follows:—

The periods within which various descriptions of animals must be watered are as follows:—

Horses, Asses and Mules	..	..	24 hours
Cattle	..	..	27 " 24 or 24 hours if destination can be reached Q 11b
Sheep and Lambs	..	..	36 "
Pigs	..	..	27 " reached

The consignee and person in charge of the train shall each be guilty of an offence against the Act, and it shall be on the person in charge to prove such a request, and the time within which the animal had a supply of water.

The Company supplying water or food may make, in respect thereof, such reasonable charges as the Minister of Agriculture by Order approve, over and above the rate of remuneration, and the amount of such additional charges shall be paid by the consumer and/or from the consumer to the Company, and can be recoverable by the Company from either of them, with costs, by proceedings, in any Court of competent jurisdiction.

If when animals are fed and watered in transit, a note must be made on the labels that this has been done, particular care to be taken that the station at which they have been attended to is shown.

A list of the Stations where the Company must provide a supply of water for animals is shown on page 16 of the General Manager's Circular, No. L S D 1441, dated September, 1907, and if a request is made at any other Station for water to be supplied to animals, the Goods Agent or Station Master should, if possible, comply with such request.

An prisoner, officer, or constable who detains an animal, horse, ass, or mule must supply it with requisite food and water during its detention and the expenses incurred by him may be recovered summarily by him in the person in charge of the animal, horse, ass, or mule, or its owner.

If there is no person travelling in charge of the consignments, the Inspector and Station Master must refer the inspector, officer, or constable to its owner for the expenses.

Construction of Trucks, etc., Used for Carriage of Animals.

1. No animal shall be carried by railway in a truck or other vehicle which is not in accordance with the provisions of this Article, unless the use of such truck or other vehicle, either generally or in special cases or in respect of a particular type of truck or vehicle or in particular cases, of a particular class of animal, is authorized by an exemption granted by the Minister.

2. Every truck or other vehicle shall be provided with a container for the collection of litter to prevent spilling, shall, in the case of a truck used for the hauling of material, be equipped with a container for the collection of litter, and shall be provided with a proper quantity of litter or sand or other proper substance.

3. The battens in a truck or other railway vehicle shall be placed across the vehicle, except between the doorways, where they shall be placed lengthways.

4 The interior of every truck or other vehicle shall be free from any boltheads, nails or other projections likely to cause suffering to animals carried therein.

5. Every truck for carrying animals shall be fitted with a roof, and with flooring having lines of a pattern approved by the Master, and all internal projections shall be rounded.

4. Every falling loading door and every gangway, passage way, catwalk or unloading board, shall be fitted with longitudinal battens or other proper footholds.

7. Every truck or other vehicle shall be so constructed as to admit of ventilation and inspection at the floor level.

4. Every truck or other vehicle shall be so constructed as to permit of its being cleaned and disinfected in the manner prescribed by this Order.

Securing of Cattle in Railway Trucks. - All Falls, whether polled or not, shall, when being carried in a railway truck or other railway vehicle, be securely tied by the head or neck.

.All horned stock carried in the same railway truck or other railway vehicle with a bull shall, unless separated therefrom by a suitable partition, be securely tied by the head or neck.

of a suitable partition.

All the Federal Comptrollers have agreed that seed rats are to provide the rat traps, excepting only the half provided by rail from the ports. Other arrangements must not be loaded with half, as the animals are liable to suffer injury through being so conveyed.

Carriage by Railway of Unfit Animals. No animal shall be permitted by the owner thereof, or his agent, or any person in charge thereof, to be carried by railway if, owing to lameness, illness, injury, fatigue, or any other cause it cannot be carried without unnecessary suffering during the intended transit by railway.

CONVEYANCE OF LIVE STOCK.

ANIMALS (TRANSIT AND GENERAL) ORDER OF 1927—Continued.

[illegible]

The statement of the respondents to the effect of their responsibility or supervising the carrying out of the foregoing instructions.

Separation of Mixed Consignments. Cows, sheep, goats and swine, if carried in the same railway car with any other than a calf, or a horse, ass or mule, shall be so loaded that this provision shall not apply to the conveyance of a cow with its unweaned calf, if they are separated from other animals.

of a cow with its unweaned calf, if they are separated from other animals. The unweaned calf may be loaded together, or may be separated, if sender so desires.

separated, if sender so desires.

and the smaller animals being loaded together on the other side of the partition.

the sums of the different head rates are cheaper.

Overcrowding. A railway company shall not allow any railway truck or other railway vehicle used for carrying animals in the railway or any compartment thereof to be overcrowded so as to cause injury or unnecessary suffering to the animals therein.

injury or unnecessary suffering to the animals therein.

[illegible][illegible]

If, for safety, in order of overruling the preliminary instructions contained herein must be strictly carried out.

Shorn Sheep. The first day of November and the next following 30th day of April both inclusive, shall be the period during which shorn sheep are being transported in interstate commerce in such a manner as to protect the wool from vermin. The Order does not apply to sheep last shorn more than 60 days before being carried.

CLEANSING AND DISINFECTION OF HORSE BOXES AND SPECIALLY CONSTRUCTED CATTLE
VEHICLES WORKED IN PASSENGER TRAINS.

Horse Boxes and Passenger-fitted Cattle Vans must be cleansed and disinfected in the manner shown below immediately they have been unloaded:—

1. By scraping and sweeping the floor and other parts with which the animal or its droppings have come in contact.

(ii.) By effectually removing therefrom, and forthwith well mixing with quicklime, the scrapings, sweepings, dung, sawdust, fodder, litter and other matter; and

d. By treating the undersides of all the interior parts of the box with which the animal is in contact. A thin layer of fat is provided specially for this purpose, and it must be applied by means of a sponge, brush, swab or spray.

purpose, and it must be applied by means of a sponge, brush, swab or spray.

Where a spillage of liquid occurs, the spillage must exist at the unloading station, the instructions of the Division Superintendent or District Label Manager must be obtained as to the action to be taken.

These Instructions apply in respect of Horse Boxes and Passenger-fitted Cattle Vans belonging to other Companies as well as the Great Western Railway.

CONVEYANCE OF LIVE STOCK.

SUPPLEMENTARY INSTRUCTIONS IN REGARD TO THE CONVEYANCE OF LIVESTOCK BY PASSENGER TRAIN.

(GUARDS' VAN TRAFFIC.)

The special instructions for Managers, Inspectors, Foremen, Guards and all concerned is directed to the necessity of securing the safe and comfortable conveyance of livestock by rail or wagon at stations as well as during the journey. It is the duty of the staff to attain this object and it is essential that the following instructions should be strictly observed:—

1. The staff must ensure that the livestock is properly secured in the van, and that ample ventilation is afforded.

2. The staff must ensure that the full name and address of the sender and the name of the station to which the traffic is to be returned, is clearly marked on the van, and that the name of the sender and the name of station to which the traffic is to be returned.

3. The staff must ensure that the livestock is properly secured in the van, and that ample ventilation is afforded.

4. The staff must ensure that the livestock is properly secured in the van, and that ample ventilation is afforded.

5. The staff must ensure that the livestock is properly secured in the van, and that ample ventilation is afforded.

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14. The staff must ensure that the livestock is properly secured in the van, and that ample ventilation is afforded.

15. The staff must ensure that the livestock is properly secured in the van, and that ample ventilation is afforded.

16. The staff must ensure that the livestock is properly secured in the van, and that ample ventilation is afforded.

17. The staff must ensure that the livestock is properly secured in the van, and that ample ventilation is afforded.

18. The staff must ensure that the livestock is properly secured in the van, and that ample ventilation is afforded.

19. The staff must ensure that the livestock is properly secured in the van, and that ample ventilation is afforded.

20. The staff must ensure that the livestock is properly secured in the van, and that ample ventilation is afforded.

CONVEYANCE OF LIVE STOCK.

HOMING PIGEON TRAFFIC.

Instructions to Stations from which Homing Pigeons are forwarded.

(1) The Secretaries of County or Town Federations of Pigeon Flying Clubs can be informed of the arrangements entered in the Coaching Arrangements Book and an advice should be sent to the Divisional Superintendent or District Traffic Manager immediately and information regarding a Federation flight is available.

(2) Senders have the option of consigning at "Owner's Risk," and where this is taken advantage of the staff should see that signature is held to the O.R. conditions.

(3) Birds should be despatched by the train specified on the label, where no train is shown, by the first available service.

(4) To facilitate transit, pigeons must be forwarded by through trains whenever possible.

(5) Baskets containing birds must never be thrown down. If this is done the pigeons are likely to receive injury by concussion. The staff should be trained to treat the birds with care and consideration. Every care must be taken in loading baskets into and out of vans. The basket should be placed flat on platform rails and not overhanging the raised sides, which causes it to tip. No heavy packages should be placed on top of the baskets.

(6) When empty baskets are returned to forwarding station latter should place in safe custody, also examine the labels to see that no baskets belong to any other club are kept for it. If any baskets received with out address labels must be immediately reported to the Divisional Superintendent or District Traffic Manager. Description and size of baskets and number of birds to be given in all cases. Loss of or flight by empty baskets not only gives rise to considerable loss to the Company in loss of revenue, and seriously inconveniences the owners, as they are not able to use them for training purposes.

(7) Any instances of overcrowding birds in baskets or failure to collect empty baskets promptly must be reported.

Instructions to Stations at which Homing Pigeons are liberated.

(8) Where practicable, Station Masters should select one or more members of the staff to specially attend to the liberation of birds. If any of the staff are interested in Pigeon Flying, they may also be engaged in station to this purpose. A site adjacent to the station, cleared of all obstructions, and free from any wires, moving or standing vehicles, must be selected and marked out for the purpose. For this purpose it is necessary to get in accordance with these instructions may result in some birds being maimed or killed.

(9) Birds going in opposite directions must not be liberated within several minutes of each other or until the released birds have got clear away, otherwise large numbers of birds are diverted from their course and in the case of young untrained birds, many are lost by being carried off by birds flying a different course.

(10) Guards and others concerned should give special attention to this class of traffic to avoid any over-carrying. In the event however of any bird being over-carried a telegraphic report should be sent immediately to the sender to the correct station in which the bird is to be released. As soon as the bird is received, a report must at once be submitted to the Divisional Superintendent or District Traffic Manager. This particularly applies in the case of young birds during the months of May, August and September.

(11) If weather is unfavourable for flying, birds should not be liberated. If necessary they should be held until the following morning and sent in a live lot, at which time they should be supplied with water.

(12) Every endeavour should be made to water all birds upon arrival at the liberation place, especially those coming from long distances, and particularly in hot weather. Baskets sent long distances are usually equipped with troughs for this purpose. It is not necessary to fill these troughs with water, but to keep them full of water, even though it is at the most two or three days old, or perhaps a few days old. If it is found expedient to hold birds overnight, it should be seen that they are supplied with food of such as cats or rats.

(13) Labels should always be examined carefully before birds are released to see that sender's instructions are complied with and the name of the station at which the birds are liberated must be stamped on the address label, and the time and date of liberation noted and initialed by the person effecting the liberation. It should also be observed from what station for travel a bird has been forwarded. It has occurred en route Pigeons cannot come in the dark, therefore they should not be liberated at such a time that it would be impossible for them to reach their lofts the same day.

(14) After liberation, the baskets should be emptied of chaff, thus if not done, causes untidy platforms and entails extra work for the staff. Empty baskets must be returned to home stations as promptly as possible.

CONVEYANCE OF LIVE STOCK.

CONVEYANCE OF LIVE POULTRY ORDER OF 1919.

The following instructions issued by the Ministry of Agriculture must be carefully observed : -

PROTECTION OF POULTRY DURING CONVEYANCE BY WATER.

1 —(i) The conveyance of poultry (domestic fowls, turkeys, geese, ducks, guinea fowls, and pigeons) on a vessel to or from a port in Great Britain shall comply with the following conditions : -

(ii.) The poultry shall while on board or on premises of a shipping company in connection with their conveyance by water be protected as far as practicable from exposure to bad weather or sea water, or excessive heat :

(iii.) Poultry shall be carried in such parts of the vessel as are sufficiently ventilated, and receptacles containing the poultry shall be disposed so as to allow sufficient ventilation to reach each receptacle, and so as to afford access to each receptacle for inspection of the poultry as occasion may require :

(iv.) Receptacles containing poultry shall as far as practicable be secured so as not to be liable to be shifted by the motion of the vessel :

(v.) A receptacle containing poultry may be placed on another such receptacle only if sufficient ventilation is left for each receptacle :

(vi.) The poultry, whether carried in receptacles or otherwise, shall not be so overcrowded as to cause injury or unnecessary suffering to the poultry.

(2) If any head of poultry while being conveyed by sea on a vessel for sale or exposure for sale is found to be injured or to be suffering from any disease or to be in such a state of distress that it is desirable to prevent unnecessary suffering, the master or other person in charge of the vessel may cause it to be killed.

(3) If any poultry are conveyed in contravention of this Act by the shipping company and the master of the vessel, each according to his liability in respect of their own acts or omissions, be deemed guilty of an offence against the Act of 1894.

PROTECTION OF POULTRY DURING CONVEYANCE BY RAILWAY.

2 —(i) The conveyance of poultry by railway to Great Britain shall comply with the following conditions :

(ii.) The poultry shall, while conveyed in a railway truck or other vehicle, or while on premises of a railway company in connection with such conveyance, be protected as far as practicable from exposure to bad weather or excessive heat :

(iii.) Every railway truck or other vehicle in which poultry are conveyed shall be sufficiently ventilated :

(iv.) Receptacles containing poultry shall be so disposed as to allow sufficient ventilation to reach each receptacle, and so as to afford access to each receptacle for inspection of the poultry as occasion may require :

(v.) Receptacles containing poultry shall, as far as practicable, be secured so as not to be liable to be shifted during transit :

(vi.) A receptacle containing poultry may be placed on another such receptacle only if sufficient ventilation is left for each receptacle :

(vii.) The poultry, whether carried in receptacles or otherwise, shall not be so overcrowded as to cause injury or unnecessary suffering to the poultry.

(2) If any head of poultry while being conveyed by railway in Great Britain or while on the premises of a railway company is found to be injured or to be suffering from any disease or to be in such a state of distress that it is desirable to prevent unnecessary suffering, the officer may cause it to be killed.

(3) If any poultry are conveyed in contravention of this Act by the railway company or by the railway or carrier or person in charge of the conveyance, each according to his liability in respect of his acts or omissions of the company, be deemed guilty of an offence against the Act of 1894.

(4) Poultry traffic shall not be accepted for conveyance by Freight train when a service is available of more than 30 hours.

PROTECTION OF POULTRY DURING CONVEYANCE BY ROAD OR EXPOSURE FOR SALE.

3 Poultry, while being conveyed by road or exposed for sale or while in a market or other place, where poultry are habitually exposed for sale, shall in Great Britain

(i.) be protected as far as practicable from exposure to bad weather or excessive heat ; and

(ii.) not be confined in a receptacle which is not of a height and size reasonably sufficient for the poultry and the number contained therein, or which is so constructed as to be likely to cause injury or unnecessary suffering to poultry confined therein or which does not allow sufficient ventilation, but this shall not be deemed to prohibit the conveyance of poultry in "swills" or shallow baskets having a net over the top with mesh sufficiently small to prevent protrusion of the heads of the poultry ; and

(iii.) not unnecessarily be tied by the legs or be allowed to remain so tied for a longer period than is necessary ; or unnecessarily be carried head downwards ;

and in the event of any failure to conform to these provisions, the owner and the person in charge of the conveyance, in relation to whose failure occurs shall, each according to and in respect of his own acts or omissions, be deemed guilty of an offence against the Act of 1894.

CONVEYANCE OF LIVE STOCK.

CONVEYANCE OF LIVE POULTRY ORDER OF 1919—*Continued.*

CONVEYANCE OF UNFIT POULTRY.

4. Poultry shall not be permitted, by the owner or person in charge thereof, to be conveyed on a vessel from a port in Great Britain to any other port or by railway or by road in Great Britain, if from injury or any other cause such conveyance of the poultry may reasonably cause or necessarily suffering to them.

USE OF UNSUITABLE RECEPTACLES.

5. (1) Any person who in Great Britain, with a view to the conveyance of poultry by railway, water or road, shall place poultry in a receptacle, or portion of a receptacle, which is not of a light and size reasonably sufficient for the poultry and the number placed therein, or which is so constructed as to be likely to cause injury or unnecessary suffering to poultry carried therein, or which does not allow sufficient ventilation, or in the case of conveyance by vessel or by railway, or by a public carrier, which is not so constructed as to protect the poultry from injury by protrusion of the head, legs or wings through the top or bottom or sides of the receptacle, shall be deemed guilty of an offence against the Act of 1894, and any person who, in Great Britain, being the owner or person in charge of poultry, shall if it or cause to be put on board any vessel or consign by railway or by a public carrier the poultry which is contained in any such receptacle, or portion of such receptacle, shall be deemed guilty of an offence against the Act of 1894.

(2) The provisions of this Article relating to the height of receptacles shall not be deemed to prohibit the use for the conveyance of poultry of swills or shallow baskets having net over the top with mesh sufficiently small to prevent protrusion of the heads of the poultry.

(3) A receptacle shall be deemed to be so constructed as to be likely to cause injury or unnecessary suffering, for the purpose of this Article, if it measures more than twenty-four square feet, or if any side of it measures more than ten square feet, or if the receptacle is not sufficiently strong and rigid for the purpose for which it is intended to be used.

MIXED CONSIGNMENTS.

6. Turkeys, geese and ducks, which are placed in the same receptacle with other poultry, shall not be conveyed by a vessel from a port in Great Britain or by railway in Great Britain, unless the turkeys or geese or ducks, as the case may be, are in a separate compartment, and if any poultry are conveyed in contravention of this Article the owner and consignor of the poultry, and the master of the vessel or the railway company, as the case may be, shall each according to his liability in respect of his or their own acts or omissions, be deemed guilty of an offence against the Act of 1894.

HANDLING OF RECEPTACLES CONTAINING POULTRY.

7. Receptacles containing poultry shall, during conveyance on a vessel to or from a port in Great Britain, or on a railway track or other vehicle in Great Britain, or while being loaded or unloaded thereon, or while being carried and deposited with, and in such manner as to avoid injury or unnecessary suffering being caused to the poultry, and any person handling, carrying or depositing a receptacle who fails to comply with this Article shall be deemed guilty of an offence against the Act of 1894.

INFORMATION TO BE FURNISHED TO INSPECTORS.

8. A carrier of poultry by railway, water or road in Great Britain shall, if so required by an Inspector of the Ministry, furnish him with the names and addresses of the consignors and consignees of the poultry so far as they are known to the carrier.

CONFINING OF POULTRY IN RECEPTACLES FOR UNNECESSARY TIME.

9. Any person who in Great Britain, in connection with the conveyance of poultry, delays to deliver or receive under his charge, shall cause or permit the same to be confined in a receptacle for a longer time than is reasonably necessary, shall be deemed guilty of an offence against the Act of 1894.

CLEANSING OF RECEPTACLES.

10. A receptacle which has been used for the conveyance of poultry by rail or water to any place in Great Britain shall be thoroughly cleansed by the owner or person in charge thereof before being again so used, and if sent by railway or vessel before being so sent.

FEEDING, ETC., OF POULTRY IN EXCEPTIONAL CIRCUMSTANCES.

11. Where the conveyance of poultry by a vessel, or by railway has been so protracted, or exceptional circumstances so require, it may be necessary to supply the poultry with food or water in order to protect the poultry from unnecessary suffering; the master of the vessel or the railway company, as the case may be, shall cause the poultry to be supplied with sufficient food and water.

COMMENCEMENT.

12. This Order came into operation on the sixteenth day of September, nineteen hundred and nineteen.

SECTION IV.

INSTRUCTIONS CONCERNING STATION WORK.

SECTION IV.

INSTRUCTIONS CONCERNING STATION WORK.

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INSTRUCTIONS CONCERNING STATION WORK.

PROMPTITUDE IN DEALING WITH ACCIDENTS AND OTHER EMERGENCIES.

1. In the unfortunate event of an accident to a train conveying passengers it is of the first importance that immediate steps should be taken by Station Masters, Inspectors, Foremen, Guards and other servants of the Company who may be concerned to relieve any who may have sustained injury, as well as to adopt all precautionary measures which the circumstances may render desirable, in the direction of limiting the effects of the occurrence. The following important points are therefore set out as being necessary to be borne in mind -

- (a) Examine train to see if there is any sign of fire from live engine coal or broken gas cylinders or connections, and take what steps are possible to extinguish it.
- (b) Ascertain where the greatest damage has occurred, and take instant steps to release any passengers who may be entangled in the wreckage.
- (c) Ensure prompt telegraph or telephone communication with the nearest likely places for Doctors, Nurses, Ambulances, Refreshments, etc., according to circumstances.
At stations where Ambulance Sections are formed, in the event of a train accident in the vicinity, the Station Master must arrange immediately to despatch one or more of the organised Ambulance Sections, with their equipment, to the scene of the accident.
- (d) Enlist the services of any uninjured and willing passengers or other persons to convey messages, and to assist generally, under the direction of the Railway Staff.
- (e) Use cushions taken from the coaches in such a manner as to form comfortable couches for any injured persons, until such persons can be removed.
- (f) Advise the nearest Station Master, and other officials, and also the Permanent Way men, at the earliest possible moment.

2. The attention of Station Masters is specially directed to the importance of being at all times prepared to deal with the various emergencies incidental to railway work, and especially those which are most likely to arise at their own Stations, or on the section of line for the working of which they are directly responsible. Station Masters are reminded that not only as it is their duty that they should themselves give special attention to the whole subject as well as give them, could not, in dealing with emergencies when they arise, but they should also take steps to satisfy themselves that the members of their Staff who are in charge of it. Station Masters should also be constantly kept apprised of the character of the duties which in an emergency they would be called upon to perform, and particularly as to the course of action which should properly be taken in the following circumstances:-

(a) Working the Traffic of a Double Line over a Single Line of rails. See Rules 189 to 205 in the Rule Book.

(b) Receiving a broken down Train or Engine from the front by Engine or Broken-down Train running on the wrong road. See Rule 183.

(c) Drawing or pushing an Engine, Train, or portion of Train on the wrong Line back to the next Station or Signal Box in the rear. See Rule 184.

(d) Working a Single Line by Pilotman owing to the failure of the Electric Token apparatus. Clause 25 of the Electric Token Block Regulations.

(e) Working a Single Line by Pilotman on one side of an obstruction and by Train Staff on the other.

See Clause 14c of the Electric Train Token Block Regulations.

" " 20 " Train Staff and Ticket Regulations.

" " 13 " One Engine in Steam Regulations

(f) Transferring passengers from train to train at an obstruction. No specific instructions are given as to the manner of dealing with cases of this kind, beyond the general principle of working by Pilotman on the wrong line in each direction back to the nearest cross-over road after transferring passengers from train to train, provided for in Rules 189 to 205 of the Rule Book, and in the general Instructions for working Single Lines to be found in this Book.

3. Each Station Master, with his principal assistant should be not have been so already, is asked to thoroughly work out on the principles laid down in the Regulations referred to, the details of the action he would be required to take were any of these emergencies to occur at or near his station. He should thoroughly rehearse in his own mind the exact course to be followed in the various forms

Reference to the following to be made on page 263 :—

INSTRUCTIONS TO BE OBSERVED WHEN ASSISTANCE OF FIRE BRIGADES IS REQUIRED IN CONNECTION WITH TRAIN ACCIDENTS.

1. In any serious train accident where passengers or railway staff may be trapped in the wreckage and in any accident where fire has broken out or would appear to be imminent, the nearest Fire Brigade must be immediately advised by the Operating Department Official on duty in charge of the station or section of the line concerned.
2. The fullest available information must be given to the Fire Brigade including the precise site, the best means of access to, and the extent of the accident.
3. The telephone number of the nearest Fire Brigade must be recorded at all stations (on Fire Notice "B") and in all Signal Boxes.
4. The Officer in charge of the Fire Brigade must be requested to report to the Railway Official in charge of the incident and must be advised where to find him.
5. When the railway breakdown vans arrive, the Official in charge of the vans must contact the Officer in charge of the Fire Brigade and co-ordinate the efforts of the rescue squads.
6. It will be the responsibility of the Operating Department Official in charge to ensure the safety of men working at the site from the movement of traffic on adjoining lines.
7. Divisional Operating Officers should ensure that all stations are advised of these instructions and that the Fire Brigades in the district are acquainted with the instructions issued to the railway staff.
8. The ~~Breakdown Van Driver~~ **Motive Power Supt** will satisfy himself that the Officials in charge of breakdown vans are advised of these instructions and will be prepared to direct the efforts of railway and fire personnel at the site of the accident.

(G.A.23—7/49. L.K.1, '8669/33.)

PROTECTION OF GOODS IN TRANSIT—MISHAPS.

In the event of mishap involving derailment of loaded wagons where the contents are liable to misappropriation, immediate action must be given by the District Superintendent or District Traffic Manager, who will arrange with the Divisional Police Officer for police protection of the goods to be provided.

(G.A. 18. 11/47. L.K.2/15868/3.)

Reference to the following to appear on page 263 :—

EMERGENCY ARRANGEMENTS.

In an emergency, necessitating speedy transit of materials, etc., on behalf of the Engineering Department, arrangements for the transport of materials and plant as necessary, either (a) by special train, (b) by prompt despatch on ordinary freight train, or (c) by passenger train, according to circumstances, will be made with the Superintendent of the Line by the Chief Engineer or Stores Superintendent, who will supply all requisite information such as source, wagon numbers, time available, etc. In the case of materials and plant from local sources, Divisional Engineers should keep in touch with Divisional Superintendents (or District Traffic Managers) or the Control Office, who will require similar information of the traffic expected to pass. Local arrangements for delivery to site of materials coming from a distance should also be made between the Divisional Engineers and Divisional Superintendents (or District Traffic Managers) or the Control Office.

The special attention of all concerned is directed to the importance of ensuring prompt transit of materials and plant, as ordered.

G.A. 1. 3/37. E/74645/H.)

STATION INSTRUCTIONS.

PROMPTITUDE IN DEALING WITH ACCIDENTS, ETC.—Continued.

in which an accident or casualty may be likely to present itself, so that if the need should arise, he may be quite familiar with the details of the exceptional arrangements to be made, and may act with full confidence in adherence to established Regulations which it is so necessary should be observed; but it must be remembered, that it is preferable to set about the adoption of the special working with speed, than to wait until the best arrangements being made, rather than to err from want of sufficient knowledge of mind or calm judgment. The necessary materials in the shape of Pilot Forms for single line working, the forms shown in Rules 183 and 184, and the Pilot's knowledge, should be kept together in a convenient place in the Signal Box, and it is the Station Master's duty to satisfy himself from time to time that they are ready for immediate use. Attention is also called to Rules 180, 181, 197 and 196 of the Rule Book, and on page 265 of this Book with respect to the fastening of loose Runaway Catch Points before commencing to work Single Line.

4 The ramps provided at some Stations and shown in the Appendices to the Working Time Books for the purpose of regulating the movement of other vehicles which may get off the Line, should also be kept in a convenient place well known to the staff at the Station, preferably near to where most of the shunting operations take place.

5 The proper method of making the Pilot arrangements, in the circumstances shown in the diagram, is as follows:—



The Line is supposed to be blocked at the point X, and in such a case two Pilots should be sent, one to work between A and C, and the other between B and D, the latter to be instructed to write on the Single Line Form not to take a train intended for A further than the home signal at the station B, from which point it must be taken onward to A by the Pilotman working between A and C.

When there is a frequent service of trains, the Pilotman working between A and C may find it convenient to work a train from A to C, to admit and send the train on to A by verbal order if convenient. But in such cases either before or approaching that point from D, the train going to A. If a train is going to A from D, it should be put into force, and if the train from D for A, the first train to be sent on first, and afterwards follow himself.

In such cases the Pilotman working between A and B, and the Pilotman working between B and D, should be put into force. In such cases the Pilotman may take a train to the station B, where it must be handed over to the Pilotman of the section to be worked.

6 Should the mishap involve the derailment of an Engine or Vehicles, and any of the running roads be blocked, the breakdown gang, if required, must be wired for at once. While doing so, it must be ascertained which line is blocked, and such other information given as may be useful. Single line working arrangements should then be put into force, and if the breakdown gang has not arrived, the Traffic Staff should, if possible, be used by the Traffic Staff to clear the line by any means at their disposal. In no case should the running roads be made use of for traffic purposes after a vehicle has been off the line until the Permanent Way Inspector or Gang or other competent member of the Engineer's Staff has certified that it is fit for use, nor should a vehicle which has been off the rails be allowed to run in a train until it has been examined and certified as fit to do so by a carriage and Wagon Inspector or Examiner. The working should also be carefully examined if it has been interfered with in any way through the accident.

7 Each Station Master must make himself acquainted with the facilities for working the traffic of a single line over a single line, not only at his own station, but at the stations on each side of his, and he must keep exhibited in a conspicuous place at the station, accessible to all the Traffic Staff, a list showing the extent for the nearest breakdown gang, and the names and addresses of the men from the Signal and Engineering Departments. Special attention is also directed to the instructions contained in Rule 177 of the Rule Book as to the Officers to whom information is to be telegraphed.

8 Should an accident happen of a character to cause a total stoppage of traffic by the usual routes, or a considerable block, prompt arrangements should be made for diverting the trains by any other route that may be practicable, care being taken in such cases that all necessary precautions are taken to the satisfaction of the persons concerned. In all cases the best arrangements practicable must be made for the comfort and general convenience of passengers who are detained, and the best arrangements made for the relief of the natural anxieties of the public who may be awaiting the arrival of their friends.

9 In the case of a fire or a fire involving damage to Goods Rolling Stock or contents, the Station Master must be sent promptly to the District Goods Manager, and Local Goods Agents, so that steps may be taken to deal with any urgent or perishable traffic.

STATION INSTRUCTIONS.

CROSSING OF RAILWAY BY OVERHEAD ELECTRIC POWER LINES.

In connection with the Electricity Grid System, arrangements are being made by the Central Electricity Board to fix at towers on each side of the railway an additional label giving the telephone number of the Board's Control Room under whose jurisdiction each particular line comes and particulars are also shown in the Signal Box and/or Station on each side of the power cables crossing the lines. Where the Signal Box on either side is switched out, the particulars are also recorded in the nearest Signal Box open.

The labels will be lettered in white on red background, and the following is a specimen of the wording:

<p>CONTROLLED FROM TELEPHONE NO BRISTOL 35055 BRISTOL.</p>	}	<p>The number and town applicable will be shown.</p>
--	---	--

For installations of Electricity Power lines other than those of the Central Electricity Board labels are not in all cases fixed on the pylons, but particulars of the Electricity Power Company to be approached are provided at the Signal Box and/or Station on each side of the power cables crossing the lines.

In the event of wires breaking or sagging and fouling any of the lines, signals, or other parts of the Company's property, the person becoming aware of the mishap must immediately advise the nearest Signal Box and/or Station Master, stating if possible the name of the Company or Authority supplying the power. The Station Master or Signaller, who will be furnished with the address of the Central Electricity Board's Control Room, or Power Company's responsible officer must communicate at once with the following for his Engineer on duty, giving particulars with the appropriate identification letters and numbers and obtain instructions. The obstruction must be protected as soon as practicable in accordance with Rule 217.

THE STAFF ARE SPECIALLY REMINDED THAT NO ATTEMPT MUST BE MADE TO INTERFERE WITH OR TOUCH A BROKEN WIRE OR ANY RAIL OR OTHER METAL STRUCTURE IN CONTACT WITH THE BROKEN WIRE UNTIL NOTIFICATION HAS BEEN RECEIVED FROM THE ELECTRICITY COMPANY'S ENGINEER THAT THE CURRENT HAS BEEN CUT OFF AT THE POWER HOUSE, AND THE WIRE IS SAFE TO HANDLE.

In addition to the steps mentioned above, an advice must immediately be sent to the nearest telegraph Lineman, and if the section of line on to which the electric power is fed is track covered by telegraph wires alongside the line an advice must be despatched promptly to the Telegraph Lineman.

REPORTING OF ACCIDENTS.

Injury to
employees.

Employees sustaining any injury in the course of their work must make a written report of the circumstances to their superior officers.

Wagon brakes.

Any employee concerned in, or who may have witnessed an accident, must make a written report giving full particulars of his knowledge of the occurrence and hand it to his superior officer.

Injury to other
persons.

If an accident happens to a train belonging to the Company whilst running upon the line of another Company, Guards must report the occurrence to their superintendents by telegraph if necessary.

Accidents to
passengers.

In the case of accidents to passengers and other members of the public whilst on station premises, full particulars of the accident if any must be obtained.

Cause of accident.

The cause of the accident must if possible be obtained from the injured person, and dealt with in the statement of the members of the Staff who have knowledge of the case.

Wagon Brakes.

If an accident occurs to an employee whilst applying or releasing a wagon brake, the type of brake and owner of the vehicle must be given in the statement. Any defect in the brake must be described.

Examination of
site of accident.

If a passenger sustains an injury by falling on steps or platform, or when joining or leaving trains or approach roads, the place where the mishap occurred (including the footboard of the coach if the injured person was boarding or alighting from a train) must be noted at the time and an examination made, so that the cause of the accident may be ascertained definitely. Similar action must be taken by the Guards and Travelling Ticket Collectors if a passenger sustains injury due to any alleged defect in carriages, etc.

Independent
witnesses.

In all cases of accident causing injury to members of the public or to Company's employees, or damage to property either on the Company's premises, or in public thoroughfares or on private premises in connection with the Company's road vehicles, it is important that the names and addresses of any independent witnesses to the occurrence should be taken.

1. Referring to Rules 195 and 196 in the Rule Book, when it becomes necessary to work the traffic of a ~~single~~ over one part of a run which there is a self a time use runaway each-point, one of the following arrangements may be used to fasten the loose point against the stock rail so as to make the line continuous.

- 1st - A clip passes through the switch tongue and through the stock rail to which is attached a padlock.
- 2nd - A clip passes under the stock rail and the switch tongue so as to embrace them and bring them close together by means of a screw.

These appliances, which will be kept in the Signal Boxes, are designed for the purpose of firmly securing the switch tongue so as to make it safe for trains passing along the running line in the facing direction.

2. To enable the switch tongue to be closed against the stock rail, a loose lever for insertion in the frame opposite the catch-point is kept in the signal box.

3. Before stage line working is commenced, a PM man positioned with hand signals and a PL man positioned with the key in the PL man's hand, will be in the position, one step back from the stage line, and the PL man will be in the position, one step back from the stage line. Before taking the first throw over the stage point, the PM man must assure himself that the rats are properly held, or copped and prepared, and when he has so related, the key of the primes from the rats at the points and accept this as possible to take the stage line working has ceased. The man in charge of the points must remain at the point of the throw, and when the stage line is cleared, it must be the PL man's job to take the key from the PL man and place it in the PL man's hand, and when the stage line is cleared, it must be the PL man's job to take the key from the PL man and place it in the PL man's hand, and when the stage line is cleared, it must be the PL man's job to take the key from the PL man and place it in the PL man's hand.

4. Boards indicating the position of catch points are provided fixed on the skew as 15 feet Drivers and worded "Catch Points."

As incidents occur from time to time when an clearly calls for Israeli to those who are in a position which may be a friend of the streets for the staff generally states their presence and authority in order that unnecessary risks may be avoided.

It is also a matter to employees as a result of their using unauthorised and in any case, the use of unauthorised routes for work and also in irregular use of the line with passing of the line. The use of unauthorised routes in any case, in particular, capacity to the unauthorised route, so as to avoid the use of an authorised route with going to or from work or whilst on duty.

Attention is particularly directed to—

12. As to coupling and uncoupling of vehicles, and enforcing the use of coupling poles wherever practicable.

Rule 11. Warning servants of the Company not to expose themselves to danger.

Rules 14 and 15. As to trespassing and walking on the Railway.

Rule 201 (a) As to Flag signalman standing in a position of safety when giving hand signals to Drivers.

Rules 110, 111 and 112. As to the precautions to be taken before vehicles are moved or shunted.

Rules 234 to 236 inclusive, which deal with the precautions to be observed by Lengthmen and others engaged upon the permanent way.

Rules 25, 126, 163 and 185, also specially relate to matters of safety.

The use of shunting poles for any other purpose than that of coupling or uncoupling vehicles is strictly prohibited. Shunters and others are specially warned against the dangerous practice of riding on shunting poles or brake sticks.

STATION INSTRUCTIONS

ACCIDENTS TO COMPANY'S SERVANTS—Continued.

Treatment of
minor injuries.

Station Masters, Clerks, Agents, and others employed are required to have the following notice exhibited in a prominent place for the information of the staff:—

NOTICE TO THE STAFF.

MINOR INJURIES.

Personal injuries are sometimes sustained by the Staff, particularly those engaged in handling traffic, which, whilst appearing to be slight and not needing attention at the time, subsequently develop into blood poisoning, owing to the wound becoming dirty.

Much suffering might be avoided if members of the Staff who meet with slight injuries such as cuts or abrasions sought treatment from a qualified ambulance worker without delay.

Persons qualified to render "First Aid":—

..... Office,
.....
.....
.....

Supplies of the notice, if required, can be obtained on application to the Divisional Superintendent, District Goods Manager or District Traffic Manager.

PREVENTION OF ACCIDENTS.

Trains must not be shunted about station yards with the doors of wagons left open, either hanging down or swinging about.

The attention of all concerned is directed to Clause (b) of Rules 112 and 115a, which states that vehicles must not be moved unless the doors are properly closed and fastened or in such a position that they will not come into contact with any obstruction when the wagons are moved.

Level Crossings At stations where passengers have to cross over the rails from one platform to another, the Staff must exercise the utmost supervision to prevent the risk of accident.

At all stations where foot-bridges are provided special steps should be taken to prevent passengers using the level crossings.

At level crossings Passengers carrying heavy goods, cycles, or motor cycles of luggage must be very careful when they descend the platforms at level crossings in all cases give timely and efficient warning of their approach.

Closing Carriage Doors To avoid injuries to passengers by reason of their fingers being caught in the carriage doors, the Staff must exercise every reasonable care to avoid inflicting personal injury.

Securing of Platform Seats. Platform seats, if any, and places must be secured to prevent them from being blown off the platform.

Inspection of Roof The Staff must see that the roof of the coaches is properly secured to prevent the boards from falling from express trains, the boards must be properly placed in the slots provided for their reception before the coaches start on their journey, and the attention of Station Masters, Inspectors and others concerned is specially directed to the importance of seeing that this is done before despatching the trains.

Guards' Brake Van Doors. Guards working Passenger Trains must not allow their van doors to be open in such a way as to incur danger to persons on the platform while their trains are running into and out of station.

PLATFORMS AND STEPS IN FROSTY WEATHER.

In frosty weather the Station Master, or person in charge, must, the first thing in the morning, or at other times, see that either ashes, sand or fine gravel are sprinkled on all parts of platforms, stairs, foot crossings between platforms, or other necessary places used by passengers.

This must also be done with regard to the steps of carriages.

PREVENTION OF ACCIDENTS—Page 266.

The following to be inserted as paragraph 5 :—

Whitening of
Platform
Edges.

The edge of platforms must be whitened for a width of approximately 5 inches
The white lines must be renewed not less than once a month in Summer and weekly
in Winter (G.A.23-7 49. L.K.I. 9678 Gen.)

The edges of the ramp must not be whitened G.A. 30

PREVENTION OF ACCIDENTS.—Page 266.

The following to be added to these instructions :

Maximum
load gauges.

Load gauges having adjustable ends must be left in the normal or upright position when
not in use. (G.A. 1 3, 37. L.K.I. 3948/14.)

STATION INSTRUCTIONS.

SECURING AND HANDLING OF PLATFORM TROLRIES, BARROWS, &c

To prevent mishaps through platform trolleys or barrows falling on to or from any of the lines, the special attention of the Staff concerned is directed to the following instructions:—

Securing Trolleys, Barrows, &c. on Platforms—Luggage trolleys, barrows, &c., when left standing, will be secured by the Staff. They must be properly secured to be on a safe position. They must be kept away from the edge of the platform, and when not in use, near fences or the edge of the platform, they must be secured by the Staff. They must be kept away from the edge of the platform, and when not in use, near fences or the edge of the platform, they must be secured by the Staff. They must be kept away from the edge of the platform, and when not in use, near fences or the edge of the platform, they must be secured by the Staff.

Trolleys fitted with automatic brakes must, before being left, have the handles raised so as to apply the brakes, and must be secured by the Staff to prevent the handles being placed in this position.

These instructions must be observed by the Staff, and also by privately owned trolleys or barrows used for station platform purposes.

The unauthorised use of platform trolleys must be prevented, and the utmost vigilance should be exercised by the staff generally in preventing any unauthorised persons, especially school children, newspaper, refreshment and telegraph boys or girls from interfering with or wheeling them.

MOVEMENT OF TROLRIES Before attempting to cross a line or lines, the man in charge of a trolley or barrow must stop and look both ways to assure himself that no train or engine is approaching. Care must be taken to load the trolley or barrow so that nothing is likely to fall off, and that the load is not too heavy to be taken across the line in safety.

Platform trolleys must be secured by the Staff when left on the platform at any time, and care must be taken to prevent them from being used for any other purpose, or from being used through the station, to keep them as far from the edge of the platform as possible.

Other instructions relating to the use of platform trolleys, barrows, &c., are given in the following instructions:—
To be observed by the Staff and by the public:—
Other instructions relating to the use of platform trolleys, barrows, &c., are given in the following instructions:—
To be observed by the Staff and by the public:—
Other instructions relating to the use of platform trolleys, barrows, &c., are given in the following instructions:—
To be observed by the Staff and by the public:—

OPENING GOODS TRUCK DOORS.

When a goods train is standing at a station, the doors of the trucks must be opened in such a manner as to prevent any person from falling on to or from any of the lines, or from being struck by any part of the train when it starts.

When a goods train is standing at a station, the doors of the trucks must be opened in such a manner as to prevent any person from falling on to or from any of the lines, or from being struck by any part of the train when it starts.

ROAD VEHICLES IN STATION YARDS.

Road vehicles must be kept at least six feet from the nearest line or siding, and must be kept at least six feet from the nearest line or siding.

When a road vehicle is in a station yard, it must be kept at least six feet from the nearest line or siding, and must be kept at least six feet from the nearest line or siding.

ACCIDENTS TO CHILDREN.

Company's servants are warned that young children must not be allowed to walk on or cross the permanent way for the purpose of bringing meals to their father or any other persons.

HOW TO STOP BLEEDING, WITH OR WITHOUT TOURNIQUETS

1. When a leg or arm is severely wounded, the first thing to be done is to stop the bleeding. This can be done by pressing the wound with the fingers, or by using a tourniquet.

2. Should there be much bleeding, raise the limb as high as you can above the body. To control bleeding without a tourniquet, press the wound with the fingers, or use a tourniquet. To control bleeding with a tourniquet, press the wound with the fingers, or use a tourniquet.

If you cannot find the pressure point, put a roller or a strip of cloth round the limb above the wound. A tourniquet (or strap) should not be applied to the bare limb, but over the clothing.

CRISTO

**THE RAILWAY EXECUTIVE
(WESTERN REGION).**

OFFICE OF COMMERCIAL SUPERINTENDENT,
PADDINGTON.

OFFICE OF OPERATING SUPERINTENDENT,
PADDINGTON.

INSTRUCTIONS TO STATION MASTERS AND OTHERS CONCERNED.

**Provision of emergency road services when rail
services are interrupted in consequence of accident,
flood, subsidence, or other unforeseen cause.**

The following revised instructions are given for the guidance of Station Masters and others who are concerned with the provision of emergency road services in the event of interruption of railway passenger road and will supersede the instructions contained in page 208 of the General Appendix to the Rule Book.

AUTHORITY FOR THE OPERATION OF ROAD SERVICES.

Under the provisions of the Transport Act, 1927, the British Transport Commission has obtained from the Licensing Authorities a general approval of the operation of road services in cases of emergency of passenger road services in aid of rail or boat services operated by the Railway Executive. This approval is subject to compliance with certain conditions which include the following:—

(1) NOTICE TO BE GIVEN TO THE LICENSING AUTHORITY AND POLICE.

The Licensing Authority and the Police are to be advised of all emergency road services operated, the route to be taken between which the service will be operated and the routes to be followed, if possible, before commencing to operate the road service, and if not so possible as soon as practicable thereafter.

(2) ROUTES TO BE FOLLOWED.

It will be necessary to comply with any Police instructions regarding routes to be followed, or otherwise, for the direction or control of traffic.

(3) NOTICE OF CESSATION OF ROAD OPERATIONS.

The Licensing Authority and the Police are to be advised immediately the road service is withdrawn.

PROCEDURE TO BE FOLLOWED.

In the normal course there will be consultation with the Office of the District Operating Officer prior to the introduction of road services, but if for any reason that Office cannot be contacted, it will be necessary for Station Masters to act on their own initiative.

To comply with the cases (1) to (3) above, it will be the responsibility of the person ordering and terminating the road transport arrangements:—

- (a) to advise, by telephone, the local police officer responsible for the area in which the service is to be operated, concerning, if necessary, with the operator engaged to provide the service as to the route which will be followed by the vehicles;
- (b) to advise, by telephone or telegram, the District Commercial Officer of the arrangements, including the route to be followed in order that the District Commercial Officer can give the required advice to the Licensing Authority concerned;
- (c) to advise the local police officer and the District Commercial Officer of the termination of the road service and to furnish to the latter a report of the arrangements, giving the following information:

- (i) Name of operator engaged;
- (ii) Dates on which the road service was operated;
- (iii) Number of trips run between the various points each day;
- (iv) Number of passengers carried each day;
- (v) Number of vehicles hired each day (stating whether any vehicles hired but not utilised for any reason);
- (vi) The vehicle mileage each day.

Wherever possible, with reasonable convenience, an associated undertaking should be called upon to provide road services in preference to a non-associated operator.

If there is doubt as to the operator to be approached in an emergency, guidance should be sought forthwith from the District Commercial Officer concerned.

If questions are raised by operators as to the legality of the services they are asked to provide, they should be informed that the necessary authority is held by the British Transport Commission.

It is the intention that vehicles engaged on these services shall bear a notice "Emergency Service for Rail Passengers". These labels are provided by the Railway Executive and arrangements have been made for Associated Omnibus Companies to hold supplies at their various garages and to make a label to each vehicle hired to the Railway Executive. Stations should, however, hold a supply for the name of the District Commercial Officer for fixing to vehicles hired from other operators.

All classes of railway tickets will be honoured on the special road services between the points for which they are available and only passengers holding rail tickets are to be allowed to travel on those services.

Rail tickets will not be collected on the road services except when the stage traversed by road represents completion of the journey covered by the rail ticket. It is desirable, where practicable, that the railway staff should examine and collect the tickets but when the collection is undertaken by the bus staff, such tickets should be handed to the responsible railway official at the end of the road journey.

Accounts subsequently received for the operation of road services under these arrangements should be certified, if correct, and passed to the District Commercial Officer.

The above instructions relate to instances where special road services are necessary. If there is no existence of a conventional road service linking stations between which it is required to provide for the conveyance of railway passengers by road, it may be possible to arrange for the passengers to be conveyed on this ordinary service. The railway tickets being honoured by the operator and an account (accompanied by any receipts etc.) rendered later, when the Railway Executive is notified of the arrangements. The Associated Omnibus Companies have agreed that such arrangements will be brought into operation on the written authority of the Station Master or other responsible official. In such cases it should be necessary to advise the Licensing Authority or Police of the arrangements but a full report of the circumstances must be made to the District Commercial Officer.

C. FURBER,

COMMERCIAL SUPERINTENDENT.

(B7/164/C.)

GILBERT MATTHEWS,

OPERATING SUPERINTENDENT.

(A2/71106/22.)

April, 1951.

Sheffield United Tours, Ltd., 88, Kingsway, London, W.C.2.
Simpsons and Forresters, Ltd., 1, Market Street, Dunfermline.
Southdown Motor Services, Ltd., 5, Steine Street, Brighton.
Southern National Omnibus Co., Ltd., 48-50, Queen Street, Exeter.
Southern Vectis Omnibus Co., Ltd., Nelson Road, Newport (I. of W.).
Sunderland District Omnibus Co., Ltd., Philadelphia, Co. Durham.
*Thames Valley Traction Co., Ltd., Lower Thorn Street, Reading.
Tadmor & Co., 1, 3, 5, 7, 9, 11, 13, 15, 17, 19, 21, 23, 25, 27, 29, 31, 33, 35, 37, 39, 41, 43, 45, 47, 49, 51, 53, 55, 57, 59, 61, 63, 65, 67, 69, 71, 73, 75, 77, 79, 81, 83, 85, 87, 89, 91, 93, 95, 97, 99, 101, 103, 105, 107, 109, 111, 113, 115, 117, 119, 121, 123, 125, 127, 129, 131, 133, 135, 137, 139, 141, 143, 145, 147, 149, 151, 153, 155, 157, 159, 161, 163, 165, 167, 169, 171, 173, 175, 177, 179, 181, 183, 185, 187, 189, 191, 193, 195, 197, 199, 201, 203, 205, 207, 209, 211, 213, 215, 217, 219, 221, 223, 225, 227, 229, 231, 233, 235, 237, 239, 241, 243, 245, 247, 249, 251, 253, 255, 257, 259, 261, 263, 265, 267, 269, 271, 273, 275, 277, 279, 281, 283, 285, 287, 289, 291, 293, 295, 297, 299, 301, 303, 305, 307, 309, 311, 313, 315, 317, 319, 321, 323, 325, 327, 329, 331, 333, 335, 337, 339, 341, 343, 345, 347, 349, 351, 353, 355, 357, 359, 361, 363, 365, 367, 369, 371, 373, 375, 377, 379, 381, 383, 385, 387, 389, 391, 393, 395, 397, 399, 401, 403, 405, 407, 409, 411, 413, 415, 417, 419, 421, 423, 425, 427, 429, 431, 433, 435, 437, 439, 441, 443, 445, 447, 449, 451, 453, 455, 457, 459, 461, 463, 465, 467, 469, 471, 473, 475, 477, 479, 481, 483, 485, 487, 489, 491, 493, 495, 497, 499, 501, 503, 505, 507, 509, 511, 513, 515, 517, 519, 521, 523, 525, 527, 529, 531, 533, 535, 537, 539, 541, 543, 545, 547, 549, 551, 553, 555, 557, 559, 561, 563, 565, 567, 569, 571, 573, 575, 577, 579, 581, 583, 585, 587, 589, 591, 593, 595, 597, 599, 601, 603, 605, 607, 609, 611, 613, 615, 617, 619, 621, 623, 625, 627, 629, 631, 633, 635, 637, 639, 641, 643, 645, 647, 649, 651, 653, 655, 657, 659, 661, 663, 665, 667, 669, 671, 673, 675, 677, 679, 681, 683, 685, 687, 689, 691, 693, 695, 697, 699, 701, 703, 705, 707, 709, 711, 713, 715, 717, 719, 721, 723, 725, 727, 729, 731, 733, 735, 737, 739, 741, 743, 745, 747, 749, 751, 753, 755, 757, 759, 761, 763, 765, 767, 769, 771, 773, 775, 777, 779, 781, 783, 785, 787, 789, 791, 793, 795, 797, 799, 801, 803, 805, 807, 809, 811, 813, 815, 817, 819, 821, 823, 825, 827, 829, 831, 833, 835, 837, 839, 841, 843, 845, 847, 849, 851, 853, 855, 857, 859, 861, 863, 865, 867, 869, 871, 873, 875, 877, 879, 881, 883, 885, 887, 889, 891, 893, 895, 897, 899, 901, 903, 905, 907, 909, 911, 913, 915, 917, 919, 921, 923, 925, 927, 929, 931, 933, 935, 937, 939, 941, 943, 945, 947, 949, 951, 953, 955, 957, 959, 961, 963, 965, 967, 969, 971, 973, 975, 977, 979, 981, 983, 985, 987, 989, 991, 993, 995, 997, 999, 1001, 1003, 1005, 1007, 1009, 1011, 1013, 1015, 1017, 1019, 1021, 1023, 1025, 1027, 1029, 1031, 1033, 1035, 1037, 1039, 1041, 1043, 1045, 1047, 1049, 1051, 1053, 1055, 1057, 1059, 1061, 1063, 1065, 1067, 1069, 1071, 1073, 1075, 1077, 1079, 1081, 1083, 1085, 1087, 1089, 1091, 1093, 1095, 1097, 1099, 1101, 1103, 1105, 1107, 1109, 1111, 1113, 1115, 1117, 1119, 1121, 1123, 1125, 1127, 1129, 1131, 1133, 1135, 1137, 1139, 1141, 1143, 1145, 1147, 1149, 1151, 1153, 1155, 1157, 1159, 1161, 1163, 1165, 1167, 1169, 1171, 1173, 1175, 1177, 1179, 1181, 1183, 1185, 1187, 1189, 1191, 1193, 1195, 1197, 1199, 1201, 1203, 1205, 1207, 1209, 1211, 1213, 1215, 1217, 1219, 1221, 1223, 1225, 1227, 1229, 1231, 1233, 1235, 1237, 1239, 1241, 1243, 1245, 1247, 1249, 1251, 1253, 1255, 1257, 1259, 1261, 1263, 1265, 1267, 1269, 1271, 1273, 1275, 1277, 1279, 1281, 1283, 1285, 1287, 1289, 1291, 1293, 1295, 1297, 1299, 1301, 1303, 1305, 1307, 1309, 1311, 1313, 1315, 1317, 1319, 1321, 1323, 1325, 1327, 1329, 1331, 1333, 1335, 1337, 1339, 1341, 1343, 1345, 1347, 1349, 1351, 1353, 1355, 1357, 1359, 1361, 1363, 1365, 1367, 1369, 1371, 1373, 1375, 1377, 1379, 1381, 1383, 1385, 1387, 1389, 1391, 1393, 1395, 1397, 1399, 1401, 1403, 1405, 1407, 1409, 1411, 1413, 1415, 1417, 1419, 1421, 1423, 1425, 1427, 1429, 1431, 1433, 1435, 1437, 1439, 1441, 1443, 1445, 1447, 1449, 1451, 1453, 1455, 1457, 1459, 1461, 1463, 1465, 1467, 1469, 1471, 1473, 1475, 1477, 1479, 1481, 1483, 1485, 1487, 1489, 1491, 1493, 1495, 1497, 1

TOTAL

DEFECTIVE DOOR LOCKS ON PASSENGER COACHES.

Should a defect in the door handle lock of a corridor coach come under notice, the vehicle must be labelled for repairs, but the coach may be allowed to continue temporarily in traffic if the door handle is tied, the door locked with the carriage key and the hole of the carriage key lock plugged with timber.

In the event of a defect being observed in the door handle lock of a non-corridor coach, the passengers must be advised that the vehicle is not to be used, the blinds must be drawn, door handles tied, both doors locked with the carriage key and the holes of the carriage key locks plugged with timber. The coach must be labelled for repairs.

If both the door handle lock and the carriage key lock are defective, the coach must not be allowed to go forward.

(G.A.16. 5/46. L.K.2/10482/N.)

The following instruction to be inserted on page 270:—

MISSING WINDOW STRAPS IN NON-CORRIDOR STOCK.

When window straps are missing in compartments of a non-corridor stock and there is no means of communication with the Guard or other staff concerned must draw the blinds, close the windows and lock up the compartment.

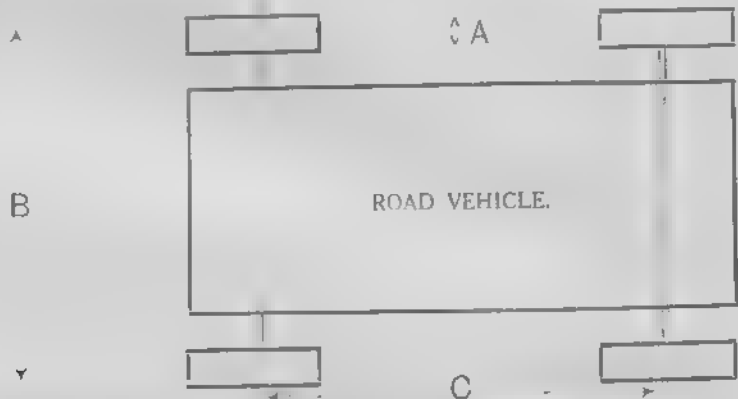
A report on a carriage examiner must be drawn to the defects and the Guard must record particulars on his train journal.

If the vehicle is to remain in service until repair straps are available the key holes must be plugged immediately after the vehicle has completed the journey on which the defect was noticed, in order to prevent irregular entry.

(G.A.16. 5/46. L.K.2/1437 L.P.)

The following are particulars, as per diagram below, of the road vehicle which will be used. —

(a)	Width of wheels	..	..	..
(b)	Width of road vehicle over wheels			
(c)	Length of wheel base	..	..	..



Now, when we have three lawyers, we must have to the Secretary Department, in order that proper care may be taken of the papers of the deceased.

When it is necessary to have the papers of a deceased person examined, the District Attorney may take the papers, and after examining them, may send them to the District Attorney, or his representative, personally, for report to the Secretary Department.

TRESPASSING ON THE RAILWAY.

When an application is made for a certificate of trespassing upon the line, the following particulars must be given on the standard form:—

1. Christian and Surname of Offender.
2. Address and Occupation
3. Whether address is a verified one.
4. Date and place of offence.
5. Name of the station or place where the offence took place, and if so to what Act of Parliament they refer.
6. Whether any trains were due. If so, name them.
7. If not, state if the trespasser was exposing himself to danger, and, if so, how.
8. State particulars of refusal to quit, if any.
9. Particulars of any previous irregularities.
10. If offenders are young, state age.
11. General observations on facts of case.

11. General observations on traffic signs
 The present traffic signs used to control and regulate the traffic have had special powers
 under the Motor Vehicle Act and the Traffic Signs Regulations. It is suggested that any
 new traffic signs proposed, any of the signs of the Motor Vehicle Act or the Traffic Signs
 Regulations, or any other sign, which is not a sign of the Motor Vehicle Act or the Traffic
 Signs Regulations, should be subject to the provisions of the Motor Vehicle Act No. 188, any person
 who contravenes the provisions of the Motor Vehicle Act No. 188, shall be liable to a fine of
 \$100 or to imprisonment for a term not exceeding 3 months or to both such fine and imprisonment
 (if any).

[illegible]

INSTRUCTIONS TO TICKET COLLECTORS AND EXAMINERS.

A case has occurred where a passenger holding the backward half of a third class return ticket desired to travel first-class for a portion of the journey only. The excess was collected, and the ticket given back to the passenger but without being punched. The ticket held by the passenger was incorrectly shown on the Excess Note, and the Excess Note was therefore accepted for the journey from and to the Stations shown upon it, thus enabling the passenger to travel the backward half of the original ticket, and as there was no punch mark to indicate that it had been used a Return Fare Claim was made.

2. Petty Cash Where considered necessary by the Divisional Superintendent of Taxes & Collectors of the persons who are regularly occupied in collecting or examining the tax and interest on the supply of, or the Station Master or Booking Clerk with a sub-tenant amount of petty cash, a provision may be made so that there may be no difficulty in giving change to passengers when they are excoressed.

4. Receipts for Excess Fares. No ticket sales have been made where the collecting staff have received fares at less than the published rates. Any staff member who receives fares at less than the published rates should report the same to the Station Manager. The Station Manager should ensure that the staff members of the Supervisory Staff should see that this instruction is strictly carried out.

5. **Cancellation of Tickets.** All tickets booked must be cancelled as except in the circumstances set out below, carrying penalty. Tickets upon which excess has been paid in accordance with the provisions of the above paragraph shall be returned to the Office of the Secretary (Form 305 1), and dealt with in accordance with Clause 198c of Station Accounts Instruction Book.

The special envelope provided for the conveyance of these tickets must be used in all cases.

6. **Examination and Collection of Tickets.** Upon the arrival of a train at a station where the Tickets have to be examined or collected the Ticket Collectors must be on the platform ready to do so, and as soon as possible after the arrival of the train, the work of examination must be commenced from the first to the last carriage, and the door of each compartment must be locked until the train is nearly ready to start.

One will if its intent be made to ensure that persons over 14 years of age do not travel with half tickets, nor children over three and under 14 without any ticket at all.

7. Collecting Stations. Under no circumstances may rail tickets be issued to cover excess received at collecting stations either in the train or at the exit doors.

8. **Intent to Defraud.** When cases arise of excess having to be collected, the Ticket Collector must deal with the matter in such a way as to cause the least possible delay to the train. If he has good reason to believe that the passenger is committing an irregularity, with intent to defraud the Company, the excess fare should not be demanded, but the attention of the Station Master or other person in charge must be called to the matter immediately.

9. Collection of Tickets at "Way Out" Gates - The Ticket Collector must collect tickets at the "Way Out" Gates must observe as far as possible the class of carriage from which passengers alight, so that he may have some check upon the tickets given up.

10. Necessity of Ticket Collector travelling to next stopping Station.—It may be necessary in certain instances (such as the passenger having a time limit ticket or making the attempt to avoid payment of the excess, rather than delay the train, for a Ticket Collector to proceed with the train to the next stopping station, but this must not be done without the concurrence of the Station Master. If the Ticket Collector cannot accompany the train the Guard must be informed and a

STATION INSTRUCTIONS

INSTRUCTIONS TO TICKET COLLECTORS AND EXAMINERS *(Continued)*

to prevent further delay. If a passenger is found to be in possession of a ticket, the Ticket Collector or Examiner should be instructed to take the passenger to the station and to cause further delay to the train at the examining station.

11. Sailors without Tickets. When a passenger is found to be in possession of a ticket, the Ticket Collector or Examiner should be instructed to take the passenger to the station and to cause further delay to the train at the examining station. Mess No., and any further details likely to assist in tracing the man, when the Authorities are written for the fare due, should be taken.

For full instructions in regard to Examination and Collection of Tickets and charging and accounting for Excess Fare, see re issue of Circular 1807, dated ~~October, 1923~~ **April 1938**.

NOTE.—The special attention of all concerned is directed to Superintendent of Line's Circular ~~1807~~, **July, 1949**. **August 1938**

EXAMINATION AND COLLECTION OF TICKETS.

1. Examination of Lavatories in Trains. Station Masters should be instructed to cause examination of Lavatories in trains when tickets are being examined, or collected for open stations.

(a) Station Masters should be instructed to cause examination of Lavatories in trains when tickets are being examined, or collected for open stations.

(b) Lavatories in Non-Corridor Coaches must be systematically examined.

(c) UNDER NO CIRCUMSTANCES IS A LAVATORY TO BE EXAMINED BY LOWERING THE WINDOW FROM THE OUTSIDE.

(d) In the event of difficulty owing to a Lavatory being "engaged," the Station Master, or person in charge, should be instructed to obtain assistance for dealing with the case at his discretion.

2. Entrances and Exits at Stations Left Open Unnecessarily. No instructions have been issued to the effect that the entrances and exits at stations should be left open unnecessarily.

(a) Station Masters should be instructed to cause examination of Lavatories in trains when tickets are being examined, or collected for open stations.

(b) Lavatories in Non-Corridor Coaches must be systematically examined.

(c) UNDER NO CIRCUMSTANCES IS A LAVATORY TO BE EXAMINED BY LOWERING THE WINDOW FROM THE OUTSIDE.

(d) In the event of difficulty owing to a Lavatory being "engaged," the Station Master, or person in charge, should be instructed to obtain assistance for dealing with the case at his discretion.

3. Scrutiny of Tickets at the Time of Collection, or when Examinations are being made. Station Masters should be instructed to cause examination of Lavatories in trains when tickets are being examined, or collected for open stations.

(a) Station Masters should be instructed to cause examination of Lavatories in trains when tickets are being examined, or collected for open stations.

(b) Lavatories in Non-Corridor Coaches must be systematically examined.

(c) UNDER NO CIRCUMSTANCES IS A LAVATORY TO BE EXAMINED BY LOWERING THE WINDOW FROM THE OUTSIDE.

(d) In the event of difficulty owing to a Lavatory being "engaged," the Station Master, or person in charge, should be instructed to obtain assistance for dealing with the case at his discretion.

STARTING TRAINS AT STATIONS WHERE TICKETS ARE EXAMINED.

Station Masters, Inspectors and others in charge of Stations are instructed to take care that when the train is about to start, the Ticket Collector or Examiner is in the carriages or on the carriage steps.

At the starting point, the Ticket Collector or Examiner should be instructed to take the tickets from the passengers and to cause further delay to the train at the examining station.

Where the Tickets are examined at the starting point, the examination must be completed before the train is due to leave.

Where the Tickets are examined at the starting point, the examination must be completed before the train is due to leave.

Where the Tickets are examined at the starting point, the examination must be completed before the train is due to leave.

COLLECTION OF TICKETS FOR DOGS, BICYCLES, &c

Tickets for Dogs, Bicycles, etc., conveyed in the Guard's van are not to be collected until the Guard has arrived at his destination, and no Dog, Bicycle, or other article for which a ticket should be delivered to a Passenger until he produces such ticket, or pays the correct fare for such Dog, Bicycle, or other article, whether the Station be an open one or not.

STATION INSTRUCTIONS.

COLLECTION OF TICKETS FOR DOGS, BICYCLES, ETC.—Continued.

It must be clearly understood that although these tickets are not to be collected until passengers claim the articles from the Guard's van in the case of an "open" station, or until they are leaving the gates at a closed station, they must be *collected and punched* on a basis as if the passengers' tickets are so dealt with *en route*, or when collection is being made for the destination station, if an "open" one. The attention of Train Ticket Collectors is specially directed to this requirement.

Great care must be taken to ensure that Dogs, Bicycles, and other articles for which tickets are required, are properly booked at the starting point.

INSTRUCTIONS REGARDING THE ISSUE OF PLATFORM TICKETS AT CLOSED STATIONS.

General. 1. A charge of one penny is made for admission to the platform for all persons, including children over three years of age, who are not in possession of a railway ticket, or who are not otherwise authorised to be thereon; and the following instructions are to be imparted to all concerned, the Station Master and Chief Booking Clerk seeing that they are carried out properly.

Tickets. 2. A specimen is given below of the tickets, which will be issued from Automatic Machines in numerical order, and dated on the back.

ACTUAL SIZE.

FRONT OF TICKET.

21 11 01 6 | 8 | 2



GREAT WESTERN RAILWAY.

The holder is prohibited from entering the Company's Trains. **NOT TRANSFERABLE.**

Admit ONE to PLATFORM

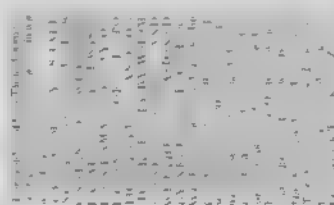
Available ONE HOUR on DAY of ISSUE ONLY ^{1^p}

This Ticket must be given up on leaving Platform

FOR CONDITIONS SEE BACK

1 | 2 | 3 | 4 | 5 | 6

BACK OF TICKET.

**Punching.**

3. The numbers printed on the margin of the tickets indicate the hours of the day. The hour at which a person holding a ticket is admitted to the platform must be indicated by the Ticket Examiner punching out, with the ticket mippers provided, the hour at which the holder is admitted to the platform, and the ticket is available for one hour only.

For example, a ticket presented at any time between 10 o'clock and 10.59 is to be punched in the space marked 10, and a person in possession of a ticket so punched is not to be challenged until 12 o'clock.

Collection.

4. From each person leaving the platforms who does not surrender or produce a railway ticket, hold a pass, pay an excess fare, or is not otherwise authorised to be upon the platform, a platform ticket is to be demanded. Particular attention is to be paid to the following

(a) That the ticket bears the current day's date.

(b) That the ticket is properly punched. **IF A PLATFORM TICKET BE COLLECTED NOT PUNCHED, AN EXPLANATION OF HOW THE TICKET WAS OBTAINED MUST BE ASKED FOR.**

(c) A ticket punched in the space marked 10, for example, indicates that the holder passed the barrier some time between 10 o'clock and 10.59, and should such a ticket be presented after 1 o'clock an explanation must be obtained as to how the holder has occupied the interval, on the assumption that a journey may have been made to an adjacent station and back without paying the fare.

(d) When platform or other tickets are not produced or surrendered, or the former are held for more than the stipulated period, the necessary excess fares are to be collected.

**Disposal of
collected Plat-
form tickets.**

5. Collected platform tickets are to be cancelled, sorted into numerical order, and sent to the Audit Office daily, in the same way as collected railway tickets. Under no circumstances must a collected platform ticket be re-issued.

**Monthly
Return.**

6. A monthly return of platform tickets issued must be rendered to the Superintendent of the Line.

**Persons
admitted to
platforms.**

7. In cases where the Station Master's Office, or the Cloak Room, is on the platform the usual arrangements for people entering to make enquiries at these Offices, and before admitting them through the barrier without a ticket it will be the duty of the Examiners to satisfy themselves that such persons are on legitimate business.

STATION INSTRUCTIONS.

ISSUE OF PLATFORM TICKETS AT CLOSED STATIONS—*Continued.*

Where there is not an Enquiry Office outside the ticket barrier all enquiries regarding train services (except arrangements &c.) must be dealt with in the first instance at the Booking Office, and only be referred to the Station Master's Office if it be impossible to deal with them at the former place.

The following may also be admitted to the platforms without a platform ticket:—

(a) Members of Messrs. Wyman's library wishing to exchange their books, on production of their library ticket.

(b) Messrs. Wyman's staff employed at the book stalls when on duty.

(c) Policemen and Postmen on duty, in the exercise of their duties.

(d) Wives and children of Company's servants, employed at the station, when bringing meals.

(e) "Boots" from the principal Hotels, when in livery.

Such persons, when admitted, must be asked to leave the station through the barrier by which they entered, and it should be an easy matter to make a statement made by a person leaving the platform without a platform ticket if the ticket is not the first time the ticketing man, and his course must be adopted if necessary, but at all times the ticket must be taken and sent back to the Company.

8. Passengers who allege that they have travelled from Platform Ticket Issuing Stations without a platform ticket are to be asked for the platform tickets, and if surrendered, excess fares are to be charged. If not surrendered, and no platform ticket is shown, the absence of satisfactory explanation, excess fares are to be charged from the station at which the ticket was exchanged prior to the arrival of the passengers at the station they state they have travelled from.

The return of the platform ticket when surrendered must be sent against the Excess Fare Entry, and the ticket is to be sent to the Audit Office with the collected tickets for the day.

9. Automatic Machines, actuated by means of a spring motor put in motion by the insertion of a penny coin, or which is supplied for issuing the necessary tickets, and will be placed in a position as posted on the station to the effect of "If possible it is a good idea to put the Ticket Collectors can see that they are not tampered with."

10. The Ticket Collectors of 1000s of 3000s and 10s of tickets to be placed in the machine, and the station provided for the tickets at the Booking Machines with tickets. The Ticket Collectors are to be placed in the machine, and the station provided for the tickets at the Booking Machines with tickets. The Ticket Collectors are to be placed in the machine, and the station provided for the tickets at the Booking Machines with tickets.

11. The Clerk in charge is to arrange for the machines to be kept replenished with tickets. The Ticket Collectors are to be placed in the machine, and the station provided for the tickets at the Booking Machines with tickets. The Ticket Collectors are to be placed in the machine, and the station provided for the tickets at the Booking Machines with tickets.

12. The spring motor operates about 500 times before it needs re-winding. A Whirling. The Ticket Collectors are to be placed in the machine, and the station provided for the tickets at the Booking Machines with tickets. The Ticket Collectors are to be placed in the machine, and the station provided for the tickets at the Booking Machines with tickets.

13. If the spring motor they can be released by pressing a button (or a detector which is fitted on the outside of the machine). The coin detector is to be handled carefully, and must not be dropped or roughly used.

14. A screw is fixed at the machine for altering the date type. At the close of each day the date is to be altered to that of the following day (see Clause 19). The dating device must be inked periodically, AT LEAST ONCE A MONTH.

15. EXTERIOR.

(a) The unknelt parts are to be rubbed occasionally with a suitable metal polish. (See Clause 19.)

polish.

IT IS IMPORTANT THAT THE WORKING PARTS OF THE MACHINE SHOULD BE KEPT CLEAN, AND THE FOLLOWING INSTRUCTIONS MUST BE CARRIED OUT ON THE FIRST DAY OF EACH MONTH.

INTERIOR.

(b) Feeding device. - This is to be dusted with the soft brush provided, care being taken not to damage the metal prongs.

(NOTE. Omission to do this is liable to cause failure of the machine.)

STATION INSTRUCTIONS.

ISSUE OF PLATFORM TICKETS AT CLOSED STATIONS.—*Continued.*

(c) Coin detector. This can be taken off by unscrewing the nut at the back, and opened out by loosening a red screw; this will then permit of the removal of dust and dirt.

(d) Chutes. These and the coin detector are best cleaned by means of sticks with linen wrapped upon them.

All the working parts of the mechanism should be oiled with pure machine oil, except the metal prongs in the feeding device, which must be cleaned but not oiled.

Defects

16. Any defects in the machine which cannot be remedied by the Station staff are to be reported to the Divisional Superintendent or District Traffic Manager at once, and by wire if necessary. In the meantime the tickets are to be transferred to the Booking Office, and issued therefrom if the machine will not work. In cases of breakdown a telegraphic advice to be sent to the Stores Superintendent, Swindon, who will arrange for a machine to be sent by return to take the place of the machine requiring repairs, the latter on return to Swindon being held for subsequent re-issue.

Cash.

17. The cash receptacle is to be cleared of cash at the close of each day, or more often if the circumstances require it, by the Chief Booking Clerk.

Record of Issues
and Passes
General
Account
No. 874
tickets.

18. The platform ticket issues are to be recorded in the Booking Clerk's Train Book similarly to railway tickets, and accounted for monthly in the space provided for the purpose on the Passenger General Account (874).

19. As the machine on issuing a ticket also dates the next ticket to it, at the close of each day the ticket next to be issued is to be obtained by the Booking Clerk after the date has been altered (see Clause 14), treated as non-issued, and sent to the Audit Office with the Passenger General Account 874.

20. At some Stations Machines supplied by the British Automatic Machine Co. are in use, and the paragraphs 21-28 take the place of paragraphs 9-15.

Operating
Machines

21. Card tickets of different designs for adjacent stations are issued.

22. To open machine, unlock and pull side cash drawer half way out; then remove the front plate. The latter should always be pushed right home before closing cash drawer.

Tickets

23. All tickets placed in machine must be of uniform thickness and absolutely flat.

Care must be taken that the edges of tickets are not damaged or frayed out in any way. The marks left by string when tickets are tied up in bundles should specially be looked for.

The weight should always be left on top of tickets in column.

Refilling
Machine

24. To re-fill machine with tickets, open the front door; then remove front plate and weight on top of the tickets. See that the tickets are put in the column squarely and flat.

Dating Device.

25. When changing date in type cylinder, remove inking roller, then pull the drawer right out, and allow the pins in side pawl to rest in the last tooth in the top rack; then push fork in the holes of the locking blade of type cylinder with the right hand, at the same time gently pressing on the type to prevent it jumping out with the left hand; then change type as required by means of tweezers supplied with the outfit; then remove fork and press down with the wooden block also provided for this purpose. To renew ink on roller, a very small portion of ink supplied should be spread on the glass slab with the palette knife and carefully rolled with hand roller; then roll type roller over surface of slab, and this to be repeated as required.

The ink must in no case be allowed to get between the type in cylinder, and must never be allowed to dry on same. This can be prevented by occasionally removing type and cleaning it with turpentine.

Putting Machine
out of use.

26. To put machine out of use, pull handle slightly forward and turn key in back of machine.

Coin Hopper

27. If the coin hopper should become blocked, the obstruction can be removed by taking out the two screws and lifting front of hopper; but care must be taken that screws are put back and screwed up tightly.

Care of
Machine

28. All the working parts of the machine should be oiled, and a good sperm or lard oil must be used. Care, however, should be taken not to let the oil go near the tickets in column of dating cylinder.

All screws should be examined from time to time to see that they are tight.

The inking roller will probably require rolling on the slab after the issue of each 5000 tickets. This will, however, depend on the absorbent character of the tickets issued through the machine.

**REGULATIONS FOR WORKING ELECTRIC AND PETROL PLATFORM
TROLRIES AND TRAILERS—page 277.**

The following to be inserted as Clause 16 A:—

Except in cases where an attendant is returning to the point from which loaded
trailers have been taken MEMBERS OF THE STAFF ARE FORBIDDEN TO RIDE
WITH DRIVERS ON THE TROLRIES

(G.A.29.Op.—5/52. LK1/10088/1.)

STATION INSTRUCTIONS.

REGULATIONS FOR WORKING ELECTRIC AND PETROL PLATFORM TROLLIES AND TRAILERS.

It is of the greatest importance that mechanical trolleys should be moved about with every care at a speed never exceeding eight m.p.h., so as to prevent inconvenience, annoyance or possible injury to passengers on the platforms and the special attention of all concerned is directed to the following:

1. The method of charging electric-driven trolleys varies according to the type of trolley and system of electrical supply. Special instructions will be issued to drivers to suit the local conditions.
2. No person must be allowed to drive any trolley unless he has been passed as competent to do so by the Road Transport Department. (Or by the responsible official in the case of the Docks Department Warehouses).
3. Each driver must see that the Log is on the vehicle, and that the instructions relative thereto are carried out. (Not applicable at Dock Warehouse).
4. Drivers must immediately enter in the Vehicle Log any defect they may notice, stating if possible whether the defect is mechanical or electrical, and present same to the Station Master, or to the officer under whom the driver is directly employed. (At the Docks the defects must be reported by the drivers to the responsible Foremen, who will draw the attention of the Electrical Department to the matter).
5. Drivers will be responsible for a good state of the vehicle and for its proper use. (Except at Dock Warehouses, where the maintenance of electrical trolleys is attended to by the Electrical Assistant.)
6. Drivers must see that the EXTINGUISHERS are in good order, and must make himself familiar with the use of same. (Not applicable at Dock Warehouses where extinguishers are placed at certain points in the Warehouses)
7. No trolley must be used to carry any load other than the load specified in the instructions.
8. These trolleys must not be used to pull any vehicle or trailer, or to assist in the coupling or uncoupling of any vehicle.
9. Every trolley must have a warning signal, which must be in good order and give a loud sound. Trolleys must be driven at a speed which will give a warning signal which is not more than is necessary, but sufficient audible warning must be given.
10. The maximum number of trolleys which may be used at any one time during the day is shown below. During quiet intervals at night the number may be increased, at the discretion of the supervisory staff, within the limits shown:—

TYPE OF TROLLEY.	ON LEVEL.			ON RISING GRADIENT.		
	General	News-papers	Empty.	General parcels.	News-papers.	Empty.
During the day.						
Platform trolley	4	4	6	4	3	6
Cob tractor	4	4	6	4	4	6
At night.						
Platform trolley	6	4	12	4	3	12
Cob tractor	10	8	12	8	6	12

11. These trolleys must be driven at reasonable speed and drivers must always have the vehicles completely under control, and be prepared to stop immediately, without warning, if necessary.
12. Trolleys must not be run about unnecessarily.
13. Trolleys must not be run alongside loaded trains whilst they are drawing in to the platform and are still on the move. As far as possible care must be taken to avoid moving trolleys along the platform just before the train is starting.
14. Opportunities should be taken to move trolleys when platforms are not crowded with passengers. When it is seen that there is a number of passengers at one particular point on the platform, time should be given for these to disperse before attempting to move through them.
15. A second man must always be in attendance when more than two loaded trailers are attached to a trolley (not applicable at Dock Warehouses)
16. The second man accompanying must ride on, or walk behind, the last vehicle and keep a good look-out forward to prevent passengers coming into contact with the moving vehicles and see that packages do not fall off.

STATION INSTRUCTIONS.

WORKING ELECTRIC AND PETROL PLATFORM TROLLEYS AND TRAILERS (continued)

17. Trolleys must always have a driver in attendance unless put away in their recognised place. The trolleys must be driven only by authorised men, and the driver must not on any occasion leave the trolley with the starting handle or connecting plug with the trolley unattended. If, for any reason it is necessary to leave the trolley, the driver must, on a key as provided, connect the handle or plug and put it in the receptacle on the trolley provided for the purpose, so that no unauthorized person can start the trolley or engine in his absence.
18. Recognised parking places must be kept ready by the Department working the trolleys. (Except at Dock Warehouses, where the Maintaining Department does this work.)
19. Trolleys must not be left standing in the proximity of lighted lamps unless the petrol is turned off.
20. Trolleys must not be run on the public highway unless provided with a registration number on the front and rear part of the machine and carry the Road Fund Licence and Certificate of Departmental approval to trolleys running *about* between Dock Warehouses or between Docks Warehouses and charging or maintenance points.
21. When trolleys are run on the public highway, only one trailer, whether loaded or empty, may be hauled at one time.
22. Engines must be run as quietly as possible and not left running while trolleys are standing unattended. When the trolley is stopped, the petrol and oil must be turned off.
23. The Area Assistants will arrange for the trolleys to be run in every district each week.
24. Supplies of oil, waste, grease, etc., can be obtained from the Stores Department, Swindon.
25. Inspectors and Foremen must give special attention to, and supervise the movement of all trolleys, so that every one is controlled by the men in charge of them, irrespective of the Department to which the men are attached.

AUTOMATIC BRAKES ON PLATFORM TROLLEYS.

All platform trolleys are fitted with the automatic brake and in order that the brake apparatus should be found in efficient condition, the following arrangements must be carried out:—

The Station Master must appoint a suitable member of his Staff to be responsible for MAINTAINING THE BRAKES IN AN EFFICIENT CONDITION.

The man appointed must test the brake on each trolley twice a week, and make any adjustment that may be necessary.

If any defect is discovered, it must be reported at once to the Station Master, who must at once report to the Chief Engineer and Works Department Depot and the same will be despatched to do what is necessary.

Trolley drivers are instructed for the purpose of instructing the specially selected men at the stations in the adjustment of the brake, and for carrying out any major repairs to the services of the trolleying divisions may be obtained on application to the Divisional Superintendent or District Traffic Manager.

Station wheels are supplied with two double-ended spanners ($\frac{1}{2}$ inch and $\frac{3}{4}$ inch) for the adjustment of the brake gear.

The following instructions must be observed by the men engaged in the repair and adjustment of the brake:—

(a) The brake must be adjusted so that when the handle is upright the blocks are sufficiently tight on the wheels to prevent the trolley running away with standing unattended on a level station platform ramp, but when the handle is in a "pulling" position the blocks must be clear of the wheels.

(b) Adjustment must be made principally by means of the eyebolt between the double and single chains.

(c) Should the springs be loose, further adjustment can be made by means of the eyebolts to which they are attached.

(d) Particular care must be taken that the nuts are not left slack.

(e) Worn out or broken parts must be changed for new ones before the trolley is allowed to be used again.

Trolleys must be kept under close observation by Station Masters, Inspectors and Foremen, and special care must be taken to ensure that there is always at least one man at the station who is responsible for the trolley. The brake should be tested at frequent intervals by the person in charge, who will be responsible for seeing that these instructions are carried out.

If it is found that the brake is not being maintained in efficient condition, particulars must at once be reported to the Divisional Superintendent or District Traffic Manager.

Trolleys must not be used while the brakes are defective.

Rubber tyres on trolleys should not be permitted to wear down to such an extent that the rims of the wheels run on the platform paving.

STATION INSTRUCTIONS.

CONSUMPTION OF GAS AND WATER AT STATIONS, DEPOTS, &c.

All gas and water meters should be read daily in order that leakages may be detected and remedied with as little delay as possible. The readings should be recorded so that a comparison can be made of the consumption of gas and water one day with another, and increases should at once be enquired into.

PLATFORM OIL LAMPS.

Whenever it is necessary for any of the outer cases or interiors of platform oil lamps to be sent for repairs, the instructions must be obtained in their place before they are sent away, and the following instructions must be strictly observed:—

OUTER CASES.

The cases should be dealt with by the Stores Department, and the attention of the Permanent Way Inspector must be drawn immediately to any case of disrepair.

INTERIORS.

If an interior is found to be defective by the Stores Department, Swindon. Immediately an interior is found to be defective, it must be marked as defective, and sent to the General Storekeeper, Swindon, for repairs. The defective interior must be sent to the General Stores, Swindon, where it will be repaired and returned in place of that sent to the station.

SPARE CASES OR INTERIORS.

Any spare cases or interiors found at Stations must be sent to the Local Engineering Department, Swindon, or to the General Stores, respectively. Whenever cases or interiors are sent away they must be marked as defective, and the date of receipt at the Stores Department must be noted, and an advice sent to the Permanent Way Inspector, Swindon, as to the date of receipt at the Stores Department, and on what day they are dispatched.

Cases must be taken by the Stores Department, Swindon, and the cases must be ensured that they are not being damaged during their transit to or from the Depot or Stores Department.

LAMPS SENT FOR REPAIRS.

The following instructions apply to the removal of oil from the reservoir of lamps which are sent away for repairs. The staff must ensure that the reservoirs are emptied before lamps are put into a train.

STORAGE AND CONSUMPTION OF PETROLEUM.

The special attention of Station Masters, Goods Agents, and other members of the Staff is directed to the necessity for strict economy in the use of Petroleum. The following Regulations must be strictly observed:—

1. Petroleum must never be poured in a room where there is a fire, and must be kept in a building shed or locker apart from the Station buildings.

2. Petroleum must be kept in a safe place, and at least 12 feet away, and further if practicable.

3. Petroleum for daily use should be carefully estimated, so that a sufficient quantity may be taken from the place of storage, and after use should remain over it must at once be returned to the store.

4. To avoid any risk of accident or failure, immediate application should be made to the General Stores, Swindon, to replace any cask or can found to be defective.

5. The quantity of petroleum supplied for lighting lamps should be carefully estimated for the purpose. See Clause 14, page 28 of the Rules. If petroleum is found to be in any way inferior, or if the quantity is found to be less than that required, the matter should be at once reported to the Stores Department, Swindon, and the quantity of the petroleum should be stated, giving the number and brand marks of the barrel, together with the date of receipt.

6. The consumption of petroleum is small and does not warrant the supply of a large quantity from the Stores, Swindon, and the quantity should be drawn from the casks as required, and not in any case put into the quantity drawn from the cask, and the quantity required should be noted.

7. Any deficiency should be promptly reported to the Stores Department, Swindon, and the petroleum should be used in the order in which it is received. The date of receipt should be chalked upon the barrels; the chalk date being rubbed off when the empty barrels are returned.

STATION INSTRUCTIONS.

STORAGE AND CONSUMPTION OF PETROLEUM—Continued.

8. Care shall be taken to avoid damage to the barrels. Cases have occurred in which barrels have been damaged by being thrown against the wall or by being dropped on to the floor from the back of a truck or lorry.

9. No unauthorised person must be allowed to have access to the lamp rooms or lamp huts, which must be kept locked when unattended, and the key placed in charge of a responsible person.

10. The special instructions in the petroleum folder to the barrel account must be carefully adhered to. Barrels must be returned when empty in a cask with the lid or as issued by the Stores Department, and in no case must an empty barrel be appropriated for station use unless a Stores Order has been given to that effect and the Stores Department has been notified of its receipt.

INSTRUCTIONS FOR REQUISITIONING, STORAGE AND DISTRIBUTION OF PETROLEUM FOR TRAFFIC AND GOODS DEPARTMENT—Page 280.

The existing instructions on page 280 to be cancelled and the following substituted:—

INSTRUCTIONS FOR REQUISITIONING, STORAGE AND DISTRIBUTION OF PETROLEUM FOR TRAFFIC AND GOODS DEPARTMENTS.

Consumption data. Petroleum is to be ordered on Requisition Form 6826 by the local Officer on the basis of data entered by the Station Master or Goods Agent in Book No. 8288.

Record of estimated requirements.

The book must be made up and forwarded to the local Officer before the 12th of the month preceding the quarter for which supplies are needed. The Station Master or Goods Agent will enter in this book the number of every type of lamp which will be IN USE during each month of the quarter against the "hours burning daily" figure which applies in each case. He must not enter any figures in the quantity columns. The local Officer, after reviewing the data in the light of Traffic requirements, will fill in the quantity columns and summary page, certify the book and despatch it to the Storekeeper, General Stores, Swindon, in time to arrive on or before the 24th of that month.

Form 6806

On the first day of each month Station Masters—and Goods Agents also where barrels are supplied to their Depots—must send in advance on Form 6806 of stocks of petroleum on hand at all their storage points to the local Officer, who will complete and certify the returns for his district and despatch them to the Storekeeper, General Stores, Swindon, in time to arrive on or before the 4th day of the month. To obviate delays in the issue of petroleum these returns must be rendered punctually.

Petroleum will as a rule be supplied in accordance with the tonnage ordered by the local Officer, but the requisition will be reviewed in the office of the Storekeeper, General Stores, Swindon. Any differences between the quantities recommended for supply and those ordered by the Stores Department will be notified to the local Officer in order that his records may be amended.

Local Officers records

The local Officer must keep a record of requirements at each station in his district for the purposes of compiling his requisitions and controlling consumption.

Supply of petroleum

In order to be in a position to accept delivery when made by road the Station Master or Goods Agent must ensure that sufficient empty drums are available to take the quantity ordered. Also drums must be empty and free from water before they are refilled. The drums are of 40 gallon capacity and supplies are made in units of 40 gallons as far as possible.

Road delivery

The Firm's delivery notes must be signed by the Station Master or Goods Agent and sent to Swindon immediately to enable invoices to be released.

Rail delivery

When oil is delivered by rail, the charge notes (Form 4943A) for full barrels must be signed and sent to Swindon on the day of arrival and empty barrels forwarded by the first possible freight train, to the Contractor's Depot mentioned in the package note (Form 4943C). Delay in this respect increases the hire charges payable by the railways. Care must be taken to drain the petroleum from the barrels before being returned as empty and to ensure that the correct n.d.d. barrel identification numbers are entered on Forms 4943A and 4943C before they are forwarded to Swindon.

Storage

Wherever possible oil must be stored in oil huts, the keys of which must be in the charge of a responsible person who, before going off duty, must deposit them in the Station Master's (or Goods Agent's) Office. Any issue of oil, in the absence of the person normally responsible for issues (whether the oil be stored in an oil hut or not) must be made in the presence of an Inspector or other authorised person, who, where an oil hut is in use, will see that the keys are replaced in the Office.

Where huts are not provided, barrels must be placed on timber supports or concrete slabs. On no account must they be stored in ashes as the sulphur content eats into the metal. Attention is also directed to the instructions as to storage of petroleum spirit contained in Schedule 4, page 11 of Circular No. 3678, Regulations for the Prevention and

Extinction of Fire.

40-30

Records of sub-
supplies.

When petroleum is sent to a station for redistribution to other stations where the consumption is too small for direct issue to be made, the Stores Department will notify the issuing and receiving stations on Forms 6840 and 8064 respectively. A record of these supplies must be kept by the issuing station in a book available for inspection by the Stores Department, indicating the number of gallons supplied to each station, and the date and train on which the oil is despatched. Similar records must be kept by the receiving stations.

If for any reason the full supply of oil authorised by the Stores Department to be issued during any month is not taken, or if, through emergency, more is supplied than authorised, an advice must be forwarded to the Storekeeper, General Stores, Swindon, by both the issuing and receiving stations, stating the quantity actually issued and received respectively for that month. These advices must be dispatched in time to reach Swindon not later than the 4th day of the following month, and will be the basis for adjusting the respective credits and debits (in gallons) for the stations concerned.

Standard
method

No method of storage and issue other than the standard combination of drum barrel stand and tap may be adopted without the local Officer's authority. The use of cans must be kept down to a minimum.

Drum
must
be

Drums must always be laid with the bung uppermost. Bungs and tap-plugs are to be tightened up before the drums are left in case they may have been loosened during transit. All drums must be carefully watched for leakage and receive daily attention before and after tapping. (Any found to be in leaking condition to be emptied and returned with an advice of despatch to General Stores, Swindon, for repair or replacement.)

Barrel

Barrel stands, spanners, washers, etc. (for releasing and tightening bungs) and tap-plugs, for use with steel drums if authorised by the local Officer, are to be ordered on Requisition Form 224.

For

(G A 25-1 50 SS-C R P P 39242.)

Required should be paid for by the local Officer. The local Officer should forward to the Divisional Superintendent or District Traffic Manager for certification.

CLEANING PETROLEUM LAMPS.

All petroleum lamps, other than long burnin lamps, must be cleaned, trimmed, and the tanks filled with the requisite quantity of oil at the Lamp Room every day during daylight. Care being taken to remove any sediment or water from the tank, after filling, to wipe every part of the lamp perfectly dry, the wick is to be renewed frequently.

That the
Petroleum
Lamps.

Signal Lamps.

The use of signal lamps will be properly regulated. The Signal Box, special care must be taken to adjust the wick.

The wick when lighted shall be regulated. The person lighting the signal lamps must have with him suitable means for cleaning purpose.

Lamp wick should be sufficiently long to reach the bottom of the tank. When a new piece of wick is required it should be placed in the lamp. The wick is not to be placed in the lamp until the wick is not dirty. The wick must be renewed when dirty, and when too long.

The cleaned portion of the wick must be cut off at each time the lamp is regulated, and the wick left clean.

In trimming the wick, the wick shall be cut with the wick trimmer, and the corners shall be cut off. The wick shall be cut off at the bottom of the tank.

Persons who may be employed at or near the buildings, especially if the latter are crowded, should be very careful not to leave any oil or dirty waste about, and, upon leaving, to make sure that no oil or dirty waste is left in or about the structure. The lamp must be kept perfectly dry, and no oil or dirty waste is to be left about on the benches or on the floor. Grass or any other matter which may be about the lamp should be removed.

Special care must be used to avoid dropping burning matches on the floor or cleaning bench of the lamp hut. Only safety matches must be used.

GA 31

STATION INSTRUCTIONS.

LIGHTING AND EXTINGUISHING SIGNAL LAMPS.

At places where signal lamps are not lighted during the summer months, the interiors must be removed from the lamp cases, and taken to the lamp huts where they must be emptied, cleaned and placed on a shelf.

placed on a shelf.

Remembering Rule 3, note there is more than one shelf on a post, or where there are two or more posts and a shelf or two after the main one. The shelves must be kept turning during the whole of the performance so that the rifles are always in the hands of a driver moving by its relative position, so that the shelves will not have to be turned during the night as they can when the whole of the arms are visible during the day.

The lighting of a fire must be in accordance with Rule 102 of the Book of Rules and Regulations.

[illegible][illegible]

Except in the case of sections worked by Pilotman, trains calling at such sidings must be instructed by the Pilotman to proceed to the siding and to be advised that the train has work to do at the siding.

train has work to do at the siding.

the Guard of the train at the siding. The Engineer, in order to save work for the Pullman, that is, that at such a siding as this, where the Pullman is not to be used, the Pullman must be told by the Guard that the train has work to do at the siding. The Pullman, however, the St. Louis

that the train has work to do at the siding. The Engineer will then take the engine and train to the siding, and will be accompanied by a Pilotman.

"LONG BURNING" SIGNAL LAMPS.

1. ~~When cleaning the lamp, use only the instructions given in the instructions to the precautions to be taken with the cleaning and tripping of the lamp. The following instructions should be strictly observed when using the "Long Burning" Signal.~~

[illegible][illegible]

Long Burning Signal Lamps—Pages 282-285.

The instructions under this heading to be deleted and substituted by the following:—

LONG BURNING SIGNAL LAMPS—EQUIPMENT.

The undermentioned articles may be ordered by Signal Lampmen from the Stores Department on Requisition Form No. 224 through the Station Master at their Home Station, and arrangements should in future be made accordingly —

Glasses for Lamp Case Interiors.

Standard Pattern Interior Circular Tank.

Front, $4\frac{1}{4}" \times 4\frac{1}{4}"$.

Back, $3\frac{1}{4}" \times 2\frac{3}{4}"$.

Old Pattern Interior Shallow Circular Tank.

Back and Front, $3\frac{1}{2}" \times 3\frac{1}{2}"$.

Semaphore 9" and 11" Cases.

Back and Front, $5\frac{1}{2}" \times 5"$. (Bottom corners cut away)

Back and Front, $5\frac{1}{2}'' \times 5''$. (Bottom corners cut away.)

Articles to be
received by
October 1
for publication

to the fact that the same person can be called by many different names. It is not the same person, but the same name. Master at the same time, the same person can be called by many different names.

GLASSES FOR 1 AMP CASE INTERIORS

Standard Pattern Interior Circular Tank.

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13 2 7

Old Pattern Interior Shallow Circular Tank.

4. 1. 1. 5. 1. 6.

Semaphore 9" and 11" Cases.

Fac and Tent, by a 5" ~~Atom~~ corners cut away)

GLASSES FOR LAMP CASE INTERIORS.—Page 285.

Delete the entry in respect of Route Indicator and Permanent Speed Indicator Lamps and insert the following:—

Route Indicator Lamps
4 in. x 5½ in.
Permanent Speed Indicator Lamps
Front glasses 5 in. x 5 in.
Side glasses 3 in. x 2½ in.

(G.A.30 Op.—9/54 LKl/10676/26).

INSTRUCTIONS FOR USING PARAFFIN VAPOUR LAMPS—"TILLEY" TYPE— Pages 285-287.

The instructions under this heading to be deleted and substituted by the following —

PARAFFIN VAPOUR LAMPS—"TILLEY" TYPE—SPARE PARTS.

The Operating Department Staff can order the following, but care must always be taken to quote the correct number of the respective part when ordering —

Part No.	Name of Part.	Part No.	Name of Part.
123-90	Globes, Clear (outside lighting).	133-46	*Pumps, without connections.
123-91	Globes, Clear and Frosted (inside lighting).	133-74	*Vaporisers, complete.
133-40	Mantles, Inverted, Large, No. 2.	133-81	Washers, XN Cock Black, No. 160.
133-42	Mantles, Inverted, Small, No. 1, Indoor Lamp (I.L. 37).	119-31	*Cans, Oil, with spout
133-39	Mantles, Inverted, Floodlight Projector.	119-165	*Angle Funnels, Tin, Gas strainer.
133-79	Washers, Vaporiser, Black, No. 153	133-22	*Carriers, Wire, "A," for P.L. and 56 lamps.
133-19	*Caps, Screw, Filler "F."	133-23	*Carriers, Wire, "B," for other lamps.
133-25	*Connections, Pump, 15".	130-10	Balls, ½", No. 147.
133-37	*Lighters, Paraffin.	109-31	Special Brush for cleaning burner.

* These items are obtainable only on Repairs Requisitions (Form No 224), and the old articles must be returned to Swindon.

Spare Parts must be kept by the person in charge of the place where the lamps are installed, i.e., Station, Yard, Depot or Signal Box.

(G.A.31—7/56 B.R. 29611)

STATION INSTRUCTIONS.

INSTRUCTIONS FOR USING PARAFFIN VAPOUR LAMPS—"TILLEY"

TYPE 49.

FIXING MANTLE. When it is necessary to fix a new mantle "M" (see Fig. 1) it is necessary to slip it over the handle "H" so that it rests in the hole at the top of the burner. When the mantle is fixed into the hole at the top of the burner, it will shape itself correctly when the lamp is lit.

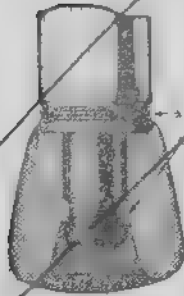
FOR HEATING BY SELF-FILLING LIGHTER

FIG. 3.

1. Slide the lighter on to the vaporiser. Slacken the cap "A" as far as it will go. If the cap is not as far as it will go, it will not light.
2. Avoid a draught whilst lighting.
3. Turn the handle "H" to the right until the handle "H" rests in the hole at the top of the burner. The handle "E" just sufficient to allow the flame to burn evenly all round. When the flame is burning evenly, the handle "E" should be turned slowly to the right until the paraffin in the cup "C" gradually turns on the vapour by the handle "E".
4. The cup "C" should not be filled completely, otherwise the paraffin will creep over the edge, causing smoke. When filled up too much, a charge has been used, the cup may be refilled when the first charge is exhausted.

FINAL PUMPING. After the lamp has been lit, pump until the centre part of the burner is covered with isother wall.

EXTINGUISHING.—Turn button "B" as far as it will go to the right, when the lamp will immediately go out. The air pressure need not be released.

OIL STRAINING. It is most essential that the oil funnel with gauze strainer be used in order to remove any dirt from the paraffin. A lamp will not burn correctly if unstrained oil is used.

BURNING HOURS. The following table shows the burning time for the following lamps—

O.L. 50	..	..	32 hours	..	..	Outdoor Lamp.
O.L. 49	..	..	2	..	..	Indoor Lamp.
O.L. 48	..	..	1	..	..	Indoor "General Utility" Lamp
O.L. 47	..	..	1	..	..	Outdoor "General Utility" Lamp
O.L. 46	..	..	1	..	..	"

Occasional pumping is sometimes necessary.

There are some O.L. 49 lamps on the G.W. system which are being superseded by the O.L. 50 type. O.L. 49 Lamps must not be used indoors.

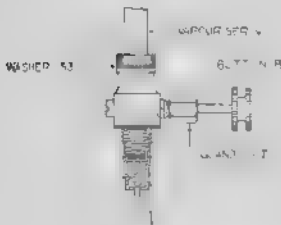


FIG. 4.

DIRT IN VAPORISER.—To clear dirt which may collect in the hole at the top of vaporiser "V" turn button "B" backwards and forwards quickly; this can be done while the lamp is alight. Should the lamp "light-back" inside the burner when this is done, causing a roaring noise, turn button "B" again and re-light. (See Fig. 5.)

IMPORTANT.

CLEANING BURNER. When fixing a new mantle, unscrew the brass cap at the top of the Burner and thoroughly wipe away dust which has collected inside. See that the small holes at the bottom of the burner are free from dirt and obstruction.

RISE and FALLING of the heat is an indication that the oil is nearly exhausted.

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**INSTRUCTIONS FOR USING PARAFFIN VAPOUR LAMPS—"TILLEY" TYPE—
PREHEATING BY SELF-FILLING LIGHTER—Page 286.**

The following sentence to be added to the Note at the end of paragraph —
See that button B is turned off before refilling (GA 23 7 49 LK I 6717 Gen 4)

LIGHTING OF LAMPS IN LAMP HUTS.

The number of lamps which may be lighted at a time in a lamp hut must not exceed 4. Where more than this number are required to be dealt with they shall be dealt with in batches not exceeding 4 in each batch. In cases where special authority is given the lighting of particular lamps must only be undertaken in a lamp hut or lamp room properly constructed of either brick or corrugated iron and having a brick or concrete floor. The lamp must not be left unattended.

DEFECTIVE CONTAINERS

Should a leak be discovered in the container of a lamp or lamp this must be reported immediately to the Engineering Department and the circumstances of the leak noted.
(G.A.3.—12/37. L.K.1/4391/23.)

STATION INSTRUCTIONS.

INSTRUCTIONS FOR USING PARAFFIN VAPOUR LAMPS—"TILLEY"
TYPE—Continued.

FIXING AND RENEWAL OF VAPORISERS. The vaporiser is to be fixed with one washer only, finger tight. The vaporiser is then to be replaced. The vaporiser is then to be replaced.

MAINTENANCE PROCEDURE. The vaporiser is to be fixed with one washer only, finger tight. The vaporiser is then to be replaced. The vaporiser is then to be replaced.

ALL OTHER REPAIRS ARE TO BE REFERRED TO THE DIVISIONAL ENGINEER.

In all cases the defective lamp is to be forwarded complete to the Divisional Engineer on label.

DAMAGE. Breaking of the lamp or of the pump or of the vaporiser is to be reported by the person in charge of the lamp. The proper action must be taken to prevent further damage to the pump with these lamps.

SPARE PARTS. The correct number of the respective part when ordering:—

Part No.	Name of Part.	Part No.	Name of Part.
123-30	Globes, Clear (outside lighting).	133-37	*Lighters, Paraffin.
123-31	Globes, Clear and Frosted (inside lighting).	133-46	*Pumps, without connections.
123-32	Mantles, Inverted, Large, No. 2.	133-74	*Vaporisers, complete.
123-33	Mantles, Inverted, Small, No. 1, Indoor Lamp (I.L. 37).	119-31	*Cans, Oil, with spout.
133-34	Mantles, Inverted, Floodlight Projector.	119-165	*Angle Funnel, Tin, Gauze strainer.
133-79	Washers, Vaporiser, Black, No. 153.	133-22	*Carriers, Wire, "A," for P.L. 55 and 56 lamps.
133-19	*Caps, Screw, Filler "F."	133-23	*Carriers, Wire, "B," for all other lamps.
133-25	*Connections, Pump, 15".	130-10	Balls, 1/2", No. 147.
		109-31	Special Brush for cleaning burner.

* The correct number of the respective part when ordering:—

Spare Parts must be kept by the person in charge of the place where the lamps are installed, i.e. Station, Yard, Depot or Signal Box.

STEEL PETROLEUM BARRELS.

The side bung and tap plug must also be properly secured when a barrel is empty.

SPARE LAMPS FOR SIGNALS, LEVEL CROSSINGS, &c.

The Signal Department is to be supplied with spare lamps for signals, level crossings, &c. The number of spare lamps must not be increased more than is absolutely necessary.

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STATION INSTRUCTIONS.

FIRE APPLIANCES.

Where fire appliances are provided the Station Master, Goods Agent, Fire Warden or person in charge must see that the regulations for the guidance of the staff are thoroughly understood by all concerned, and that they are carried out when the necessity arises.

The hydrants, stand pipes and hose must be tested not less than every three months, and all appliances after use must be kept in good order, and the receptacles provided for their accommodation.

Where fire buckets only are supplied, the Station Master, Goods Agent or person in charge must appoint a man to keep the buckets filled with water, and examine them daily and satisfy himself they are kept ready for use and in good order. The water in the buckets should be changed at least once a month, and during frosty weather measures must be taken to prevent the water becoming frozen.

Fire buckets, dip tanks or tubs, which normally contain water, are to be kept clear of ice during frost. In severe frost, buckets which are in exposed situations should be placed in sheltered or warm positions where practicable. Where this cannot be done, the buckets should be emptied and placed near the most suitable taps or hydrants for quickly filling them in case of fire.

Station Masters, Fire Wardens or other persons in charge are specially requested to see that these arrangements are applied whenever weather conditions suggest the desirability of adopting this course.

(See General Manager's Circular No. 3136, dated October, 1930, for full details as to fire arrangements.)

REPORTING OF FIRES.

All cases of fire, whether upon the railway or at the Company's premises in city or town, must be immediately reported to the superior officer concerned, so that, in those instances where insurance is involved, a proper notice may be promptly given to the Insurance Company.

See also General Manager's Circular No. 3136, dated October, 1930, in regard to reporting fires immediately by wire to the Secretary, Paddington Station, and Superintendent of the Line.

CONVEYANCE OF MOTOR VEHICLES BY RAIL.

Petrol may be left in the tanks of Motor-Cars conveyed by Passenger trains providing that:—

- (i.) The flow of petrol to the carburettor has been stopped.
- (ii.) All pressure has been released from the tank.
- (iii.) The Motor Car is free from leakage of petrol. Where the flow of petrol to the carburettor is stopped by means of a shut off cock, the engine must be run by the person in charge of the car until the petrol in the carburettor is exhausted and the engine stops automatically.

When, however, it is necessary for petrol or other inflammable liquid to be discharged from the tanks of Motor-Cars, it should be done in a safe place, and the liquid should be discharged in connection therewith.

Vapour from petrol, which is heavier than air, is liable to collect in any pit or depression. Being heavier than air the vapour sinks, and lies close to the ground or in any pit or depression.

When practicable the liquid should be discharged from the vehicles by daylight. If the operation cannot be performed by daylight, a man must be specially detailed to put out any lights, including hand lamps and those on vehicles, other than electric lights, within 15 yards, which are less than 8 feet from the ground level, warn anyone approaching with a light of any kind, and stop persons smoking within the prescribed distance. No lighted lamp must be placed on the ground or platform within 20 yards of a motor vehicle from which petrol is being withdrawn.

Where the transit of cars from or to a station is frequent, a special point at which cars must be loaded or unloaded must be arranged in conjunction with the Road Transport Department. Where no special spot is appointed, the cars must be loaded or unloaded in a position where draught is likely to blow the vapour into any pit or depression. Discretion must be used where circumstances are exceptional, e.g., if the movement of petrol takes place between high walls where the vapour is not readily dispersed, or if there is any kind of ditch along which the vapour would be readily carried. The direction of the wind should also be noted.

At places where the Company have no Agent appointed to do the work, the Company's servant in charge of the loading of the vehicle will be responsible for seeing that all petrol withdrawn is taken away from the station premises. On no account must it be thrown down a drain.

Conveyance of Motor-Cars with Spare Wheel attached. When a spare wheel forms part of the equipment of a motor-car, in addition to utilising the holder provided on the motor-car, senders must secure the wheel by a strap or other equally suitable fastening, or remove and place it in the body of the car in such a position that it cannot fall out.

HANDLING OF MOTOR CYCLE TRAFFIC.

The following instructions must be observed when dealing with motor cycle traffic:—

- (a.) In loading, the front wheel must be placed on the van floor, one loader to hold handle bars and another to lift rear wheel into van.
- (b.) When more than one machine is placed in a van, they must be loaded front to rear alternately.
- (c.) The machines must not be placed on their stands when loaded in train vans, as the oscillation is likely to cause them to fall.
- (d.) When unloading the rear wheel must be lowered on to the platform by one loader, another loader holding the handle and lowering the front wheel.

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HANDLING OF MOTOR CYCLE TRAFFIC.—Page 288.

The following to be inserted as clause (e):—

At stations where special motor cycle loading boards are provided they must always be used for loading or unloading motor cycles into or out of vans. The boards must be carefully handled and after use they must be returned to their storage point.

(G A.30 Op.—9/54. A2/154,55.).

STATION INSTRUCTIONS.

PROTECTION OF GAS AND WATER PIPES AND SANITARY FITTINGS AGAINST FROST.

During frosty weather full use must be made of stop valves where provided. These must be turned off during the night and, if the frost be severe, during the day, and tanks and service pipes emptied.

Gas and water service pipes, pumps, etc., whether for w.c.'s, urinals, or taps, must, wherever possible, be protected by a covering of straw. Where fire bricks are especially to be used, the Master Signalman must see that there are sufficient in number and that they are properly stacked on and so that no steam is ever allowed to escape from any such apparatus used by the Stores Department.

Open fireplaces, chimneys, etc., must be covered with straw, or other such material. Straw, old sacks, or other such material should be placed over fish tanks, and stopcock hydrant and meter pits must be filled with straw.

When the water is turned on, the Master Signalman of the London and North Western Railway should be informed of the practice, but no such notice is to be given to the Master Signalman of the Great Central Railway. If water is turned on, the Master Signalman of the Great Central Railway should be informed. In some instances, however, it has been found that the pipes, valves, and valves have been damaged in breaking ice, and these practices must be avoided.

ELECTRIC TRAIN LIGHTING—DYNAMO BELTS.

In the case of electric trains, the dynamo belts should be kept in good order, and the dynamo should be forward in the train, and the dynamo should be forward in the train.

ARTICLES FOUND ON LINE.

If any article is found on the line, it should be reported to the Master Signalman of the London and North Western Railway, and the Master Signalman of the Great Central Railway, and the Master Signalman of the Great Central Railway should be informed of the article found on the line.

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REGULATIONS FOR DEALING WITH MAIL BAGS ON NON-STOPPING TRAINS.

1. The G.W.R. mail trains by night are distinguished by the MAIL VEHICLE carrying white lights at the end by the signal box. The L.M.S. Mail Vehicle carries a small white light showing towards the engine, so that the mail train can be easily recognised before it arrives by the Signalmen and P.O. Office Officials attending to the mail apparatus. Persons in charge of stations from which mail trains are started must see that the mail train is so equipped.

2. It sometimes happens, however, that from various causes the mail trains are running out of course, or are divided, and in order that the practice in such cases may be as uniform as possible, the following regulations must be carried out:—

3. At those places at which electrical or mechanical gongs are fixed between the signal box and the hut near the mail apparatus, the gong must be worked for every mail train carrying the mail apparatus during the time the signal box is open, the following bell code being used:

Down Mail approaching	..	..	..	2 beats.
Up	..	..	..	4 "
Cancel Signal	..	..	..	8 "

and on each occasion when the Mailman comes on duty at the hut a test signal of one beat must be exchanged between the signal box and the mail hut.

4. The bell signals must be sent steadily by the Signalsman, and must be returned by the Mailman in each case to denote that they are understood. If incorrectly returned, the Signalsman must send the signal again until it is correctly returned.

At each place a time at which the Mailman must be at the apparatus must be fixed upon with the local Postmaster so that the former may be present to receive the gong signal from the box.

When the Mailman is communicating with the mail apparatus, the Mailman must inform the Signalsman on each occasion when he comes on duty by calling him up on the telephone or by the signal box, and he must be apprised of the mail train by passing the appropriate one of the following messages:—

Down Mail approaching.
Up Mail approaching
My advice in regard to Up Mail approaching is cancelled.
Down

These messages must be repeated by the Mailman to shew that they are understood.

STATION INSTRUCTIONS.

REGULATIONS FOR DEALING WITH MAIL BAGS—*Continued.*

5. Immediately the "Is Line Clear?" signal (or "Train entering Section" signal where that is early enough) is received from the next station in circuit in the rear, for a train which carries the mail van, the Signaller must send the number of beats on the bell, as per bell code, to the mail hut, and the man in charge of the latter must at once return the same number of beats to the Signaller.

6. The bell signal sent to the mail hut will simply be a warning to the Mailman that the mail train is approaching, but in no way is it to serve as a guide as to the time he shall prepare the apparatus, which is to be done as the train is signalled by the train in reaching the latter and depend upon the station from which the mail train is signalled.

7. When the signal, as per bell code, has been sent to the mail hut for an up or down train, and it is found to be necessary to cancel the signal, the Signaller must send the "Cancel" signal to the mail hut, which signal must be acknowledged by being returned. If the Mailman has already set the apparatus at a time when the train is signalled, he must place the apparatus in its normal position clear of the main line.

8. When the mail trains are running in duplicate, Stations at which the first mail train telegraph to the place at which the mail apparatus is set, sending whether the train is running late or second part of the train, and if on the second, how late it is running. A telegraph message must also be sent to the mail apparatus station in those cases where the mail is running late and another passenger train is allowed to go in front of the mail and in its running times. The message must be received by the clerk in charge of the station from which the mail train is signalled, and must be sent to the mail train.

Such messages must be shortened, as under:—

When mail is divided send the words "Mail first part (—) mins. late."

Or "Mail second part (—) mins. late."

When mail is late and out of course, send the words:—

"Mail (—) mins. late (—) Train in front."

9. Station Masters or persons in charge of stations, excepting places covered by clause 3, should arrange with the Post Office for a Mail van to be kept at the station, and the signal box when a passenger train is running in duplicate, to be signalled as to whether the mail van is on or not, and which part of the train the mail van is on.

10. At those places where gongs are not fixed, but where it is possible, owing to the short distance between the mail hut and the signal box, to send a message by the gong, if the station is to advise the Mailman when the mails are running late or in duplicate, this must always be done, and particular care is to be taken that the Mailman is always advised of the train conveying the Mail Van.

11. When the Mailman is not at the station, the Station Master or person in charge must advise the Mailman by the gong or by the signal box, to ensure the apparatus being set to the proper position, the instructions in clause 3 must be strictly carried out.

12. At those places where gong men are not next door and where it is not possible to advise the station to be advised when trains are running in duplicate without written or printed notice, it will be the duty of the Post Office to provide a notice board, with a printed notice, or one fixed in those cases in which the mail trains are signalled by notice the instructions in clause 11 must be strictly carried out.

13. These arrangements do not in any way relieve the Mail Attendant from responsibility in a case where a passenger train is signalled to pass the Mail van, and the Mailman is not responsible for any delay or accident which may arise from any failure to advise the Mail Attendant as to the running of the mail trains.

14. Whenever it is necessary for a train that picks up or sets down mail bags by means of the apparatus, to be diverted from the line upon which it usually runs and for which the apparatus is fixed, the Station Master or person in charge of the station where the train is to be diverted must take steps to stop the train, and to place the apparatus in its normal position, and to be signalled by the apparatus. In all such cases the Post Office Officials must be previously advised if it is possible to do so.

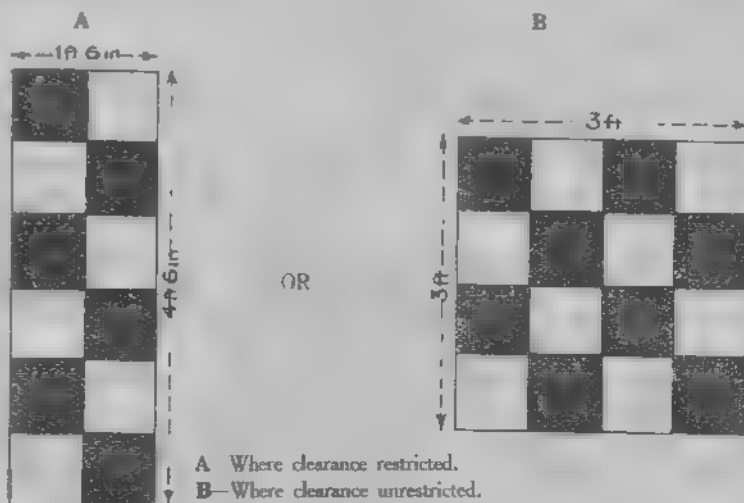
LAMPS ON POST OFFICE WAYSIDE MAIL BAG APPARATUS STANDARDS.

Brackets have been fixed to the standards of the Post Office mail apparatus, on which a lamp showing a white light will be exhibited at night when the arm of the apparatus is in working position. The lamp is about five feet above the ground.

Reference to the following to be made on page 290:—

MAIL BAG APPARATUS.

Enamelled plates with 9 inch yellow and black squares as per diagram below, are erected vertically at ground level, about 20 yards on the approach side of Post Office mail apparatus.



The enamelled plates, which are illuminated at night when pouches are swung towards the Line, are provided in order to give warning to Enginemen and others of the position of the mail apparatus.

(G.A. 239 LK 1432b 3)

STATION INSTRUCTIONS

TELEGRAPH AND TELEPHONE MESSAGES.

Telegrams should be worded as briefly as possible and use made of the code specially designed to reduce the length of telegrams.

By
Wording of
Telegrams.

Telegrams should not be handed in at a late hour unless it is certain they will reach their destination.

Telegrams not
to be handed in
after 10 p.m.

Telegrams for stations only a short distance from the office of origin, or between stations, should be sent as Urgent Train Messages.

Urgent Train
Messages.

Between 5.0 p.m. and 6.0 p.m. a good many offices close for the day, consequently communications cannot be delivered after that hour for these places, and would reach their destination just as quickly by ordinary letter as they do by Telegraph or Urgent Train Messages.

The need for strictly reserving the use of the telegraph system for matters which cannot be dealt with by other means is of the utmost importance, and the telegraph must be kept free for a possible emergency, so that prompt despatch can be ensured.

Telegrams sent under trunk system are subject to the same conditions as those sent by the Company's own system, and a despatch of telegrams must not be sent over the Post Office wires if there are Company's circuits available.

Railway Trunk Telephone Service.—Railway trunk telephone service is available for use on matters of importance should only be used for messages which are urgent or of an important character.

Public Telephone Service.—The Post Office Telephone Exchange connections which exist on the Company's system provide the following facilities:—

Pass forms are forwarded to the particular Post Office on the same day the messages are despatched. It is most important that the pass forms should be sent in by the messenger, and that the telegrams will be delivered at the special inland rate.

When it is desirable to make use of the telephone to any considerable extent for the purpose of sending telegrams, it is recommended that the necessary arrangements should be made at the Post Office periodically, in order to avoid the necessity of sending them day by day.

For the purpose of sending telegrams, it is recommended that the necessary arrangements should be made at the Post Office periodically, in order to avoid the necessity of sending them day by day.

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A Trunk fee is a charge for the use of the Post Office Trunk Lines.

Fees for
Trunk Calls

The time occupied by a trunk call should be limited to 3 minutes' conversation wherever possible. Double fees are charged for 6 minutes' conversation.

A reduction is made in trunk call charges between the hours of 5.0 a.m. and 9.0 a.m. and 2.0 p.m. and 7.0 p.m. and after 7.0 p.m., and it should be arranged as far as possible for trunk calls to be made during these times provided this can be done without detriment to the Company's business.

It should be borne in mind that these charges are made to the Company only in the case of originating calls, and not in respect of inward calls.

It is of importance that trunk calls should be restricted to matters of pressing business, and that calls on the Public Telephone must be confined to the Company's business.

Restriction for
Trunk Calls.

STATION INSTRUCTIONS

CUSTODY OF WORKING NOTICES, PROGRAMMES, INSTRUCTIONS AND OTHER DOCUMENTS RELATING TO RAILWAY BUSINESS.

It is desired that the staff should very clearly understand the importance of retaining in safe custody all notices, programmes, instructions or other documents relating to Railway business.

Under no circumstances must printed, written or verbal information as to working or train arrangements (other than those notified to the public) be communicated in any quarter outside the service.

Notices or instructions marked "Private and not for publication" must not be exhibited in positions accessible to the Public in Parcels Offices, &c.

Any member of the staff who becomes aware, either directly or through information received, of circumstances which point to the leakage of confidential matter relating to the railway, or of attempts made on the part of unauthorised persons to obtain such information, is requested to report the case to his superior Officer, in order that immediate action may be taken in the matter.

BICYCLES PROVIDED FOR USE OF STAFF.

(1) Each machine when not in use must be kept in the station buildings or office under lock and key and in a locked front entrance. Bicycles are to be used with the authority of the Officer in charge.

(2) Machines must be used only on the Company's business, care being observed that they are not subjected to improper treatment.

(3) Small repairs (such as punctures and repairs to outer covers of tyres, adjustment, and cleaning) should be carried out by a member of the Staff where possible.

(4) Occasional repairs or replacements in order to keep the machines in good working order, and the Stores Superintendent, or the Agent in Charge of the Station, as the case may be, to be kept advised of the cost of such work, together with the account, being subsequently forwarded to the Stores Superintendent. Should the cost of such work exceed a prescribed limit, the Stores Superintendent must be informed before the work is put in hand.

(5) The Stores Superintendent will stock all necessary accessories such as spanners, oil cans, and other tools, and will be responsible for the maintenance of the machines. Repairs may be made by Stores Requisition or by Repairs Requisition, in the latter case the old article being forwarded to General Stores, Swindon.

(6) ALL accounts must be forwarded to the Stores Superintendent and will be paid from Swindon, where a record of repairs will be kept.

(7) Applications for additional machines to be made to the General Manager through the head of the Department. All applications must be supported by the Stores Superintendent, Swindon.

(8) Care must be taken to see that all bicycles in addition to being fitted with a red reflector are equipped with "a white surface" of not less than 1' square inches in accordance with "The Pedal Cycles (White Surface) Provisional Regulations, 1934.

STORAGE OF CARBIDE OF CALCIUM.

The attention of all concerned is directed to the necessity for strictly carrying out the Regulations in regard to the safe storage of Carbide of Calcium, as laid down in the General Manager's Circular No. 1130, dated October, 1930, which reads as follows:

The quantity of Carbide of Calcium that may be kept without a licence shall be as follows:—

Where the carbide is kept in separate hermetically closed metal vessels containing not more than one pound—5 lbs. may be kept.

Where the following conditions are observed, 28 lbs. may be kept:—

- The Carbide shall be kept only in a metal vessel or vessels hermetically closed at all times at which the Carbide is not actually being placed in or withdrawn from such vessel or vessels.
- The vessels containing Carbide shall be kept in a dry and well ventilated place.
- Due precautions shall be taken to prevent unauthorised persons from having access to the Carbide.
- Notice shall be given of such keeping to the Local Authority and free access shall be afforded to their duly authorised Inspector to inspect the portion of the premises where the Carbide is kept and the generator is situated.

In the event of it being found necessary to store more than 28 lbs. of Carbide an advice must be sent immediately to the Stores Superintendent, Swindon, in order that the necessary licence may be obtained, as laid down in Clause 5 of the General Manager's Circular.

Every effort should be made to keep the stock of Carbide within the limits laid down, i.e. 28 lbs., in order to avoid more licences than necessary being obtained.

GREAT WESTERN RAILWAY.

Alterations and additions to the "General Appendix to the Rule Book."

To come into operation on 26th July, 1937.

EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES. Pages 293 to 298.

The Regulations under this heading have been revised as follows: -

Definitions.

Lifting and hauling appliances include:—

Cranes (Fixed, Movable, Portable, and Travelling); Gantries, Coal Tips; Shovels, Hoists and Lifts, Capstans and Hauling Appliances, Traversers, Overhead Runways, Chains and Ropes, and all loose tackle for lifting and hauling.

(a) *Fixed Cranes* are Cranes that are unable to travel.

(b) *Movable Cranes* are those that can travel within limits on a prepared track.

(c) *Mobile Cranes* are those that can be moved by their own power and independent of any track.

(d) *Portable Cranes* are those that can be moved from place to place.

(e) *Travelling Cranes* are Cranes that can travel on their own wheels in a train.

Maintenance includes Painting Examination and Lubrication, also Removal and Refixing of Ropes and Chains.

Ownership. The Department for whose use an appliance is provided is deemed the "Owning" Department.

Instructions to Staff respecting Working and Transit.

1. Cranes and other appliances, including chains, ropes, slings, etc., must be used only by authorized persons, who must satisfy themselves that they are in good working order.

cranes, etc., to be used only by authorized persons.

2. ANY DEFECT MUST BE IMMEDIATELY REPORTED TO THE STATION MASTER, GOODS ASSISTANT, OR IN CHARGE, WHO WILL COMMUNICATE WITH THE DEPARTMENT CONCERNED FOR REPAIR. NO ACCIDENT IS ANY CRANE, ROPE, SLING, ETC., TO BE USED UNTIL THE DEFECTS HAVE BEEN MADE GOOD. THE INSTRUCTIONS IN EACH CRANE MUST BE STRICTLY OBSERVED. WHEN CRANES ARE USED IN A PARTS MUST BE LOCKED OR OTHERWISE SECURED SO AS TO PREVENT THEIR BEING MOVED BY WIND OR OTHER FORCE.

3. ALL LIFTING AND HAULING APPLIANCES will be worked by staff provided by the Chief Mechanical Engineer's Department, except those belonging to and used exclusively by the Civil Engineering, Signal, and Marine Departments, who will provide their own staff.

Staff for working stationary lifting appliances

4. NO ACCIDENT MUST ANY CRANE OR OTHER LIFTING OR HAULING APPLIANCE BE USED FOR A HEAVIER LOAD THAN THE MAXIMUM MARKED UPON IT WITHOUT EXCESS PERMITS FROM THE ENGINEER RESPONSIBLE FOR MAINTENANCE (see Clause 42). No person must use an appliance without first ascertaining its maximum load.

Maximum marked loads not to be exceeded

5. When doubt exists as to the weight of a load being within the marked lifting capacity of an appliance, the load must not be lifted without the special authority of the person in charge of the appliance, who must, if necessary, consult the Maintenance Department.

Care with loads of unknown weight

6. WITH REGARD TO TIMBER, IT MUST BE BORNE IN MIND THAT THE WEIGHT VARIES WIDELY FROM VARIOUS CAUSES, THE EXCESS OF ACTUAL WEIGHT OVER WEIGHT ESTIMATED BY MEASUREMENT MAY VARY FROM 50 PER CENT IN THE CASE OF DRY, WELL SEASONED TIMBER, TO 100 PER CENT IN THE CASE OF RECENTLY CUT TREES.

Varying weight of timber

7. CRANES MUST NOT BE USED FOR SHUNTING OR TOWING TRUCKS NOR FOR LIFTING ARTICLES OUT OF PLACES.

cranes not to be used for loads out of plumb.

Note.—Care must be exercised to see that a load to be lifted from a stack, such as timber, is quite free and not pinned down or jammed in any way.

8. The skiving gear must be used where provided. In other cases manual power must be employed for pulling round the jibs of cranes.

Use of skiving gear.

Load or jib not
to fou any
structure

Use of brake
power

Use of Pawls.

Load is not to
remain
suspended.

Stall force
of cranes
or portable

Use of Traveling

Work self
driving
cranes on
Running Lines.

9. When a load is suspended from a crane, it is very important that neither the load nor the crane be allowed to foul any structure as this may cause the crane to slew, resulting in the handles rotating and striking the operator, and may also damage the load or structure.

10. Great care must be exercised in using the brake power, particularly in damp weather, to prevent the brake slipping. When loads are lowered by hand or foot brake, the brake must be applied, the gearing released, and the brake kept on until the loads are in position for lowering, and sufficient brake power must be applied and maintained to lower slowly. On no account must the lowering be stopped suddenly; and the man in charge must not release control of the brake lever unless it is properly secured or the load is being lowered by hand. The handles of cranes must be taken off or the handle shaft put out of gear before the operation of lowering is commenced.

When a load is being landed, care must be taken to obviate the risk of its heeling over and causing the crane to slew.

11. PAWLS MUST ALWAYS BE ENGAGED IN THE RATCHET WHEEL WHEN LIFTING, AND ONLY DISENGAGED DURING LOWERING OPERATIONS.

12. Loads must not remain suspended beyond the time necessary to adjust the crane for lowering.

13. Travelling and portable cranes bear a caution plate embodying instructions in regard to user, and these must in all cases be strictly observed. The special attention of all concerned is directed to the fact that the crane must be properly secured and adjusted before being used for lifting heavy weights. The crane must be drawn out and properly secured to the rails before being used for lifting heavy weights. If these precautions are neglected, the stability of the crane must be taken.

Whenever it is necessary for the crane chains to be used doubled, the snatch blocks must be made use of.

14. Cranes, whether capable of moving under their own power or not, must not be moved when on running lines the gradient of which is 1 in 150 or steeper, unless coupled to a locomotive.

Delete clause 15.

16. With either class of crane, the locomotive should, in all cases where no serious inconvenience to the operation would result, be placed below, rather than above the crane, i.e., the locomotive should stand lower on the gradient than the crane.

17. Where cranes are working on a gradient steeper than 1 in 260, a baulk of timber must in every case be fixed across the line and secured to the rails by means of chains at the lowest point on the gradient to which the crane, or the locomotive attached to the crane, may be required to travel, unless the locomotive is at the lower level.

(G.A. 23-7 49 R.E. Stand. Op. Com. Mn 77)
18. Where the track on which the crane is working is level, or is slightly up or down the case where a bridge is under reconstruction, a baulk of timber is in every case to be fixed to the rails as described above, independently of whether the line is level or on a gradient, and short of the point at which the break of track occurs.

19. Before the crane is allowed to work on G.W. lines it must be examined by a competent Locomotive Department Mechanical Inspector who will certify that the crane is fit to travel on G.W. lines or on the particular lines on which it is to work.

20. Whenever the crane is required to work under load on running lines, a locomotive must be provided to work with it. Clauses 14 to 18 to be applied strictly.

21. The driver of such steam crane must, before the permission is given, be required to pass a similar examination by the Locomotive Inspector, as is required of private owners' locomotives.

Petrol-electric Mobile Cranes.

22. The arrangements for the maintenance and working of these cranes are set forth in Joint Departmental Instructions issued by the Chief Mechanical Engineer and Superintendent of Road Transport jointly with the Chief Goods Manager for Traffic cranes and jointly with the Chief Docks Manager for Docks cranes.

Cranes of this type that are used generally for Traffic purposes must not be moved from station to station, or outside station premises, without the Chief Goods Manager's authority.

Ropes--

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EXAMINATION HAULING

Instructions to

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EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES.—Page 293.

Instructions to Staff respecting Working and Transit.

The following to be added as clause 13a:—

Staff must see that they do not expose themselves to danger during lifting operations by standing where the load may fall, be lowered on to them, or strike them during motion, or where they may be struck by rotation of crane handles which, through some defect, cannot be removed or disconnected as required by Clause 10. When withdrawing chains or slings clear of loads care must be taken to ensure that they do not catch in the load and cause it to overturn.

Staff must not ride on a hook or load, nor interfere with a load except to guide or prevent it swinging.

(G A.30 Op —9 54 C.R.O.—W.36027,5).

CHAINS, WIRE ROPES AND OTHER LIFTING APPLIANCES.

The expedient of using two 3 leg slings instead of a 4 leg sling or two single leg

the following to be added to Cause 29

SLINGS, WIRE ROPES AND THEIR ATTACHMENTS.

The expedient of using two 2-leg slings instead of a 4-leg sling or two single-leg slings in place of a 2-leg sling must not be resorted to, as this entails two rings or end links being placed on the crane hook. There is a possibility of one of these slipping off the hook when the load is being lifted particularly when the load is out of balance and takes up an inclined position.

When a load to be lifted requires the use of a multiple leg sling only the standard type of sling which has one ring for placing on the crane hook should be employed. (C A 29 Op — 5/52.)

about becoming obsolete. The 4-leg sling is being discontinued owing to the

Clause 21 to be amended to read.

Chains, wire ropes and their attachments.

When using 1 chain or four leg slings the horizontal distance H , between opposite hooks, shall not be greater than $\frac{1}{2}$ times L or least 6 ft. 6 in. and when using three leg slings the horizontal distance H between hooks shall not exceed $\frac{1}{2} L$. Under these conditions the vertical angle between the sling legs will be about 120° and the working end of the chain will be marked on the main part of the sling.

Use of multiple slings.

The angle between legs shall not exceed 90° except in special cases, when the load must not exceed that shown on the table posted on the premises where the sling is used.

When using multiple leg slings care must be taken that each leg carries an equal share of the load and where this is not possible a tag end shall be used.

Note. The fitting of a tablet on the sling is being discontinued owing to the tablet becoming discolored in use.

Ropes—wire and fibre.

23. Each rope (wire or fibre), including slings, used for lifting, must bear a ferrule with the number and its maximum working load, and in no circumstances shall the load be exceeded.

24. Ropes must not be used with the same freedom as chains or fibre ropes, and must not be used to round small objects such as crane hooks, or used in any way which would cause a sharp bend or kink. This practice injures the wires and causes them to break under load.

25. It is very important that the Jib Head Sheave of a crane, or the hook of an appliance, should be directly over the load before lifting is commenced, or otherwise liable to be jammed at the side of the sheave, causing it to break.

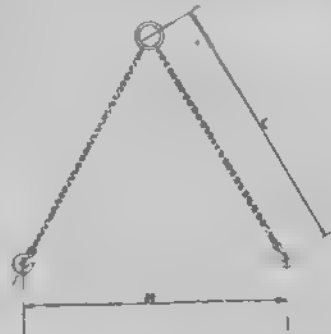
26. All wire ropes in good condition they must be kept well lubricated internally and externally. Supplies of suitable lubricant (described as "Lubricant; wire ropes") can be obtained from the Stores Department.

27. All ropes and fittings (wire and fibre), must be kept under cover in suitable sheds.

Wire Ropes, and their attachments.

28. The Company's own slings, hooks, chains, etc., should be used as far as practicable. Appliances belonging to the Company should not be used unless special authority is given by the Company, and in all such exceptional cases a responsible man must first see that they are in good condition and of ample size, and the man must accept all responsibility for their use.

used
ten times
the weight
of the load
it is
a strong steel must be
used in the opinion of the
engineer, give the necessary



Numbers and maximum loads indicated on ropes.

Wire ropes, avoidance of sharp bends.

Care in fitting with wire ropes.

Lubrication of wire ropes.

Ropes to be kept under cover.

Prohibited appliances to be used only with special authority.

Use of double slings.

29. All rings must be large enough to pass easily over crane hooks. If the rings are too small a shackle large enough to fit the crane hook, must be used.

30. Cranes should be equipped with a suitable shackle, which will be supplied by the Mechanical Engineer on receipt of a requisition on form 2432.

31. Cranes must be run on their own wheels.

32. Steam and hand travelling cranes must be conveyed from place to place as far as possible, on the main line freight trains and then subject to the following conditions:

- (a) All cranes to be placed next in front of the rear brake van.
- (b) Not more than one crane, with match truck, of combined dead weight exceeding 30 tons may be carried on a suitable freight train, and only then provided no other vehicle conveying an exceptionally heavy load is also attached.

Cranes of greater lifting capacity than 25 tons must not be conveyed by ordinary freight services. Cranes of greater lifting capacity than 25 tons must not be conveyed by "breakdown" or "Engineering Department train," they must always be worked from point to point as a special train.

33. No travelling crane of 10 tons capacity or over and no steam travelling crane must be allowed to be moved without an attendant (supplied by the Chief Mechanical Engineer's Department on application). The attendant will ride in the Guard's brake van, except when the crane is in steam, or when it is to be raised en route, when he will ride in the cab of the crane. The crane attendant, with the Guard, will be responsible for safe running.

34. The transit of steam travelling cranes on certain lines and branches is subject to the following conditions: (a) The transit of cranes must be governed by the observation of the following governing the transit of all steam travelling cranes.

35. The jibs of cranes not fitted with derricking gear must not be lifted or lowered with the Slack Block in position, the hook of the crane chain must be engaged in the shackle on the jib or crane post before the jib is lifted or lowered.

36. The movable parts of travelling cranes must always be securely fastened before the cranes are allowed to travel. The jib must be properly lowered and secured so as to pass under the load gauge, and in cases where the jib is not carried on the tie rods, it must be lowered on to the match truck.

Special precautions for securing movable parts, etc.

Position of jibs
of cranes when
travelling.

Cranes with jibs
carried by
trucks
Special
Precautions.

Movement of
cranes in yards,
etc.
Cranes removed
from running
lines

Travelling
cranes in
Severn Tunnel

35. Steam and hand travelling cranes with the jib properly secured on a specially constructed match truck fitted with roller on which the jib can rest and traverse may be permitted to travel with the jib leading or trailing. Other steam or hand travelling cranes must, when practicable, travel with the jib trailing.

36. **CRANES WITH THE JIBS CARRIED BY THE TRUCKS MUST TRAVEL WITH THE JIBS POINTING TO THE REAR OF THE TRAIN.** If, however, a crane is at a terminal or other station where facilities do not exist for turning it may be allowed to travel with the jib pointing in the direction the train is travelling to the nearest point at which it can be turned. Special care must be taken when the load is trailing, to see that the jib is securely chained or roped to a match truck or wagon, sufficient allowance being made to prevent rigidity in passing round curves.

37. When moving travelling cranes in station yards, the jibs must be lowered to be within the load gauge and properly secured.

38. If travelling cranes have to be removed from running lines or sidings this must be done only by the Chief Mechanical Engineer's Department who will furnish the necessary assistance and timber loaders or other experts if available. After replacement the cranes must not be allowed to travel over running lines or sidings until they have been examined by a representative of the Chief Mechanical Engineer's Department. Such examinations will be made by wagon Examiners where available, at other places the nearest Divisional Locomotive Superintendent will arrange on receipt of an advice.

39. Steam travelling cranes with the jib properly secured on a specially constructed match truck fitted with roller on which the jib can rest and traverse may pass through the Severn Tunnel with the jib leading or trailing. The speed at which these cranes are conveyed, whether ordinary freight trains or special trains, must not exceed a speed of 25 m.p.h. through the Tunnel.

Other steam cranes and hand cranes must not be allowed to pass through the Severn Tunnel except on trains specially arranged for the purpose, and the crane must be properly adjusted for travelling, and the jib is pointing forward. The speed of the train conveying such cranes must be limited to 25 m.p.h. through the Tunnel.

Register of Appliances.

40. The Station Master, Goods Agent or other person in charge must keep a register in approved form of all Lifting and Hoisting appliances, used or used, under his control.

The register must be kept posted up to date by the entry of any additional appliances provided and by marking off any that are sent away.

Quarterly Inspection

41. The Station Master, Goods Agent, or other person in charge must see each individual article in the Register at least once every three months. If stored as a rule, be accompanied in this inspection by the man appointed to make the weekly examinations (paragraph 61) and should satisfy himself that these instructions are properly understood and carried out.

Responsibility of Departments for Maintenance, Testing and Registration.

42. The departmental responsibility for maintenance is as under:—

- | | |
|--|----------------------------|
| (a) All appliances, including structures and foundations, used by the Locomotive, Carriage and Wagon and Stores Departments at the Works and Running Sheds at Swindon and Wolverhampton, and Severn Tunnel Pumping Stations. | Chief Mechanical Engineer. |
| All Travelling and Portable cranes. | |
| All appliances (including those on floating craft) other than provided for in (b) and (c). | |
| (b) Manual appliances (including those on floating craft) used exclusively by the Engineering Department, and: | Civil Engineer. |
| All foundations, including masonry, metal and timber, and all tracks other than provided for in (a) | |
| (c) All appliances on floating craft under the supervision of the Docks Department, including those subject to Lloyd's survey. | Chief Docks Manager. |

Fixed appliances provided for the Signal and Marine Departments will be painted by the owning departments.

It is recognised that in certain cases it will be convenient for one department to carry out work for and at the expense of another department.

Weight testing

43. THE WEIGHT TESTING OF ALL APPLIANCES WILL BE CARRIED OUT BY THE CHIEF MECHANICAL ENGINEER.

Department's
responsibility
for
maintenance

CHAINS—ANNEALING AND TESTING.

References to "Rail Clips for Traveling Cranes," in Clause 50, to be deleted.

The following to be added to Clause 50—

Wrought iron chains are in some cases being replaced by steel chains which do not require annealing after use but normalising after manufacture and before use. It will, therefore, only be necessary to forward steel chains and their attachments to the Testing House, Locomotive Works, Swindon for testing at three yearly intervals except those on land cranes fixed and movable and on manual appliances of all kinds which should be sent in for testing at six-yearly intervals. To enable examiners to identify steel chains these will be stamped with the letter 'S' at the end of the register number on the tab attached to the chain or on the ring of multiple egg slings. The tab will be attached to the chain by a steel wire in the form of a closed 'S'. A similar piece of wire will also be attached to the ring of multiple egg slings made of steel.

(G A 29 Op 5 52)

(1) Any chain or attachment
marked 'S' instead of annealed?

(G A 1-11 17 G VI—w)

49. The date of the testing and examination of the appliances must be...

The following to be added to Clause 101:

The following classes of chain and lifting tackle are exempt from annealing.

(1) Chains made of malleable cast iron

(2) Plate and chain

(3) Chains, rings, hooks, shackles and swivels made of steel or of any non-ferrous metal

(4) Fitted chains working on sprocket or socketed wheels

(5) Rings, hooks, shackles and swivels permanently attached to pitched chains, pulley blocks or weighing machines

(6) Hooks and swivels having screw threaded parts or ball-bearings or other case-hardened parts

(7) Shackle shackles secured to wire ropes by white metal capping

(8) Loaded connections

(9) Any chain or lifting tackle which has been subjected to the heat treatment known as "normalising" instead of annealing

(G.A. 18 11 15 G.M. - W 36 27 5)

as forming part of the chains with which attachments, Boards, etc., chains and

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44. Each crane must bear a registered and consecutive number, indicated by means of a plate. The registered numbers will be appropriated, and plates supplied, by the Chief Mechanical Engineer. As new numbers are allocated, all old number plates and painted numbers must be deleted. Prefix letters as under, will be used in connection with the numbering, to indicate the Group and Power of each crane:

Registration of cranes.

GROUP.	POWER.
F. (Fixed).	M. (Manual).
M. (Movable).	S. (Steam).
P. (Portable).	E. (Electric).
T. (Travelling).	H. (Hydraulic).
	O. (Petrol, Oil, etc.)
e.g. F.M.100	-- Fixed Manual.
P.S.100	-- Portable Steam.

For the convenience of the users, the Maximum Load must be clearly indicated upon each appliance.

Indication of maximum load on appliances.

Instructions respecting Testing and Examination.

46. All appliances will be weight tested when new, or when re-erected after extensive repairs. It is recommended that there may be exceptional cases in which special weight tests are desirable; such tests will be a matter of arrangement between the department concerned and the Chief Mechanical Engineer.

Weight testing of appliances.

47. A THOROUGH EXAMINATION OF ALL CRANES, OTHER LIFTING AND HAULING APPLIANCES (EXCEPT LIFTS, FOR WHICH, SEE BELOW) AND ALL LOOSE TACKLE MUST BE MADE ONCE EVERY TWELVE MONTHS BY A

Examination by Maintenance

48. Every lifting appliance must be examined by all lifting and hauling appliances, and the maintenance of these appliances.

Records of examination and testing.

49. The lifting and hauling examination of Cranes and all lifting and hauling appliances must be legibly marked upon them.

Dates of testing and examination to be marked on cranes.

Chains—Annealing and Testing.

All chains, slings and their attachments, including Shackles, Rings, Hooks, Dog-Hooks, and Travelling Hooks, Links and Snatch Hooks must be sent to the owning department periodically to the Testing House, Locomotive Works, Swindon, for annealing and testing, as set out below.

Prescribed Annealing and Testing

The intervals between Annealing and Testing not to exceed—

Chains $\frac{1}{2}$ " diameter or less in use at Docks, Wharves, Quays and premises subject to the Factories Act, also all chains used in connection with molten metal or slag ... 6 months

6 months

Chains $\frac{3}{4}$ " diameter or more on Travelling and Portable Cranes, and on all power appliances.

50. Chains and attachments, such as Coal Trays, Scaffolding Boards, etc., must be tested, stamped with the working load and a certificate issued, which will cover these items until such time as they may require heavy repairs, when they would again be tested and a fresh certificate issued.

Chains and attachments

51. Chains and attachments to which chains are not permanently attached, such as tubs, buckets, trays, etc., must be tested, stamped with the working load and a certificate issued, which will cover these items until such time as they may require heavy repairs, when they would again be tested and a fresh certificate issued.

52. If a chain is found on examination (see paragraphs Nos. 40-44) to be badly defective, it must be sent to Swindon for testing forthwith, even though not due for testing as specified above.

Defective chains etc. to be tested.

53. The Station Master, Agent or other person affected must keep a register of the chains, slings, and other loose lifting tackle under his charge (a special book being provided for large stations), and care must be taken to ensure that the dates on which chains, etc., are sent for testing and returned are accurately recorded. The register must be periodically examined by the Station Master, or other person having charge of chains, etc., so as to ensure that every appliance requiring testing is dealt with in accordance with instructions. This does not refer to chains $\frac{1}{2}$ " in diameter or smaller, in use at Docks, etc., which must be specially dealt with as directed in Statutory Order No. 279 issued by the Home Office.

Register of chains, etc.

54. Every chain, sling, or other lifting tackle tested will have its number stamped on the end link, and a small label attached to the end link bearing the number of the chain as recorded in the Station Master's register, and the maximum working load when used as a single chain. This label must on no account be removed.

Chains to bear metal number labels

The following to be added as Clause 54 A:—

Unregistered lifting tackle.

Unregistered lifting tackle should not be used, but, if found, should be sent to Swindon for registration.

(G.A.29.Op.—5/52.)

Chains $\frac{1}{2}$ " diameter or less in use at Docks, Wharves, Quays and premises subject to the Factories Act, also all chains used in connection with molten metal or slag	6 months
Chains over $\frac{1}{2}$ " diameter on Travelling and Portable Cranes, and on all power appliances	
Chains $\frac{1}{2}$ " diameter and under on Hand Cranes Fixed and Movable, and on manual appliances of all kinds	2 months
Chains over $\frac{1}{2}$ " diameter used on Cranes and other hoisting appliances worked by hand at the Docks, and subject to Statutory Rules and Orders, 1934, No. 219	2 years
Chains over $\frac{1}{2}$ " diameter on Hand Cranes, Fixed and Movable, and on manual appliances of all kinds	3 years

Regulations for repairs during repairs and testing.

55. When a crane chain, a drag sling, or other loose lifting tackle requires testing, and cannot be spared for the time necessary for it to be sent to Swindon and returned, application must be made to the Chief Mechanical Engineer, Swindon, on Form No. 2432 for another to take its place. A test it has with the be safe, and when received, the chain or other testing must be sent to Swindon together with an advice on Form No. 2432. The chain or other testing is retained as the permanent chain until it, in turn, requires testing. Form No. 2432 must be used in ordering new chains and for lifting tackle, as the proper number quoted as shown on diagram of Standard Crane Lifting Tackle. A chain or other testing chains should quote the length of chain in the maximum lifting capacity of the appliance.

Regulations for the testing of chains

56. Form 2432 must always be used when a duplicate chain has been received in reply to a request on Form 2432, the form relating to the one which is due for testing must be endorsed—"Chain not required to be returned."

Loan of lifting tackle

57. ~~Form 2432 must always be used when a duplicate chain has been received in reply to a request on Form 2432, the form relating to the one which is due for testing must be endorsed—"Chain not required to be returned."~~
57. ~~Form 2432 must always be used when a duplicate chain has been received in reply to a request on Form 2432, the form relating to the one which is due for testing must be endorsed—"Chain not required to be returned."~~
Form 2432 must always be used when a duplicate chain has been received in reply to a request on Form 2432, the form relating to the one which is due for testing must be endorsed—"Chain not required to be returned."

In the case of tackle for loan to outsiders, the Chief Mechanical Engineer must be furnished with particulars to ensure that the appliances are returned in the same condition as when they were issued. In urgent cases application may be made by telephoning to the Chief Mechanical Engineer.

Chains, etc., to be kept under continuous observation

58. Chains, wire ropes, and their attachments, drag-hooks attached to tow-ropes and other loose fittings must be used in the same condition as received from Swindon. They must not be altered, cut, or altered in any way. If any chain, hook, or fitting requires alteration it must be sent to Swindon for repair.

Chains, etc., to be kept under continuous observation

59. All chains and loose lifting tackle sent to Swindon must be addressed to the Chief Mechanical Engineer, to whom application should be made for a receipt sheet for a duplicate to accompany them. An advice of receipt should be sent to the Chief Mechanical Engineer, and the chains and other tackle kept free of charge. The chains and other tackle received from Swindon must be returned to the Chief Mechanical Engineer.

Examinations of chains, ropes, and other tackle

60. Examination of chains, etc., as set out below, will be arranged by the Engineers responsible for maintenance, who will notify the Chief Mechanical Engineer when replacements are necessary.

To be examined at intervals not exceeding—

Chains, ropes (wire or fibre) and other fittings on all cranes and lifting appliances worked by power, including all loose chains and other tackle	1 month.
Wire ropes on manual appliances	3 months.
Chains, and fibre ropes on hand cranes and on all manual appliances, including loose chains and other tackle ..	6 months.

Examination by Owing Department.

Chains, etc., to be kept under continuous observation

61. In addition to the periodical testing and examination it is necessary for all chains, ropes and loose fittings to be kept under the continuous observation of the using department. A competent man must be appointed at each station by the Station Master, Goods Agent, or other person in charge to examine each week (preferably on Monday morning), all lifting and hauling appliances including chains, ropes, slings, tow-ropes, and having rope attachments, rail clips, etc., and all other fittings covered by these instructions and report upon their condition.

Reports of weekly examination.

When making his weekly reports, the person so appointed must state whether the loose lifting tackle indicated on list supplied to him is safe, or not, and if not, in every respect.

Procedure with defective ropes wire and fibre,

62. Any fibre rope found defective must be at once cut in two so as to prevent its being used. The old rope must be sent to Swindon, and application made on Chain Requisition Form No. 2432 to the Chief Mechanical Engineer for a new one. If any wire rope, wire sling, etc., is found on examination to have 10 per cent. of the total number of wires broken in any length equal to eight times the diameter it should be condemned and sent to Swindon, and application made to the Chief Mechanical Engineer's Department, on Wire Rope Form No. 3171, for a replacement.

63. The weekly examination will be independent of the periodical testing and examination of chains referred to in paragraphs 50 and 60.

Weekly examination independent of periodical examination of Dock Buoys, etc

64. All Dock Buoys and all Dock Mooring Chains are to be examined every five years by a Diver, and must be tested, repaired and renewed as and when found necessary.

Chains—Annealing and Testing.

Amend the first sentence of clause 56 to read:—

Form 2432-I must always be rendered in duplicate when a chain is forwarded for testing.

(G.A.30 Op.—9/54. C.R.O.—W.36027/5).

Amend the first sentence of clause 57 to read:—

Loose lifting tackle required for temporary use should be requisitioned on form 2432 rendered in duplicate amended as necessary, and the numbers quoted as appearing in the diagram of "Standard Lifting Tackle."

(G.A.30 Op.—9/54. C.R.O.—W.36027/5).

JAMES MILNE,

General Manager.

July, 1937.

Each member of the Staff receiving a copy of this Circular is requested to read carefully and note the amendments supplied with a copy of the General Appendix, to alter, or cancel in ink the present copy of the General Appendix, afterwards passing the Amendments in the proper places in the General Appendix.

Station Masters are responsible for seeing that copies of the General Appendix supplied to Seafarers, after their examination, are corrected in accordance with this Circular.

(This form must be detached and forwarded to the Head of Department.)

1937.

Station.

RECEIVED copy of Circular G.A. 2 dated July, 1937, containing Alterations and Additions to the General Appendix to the Rule Book.

STATION INSTRUCTIONS.

EXAMINATION AND TESTING OF CHAINS.

The instruction classes Nos 48 and 49 of General Manager's Circular No. 2032, dated November 1st, 1924, are hereby observed by a competent person.

48. When a chain is required for testing, the necessary requisitions for replacements during repairs and testing must be placed in the hands of the Chief Mechanical Engineer, Swindon, on Form No. 2432 for the purpose. The tested chain will then be supplied with an entry on Form No. 48, which is to be placed in the chain as the permanent identification of the chain. Form No. 2432 must be sent to the Chief Mechanical Engineer, Swindon, and the requisition must be quoted as shown on diagram of "Standard Lifting Tackle."

Form No. 2432 must always be used when a chain is forwarded for testing. If a duplicate has been received in respect of a requisition Form No. 2432, the duplicate must be returned.

49. All requisitions for lifting tackle sent to Swindon must be addressed to the Chief Mechanical Engineer, to whom application should be made for the necessary requisitions. The requisitions must be sent to the Chief Mechanical Engineer, Swindon, and the requisition must be quoted as shown on diagram of "Standard Lifting Tackle."

It shall be particularly noted that, in every case, the chain requisition must be forwarded to Swindon on the same day as the chain is despatched.

EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES.

Definitions.

Lifting and hauling appliances include —

Cranes; Travelling cranes; Overhead Runways; Hoists and Lifts; Capstans and Hauling Appliances; Traversers; Overhead Runways; Chains and Ropes, and all loose tackle for lifting and hauling.

(a) Fixed Cranes are cranes that are unable to travel.

(b) Movable Cranes are those that can travel within limits on a prepared track.

(c) Portable Cranes are those that can be moved from place to place.

(d) Travelling Cranes are cranes that can travel on their own wheels in a train.

When a crane is being examined, it shall be marked with a red band and Refixing of Ropes and Chains.

Ownership.—The Department for whose use an appliance is provided is deemed the "owning" Department.

Instructions to Staff respecting Working and Transit.

1. Cranes and lifting appliances, hoists, capstans, slings, etc., must be used only by persons who must satisfy themselves that they are in good working order. be used only by authorized persons.

2. ANY DEFECT MUST BE PROMPTLY REPORTED TO THE STATION MASTER, GOODS AND OTHER PERSON IN CHARGE, WHO WILL COMMUNICATE WITH THE DEPARTMENT RESPONSIBLE FOR MAINTENANCE. ON NO ACCOUNT IS ANY CRANE, ROPE, SLING, ETC., FOUND DEFECTIVE, TO BE USED UNTIL THE DEFECTS HAVE BEEN MADE GOOD. THE INSTRUCTIONS ON EACH CRANE MUST BE STRICTLY OBSERVED. WHEN CRANES ARE OUT OF USE, ALL PARTS MUST BE LOCKED OR OTHERWISE SECURED SO AS TO PREVENT THEIR BEING MOVED BY WIND OR OTHER FORCE.

3. All Station Lifting and Hauling Appliances will be worked by staff provided by the Mechanical Engineering Department, except that they may be used exclusively by the Civil Engineering and Naval and Marine Departments who will provide their own staff. Station work will be done by the Civil Engineering and Naval and Marine Departments.

4. ON NO ACCOUNT MUST ANY CRANE OR OTHER LIFTING OR HAULING APPLIANCE BE USED FOR A TRAVELLING LOAD THAN THE MAXIMUM MARKED UPON IT. No person must use an appliance without first ascertaining its maximum load. Never use a crane for loads not marked on it.

5. When doubt exists as to the weight of a load being within the marked lifting capacity of an appliance, the load must not be lifted without the special authority of the person in charge of the appliance, who must, if necessary, consult the Maintaining Department. Care with loads of unknown weight.

See G. A. 2.

STATION INSTRUCTIONS.

EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES—Continued.

6. WITH REGARD TO TIMBER, IT MUST BE BORNE IN MIND THAT THE WEIGHT VARIES WIDELY FROM VARIOUS CAUSES; THE EXCESS OF ACTUAL WEIGHT OVER WEIGHT ESTIMATED BY MEASUREMENT MAY VARY FROM 50 PER CENT. IN THE CASE OF DRY, WELL SEASONED TIMBER, TO 100 PER CENT. IN THE CASE OF RECENTLY CUT TREES.
7. Cranes must not be used for shunting or towing trucks nor for lifting articles out of plumb.
- (NOTE. Care must be exercised to see that a load to be lifted from a stack, such as timber, is quite free and not pinned down or jammed in any way.)
8. The slewing gear must be used where provided. In other cases manual power must be employed for pulling round the jibs of cranes.
9. Great care must be exercised in using the brake power, particularly in damp weather, to prevent the brake slipping. When loads are to be lowered by means of a hand or foot brake, the brake must be applied, the gearing released, and the brake kept on until the loads are in position for lowering, and sufficient brake power must be applied and maintained to lower slowly. On no account must the lowering be stopped suddenly; and the man in charge must not release control of the brake lever unless it is properly secured or the load is being lowered by hand. The handles of cranes must be taken off or the handle shaft put out of gear before the operation of lowering is commenced.
10. PAWLS MUST ALWAYS BE ENGAGED IN THE HATCHED WHEEL WHEN LIFTING, AND ONLY DISENGAGED DURING LOWERING OPERATIONS.
11. Loads must not remain suspended beyond the time necessary to adjust the crane for lowering.
12. Travelling and portable cranes bear a caution plate embodying instructions in regard to user, and these must in all cases be strictly observed. The special attention of all concerned is directed to the necessity for seeing that when such cranes are used whilst stationary for lifting heavy weights, the extension girders on both sides, when provided, are drawn out and properly packed and the crane secured by means of the rail clips when provided. If these precautions are impracticable, other means of securing the stability of the crane must be taken.
- Whenever it is necessary for the crane chains to be used doubled, the snatch blocks must be made use of.
13. Cranes which are incapable of moving under their own power must not be moved, when on running lines, the gradient of which is 1 in 100, or steeper, unless coupled to a locomotive.
14. Cranes which are capable of moving under their own power must not be moved, when on running lines, the gradient of which is 1 in 100, or steeper, unless coupled to a locomotive.
15. With either class of crane, the locomotive should, in all cases where no serious inconvenience to the operation would result, be placed below, rather than above the crane, i.e. the locomotive should stand lower on the gradient than the crane.
16. Where cranes are working on a gradient steeper than 1 in 200, a baulk of timber is in every case to be fixed across the line and secured to the rails by means of chains at the lowest point on the gradient to which the crane, or the locomotive attached to the crane, may be required to travel.
17. Where the track on which the crane is working is broken, as is frequently the case where a bridge is under reconstruction, a baulk of timber is in every case to be fixed to the rails as described above, independently of whether the line is level or on a gradient, and short of the point at which the break of track occurs.
- Ropes—wire and fibre.*
18. Each rope (wire or fibre), including slings, used for lifting, must bear a ferrule giving its registered number and its maximum working load, and in no circumstances must the working load be exceeded.
19. Wire ropes cannot be used with the same freedom as chains or fibre ropes, and must not be bent round small objects such as crane hooks, or used in any way likely to produce a sharp bend or kink. This practice injures the wires and causes them to break under load.
20. It is very important that the Jib Head Sheave of a crane, or the hook of an overhead crane, should be directly over the load before lifting is commenced, otherwise the WIRE ROPE is liable to be jammed at the side of the Sheave, causing it to break.

Varying weights of timber

Cranes not to be used for loads out of plumb.

Use of slewing gear

Use of brake power

Use of Pawls.

Loads not to remain suspended
Strict observance of instructions on caution plates

Use of Travelling Cranes on running lines.

Use of Ropes.

Numbers and maximum as indicated on ropes

Use of ropes of sharp bends.

Care in lifting with wire ropes

The following to be substituted for paragraph 25 :—

No hand travelling crane of 10 tons capacity or over, and no steam travelling crane must be allowed to travel by freight train without an attendant (as supplied by the Chief Mechanical Engineer's Department on application). The attendant will ride in the Guard's brake van, except when the crane is in steam, or steam is required to be raised en route, when he will ride in the cab of the crane. The crane attendant with the Guard will be responsible for safe running.

(G.A. 1. 3/37. LK.1/3946.12.)

STATION INSTRUCTIONS.

EXAMINATION, MAINTENANCE TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES—Continued.

21. To maintain wire ropes in good condition they must be kept well lubricated internally and externally. Supplies of suitable lubricant (described as "Lubricant; wire ropes") can be obtained from the Stores Department.

Lubrication of wire ropes.

22. Loose ropes and fittings (wire and fibre) must be kept under cover in suitable place.

Ropes to be kept under cover.

(Chains, Wire Ropes, and their attachments.

23. The Company's own slings, hooks, chains, etc., should be used as far as practicable. Appliances belonging to traders should not be used unless special authority is given by the Company, and in all such exceptional cases a responsible man must first see that they are in good condition and of ample size, and the owners must accept all responsibility for their use.

Traders' appliances to be used only on special authority.



24. When using double slings, care must be taken that the horizontal distance H, between the hooks, is not greater than L, the length of the sling.

Use of double slings.

If H is greater than L, a stronger sling must be used such as will, in the opinion of the person in charge, give the necessary security.

Cranes travelling by train on their own wheels.

25. Cranes travelling by train on their own wheels must be fitted with derriking gear and no steam travel.

26. The jibs of cranes not fitted with derriking gear must not be lifted or engaged in the shackle on the jib or crane post before the jib is lifted or lowered.

Safety precautions for securing the jib to derriking gear.

27. The movable parts of travelling cranes must always be securely fastened before the cranes are allowed to travel. The jib must be properly lowered and secured so as to pass under the load gauge, and in cases where the jib is not carried on the tie rods, it must be lowered on to the match truck.

28. Steam and hand travelling cranes, with the jib properly secured on a specially constructed match truck fitted with roller on which the jib can rest and travel, may be permitted to travel with the jib leading or trailing. Other steam or hand travelling cranes must travel with the jib trailing. (See Note at foot of page.)

Positions of jibs of cranes when travelling.

29. CRANES WITH THE JIBS CARRIED BY THE TIE RODS MUST TRAVEL WITH THE JIBS POINTING IN THE DIRECTION THE TRAIN IS TRAVELLING. Where facilities do not exist for turning, it may be allowed to travel with the jib pointing in the direction the train is travelling, to the nearest point at which it can be turned. Special care must be taken (whether jib is leading or trailing) to see that the jib is securely chained or roped to a match truck or wagon; sufficient allowance being made to prevent rigidity in passing round curves.

Crane with jibs carried by the rods. Special Precautions.

30. When moving travelling cranes in station yards, the jibs must be lowered to the load gauge and properly secured.

Movement of cranes in yards, etc.

Cranes of greater lifting capacity than 25 tons must not be conveyed by ordinary freight services. When not forming part of a breakdown or Engineering Department train, they must always be worked from point to point as a special train. Two or more of these cranes may be conveyed by the same special train where it is convenient to do so and provided the route is authorised for the passage of such cranes.

No hand travelling crane of 10 tons capacity or over, and no steam travelling crane must be allowed to travel by freight train without an attendant (supplied by the Chief Mechanical Engineer's Department on application). The attendant will ride in the Guard's brake van, except when the crane is in steam, or steam is required to be raised en route, when he will ride in the cab of the crane. The crane attendant, with the Guard, will be responsible for safe running.

Note.—An addendum to Circular No. 3450 will be issued at a future date.

(G.A.13. 9/43. G.M. —W.36027/5)

[illegible]

STATION INSTRUCTIONS.

EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES—Continued.

Other steam cranes and hand cranes must not be allowed to pass through the Severn Tunnel, except on trains specially arranged for the purpose, and provided the crane is properly adjusted for travelling, and the job is pointing towards the rear of the train, but the speed of the train conveying such vehicles must be limited to 25 m.p.h. through the Tunnel.

Register of Appliances.

33. The Station Master, Goods Agent or other person in charge must keep a register in approved form of all lifting and hauling appliances, loose or fixed, under his control.

The register must be kept posted up-to-date by the entry of any additional appliances provided and by marking off any that are sent away.

Quarterly Inspection.

34. The Station Master, Goods Agent or other person in charge must see each individual article on the Register at least once every three months. He should, as a rule, be accompanied on this inspection by the man appointed to make the weekly examinations (paragraph 52), and should satisfy himself that the instructions contained in this Circular are properly understood and carried out.

Responsibility of Departments for Maintenance, Testing and Registration.

Departmental
responsibility for
maintenance.

35. The departmental responsibility for maintenance is as under:—

(a) All appliances, including structures and foundations, used by the Locomotive, Carriage and Wagon and Stores Departments at the Works and Running Sheds at Swindon and Wolverhampton, and Severn Tunnel Pumping Stations, and:—

Chief
Mechanical
Engineer

All Travelling and Portable cranes and all appliances (including those on floating craft) other than provided for in (b) and (c).
All electrical appliances.

(b) Manual appliances (including those on floating craft) used exclusively by the Engineering Department, and:—

Chief
Engineer

Appliances used for moving timber, and all tracks other than provided for in (a).

Appliances used for the movement of the Marine Department, and subject to Lloyd's Survey.

Master
Superintendent

Fixed appliances provided for the Signal and Marine Departments will be painted by the owning departments.

It is recognised that in certain cases it will be convenient for one department to carry out work for and at the expense of another department.

Weight testing.

36. THE WEIGHT TESTING OF ALL APPLIANCES WILL BE CARRIED OUT BY THE CHIEF MECHANICAL ENGINEER.

Registration of
cranes.

37. Each crane must bear a registered and consecutive number, indicated by means of a cast-iron plate to be supplied by the Chief Mechanical Engineer. As new appliances are introduced, the old numbers must be deleted. The following are the codes used to indicate the Group and Power of each crane:—

GROUP.	POWER.
F. (Fixed).	M. Manual.
M. (Movable).	S. Steam.
P. (Portable).	E. (Electric).
T. (Travelling).	H. (Hydraulic).
	O. (Petrol, Oil, etc.).
e.g. F M.100	Fixed Manual
P S.100	Portable Steam.

Indication of
maximum load
on appliances.

38. For the guidance of the user, the Maximum Load must be clearly indicated upon each appliance.

Instructions respecting Testing and Examination.

Weight testing
of appliances.

39. All appliances will be weight tested when new, or when re-erected after extensive repairs. It is recognised that there may be exceptional cases in which special weight tests are desirable, such tests will be a matter of arrangement between the Department concerned and the Chief Mechanical Engineer.

STATION INSTRUCTIONS.

EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES—Continued.

40. A THOROUGH EXAMINATION OF ALL CRANES, OTHER LIFTING AND HAULING APPLIANCES AND ALL LOOSE TACKLE MUST BE MADE ONCE EVERY FIVE MONTHS BY A COMPETENT MECHANIC PROVIDED BY THE MAINTAINING DEPARTMENT, AND A CERTIFICATE OF SUCH EXAMINATION FORWARDED TO THE OFFICE OF THE ENGINEER RESPONSIBLE FOR MAINTENANCE.

Examination by
Maintaining
Department.

41. Records of the examination of all lifting appliances must be kept in a book provided for the purpose for the maintenance of these appliances.

Records of
examination
to be kept in
a book.

42. The date of the examination of chains and all lifting appliances must be legibly marked upon them.

Records of
examination
to be marked
on chains.

Chains—Annealing and Testing.

43. All chains, fittings and their attachments (including Shackles, Slings, Hooks, Rigging, Ropes for Trailing Cars, Pulling Steel Lines) must be sent by the owning department periodically to the Testing House, Locomotive Works, Swindon, for annealing and testing, as set out below:—

Periodical
annealing and
testing.

The intervals
between testing
not to exceed—

Chains $\frac{1}{2}$ in. diameter or less in use at Docks, Wharves and Quays (see Circular No. 2,000, December, 1904) ..
Chains on Travelling and Portable Cranes, and on all Power appliances and Dredging Plant
Chains on Hand Cranes, fixed and movable, and on Manual appliances of all kinds

6 months.

12 months.

44. Hooks and other attachments, such as Coal Grabs, Scale Boards, etc., having chains attached, must be treated as forming part of the chains with which they are used.

Chains and
attachments.

45. If a chain is found on examination (see paragraphs Nos. 51-55) to be badly worn or to show defects, it must be sent to Swindon for testing forthwith, even though not due for testing as specified above.

Defective chains,
etc., to be tested.

46. Every Station Master, Goods Agent or other person affected must keep a book being provided for large stations, and care must be taken to ensure that the dates on which chains, etc., are sent for testing and returned are accurately recorded. The register must be periodically examined by the Station Master, or other person having charge of chains, etc., so as to ensure that every appliance requiring testing is dealt with in accordance with instructions. This does not refer to chains $\frac{1}{2}$ in. diameter or smaller, in use at Docks, etc., which must be specially dealt with as directed in General Manager's Circular No. 2,000.

Register of
chains, etc.

47. Every chain, after being tested, will have its number stamped on the end link, also a small metal label attached to the link bearing the number of the chain as recorded in the Testing House register, and the maximum working load when used as a single chain. This label must on no account be removed.

Chains to bear
and be number
labelled.

48. When a crane chain, a drag sling, or other loose lifting tackle requires testing, and a replacement is necessary, it must be sent to Swindon for testing. Application must be made to the Chief Mechanical Engineer, Swindon, on Form No. 2,432 for another to take its place; a tested chain will then be supplied, and when received, the chain requiring testing must be sent to Swindon (together with an advice on Form No. 2,432-1), the one supplied in its stead being retained as the permanent chain until it, in turn, requires testing. Form No. 2,432 must be used in ordering new chains and loose lifting tackle, and the progressive number quoted as shewn on diagram of "Standard Lifting Tackle."

Requisitions for
replacements
to be sent to
Swindon for
testing.

49. Chains, wire ropes, and their attachments, drag-hooks attached to tow ropes, etc., must be sent to Swindon for testing, and must not be heated, cut, or altered in any manner. Should any chain, hook or fitting require alteration it must be sent to Swindon for this purpose.

Chains, etc., not
to be cut, heated
or altered.

50. All chains and loose lifting tackle sent to Swindon must be addressed to the Chief Mechanical Engineer, Swindon, and should be accompanied by the necessary sheet-iron address labels to accompany them. An advice of despatch must be sent, and the chains or other tackle booked free of charge. The sheet iron address labels received from Swindon must be returned to the Chief Mechanical Engineer immediately.

Chains sent to
Swindon for
testing must be
accompanied by
labels.

EXAMINATION, MAINTENANCE TESTING AND WORKING OF LIFTING AND HAULING
APPLIANCES—Continued.

51. Explain the difference between a substitution and a replacement, with the aid of the following example. When a replacement is necessary, the original Department will be notified.

To be examined
at intervals
not exceeding—

Chains, ropes (wire or fibre) and other fittings on all cranes and lifting appliances worked by power, including all loose chains and other tackle	1 month.
Wire ropes on manual appliances	3 months
Chains, and fibre ropes on hand cranes and on all manual appliances, including loose chains and other tackle ..	6 months.

5. In addition to the periodic testing and examination if necessary for detection of possible adulteration, the kept milk should be observed in the daily consumption. A system of constant supervision at each station by the State Milk Control Authority is to be maintained. The State Milk Control Authority may also employ a special inspector to visit the dairies to see that the milk is kept in the best condition and to see that the fittings covered by this Circular and report upon their condition.

It is the duty of every person who is required to file a report under this Circular to state whether or not the information reported is true and correct in every respect.

[illegible]

Weekly examination of chains referred to in paragraphs 43 and 51.

two copies of the same report were made and I when found necessary.

Items of Scrap Metal which have not been ~~used~~ ^{used} for the purposes mentioned above shall be thrown away, but must be collected and disposed of in the following manner:—

The MORE VALUABLE METALS, such as Brass, Copper, Lead, Zinc, are to be sent to the Salvage Warehouse, Port of New York, and the more common metals such as Iron and Steel, to the Salvage Warehouse, Port of Baltimore. The more valuable metals, especially copper, should be kept separate from the other metals, and the following instructions will be given as to disposal.

[illegible]

It is desired to impress upon all personnel the importance of the Station lock policy. It is desired to the fullest extent possible to prevent unauthorized personnel from entering the Station lock. It is desired that frequently the personnel who are authorized to enter the Station lock are the personnel who are the goods issued. It is desired that one of the personnel who are authorized to enter the Station lock are the personnel who are the goods issued. It is desired that one of the personnel who are authorized to enter the Station lock are the personnel who are the goods issued. It is desired that one of the personnel who are authorized to enter the Station lock are the personnel who are the goods issued.

STATION INSTRUCTIONS.

NEATNESS AND CLEANLINESS OF STATIONS.

The attention of Station Masters is specially directed to Rule 17 (vi).

There is need for the exercise of special care in regard to all Sanitary equipment.

Station Duty Sheets should be examined in order to ensure that due provision is made for the regular performance of the necessary cleaning work, and those upon whom this devolves should be specially reminded of the importance of the matter.

CLEARING UP STATION YARDS.

WORK TO BE DONE BY TRAFFIC DEPARTMENT.—Station Offices, Waiting Rooms, Platforms (whether under cover or not), Goods Sheds and all areas under cover, must be swept and kept clean, and the Platforms weeded, by the Traffic Department, who will also be responsible for the cleansing and disinfecting of Cattle Pens, and for depositing the rubbish, manure, etc., in a receptacle provided for the purpose, or at an agreed point.

WORK TO BE DONE BY THE ENGINEERING DEPARTMENT.—Station Yards, Sidings, Roads, Wharves and all areas under cover, must be swept and kept clean, and the Platforms weeded, by the Engineering Department, who will also be responsible for the cleansing and disinfecting of Cattle Pens, and for depositing the rubbish, manure, etc., swept together in the course of cleaning up by the two Departments. Any cash

Station Masters, Goods Agents and all other persons concerned are reminded of the necessity of keeping the Stations, Offices, etc., in a neat and clean state. The Engineering Department is responsible, are not properly looked after and kept in a neat state.

Refuse from the Platforms must not be swept on to the Permanent Way, but must be collected and conveyed to the dust-bin, or be burnt.

DISPOSAL OF STATION AND OFFICE REFUSE.

Where practicable, arrangements should be made for the Local Sanitary Authorities to remove refuse from Stations, Offices, etc., proper receptacles being provided, where needed, for the deposit of refuse in places convenient for its removal by the Local Authorities' carts. Where fixed receptacles are not available, portable bins must be requisitioned from the Stores Department.

OLD BOOKS, FORMS, AND WASTE PAPER.

Records, be preserved with care, kept in good order and conveniently arranged for reference.

2. Books, correspondence, etc., must be kept for the periods prescribed.

Period for which they are to be kept.

3. Old books, forms, and waste paper must be sent to the Stores Department for disposal. The Storeskeeper concerned for the necessary sacks:—

Northern Division	Wolverhampton Stores.
London Offices and Depots	See Clause 8.
All other Stations	Swindon General Stores.

In special cases, arrangements will be made for paper to be sent direct to Contractors, and in all cases where the quantity on hand is one ton or over application should be made to the Stores Department for instructions regarding disposal.

4. Waste paper must be collected daily from the various offices, etc., at the station or depot and placed in a suitable receptacle until a sufficient quantity has accumulated to warrant an application to the Stores Department for sacks and forwarding instructions. The responsible staff must satisfy themselves that suitable accommodation is provided for waste paper.

Daily collection of Waste Paper.

Upon application, the Stores Department will supply small sacks for storage purposes and exchange them for empty sacks when received full.

The forms shown at the end of the Appendix must be carefully packed if in a clean condition and in all cases forwarded to Swindon separately from other waste paper in order that they may be packed tightly.

Method of Packing.

Correspondence books, pens and paper fasteners must be carefully removed from old papers and retained at the station or depot for further use.

Large books must not be put into sacks but tied securely in bundles.

STATION INSTRUCTIONS

OLD BOOKS, FORMS, AND WASTE PAPER *Continued*

Old tisme and foolscap books must be packed separately, the former being sent to the Stationery Superintendent, Westbourne Terrace, Paddington, G.W.1 and the latter to the Storekeeper, General Stores, Swindon.

If the quantity is more than is required for parcelling or packing purposes at the station or depot, old time bills and posters are to be returned whole, if possible, and not torn up as is often the case. This class of paper is used to considerable advantage at Swindon for packing purposes in lieu of brown paper.

Old truck labels must not be packed with waste paper, but forwarded in separate sacks and the nature of the contents marked on the label.

Care must be taken to see that **no rubbish** is packed with waste paper. Instances have occurred where detonators, broken glass, pieces of tin, etc., have been found in the sacks, involving risk of serious accidents to the staff handling the waste.

Use of Sacks.

1. When sacks for waste paper are received they must be filled and disposed of promptly as directed by the Stores Department. They must not be used for storage purposes nor kept more than seven days except as provided in the second paragraph of clause 4 under the heading "Daily Collection of Waste Paper."

If more sacks are received than are required for immediate use, the surplus must be returned at once to the Stores Department, Swindon, a delivery note stating the number of sacks returned being sent at the same time.

Use of
Depots

Every sack and bundle of books, etc., sent away must have a label attached giving the address of the consignee, the date sent, and the name of the sending station or depot.

Travelling stores vans must be used whenever possible.

London Offices
and Depots

8. The instructions relating to the collection and disposal of old books, forms and waste paper from London offices and depots are set out in Circular No. 3010 of 17th November, 1926.

NOTE.—Books and documents relating to accounts for the year 1913 should be returned for the present, as they comprise the first and only complete year of accounts rendered in accordance with the Accounts and Returns Act of 1911.

DISPOSAL OF OLD BOOKS AND PAPERS.

TO BE RETAINED PERMANENTLY.

Comparative Returns

D.G.M.'s Weekly or Periodical printed Circular.

Dock Rate Book

Fare Books

General Instructions relating to Merchandise Traffic.

General Railway Classification and Supplements.

Guard Books containing circulars, balance sheets, etc.

Handbook of Stations and Appendix.

Important correspondence where questions of Agreements, history, principle and other similar questions have been reported upon and settled. To be specially indexed and kept apart.

Instructions to Country Stations re London Traffic.

Irish Traffic General Instructions.

Merchandise Route Books.

Pier Head Arrivals & Sailings Book.

R.C.L. Coaching Arrangements Books.

Rules, Instructions to Railway Companies Staffs relating to Passenger Train Traffic and Merchandise Traffic.

Rate Books

Regulations, General Instructions, and Scales of Charges for Warehouse Rent, Wharfage, Demurrage, Labourage, etc.

Season Ticket Rate Books.

Special Arrangements Book.

Staff Records, and correspondence connected therewith.

Station Accounts Instruction Book and Supplements.

Statistical Books

Stemming Book

Summary of Coal shipped at each Appliance

Tables for ascertaining Railway Charges on Merchandise

Towns, Villages, etc., adjacent to and served by G.W.R., and Supplements

Other printed publications and important documents not specifically enumerated, in use in General Offices, etc.

Description	Period forms retained at present.	Period forms to be retained in future.
Income Tax Records	6 years	4 years
Carmen's Delivery Sheets	6 years	3 years
Consignment Notes	6 years	3 years
Rolling Stock Requisition	1 year	6 months

STATION INSTRUCTIONS.

DISPOSAL OF BOOKS AND PAPERS *Continued.*

GOODS DEPARTMENT FORMS, ETC

TO BE RETAINED ONE YEAR AFTER COMPLETION.

Abstract forms No. 51234
 Cartmen's Daily Record.
 Cartmen's Weight Book.
 Carting Agents' Account Return.
 Cheque and Note Books.
 Coal Order form 134.
 Collection Orders
 Inaccuracy Sheet Register.
 Invoice Pro Book.
 Loading and Unloading Ships (Provincial).

Ministry of Transport statistics.
 Paid on Certificates.
 Porters' Loading Book (Provincial).
 Repairs requisition book 206.
 Returned Goods Book.
 Rolling stock daily report 5781.
~~Rolling stock requisition book 5821~~
 Shipment Declaration.
 Staff Time Books.
 Wagon Labels from local and foreign stations.

TO BE RETAINED THREE YEARS AFTER COMPLETION.

Abstracts.
 Arrival Book
 Cartmen's Bonus Record (Carters' daily returns only).
 Cartage Loading Books.
 Cash and Ledger Transfer Summaries (copies).
 Cash Transfer Voucher Copies
 Clothing Requisition.
 Correspondence (ordinary station).
 Credit and debit transfer book 7743.
 Cash Account.
 Book
 Daily Statement of Outstandings
 Ledger Account List 1546.
 Not to Hand Book.
 P.T.F. Book.
 Paying-in Slips for Banks.
 Pickford's Account Forms (copies).
 Porters' Bonus Record (Carters' daily returns only).

Porters' Loading Book (London District).
 Railway statistics.
 Rebate Vouchers (copies).
 Remittance Book 277.
 Station Account List -1386A (copies).
 Station Book 165 (when not used as signature book).
 Station Truck Lists.
 Summary Book.
 Summary of Debits.
 Summary Delivery Sheets 5297.
 Summary Cash Book.
 Tissue Copying Books :—
 Abstracts and Summaries.
 Accounts.
 Unloading Book.
 Unloading records.
 Unpaid Wages Book.
 Wagon Repairs Book.
 Weekly Revenue Returns (copies).

TO BE RETAINED SIX YEARS AFTER COMPLETION.

Advice Notes.
 Advice Notes Register.
~~Cash Book~~
 Cash Book.
 Claims Register.
~~Coal Order form 134~~
~~Collection Orders~~
~~Invoice Pro Book~~
~~Loading and Unloading Ships (Provincial)~~
 General Account 1103.
 General Account Analysis.
~~General Account Book~~
~~General Account Record~~
 Inwards (Inwards and copies of Outwards).
 General Account Book.
 Deductions Book.

Letter Register.
 Number Takers' Books.
 Overcharge Book.
 Paid on Book.
 Postage Book.
 Rebate Register.
 Receipt Book.
 Short credit account register 1543A.
 Siding Rent and Demurrage Book.
 Station Account Book.
 Station Book 165 when used as signature book.
 Sundry Charges Book.
 Transfer Orders.
 Wagon Book.
 Warehouse Book.
 Weighing Book.

STATION INSTRUCTIONS.

DISPOSAL OF BOOKS AND PAPERS—*Continued.*

PASSENGER DEPARTMENT FORMS, ETC.

TO BE RETAINED ONE YEAR AFTER COMPLETION.

Brood Mares and Stallions Certificates.
 Carmen's Journals.
 Cheque Register.
 Delays to Train correspondence.
 Guards' Time Sheets.
 Guards' and Porters' Memo. Books.
 Horse Stock and Harness Book.
 Inaccuracy Sheet Register.

Passenger Guards' Train Book 750.
 P.L.A. Forms.
 Property Found Books.
 Receiving Book.
 Return Book.
 Stable Stores requisition.
 Staff Time Books.

TO BE RETAINED THREE YEARS AFTER COMPLETION.

Accounts.
 Passenger General Account; Form No. 874
 (copies).
 Parcels General Account; Form No. 109
 (copies).
 Blank Card Registers.
 Booking Clerks' Train Book.
~~Cash Book~~
 Cash Registers.
 Cash Remittance Slips.
 Cloak Room Book to-pay.
 Cloak Room Indemnity Book.
 Cloak Room Ticket Book.
 Clothing Requisition.
 Copying Book.
 Correspondence (ordinary station).
 Dockets, Record of
 Excess Pnds.
 General Summary for Overcharge Vouchers.
~~Harbour Book.~~

Milk Register.
 News Label Register.
 Paper Ticket Book.
 Parcels Account Book.
 Parcels O'S Book.
 Parcels Stamp Book.
 Parcels Transfer Book.
~~Passenger Excess Returns.~~
 Proof Book—Daily.
 Proof Book—Monthly.
 Revenue Returns.
 Season Ticket Registers.
 Station Account (O'S).
 Summary Cash Book.
 Ticket Collectors' pay book.
 Ticket Requisition Form.
~~Warehouse Rent Book.~~
 Warehouse Rent Book.

TO BE RETAINED SIX YEARS AFTER COMPLETION.

Carmen's Delivery Sheets.
 Claims Register.
 Consignment Notes.
~~Counter Sheets.~~
~~Credit Account Book.~~
 General Summary Book Budgets.
 Income Tax Records.
~~Inward Parcels Book.~~
 Letter Register.
~~Outwards Receiving Office Sheets.~~
 Outwards Receiving Office Sheets.

Paid on Book.
 Parcels Cartage.
 Parcels Cash Book.
~~Receipt Book.~~
 Receipt Book.
 Receipt Book Counterfoils.
 Receipt Pads.
~~Warehouse Rent Book.~~

DOCKS DEPARTMENT FORMS, ETC.

TO BE RETAINED ONE YEAR.

Berthingman's Daily Report.
 Cheque Registers.
 Coal Stocktaker's Handbook.
 Delays Return.
 Dock Pass (Admission to Dock Premises).
 Dock Pass (Vessels).
 Foreman's Daily Report.
 Note Books at Machine Houses.

Passes for taking Goods Off the Dock.
 Record of Orders.
 Special Services Book.
 Staff Time Book.
 Tonnage Book (Tippers).
 Traffic Order Form.
 Weighing Machine Daily Report.

Paybill Tissues	...		
Counter Sheets	...		
Credit Account Book	...		
General Summary Book (Ledger)	...	6 years	3 years
Inwards Parcels Book	...		
Outwards Parcels Book	...		
Outward Receiving Office Sheets	...		
Parcels Cart Bills	...		
Waybills	...	3 years	2 years
Unpaid Wages Book	...	3 years	1 year
Carted Luggage Book	...	3 years	13 months
Insurance Book	...	3 years	6 months
Train Register Book	...	1 year	
Rolling Stock Returns	...		

(G.A. 8.—5/41. A2/70376-58. LK1/6236.)

STATION INSTRUCTIONS.

DISPOSAL OF OLD BOOKS AND PAPERS—*Continued.*

TO BE RETAINED THREE YEARS.

Accounts.
Berthing Note.
Cargo Labour Order Book.
Clothing Requisition.
Coal Orders.
Coal Passing various Junctions.
Coal Shipped U. E. Book.
Colliery Permits & Invoices (Uncleared Book).
Correspondence (ordinary).
Crane Sheet.
Dock Toll Collector's Daily Return.
Empty Coal Sheets.
Forwarding Instructions (Empty Wagons).
Hirers Shunting Orders (In and Out).
Journal of Domestic Engines.
Landing Orders.

Licenses, Ships, Canvassers (Counterfoils).
Marine Controllers' Daily Record.
Particulars of Current Overcharge Vouchers.
Record of Mixing Forms.
Record of Train and Traffic Advice.
Removal of Vessels Form.
Return of Hirers Wagons shunted.
Shipping Instructions Form.
Stemming List.
Stemming Note.
Train Advice Form.
Tugboat Order Book.
Unpaid Wages Book.
Waterman's Overtime Book.
Water Order Book.
Working List.

TO BE RETAINED SIX YEARS.

Accounting Books.
Arrivals and Sallings Forms.
Berthing & Working Book & Lists.
Berthing and Working Orders.
Clearance Requests.
Coal and Coke Traffic Transferred form.
Coal Shipments Day Book.
Coal Wharfage Book.
Commercial Dry Dock Charges Book.
Crane Order form.
Credit Ships.
Daily Towage Return.
Docks Daily Traffic statement.
Dry Dock Stemming Diary.
Electric Lighting Order.
Graving Dock Stemming Book.
Hirers Empty wagons standing on Sidings.
Income Tax Records.
Letter Register.
Manifest forms (Outwards and Inwards).
Masters' Reports.
Mates' Receipts.

Night Order Book.
Orders for Unshipping Hatchbeams.
Paybill Tissues.
Postage Book.
Receipt Book.
Record of Arrivals & Sallings Book.
Register of Licenses.
Retarded Wagons Permits.
Royalties Book.
Screening Invoices.
Shipment Certificate.
Shipping List.
Ship Store Journal.
Tally Books.
Time Register of Vessels Book.
Tipping Invoices.
Coal Shipping Appliance Time Sheets.
Vessels Changing Docks.
Vessels Expected Book.
Waterman's Daily Return.
Water Receipt Book.
Weighing Machine Permits.

The list, so far as the books and documents in use at Stations generally are concerned, can be regarded as comprehensive, but there may be special forms in use at some stations, in regard to which no difficulty need arise as to the classification under which they should come.

Having regard to the varying periods for the retention of books and papers, it will be necessary for a proper system of filing to be instituted at each station in order to avoid confusion and prevent unnecessary handling.

FORMS TO BE CAREFULLY PACKED FOR RE-USE AND FORWARDED TO SWINDON (SEE CLAUSE 5).

Abstracts	..	..	..	No. 1109
Abstracts	..	..	..	No. 1110
Account forms	..	..	..	No. 961
Clothing Requisition	..	..	..	No. 2635
Dockets, Records of	..	..	..	No. 5004
General Account Analysis	..	..	..	No. 3744

Guard's Time Sheet	..	..	No. 11
Half Yearly Statement	..	..	No. 347
Ledger Account	..	..	No. 1150
Porter's Bonus Record	..	..	No. 3731
Time Table Notices and Posters,			

NOTE—The above periods to be observed by Divisional and District Officers as far as comparable books and papers are retained by them.

STATION INSTRUCTIONS.

LEGAL AND OTHER NOTICES AT STATIONS.

The following Notices must be exhibited at each Passenger Station :—

Legal Notices.	BY LAWS	NOTICE
	EXPLOSIVES	"
	INSURANCE	"
	PENALTIES	"
	FELLS	"
	SMOKING	"
	EXAMINATION OF CHANGE NOTICE.	

At Goods stations which are altogether separate from the Passenger stations, the legal Notices must be exhibited. On the Monmouthshire Section of the Line Toll Notices must be displayed in addition to the foregoing.

The Divisional Superintendent District Traffic Manager and District Goods Manager must be notified when the notices become dilapidated and he will arrange for new copies to be supplied.

In no circumstances are old Notices to be taken down until new ones are received.

WINDING AND REGULATING CLOCKS

1. The winding (and as far as possible the regulating) of all the clocks at the stations must be done by the Station Master or by some person appointed by the Station Master, and it is to be seen that the clocks are wound, either daily or weekly as may be required, and that the correct Greenwich time is shown.

2. Greenwich time will be transmitted daily from Paddington to all stations at which a telegraph instrument is fixed.

3. Signals in London and Paddington will receive the signal direct from that station, and the telegraph office at the other end will receive the signal direct from the London end. Such clocks as are continuously connected with the telegraph office at Paddington will receive the signal direct from that station, and so on, till every Station having telegraph communication is reached at the same time.

4. At places where "Time" has to be transmitted by block telegraph or telephone bell, the signal must be made by giving 18 beats in the following manner :—8 pause 5 pause 5.

5. Guards must set their watches daily by the clock of the first station having telegraphic communication at which they stop, and after that time they must set the time of all stations having telegraphic communication by the time of that station as far as possible. Station Masters are responsible for seeing that such is done.

6. The clocks at outlying signal boxes which are not on through single needle circuits are to be set by the Station Master or by some person appointed by the Station Master, and the Station Master or person appointed by the Station Master must be supplied with the proper time by telegraph or otherwise, so as to ensure their having the correct time.

7. The winding and regulating of the clocks, whether done by the Station Master or by some person appointed by the Station Master, is to be performed by the same person. The Station Master must do it himself if he is capable of doing so, or if he is not, he must see that it is done by a competent person to whom he has entrusted the duty, and see that it is properly done.

8. Defective clocks and time recorders should be reported to the Signal Engineer at Reading who will arrange for attention to be given.

9. Defective movable clocks should be sent to the Signal Engineer at Reading for attention in accordance with the instructions issued.

10. Defective time pieces and watches should be sent to the Divisional Officer concerned who will despatch them to the Signal Engineer at Reading for attention.

For general instructions regarding the maintenance of clocks, etc., see General Manager's Circular No. 372—1 10/30.

PROTECTION OF WORKSHOP VANS

The following instructions are exhibited in Workshop Vans for the protection of the men engaged at work therein :

"The man in charge of this van must immediately on arrival at a station inform the Station Master (or other person then in charge of the station) of the presence of the van. As soon as the van has been placed in a siding, and when it leaves, the man in charge must exhibit a red flag during daylight and a lamp with a square glass shewing a red light after dark, if in a loop siding, at each end of the van ; if in a dead end siding, at the opposite end of the van to the stop-block. These flags and lamps must be fixed on the side of the van furthest from the running lines, so as not to interfere with passing trains."

Station Masters or other persons in charge when advised of the presence of a workshop van should arrange, if possible, to have it placed in a dead end siding, towards the stop-block end, and warn their shunters not to shunt vehicles against it.

In the case of Messrs Pooley's workshop vans, the Station Master or person in charge of the station will be held responsible for seeing that red flags or lamps are exhibited except at those places where the van has been placed on a siding which is protected by wheelstops or by the points leading to such siding being clipped in such a manner as to protect the van.

JAMES MILNE,
General Manager.

November 1st, 1945.

(G.A. 7.—3 40. LK1/5436 14)

GREAT WESTERN RAILWAY

Circular No. 3714.

(Amending Circular No. 3680.)

General Manager's Office

Paddington Station,

November 1st, 1947.

The instructions shown on pages 304 to 308 of the General Appendix to the Rule Book dated July 1936 in regard to the instructions to be observed for the protection of Workshop Vans, Carriage Cleaners, Gasmen, etc., are revised as follows :

(A) REGULATIONS FOR THE PROTECTION OF CARRIAGE CLEANERS, GASMEN, LAMPMEN AND OTHERS WORKING ON COACHING STOCK.

Where an engine is not attached to the vehicles.

1. Before any of the above-named men commence work :

(a) Upon the outside of vehicles on any line or siding on which it is possible for other vehicles to be shunted against them ;

(b) Which necessitates the use of steps or ladders inside the vehicles or involves the men placing themselves in such a position that they might lose their balance if the vehicles are moved,

a red flag by day, or a red light during darkness, fog, or falling snow, must be exhibited at the end of the last vehicle nearest the direction from which vehicles might be shunted against those on which the men are at work. If it is possible for vehicles to be shunted against both ends of the vehicles on which the men are at work, the same precautions must be taken at both ends.

When the vehicle or vehicles stand wholly inside a shed, the flag or light must be exhibited at the entrance or entrances to the shed on the line upon which the vehicles are standing.

2. If the vehicles are standing on a line adjoining a running line, the red flag or red light must be exhibited on the side farthest away from the running line. When the vehicles are standing at a platform the flag or light must be placed on the platform side in such a position as to be plainly visible therefrom.

3. Before commencing work on the vehicles each man concerned is responsible for seeing that he is protected by a red flag or a red light in accordance with the foregoing Regulations, but should more than one man or set of men be separately at work on the same vehicles, or on the same line or siding, each man or set of men must be separately protected by additional red flags or lights. Each man working alone will be held responsible for carrying out these Regulations. Where a set of men is engaged one of them must be made responsible for carrying out these Regulations.

4. The man responsible for carrying out Regulation 3 must, before work is commenced, also satisfy himself that no shunting operations are in progress affecting the vehicles on which work is to be done.

5. When vehicles in a siding adjoining a running line, or on a running line adjoining another running line, are being cleaned, gassed or watered, a good lookout must be kept and care must be taken to see that doors are not left projecting on the running line side. Planks, steps, or long-handled brushes must not be used on the side of vehicles next to a running line except when authorised by the Chief Mechanical Engineer and Superintendent of the Line.

Where an engine is attached to the vehicles.

6. Before commencing the work referred to in (a) and (b) of Regulation 1, or supplying gas or water to a train of vehicles, the man concerned must place a red flag by day, or a red light during darkness, fog or falling snow, on the side of one of the vehicles for the guidance of the Traffic Department Staff and Enginemen. In the case of a train standing at a platform, the flag or light must be placed on the platform side in such a position as to be plainly visible therefrom.

7. Passenger trains or vehicles must not be moved whilst men are at work on the top of the vehicles.

General Instructions.

8. The red flag or red light exhibited for the protection of the men must not be removed until the work has been completed, or has been suspended to admit of the vehicles being moved, and the man before removing it must satisfy himself that all the men concerned are clear. (See also Regulation 3.)

9. Vehicles protected by the red flag or red light must not be moved, nor must others be shunted against them except as provided in the second paragraph of Regulation 1. Persons responsible for starting trains must be careful to see that no red flag or red light is exhibited before giving the signal for the train to start.

1. Enginemen and Shunters are particularly warned to satisfy themselves when approaching, and before coming into contact with, vehicles standing on platform and other lines, or sidings, and before backing on to or attaching or detaching vehicles to or from trains at platforms, that no red flag or red light is exhibited for protection purposes in accordance with these Regulations.

Should, however, it be necessary to attach or detach vehicles or to change engines after the red flag or red light has been placed on the platform side of a through train at a passenger station this must be done under the supervision of the person responsible for starting the train but great care must be taken to see that the train is not moved until all the men concerned have ceased work and are clear of the train.

Shunters and others must keep a good lookout when shunting on lines and sidings adjacent to those occupied by vehicles on which men are at work.

12. Carriage Cleaners and others working on coaching stock must not pass under, over, or between the buffers of vehicles, nor between the stop block and the nearest vehicle, when less than a carriage length apart. If necessary, men may pass through a van or third class compartment when the vehicle is stationary but must take care to close and fasten the doors after them.

13. Carriage Cleaners and others working on coaching stock must not stand on the lines between vehicles or between a vehicle and the stop block unless they are properly protected in accordance with the foregoing Regulations.

14. Special attention is directed to Rule 11 in the Rule Book.

Note. Should it be necessary for work to be done underneath a vehicle the provisions of Regulation 10, relative to the protection of Brake Fitters, etc., must be observed.

(B) REGULATIONS FOR THE PROTECTION OF BRAKE FITTERS, LIFTERS, REPAIRERS AND OTHERS WORKING ON CARRIAGE OR WAGON STOCK.

These Regulations must also be observed by the employees of Private Carriage or Wagon Repairing Firms

In repairing shop sidings, and sidings specially set apart for the purpose of carrying out repairs or other work on carriage or wagon stock, where protection is afforded against shunting by means of padlock and key.

1. During the time that men are at work in the sidings referred to above, the points leading to such sidings must be kept padlocked so as to protect the men, and the key of the padlock must be held by the repair staff who will be responsible for securing and releasing the points.

2. Before any work is commenced or resumed on such a siding it is the duty of the workman or where more than one man is engaged of the man in charge, to examine the points giving access to the siding and satisfy himself that they are securely padlocked in such a manner that a shunt cannot be made into the siding or which they intend to work.

3. Before the points of such sidings are unlocked for shunting purposes, or to enable vehicles to be placed in or removed from the sidings, it must be ascertained whether any men are working on vehicles in the sidings, and the padlock must not be taken off until such men have been warned to place themselves in a position of safety, nor must work be resumed by them until the points have again been padlocked.

4. If Railway Company's staff are employed in the siding they will be responsible for carrying out these Regulations. If no Railway Company's staff are employed the duties must be carried out by the staff of the private firm or firms concerned.

5. When the siding is required for shunting purposes or on the termination of all repair work, the man holding the key of the padlock must hand it to the person in charge of the shunting. But it is not to be given until the man holding the key has satisfied himself that there are no men at work in the sidings and the work on the vehicles has reached a stage when shunting can safely be permitted.

In sidings and on lines other than those referred to above.

6. Before any work is commenced it is the duty of the workman or where more than one man is engaged of the man in charge, to go to the Station Master, Inspector, or other person in charge of the line or sidings, advise him what work is required to be done and obtain his permission for the work to be carried out.

7. A red flag by day or a red light during darkness, fog, or falling snow, must be exhibited at the end of the last vehicle nearest the direction from which vehicles might be shunted against those on which the men are at work. If it is possible for vehicles to be shunted against both ends of the vehicle on which the men are at work, the same precautions must be taken at both ends.

8. If the vehicle is standing on a line adjoining a running line the red flag or red light must be exhibited on the side farthest away from the running line. When the vehicle is standing at a platform the flag or light must be placed on the platform side in such a position as to be plainly visible therefrom.

9. Before commencing work on the vehicle each man concerned is responsible for seeing that he is protected by a red flag or a red light in accordance with the foregoing Regulations, but should more than one man or set of men be separately at work on the same line or siding, each man or set of men must be separately protected by additional red flags or lights.

10. The wheels of the vehicle upon which work is to be carried out must be secured by sprags or scotches and if other vehicles are standing on the same line or siding, one or two brakes must be applied on the vehicles both in front and in rear of the one upon which work is to be carried out, or the wheels of such vehicles must also be secured by sprags or scotches.

11. Before examination or other work is commenced underneath a vehicle attached to an engine, the Driver, and Guard or Shunter, as well as the Station Master, Inspector or other person in charge of the line or sidings, must be advised, and instead of the flag or light being exhibited as prescribed in Regulation 7, a man with a red hand signal must stand in the best possible position and keep a good lookout for the protection of the workmen.

When examination or other work is required to be carried out underneath a vehicle and there is not an engine attached, the instructions in Regulations 5, 6, 7, 8, 9, 11, 12 and 13 must be observed.

12. The red flag or red light exhibited for the protection of workmen must be removed only under the instructions of the man in charge of the work, after the work has been completed or has been suspended to admit of the vehicle or vehicles being moved. The person in charge of the shunting must be duly advised. Care must be taken to see that all concerned are clear and all scotches and sprags have been taken away before the flag or light is removed.

13. Each man working alone will be held responsible for carrying out these Regulations. Where more than one man is engaged one of them must be made responsible for carrying out these Regulations.

14. Whilst the red flag, or red light is exhibited, the vehicles so protected must not be moved, nor must others be shunted against them.

General

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General Instructions.

14. Where there are repairing shop sidings, or where other sidings are set apart for the purpose of carrying out repairs, etc., the work on vehicles must, as far as practicable, be carried out in such sidings.

15. Shunters and others must keep a good lookout when shunting on lines and sidings adjacent to those occupied by vehicles on which men are at work, and must, before commencing such shunting, warn the men engaged on the vehicles what they are about to do.

16. The special attention of the Railway Company's employees is directed to Rule 11 in the Rule Book.

17. Except on Sidings or running lines which are protected in accordance with the provisions of clauses 1 to 6 or 10, employees are forbidden to go between the buffers of vehicles or between stop blocks and the nearest vehicle, when less than 50 feet apart, without first satisfying themselves that none of the vehicles is about to be moved by engine, capstan, horse or other power, and that no shunting is going on upon the lines which they are about to cross.

18. Each Private Carriage or Wagon Repairing Firm will supply to their own staff the red flags and lamps of a pattern approved by the Railway Company concerned.

INSTRUCTIONS TO BE OBSERVED FOR THE PROTECTION OF MEN REPAIRING OR PAINTING STOP BLOCKS.

Before commencing to repair or paint a Stop Block, the man concerned must obtain the permission of the Station Master, Inspector or person in charge, who, if permission can be given, must arrange to place the line or siding temporarily out of use, during which time the points must be secured by clip and padlock. If the points are worked from a signal box, the Station Master, Inspector or person in charge must inform the Signaller of what is about to be done.

When it is not possible to place the line or siding out of use, the Station Master, Inspector or person in charge must arrange for a distance of not less than 50 yards to be maintained between the Stop Block and the nearest vehicle on the line or siding during the progress of the work. The man engaged in the work must, before commencing, exhibit a red flag at the end of the vehicles nearest to the Stop Block in a position where it can be most readily observed by Shunters, Locomotive men and others concerned, engaged in the movement of traffic, and must fix two detonators on the rail close to that vehicle in order that immediate warning will be given in the event of a movement towards the Stop Block.

PROTECTION OF WORKSHOP VANS.

The following instructions are exhibited in Workshop Vans for the protection of the men engaged at work therein:

"The man in charge of this van must, immediately on arrival at a station, inform the Station Master (or other person then in charge of the station) of the presence of the van. As soon as the van has been placed in a siding, and until it leaves, the man in charge must exhibit a red flag during daylight and a lamp with a square glass shewing a red light after dark, if in a loop siding, at each end of the van, if in a dead-end siding, at the opposite end of the van to the stop-block. These flags and lamps must be fixed on the side of the van furthest from the running lines, so as not to interfere with passing trains."

Station Masters or other persons in charge when advised of the presence of a workshop van should arrange, if possible, to have it placed in a dead-end siding, towards the stop-block end, and warn Shunters not to shunt vehicles against it.

In the case of Messrs. Pooley's workshop vans, the Station Master or person in charge of the station will be held responsible for seeing that red flags or lamps are exhibited, except at those places where the van has been placed on a siding which is protected by wicketstops or by the points leading to such siding being clipped in such a manner as to protect the van.

November 1st, 1947.

JAMES MILNE,
General Manager.

(Each man receiving a copy of this Circular is required to carefully read and note the contents, and keep it for reference.)

This Circular amends Circular No. 3630, dated 1st November 1945.

20,000--L.T.X.T. 9/47

6,000 (Reprint) 12/49.

(This form must be detached and forwarded to Head of Department.

Station

Received copy of General Manager's Circular No. 374, dated November 1st, 1947, respecting the Protection of Carriage Cleaners, etc.

To.....

STATION INSTRUCTIONS.

(A) REGULATIONS FOR THE PROTECTION OF CARRIAGE CLEANERS, GASMEN, LAMPMEN
AND OTHERS WORKING ON COACHING STOCK.

Where an engine is not attached to the vehicles.

1. Before any of the above-named men commence work:—

(a) Upon the outside of vehicles on any lane or sublan on which it is possible for other vehicles to be shunted against them.

(b) Which necessitates the use of steps or ladders inside the vehicles or involves the men placing themselves in such a position that they might lose their balance if the vehicles are moved.

a red flag by day, or a red light during darkness, fog, or falling snow, must be exhibited at the end of the last vehicle nearest the direction from which vehicles might be shunted against those on which the men are at work. If it is possible for vehicles to be shunted against both ends of the vehicles on which the men are at work, the same precaution must be taken at both ends.

When the vehicle is a vehicle that which is a shed, the flag or light must be exhibited at the entrance of the shed to the shed on the line upon which the vehicle is standing.

2. If the vehicles are standing on a line adjoining a running line the red flag or red light must be exhibited on the side farthest away from the running line. When the vehicles are standing at a platform the flag or light must be placed on the platform side in such a position as to be plainly visible therefrom.

3. Before commencing work on the vehicles each man concerned is responsible for seeing that he is protected by a red flag or a red light in accordance with the foregoing Regulations, but should more than one man or set of men be separately at work on the same vehicles, or on the same line or siding, each man or set of men must be separately protected by additional red flags or lights. Each man working alone will be held responsible for carrying out these Regulations. Where a set of men is engaged one of them must be made responsible for carrying out these Regulations.

4. The man responsible for carrying out Regulation 3 must, before work is commenced, also satisfy himself that the starting operators are in possession of the necessary training for carrying out the work and that the vehicles on which work is to be done.

5. When vehicles are stopped, and a running line or a line is being laid, the running line are being laid down, or vehicles are being moved, it must be kept in mind to be taken to see that doors are not left projecting on the running line side. Planks, steps, or long handled brushes must not be used on the side of vehicles next to a running line except when authorized by the Chief Mechanical Engineer and Superintendent of the Line.

Where an engine is attached to the vehicles.

B. Before entering the work zone from a street of Randomly Escaping Gas, rather than a main road, the driver should take place at the side of the road, away from the main road, fog or snow, with side of the field, and half the distance of the road. Before entering the platform side in such a position as to be plainly visible therefrom.

7. Passenger trains or vehicles must not be moved whilst men are at work on the top of the vehicles

General Instructions.

8. The red flag or red light exhibited for the protection of the men must not be removed until the work has been completed, or has been suspended to admit of the vessels being moved, and the man before removing it must satisfy himself that all the men concerned are clear. (See also Regulation 3.)

9. Velocities provided by the red flag or red light must not be moved, nor must others be substituted against them except as provided in the second paragraph of Rule 40.10. Two seconds must elapse for starting trains must be allowed to see that no red flag or red light is exhibited before giving the signal for the train to start.

1) Ensigns and Shanters are particularly warned to satisfy themselves when approaching, and before coming into contact with vessels standing on platform or the pier or wharves, and before backing on to or attaching red sailing vessels to or from the top of the mast that no red flag or red lights exhibit, for protection purposes, a or have with the flag at the

Should, however, it be necessary to attach or detach vehicles or to change engines after the red flag or red light has been placed on the platform side of a through train at a passenger station, this must be done under the supervision of the person responsible for starting the train but great care must be taken to see that the train is not moved until all the men concerned have ceased work and are clear of the train.

11 Shorters and others must keep a good look out when shunting, on lines and sidings adjacent to those occupied by vehicles on which men are at work.

GREAT WESTERN RAILWAY

Circular No. 3680.

(Amending Circular No. 3405.)

General Manager's Office,

Paddington Station,

November 1st, 1945.

The instructions shewn on pages 304 to 308 of the General Appendix to the Rule Book dated July 1936 in regard to the instructions to be observed for the protection of Workshop Vans, Carriage Cleaners, Gasmen, etc., are revised as follows:

(A) REGULATIONS FOR THE PROTECTION OF CARRIAGE CLEANERS, GASMEN, LAMPMEN AND OTHERS WORKING ON COACHING STOCK

1. At work on sidings referred to above, the following Regulations must be observed by the repair staff, who will be responsible for securing and releasing the points.

2. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

3. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

4. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

5. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

6. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

7. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

8. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

9. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

10. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

11. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

12. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

13. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

14. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

15. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

16. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

17. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

18. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

19. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

20. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

21. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

22. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

23. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

24. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

25. Before commencing work on a vehicle, the man concerned must satisfy himself that the vehicle is properly secured and that the points are set in the position in which they intend to work.

12. Carriage Cleaners and others working on coaching stock must not pass under cover of vehicles, nor between the stop block and the nearest vehicle, when less than a carriage or men may pass through a van or third class compartment when the vehicle is stationary but must and fasten the doors after them.

13. Carriage Cleaners and others working on coaching stock must not stand on the lines between a vehicle and the stop block unless they are properly protected in accordance with the foregoing Regulations.

14. Special attention is directed to Rule 11 in the Rule Book.

Note.— Should it be necessary for work to be done underneath a vehicle the provisions of Regulation 10, the protection of Brake Fitters, etc., must be observed.

(B) REGULATIONS FOR THE PROTECTION OF BRAKE FITTERS, LIFTERS, REPAIRERS AND OTHERS WORKING ON CARRIAGE OR WAGON STOCK

These Regulations must also be observed by the employees of Private Carriage or Wagon Repairing Firms.

In repairing shop sidings, and sidings specially set apart for the purpose of carrying out repairs or other work on carriage or wagon stock, where protection is afforded against shunting by means of padlock and key.

1. The points leading to the sidings referred to above must be kept padlocked in such a manner as to protect the men working in the sidings.

2. Before any work is commenced on such a siding it is the duty of the workman or where more than one man is engaged of the man in charge, to examine the points giving access to the siding and satisfy himself that they are securely padlocked in such a manner that a shunt cannot be made into the siding on which they intend to work.

3. Should the points be found not properly padlocked so as to afford the necessary protection, the Inspector or other person in charge of the shunting must be applied to and the points padlocked before the work is commenced.

4. Before the points of such sidings are unlocked for shunting purposes or to enable vehicles to be placed in or removed from the sidings it must be ascertained whether any men are working on vehicles in the sidings, and the padlock must not be taken off until such men have been warned to place themselves in a position of safety, nor must work be resumed by them until the points have again been padlocked.

In sidings and on lines other than those referred to above.

5. Before any work is commenced it is the duty of the workman or where more than one man is engaged of the man in charge, to get the Station Master, Inspector or other person in charge of the line or sidings, advise him what work is required to be done and obtain his permission for the work to be carried out.

6. A red flag by day or a red light during darkness, fog or falling snow, must be exhibited at the end of the last vehicle nearest the direction from which vehicles might be shunted against those on which the men are at work. If it is possible for vehicles to be shunted against both ends of the vehicle on which the men are at work, the same precautions must be taken at both ends.

7. If the vehicle is standing on a line adjoining a running line the red flag or red light must be exhibited on the side farthest away from the running line. When the vehicle is standing at a platform the flag or light must be placed on the platform side in such a position as to be plainly visible therefrom.

8. Before commencing work on the vehicle each man concerned is responsible for seeing that he is protected by a red flag or a red light in accordance with the foregoing Regulations but should more than one man or set of men be separately at work on the same line or siding, each man or set of men must be separately protected by additional red flags or lights.

9. The wheels of the vehicle upon which work is to be carried out must be secured by sprags or scotches and if other vehicles are standing on the same line or siding one or two chocks must be applied on the vehicles both in front and in rear of the one upon which work is to be carried out, or the wheels of such vehicles must also be secured by sprags or scotches.

10. Before examination or other work is commenced underneath a vehicle attached to an engine, the Driver, and Guard or Shunter, as well as the Station Master, Inspector or other person in charge of the line or sidings, must be advised and instead of the flag or light being exhibited as prescribed in Regulation 6, a man with a red hand signal must stand in the best possible position and keep a good lookout for the protection of the workmen.

When examination or other work is required to be carried out underneath a vehicle and there is not an engine attached, the instructions in Regulations 5, 6, 7, 8, 9, 11, 12 and 13 must be observed.

11. The red flag or red light exhibited for the protection of workmen must be removed only under the instructions of the man in charge of the work after the work has been completed or has been suspended to admit of the vehicle or vehicles being moved. The person in charge of the shunting must be duly advised. Care must be taken to see that all concerned are clear and all scotches and sprags have been taken away before the flag or light is removed.

12. Each man working alone will be held responsible for carrying out these Regulations. Where more than one man is engaged one of them must be made responsible for carrying out these Regulations.

13. Whilst the red flag, or red light is exhibited, the vehicles so protected must not be moved, nor must others be shunted against them.

General Instructions.

14. Where there are repairing shop sidings, or where other sidings are set apart for the purpose of carrying out repairs, etc., the work on vehicles must, as far as practicable, be carried out in such sidings.

15. Shunters and others must keep a good lookout when shunting on lines and sidings adjacent to those occupied by vehicles on which men are at work, and must, before commencing such shunting, warn the men engaged on the vehicles what they are about to do.

b. The special attention of the Railway Company's employees is directed to Rule 11 in the Rule Book

17. Except on Sidings or running lines which are protected in accordance with the provisions of clauses 1 to 4 or 10, employees are forbidden to go between the buffers of vehicles or between stop blocks and the nearest vehicle, when less than 50 feet apart, without first satisfying themselves that none of the vehicles is about to be moved by engine, capstan, horse or other power and that no shunting is going on upon the lines which they are about to cross

18. Each Private Carriage or Wagon Repairing Firm will supply to their own staff the red flags and lamps of a pattern approved by the Railway Company concerned.

INSTRUCTIONS TO BE OBSERVED FOR THE PROTECTION OF MEN REPAIRING OR PAINTING STOP BLOCKS

Before commencing to repair or paint a Stop Block, the man concerned must obtain the permission of the Station Master, Inspector or person in charge, who if permission can be given, must arrange to place the line or siding temporarily out of use during which time the points must be secured by clip and padlock. If the points are worked from a signal box, the Station Master, Inspector or person in charge must inform the Signaller of what is about to be done.

When it is not possible to place the line or siding out of use, the Station Master, Inspector or person in charge must arrange for a distance of not less than 50 yards to be maintained between the Stop Block and the nearest vehicle on the line or siding during the progress of the work. The man engaged in the work must, before commencing, exhibit a red flag at the end of the vehicle nearest to the Stop Block in a position where it can be most readily observed by Shunters, Enginemen and others concerned engaged in the movement of traffic, and must fix two detonators on the rail close to that vehicle in order that immediate warning will be given in the event of a movement towards the Stop Block.

STATION INSTRUCTIONS.

REGULATIONS FOR THE PROTECTION OF CARRIAGE CLEANERS, ETC. *Continued.*

12. Carriage Cleaners and others working on coaching stock must not pass under, over, or between the buffers of vehicles, nor between the stop blocks and the nearest vehicle, and must not stand too long apart. If necessary men may pass through a van or truck compartment when the vehicle is stationary but must take care to close and fasten the doors after them.

13. Carriage Cleaners and others working on coaching stock must not stand on the lines between vehicles or between a vehicle and the stop block unless they are properly protected in a corridor with the foregoing Regulations.

14. Special attention is directed to Rule 11 in the Rule Book.

NOTE *Should the necessary care be taken underneath a vehicle the provisions of Regulation 10, relating to the protection of brake Fitters, etc., must be observed.*

B REGULATIONS FOR THE PROTECTION OF BRAKE FITTERS, LIFTERS, REPAIRERS, AND OTHERS WORKING ON CARRIAGE OR WAGON STOCK.

These Regulations must be strictly observed by the employees of Private Carriage or Wagon Repairing Firms

In repairing shop sidings, and sidings specially set apart for the purpose of carrying out repairs or other work on carriage or wagon stock, where protection is afforded against shunting by means of padlock and key.

1. The points leading to the sidings referred to above must be kept padlocked in such a manner as to protect the men working in the sidings.

2. Before any work is commenced in such a siding it is the duty of the workman, or where more than one man is engaged of the man in charge, to examine the points giving access to the siding and satisfy himself that they are properly padlocked in such a manner that a shunt cannot be made into the siding on which they intend to work.

3. Should the points be found not properly padlocked so as to afford the necessary protection, the Inspector or other person in charge of the shunting must be applied to and the points padlocked before the work is commenced.

4. Should the points be found locked for shunting purposes, or to enable vehicles to be moved, the workman must not touch the points, and must not work any vehicle in the sidings, and the padlock must not be taken off until such men have been warned to place themselves in a position of safety, nor must work be resumed by them until the points have again been padlocked.

In sidings and on lines other than those referred to above.

5. Before any work is commenced it is the duty of the workman, or where more than one man is engaged of the man in charge, to go to the Station Master, Inspector, or other person in charge of the line or sidings, and obtain permission for the work to be carried out.

6. A red flag by day or a red light during darkness, fog or falling snow, must be exhibited at the end of the siding nearest the work, and from which vehicles must be shunted against the siding on which the men are at work. If it is possible for vehicles to be shunted against both ends of the vehicle on which the men are at work, the same precautions must be taken at both ends.

7. If the vehicle is standing on a line adjoining a running line the red flag or red light must be exhibited at the safest place away from the running line. When the vehicle is standing at a platform the flag or light must be placed on the platform in such a position as to be plainly visible therefrom.

8. Before commencing work on the vehicle each man concerned is responsible for seeing that he is protected by a red flag or light in accordance with the foregoing Regulations, but should more than one man or set of men be separately at work on the same line or siding each man or set of men must be separately protected by additional red flags or lights.

9. The wheels of the vehicle upon which work is to be carried out must be secured by sprags or scotches. If the vehicle is standing on the same line or siding, one or two brakes must be applied, and in front and in rear of the one upon which work is to be carried out, or the wheels of such vehicles must also be secured by sprags or scotches.

10. Before any work is commenced underneath a vehicle attached to an engine, the Inspector, or other person in charge, as well as the Station Master, Inspector or other person in charge of the line or sidings, must be applied to and instead of the flag or light being exhibited as prescribed in Regulation 6, a person must stand in the best possible position and keep a good lookout for the protection of the workman.

When examination or other work is required to be carried out underneath a vehicle and there is not an engine attached to it, Regulations 5, 6, 7, 8, 9, 11, 12 and 13 must be observed.

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must also be stated, as the methods of disinfecting with the new plant at Swindon vary accordingly.

47 Except on sidings or running lines which are protected in accordance with the provisions of
rules 1 to 6 or 10, employees are forbidden to go between the buffers of vehicles or between stop
blocks and the nearest vehicle, when less than 10 feet apart, without first satisfying themselves that
none of the vehicles is about to be moved by engine, capstan, horse or other power, and that no
shunting is going on upon the lines which they are about to cross.

(G.A.S.—12.37 G.M.Min.2726)

Red-banded lamps must not be used for ordinary traffic and any of these lamps on hand after the passing of the special holiday traffic must be promptly returned to Swindon Stores

(G.A. 7.—3/40. LK1/2639,23.)

Except in cases of Tubercular patients, in regard to which refer to instructions below, when a compartment is reserved for an infectious party, other passengers must not be permitted to travel in the remaining compartments in the same coach, unless it is non-corridor. All the compartments in a corridor coach must be labelled and locked, and Guards must take steps to ensure that the unoccupied compartments remain locked and are not opened up to passengers. When the journey involves changes en route a through reserved coach should be arranged if practicable.

The whole coach or coaches used are to be disinfected after use, unless non-corridor, if non-corridor only the compartments actually used are to be dealt with.

For Tubercular infectious patients the exclusive use of a coach is not necessary; the reservation of one compartment only is sufficient. In such cases the remaining compartments in the coach are to be available for other passengers. When the journey involves changes en route, a through coach containing the reserved compartment, should be arranged if practicable. (When in the notification to the Company or the journey of a Tubercular patient requiring reservation of a compartment, a competent Medical Authority states in writing there is no risk to the public on account of infection and no necessity for disinfection, no disinfection arrangements need be made. It will be necessary, however, to allow a current of air to be passed through the compartment for some hours before being used again.)

In cases of Whooping Cough providing the Medical Officer responsible does not consider disinfection necessary, it will be sufficient if the compartment is thoroughly cleaned after use, and a current of air is passed through the compartment for some hours, before being used again, no disinfection fee is chargeable in such cases.

Platform staff at entraining and detraining stations to ensure that the Guard is aware of the circumstances; in cases where Guards change en route the Guard boxing off to ensure that the relieving Guard is advised of the reservation.

In no circumstances must labels be removed, or doors unlocked, by unauthorised persons, either in the case of the compartment actually infected or the remaining compartments retained in the coach until disinfection has been carried out.

After detraining the patient the coach passing to the disinfection point should be clearly labelled throughout "FOR DISINFECTION". When only a compartment is to be disinfected the compartment concerned must be clearly labelled on both sides (red or other distinctive coloured ink to be used) "FOR DISINFECTION". The onus of carrying this out must rest upon the Station Master at the detraining point, and if suitable labels are not on hand steps must be taken to secure or improvise.

The third paragraph on page 309 of the existing instructions to be amended to read:

The following are the charges for the disinfection of coaching vehicles:

*Saloon or complete corridor coach £2 8s. 10d.

*Compartment in corridor or non corridor coach 1fs. 3d.

Vehicle containing the corpse of a person deceased as a result of an infectious or contagious disease £1 12s. 7d.

The charge to be credited to the Company performing the disinfection.

(*—Where one or more changes of train are necessary and the Saloon or coach does not work through, the above charges apply to each train. No reservation fee is charged.)

(G.A. 18. 11/47. T.87530. M.)

STATION INSTRUCTIONS.

COACHES REQUIRING TO BE DISINFECTED—*Continued.*

When application is made to convey the body of a person who has died from an infectious disease the authority of the Superintendent of the Line must be obtained. No such application must, however, be entertained until the Medical Officer of Health certifies that the body may be removed, and if necessary the vehicle used must be subsequently sent to Swindon to be disinfected.

Bodies of persons who have died from Spotted Fever must not be accepted unless the coffin is hermetically sealed and the body is received in the Local Medical Officer of Health certifying that there is no danger of infection, and is received by rail. The vans and coaches shall be disinfected.

The following are the charges agreed generally by Railway Companies for the disinfection of coaching vehicles:—

Saloon or corridor carriage or compartments in corridor carriage or complete non-corridor carriage	s.	d.
Corridor carriage	30	0
Compartment in non-corridor carriage	10	0
Guard's van conveying corpse	20	0

The charge to be credited to the Company performing the disinfection.

It is to be noted that if coaches are used for the conveyance of Foreign Emigrants shall be thereon as is stated in the district after the journey is completed, and before they are released for other traffic.

These coaches are only to be used for Foreign Emigrants travelling in the coaches but do not act as parts for other traffic. The coaches are to be used in the district as is essential that any said number should not be allowed to mix with ordinary passengers.

To ensure that all compartments in coaches being thoroughly disinfected, it will be necessary to ensure that the compartments are so arranged that the latter can make their way to the compartments should be plainly labelled to assist the staff at intermediate and terminating points.

COACHES FOR THE CONVEYANCE OF HOP-PICKERS.

Only THIRD CLASS coaches of the oldest type must be used for the conveyance of Hop-pickers. Fourth class coaches are to be used for ordinary carriages or carriages with first class compartments to be provided.

SIDE, TAIL AND HAND LAMPS.

The number of lamps to be carried on each coach is fixed and the authorised allotment to any station must not be exceeded.

Station Masters are to be responsible for the maintenance of the lamps and in charge of an Attendance Book of the lamps. The Station Master must arrange for the lamps to be ready on the platform when the van arrives.

If a coach is to be used for a purpose served by one of the Stores vans, it must be used for the purpose at a station where the van is used, where the van is used, the van is used. A special allowance showing the forwarding station must accompany the lamp.

A stock of lamps marked with two ~~white~~ ^{RED} hands is kept at Swindon and will be supplied to the Station Master to equip special trains and the divided portions of ordinary trains.

~~RED~~ ^{RED} Banded lamps must not be used for ordinary traffic purposes.

A special request for extra lamps for special events must apply to the Station Master. The Station Master will arrange to meet the requirements of the District Traffic Manager. When this request is made, the District Traffic Manager will forward the application and pass it forward to the Superintendent of the Line, when arrangements will be made for the lamps to be supplied.

ADDITIONAL LAMPS ARE NOT ISSUED FROM SWINDON STORES EXCEPT BY THE AUTHORITY OF THE SUPERINTENDENT OF THE LINE.

Tail lamps working on ordinary passenger trains are usually balanced with the coaches, but where this is not so the Station Master will be held responsible for fixing a balance.

Under no circumstances must the number of tail lamps on hand exceed the authorised supply. Any surplus lamps must be forwarded immediately to the Stores Superintendent, Swindon, and an advice sent to the Divisional Superintendent or District Traffic Manager.

The following paragraph to be added:—

Great Western tail lamps are being painted white instead of red. Side lamps will continue to be painted black.

Tail and Side Lamps.

STATION INSTRUCTIONS. SIDE, TAIL AND HAND LAMPS—Continued.

Handling of Lamps

Hand lamps on Joint Lines.

The utmost care is needed when handling lamps, so as to avoid damage.

When it is necessary for stations on Joint Lines or other stations which have Joint accounts, to send in hand lamps for exchange, the lamps must in all cases be accompanied by the special green requisition No. 224, in order that the Joint account may be correctly charged. Lamps for repairs should be handed to the Stores Van Attendant at stations at which the Van calls, or, in the case of stations at which the Van does not call, sent to the nearest exchanging station or Depot, accompanied, in all cases, by a green requisition.

TAIL LAMPS. (L.M. & S., L. & N.E. AND SOUTHERN RAILWAYS.)

The outer cases of the L.M. & S. Company's train tail lamps are aluminium colour instead of red.

The L. & N.E. Company's tail lamps are plated and allocated to certain stations and depots.

L.N.E. lamps which are painted Red must be used on Passenger Trains only, whilst those painted White must be used on Freight Trains only.

The Southern Company's passenger train tail lamps, Freight train side-tail lamps and Freight train tail lamps are being painted white instead of red.

CLEANING, TRIMMING AND LIGHTING OF TRAIN LAMPS, AND MODE OF WORKING.

All lamps must be provided at starting point with tail lamps and, where required, side lamps, cleaned, trimmed and in every respect fit for use.

The Station Master must satisfy himself that the following regulations for cleaning and trimming lamps are scrupulously observed.

Trimming petrolol side and tail and hand lamps.

Side, Tail and Hand lamps. Only petrolol must be used in these lamps, great care must be taken in trimming the wick so that the flame is not too high and is quite level before being replaced in the lamp. Every part of the lamp must be cleaned, especially the burners and vent pipes.

Wick of side and tail lamps.

The wick must be cut to the length of six inches, and be quite level before being put in the lamp. The top of the wick must be cut to the level of the burner, and the burners and the lamps must be lighted in the usual manner before they are used. The wick must be kept properly trimmed, the wick must be cut to the level of the burner, and the lamps must be lighted in the usual manner before they are used. The wick must also be changed directly it is found to have become discoloured or hardened.

Water in tank

Care must be taken to see that the tank at the bottom of the lamp is clean, and the water in the tank must be kept moving at the bottom of the tank, its position indicated by the tank having space of fresh, if there shall be any water in the tank it must be kept clean and free from dirt.

Spot in tops

Spot in tops must be kept to the point of the lamp, in order that any spot which may have accumulated may be properly removed, and if spots are provided for this purpose. The tops of the lamps must in no account be cleaned with any wash.

Defective lamps.

Lamps found to be defective or dimly burning must be examined by the Station Master and necessary exchanged for another lamp from the lamp room.

Lamps to be removed by lamp room and sent to stores.

Side and tail lamps must be removed from the train as soon as practicable after the arrival of the latter at destination; if not required for further use, the lights to be blown out, and the lamps taken to the lamp-room by the lampmen; in no case must the light be extinguished by violently jerking or knocking the lamps on the benches or floor.

Care and economy in stores.

The utmost care must be taken of the articles supplied to the lamp-room for use, and strict economy must be exercised in the use of stores.

Damage to articles in cleaning rooms, etc.

Every care must be exercised on the part of the lampmen to prevent damage by scratching or otherwise to lamps. The burners, wicks, trimming tools, as flint and wicks and articles in connection with the lamp-room must be kept clean and free from dirt and oil, wick, &c., must be kept together and removed from the lamp-room daily.

Duties.

Every lampman must strictly attend to the duties assigned to him, in accordance with his duty sheet, which shall be posted in the lamp-room.

Repairs to lamps.

Lights of all tail lamps shall be kept a bright red colour, and with the inside of the outside should be frequently cleaned with cleaning oil, when can be obtained from the Stores Department.

Guards to report defects.

The lampmen must not permit lamps to be used when in any manner defective, or require repainting, particularly in the case of tail lamps. All lamps for repairs must be exchanged at the nearest of the Stores Van.

Guards must examine the lamps on their trains at the starting stations and report defects, if any, to the Station Master, a note to be taken of the lamp at intermediate stations and, if any defects are observed, have the lamps changed, if circumstances will permit. If not, in order to avoid delay to trains, the Station Master must advise the next lamping station to be ready to do this.

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CLEANING, TRIMMING AND LIGHTING OF TRAIN LAMPS AND MODE OF WORKING.—Page 310.

The first paragraph of these instructions to be amended to read —

Side, Tail and Hand Lamps.—Only petroleum must be used in these lamps, great care must be taken in trimming the lamps to ensure that an adequate quantity is poured into the tank but that the tanks are not filled above the level of the bottom of the burner collar. The tanks must be wiped dry before being placed in the lamp, every part of which must be well cleaned especially the burners and vent holes.

(G.A 30 Op —9/54 LK1/10688/229).

7A 31

STATION INSTRUCTIONS.

INSTRUCTIONS AS TO CHARGING VEHICLES WITH OIL GAS AND ALSO FOR LIGHTING AND EXTINGUISHING OIL GAS LAMPS.

Oil Gas Works are situated at Paddington, Swindon, Bristol, Exeter, Cardiff, Stations at which Oil Gas is charged with gas.
 New at Ebbw Vale, from Llanidloes, and National Oil Refining Works at Wolverhampton and Birkenhead. Travelling gas tanks are also kept at most of the principal stations (other than those above named) at which trains are formed. These tanks, being constructed to work on Passenger Trains only, must not be conveyed by Freight Trains, and great care must be exercised with them when shunting. (See page 237.)

"A" Head Lamp Trains

GA 12

Gas tanks must not be worked on ~~Express Passenger Trains~~, except in cases of emergency. When Gas Tanks are regularly worked between stations, particulars of the trains by which they should be conveyed must be recorded in the Coach Working Programme.

Before coaches leave the Station at which they should be charged, care should be taken to ensure that the gas holders are properly filled with gas as otherwise there will not be sufficient for the out and home journey. At stations where travelling gas tanks are stationed the Carriage Department examiners will, when requested, charge the reservoirs of vehicles, upon the tank and vehicles being placed in position for this to be done. After vehicles have been charged, the valves at the ends of the indiarubber filling tube should be closed, but the gas remaining in the pipe should not be discharged into the atmosphere.

Coaches, &c., to be fully charged.

When it is necessary to use a lamp during the operation of charging the reservoirs, an ordinary hand lamp must be used and under no circumstances must an ordinary hand lamp be brought near the filling valve.

The men employed to do this work must previously be properly instructed to ensure that they are competent to do gas filling, and a person at each station where this work is carried out must be made responsible for seeing that the safety lamps are kept in proper condition and that the screw connections on the by-passes and gas charging pipes are in proper condition and a good fit.

Should the gauze in a safety lamp become pierced or otherwise damaged, or any damage occur to the body of the lamp, which would allow air to be admitted, other than through the gauze, the lamp must on no account be used, but must be sent to Swindon for repairs.

In the event of fire breaking out, the gas must at once be shut off by closing the ground filling valves and the stop valves on the ends of the reservoirs.

Before vehicles commence their journey the globes and reflectors must be examined. Care must be exercised in order to avoid damage to the incandescent mantles. In the event of a mantle being found to be defective, the attention of the Carriage examiner must be called to the matter at once.

Cleaning and lighting.

The stop valve is placed in the corridor of Corridor vehicles, and in the guard's compartment of Vans. A lever to operate the stop valve for turning the gas "off" or "on" is placed at one end of Ordinary coaches. In Double-ended Slips a stop valve is fixed in each of the guards' compartments, but it is only necessary to open one of the valves before lighting the lamps. The same valve must be operated to extinguish the light throughout, and on no account must the other valve be interfered with. On Milk Vans, etc., the lever is fixed to the sole-bar, on which the "on" and "off" positions are marked.

Incandescent Lighting.

The Bye-pass arrangement is to a great extent independent of the main lighting, and the small bye-pass lights burn continuously.

Bye-pass.

In order to raise or lower the lights, the handle of the stop valve must be turned right over to the "on" or "off" position, as the case may be, and not left intermediately, otherwise a waste of gas and damage to mantles will result.

Regulating.

Accidents have occurred through glass bowls in gas-lit coaches cracking and falling. It must be remembered that the by-pass pipes are under pressure and when they have projected a point of flame in the direction of the glass and so caused the globe to crack.

Broken Gas Globe on Carriage.

When nipples are found to be damaged, the Examiner should be advised, and the lamp should not be lighted until it is in order.

SUPPLY OF GAS IN COACHES.

Gas coaches, collected to form Special and Excursion trains, must be furnished with sufficient gas to supply light for the outward and home journeys.

When coaches are ordered by the Rolling Stock Department of the Superintendent of the Line's Office to be sent to any other Station, and gas coaches are sent steps should be taken by the person in charge at the station from which the coaches start to ascertain the amount of gas in the reservoirs, and if not sufficient, instructions should be given for the coach, or coaches, to be properly charged at

STATION INSTRUCTIONS.

SUPPLY OF GAS IN COACHES—Continued.

the nearest convenient Station *en route*, and an advice by wire should be sent by the Station Master to the charging station stating the number of coaches to be charged, in order that proper arrangements may be made with the Locomotive Department for the reservoirs to be filled with oil. If it is arranged for a Special to be run with the engine, the Station Master should advise for the train to be run *en route* responsible for sending an advice by wire giving the number of coaches, if any, to be gassed.

Should the Guard of the train pick up any such coaches on the journey, he will be responsible for arranging with the Station Master at the station where the coaches are attached for a telegram to advise to be forwarded to the gassing station, if the coaches require gas.

The Stations at which trains, or coaches, may be charged while standing at the platforms are:—

Paddington	Cardiff
Swindon	Worcester
Bristol, Temple Meads	Wolverhampton
Exeter	Birkenhead.

Any gas coaches ordered to be sent from a station at which gas can be supplied must be fully charged before they are sent away.

INSTRUCTIONS RESPECTING THE USE AND REPAIR OF GAS LIGHTING TORCHES.

Gas Lighting Torches are to be used in all cases for halftone square and hexagon gas lamps.

If not already supplied, torches and oil can be procured at Swindon, or at any of the following Stations and Depots not supplied with torches must also apply to the Stores Department, Swindon, for a torch and can, a special arrangement being made by the Stores Department for the purchase of torches. In the event of a torch being required for a special train, the Station Master should forward a telegram to the Stores Department at Swindon stating the number of torches required, and the date of the next train. If not received after a reasonable time, as specified in the telegram, the torches should be sent to Swindon Stores.

Torches sent by train from Swindon will be attached to a board to prevent damage.

On receipt of the torch, the required repairs should be at once made by the Stores Department. Swindon Stores should be kept supplied with torches and canisters, and should be provided with the torch lighting station, and the necessary form for the repair of the torches, and the necessary repairs should be rendered, and forwarded under separate cover to Swindon Stores.

Use of Torch.

Fill upper reservoir with torch oil which is supplied ready mixed by the Stores Department, Swindon.

Charge lower reservoir with a less spirit of oil of oil or benzene or petrol. (To get best results re-charge each evening.)

Use only the wick supplied for the torches by the Stores Department.

Keep lamp low, as this is not only to light the burner but also to ignite the benzene or petrol vapour which is flashed across oil flame by a pressure on metal valve.

Throw light from above or below glass of lantern, and without coming near to the burner mantle, or chimney shade.

Turn lever tap, insert top of torch a few inches into lantern, and press trigger of valve. The projected flame lights the burner.

Torches when not in use to be kept upright in a suitable place and cleaned and trimmed daily.

CARRIAGES STORED UNDER COVER.

Carrriages stored under cover must always be coupled together, and the hand-brake nearest to the exit from the shed must be tightly applied, also the automatic brake release, in order that throughout the train, in order that in the event of fire breaking out they may be drawn out without delay or risk to life and property.

LOCKING DOORS OF EMPTY CARRIAGES IN SIDINGS AND SHEDS AND LOWERING BLINDS OF DINING CARS, &c.

The doors of all empty carriages must be kept locked on both sides, whilst the vehicles are standing in sidings or carriage sheds; the windows must also be closed, the ventilators being left open. When the vehicles are left standing for any length of time, the blinds must also be lowered, in order to prevent the sunlight fading the trimmings.

All concerned must be careful to see that the window blinds are lowered when dining cars or saloons are left standing exposed to sunshine so as to prevent the cloth table covers being scorched and burned, owing to the concentration of the rays of heat caused by the sun shining through the water bottles.

STATION INSTRUCTIONS.

MAINTENANCE OF WEIGHING MACHINES, WEIGHBRIDGES, ETC.

1. The Station Master or Goods Agent is responsible generally for the carrying out of the following regulations, and each machine must be placed under the charge of one of the permanent staff. Every Machine to be in Charge of a Responsible Man.

At some stations one man may be placed in charge of all machines, but at others it may be convenient for several men to be appointed to attend to particular machines; in either case the responsible men must make themselves thoroughly acquainted with these instructions.

2. EVERY MACHINE MUST BE BALANCED FIRST THING EVERY MORNING, AND AS OFTEN AS MAY BE NECESSARY THROUGHOUT THE DAY, AND KEPT READY FOR USE. Balancing.

AT DOCKS THE MACHINES MUST BE BALANCED AT THE COMMENCEMENT OF EACH TURN OF DUTY BY THE RESPECTIVE WEIGHERS.

A machine is properly balanced when the steelyard slowly rises, from the bottom, and remains at the top of its carrier.

Should there be a failure to balance, or any reason to suppose that a machine is inaccurate, or if the Government Inspector's stamp on machines should be illegible or have become obliterated, such machine must immediately be placed out of use, and Messrs. Pooley & Son be at once notified at their nearest depot. (See clause 15.)

Machines which are constantly in heavy use, must be closely watched to see that they are working correctly.

The cleaning and oiling of machines is an important matter, and the following instructions must be strictly observed:— Cleaning and Oiling of Machines.

Every part of each machine, except those parts involving displacement or interference with working parts, to be kept cleaned and oiled when required. EVERY CLOTH NOT TO BE USED.

Steelyards and dials to be kept bright, so that the letters, figures and indicators are legible. Oil or spirits must not be used for cleaning dial faces.

The spaces between the platform and frame of weighbridges, deck and dormant platform machines to be kept clear of gravel, dirt, or other obstacle by the Department using such machines.

The surface of plates to be kept free from mud and water.

Weighbridge pits, walls, floors and corners of Traffic and Engineering Department machines, as well as frames and castings which involve no displacement, must be cleaned by the Engineering Department not less frequently than once in six months and whenever the machines are overhauled. The person in charge must advise the Permanent Way Inspector when machines are being overhauled by the contractors, which have bad drainage, or which are subject to tidal waters, cleaned out every month, and in any case, after

3. The Traffic and Carriage Department and the Signal Department are responsible for cleaning out the pits of weighbridges under their charge, as well as frames and castings which involve no displacement.

Messrs. Pooley & Son are responsible for keeping clean all under parts of weighbridges involving displacement of other parts, as well as for keeping all machines properly and efficiently painted, except bright parts. Where this is not carried out, and rust results, the Engineering Department, so far as the under parts of weighbridges are concerned, and the using Department, so far as the parts above ground are concerned, to report the cases to the Stores Superintendent. The cleaning of rust from any parts of weighing machines which should be painted is the responsibility of Messrs. Pooley & Son.

4. In case of frost binding the plates of weighbridges, fires are not on any account to be lighted thereon or underneath, but, if necessary, salt may be used, any salt water to be brushed off as quickly as possible, and great care taken to prevent salt water getting into contact with the gearing surfaces. Frost on Weighbr.

5. SHOULD THE INSPECTOR OF WEIGHTS AND MEASURES DEFACE THE STAMP OF ANY MACHINE, OR MAKE ANY COMPLAINT WITH REGARD TO IT, A REPORT OF THE CIRCUMSTANCES MUST BE SENT BY NEXT TRAIN TO THE STORES SUPERINTENDENT, SWINDON, AND TO MESSRS. POOLEY & SON, AT THEIR NEAREST DEPOT. Defacement of Stamp.

6. Particular care must be taken that each machine has its own weights, and no others. Weights must not be used as hammers, or for any other purpose than as weights. Each weight must be carefully examined daily, and if the lead therein is found to be loose, the fact must be reported at once to Messrs. Pooley & Son. WHEN NOT IN USE ALL WEIGHTS MUST BE PLACED IN PROPER CUSTODY. ON NO ACCOUNT MUST ANY WEIGHT BE REMOVED FROM ONE MACHINE TO ANOTHER. Use, Inspection and Custody of Weights.

All proportional weights are to bear the number of the machines with which they are used, and they are not to be used with any other machine.

STATION INSTRUCTIONS.

MAINTENANCE OF WEIGHING MACHINES, WEIGHBRIDGES, ETC.—

- Steelyards.** 7. No articles of any kind must be hung on the steelyards or any of the machines.
- Relieving Apparatus.** 8. Every machine provided with a relieving apparatus must be put out of gear except at the moment of weighing. Such machines must be put out of gear before being loaded, and be unloaded before being put out of gear. **NOTE.** NOT TO BE RUN OVER WEIGHBRIDGES, IF IT CAN POSSIBLY BE AVOIDED. IN SUCH CIRCUMSTANCES COMPEL THE PASSAGE OF ENGINES OVER WEIGHBRIDGES, THE WEIGHBRIDGES MUST PREVIOUSLY BE PUT OUT OF GEAR, AND THE SPEED OF THE ENGINE MUST NOT EXCEED FOUR MILES PER HOUR.
- Passage of Locomotives over Weighbridges.**
- Weigh-houses.** 9. Weigh-houses are to be kept in good repair by the Engineering Department, clean free from lumber and confined to their proper use and locked when not in use by the Department using the machine.
- Interference with Machines.** 10. Passengers, Coal Dealers, Dealers' Clerks or other unauthorised persons must not, under any pretext be allowed to make use of the weigh-houses or to interfere with any weighing instrument.
- Permanent Way Staff.** 11. The Station and Permanent Way Staff must adjourn every machine and assist, as convenient, may permit, and if required, and if it is not of Permanent Way Staff are to be made by Messrs. Pooley & Sons direct to the Permanent Way Inspector.
- Weighbridges out of use.** 12. As soon as it is known that a weighbridge is not to be used for any lengthly period the Stores Superintendent at Swindon must be advised, and he must at once communicate with Messrs. Pooley & Sons on the matter of maintenance. On the 31st December of each year the Stores Superintendent, Swindon, must be furnished with a list of all machines permanently out of service at that date, and of those which are out of use for the day, and referred to above, giving in each case the date when the machines ceased to be used.
- Weigh Tickets.** 13. No ticket or permit of weight is to be given for any road wagon, cart or other road vehicle until the tare has been duly taken and the tare from the gross weight.
- Machines transferred from one station to another.** 14. If a weighing machine is transferred from one Station or Receiving Office to another temporarily or permanently, the Stores Superintendent, Swindon, must be advised immediately.
- Spare Machines.** All machines not required must be forwarded to the General Stores, Swindon, together with an advice of the date sent and the registered number of machine.
- Repairs.** 15. Whenever a machine is broken or is out of repair, the person in charge must send a written report to the person in charge of Messrs. Pooley & Sons, Ltd., at their nearest depot as under, or to their Head Office, 72, Dean Bridge Street, Birmingham:—

Aberystwyth	..	..	..	Passenger Station Yard.
Birmingham	..	..	..	Pinfold Street.
Bristol	..	..	..	G.W.R. Goods Yard, Pylo Hill.
Chester	..	..	..	L.M. & S.R. Arches, Brook Street.
Cardiff	..	..	..	{ 39, Tynan St. Arches. 1, Westgate Street.
Crewe	..	..	..	Nantwich Road.
Dublin	..	..	..	105, Middle Abbey Street, C.9.
Exeter	..	..	..	Southern Railway, Queen Street Goods Yard.
Gloucester	..	..	..	G.W.R. Up Parcels Office.
Quernsey	..	..	..	Messrs. W. & T. Avery Ltd. (Agents), 29, Sand Street, Jersey.
Liverpool	..	..	..	4, Graysom Street, Wapping.
London	..	..	..	72A, St. Thomas Street, S.E.1.
Manchester	..	..	..	Norton Street, Greengate, Salford.
Newport (Mon.)	..	..	..	{ Docks, Top of North Dock. Grafton Lane, off Corporation Road.
Newtown (Mont.)	..	..	..	G.W.R. Goods Yard.
Plymouth Docks	..	..	..	G.W.R. Docks, Store 21.
Reading	..	..	..	G.W.R. Parcels Office.
Swansea	..	..	..	Victoria Arches, nr. Victoria St., Swansea.
Swindon	..	..	..	G.W.R. Parcels Office.
Taunton	..	..	..	G.W.R. Parcels Office.
Truro	..	..	..	G.W.R. Parcels Office.
West Easing	..	..	..	G.W.R. Goods Yard.
Worcester	..	..	..	G.W.R. Goods Yard, Henwick.

STATION INSTRUCTIONS.

MAINTENANCE OF WEIGHING MACHINES, WEIGHBRIDGES, ETC. (continued)

Any failure by Messrs. Pooley & Son to do what is required promptly, or to keep machines efficiently repaired and maintained, must be immediately reported to the Stores Superintendent, Swindon. All machines are required to be inspected by Messrs. Pooley & Son at least once in every 12 months. If a machine has not been examined for 12 months, the attention of Messrs. Pooley & Son should be called to it, whether it is in need of repairs or not, and the fact reported to the Stores Superintendent.

The officers responsible for all docks machines are the Dock Managers. In the event of any such machine getting out of order, the Dock Manager will communicate at once with Messrs. Pooley & Son, at their nearest depot, and with the Resident Docks Engineer.

The responsibility for all other machines is with the using Department, whether the machines are within the Dock areas or otherwise, and such users must communicate direct with Messrs. Pooley & Son in the event of repair work being needed.

16. When a machine is examined or repaired Messrs. Pooley & Son's workmen will present for signature a certificate specifying the nature of the work done. If the certificate presented is a PINK one, it denotes that an extra charge will be made against the Company; and in this case the person in charge must investigate and explain fully on the back of form the circumstances necessitating the repairs. All certificates, after being certified, must be handed to Messrs. Pooley's workman on the day the repairs are completed. Certificates.

Experience shows that a large number of certificates do not reach the Stores Superintendent, and persons in charge are required to see that the above instruction is strictly observed.

As a rule, no machine should be in a condition that it cannot be used for more than three days. Any such cases should be immediately reported to the Stores Superintendent.

17. All orders in respect of the maintenance, condemnation, replacement, inaccuracy, etc., also requisitions for the supply of weighing appliances must be forwarded to the Stores Superintendent, Swindon. Communications.

18. The Company undertake to supply the Contractors' repairing vans and materials for the repair and testing of the machines, and no charge must be made when such materials pass over the line; but, in order to prevent irregularity, the following instructions must be observed:— Materials.

With every consignment, whether by Freight or Passenger train, which Messrs. Pooley & Son, Ltd., hand to the Company for transit under the contract, they will hand in a consignment note, No. 3104, which will be overprinted as follows:—

"MATERIALS FOR REPAIR OR TESTING WEIGHING MACHINES.

"We certify that the traffic mentioned herein is to be used under the Agreement respecting weighing machines between the G.W.R. Company and Messrs. Pooley & Son, Ltd.

"H. POOLEY & SON, LTD.

"per

When traffic is to be forwarded by passenger train, Messrs. Pooley & Son will so endorse the Consignment Note 3104.

It will be the duty of all stations to stamp, waybill or invoice this traffic "paid." The rate of carriage for this traffic is 11 per van per mile, and the loose materials at the ordinary rates, and in the case of goods traffic the invoice must be endorsed "contract."

Amounts in respect of Passenger train traffic must be taken credit for through Column "A" or "B," as the case may be, of the Parcels General Account "109," and those in respect of Goods train traffic on the Goods General Account "1103" in the space provided thereon.

The Accounts for the traffic, on form 964 Goods, or form 971-1a or 971-2a Parcels, together with the special consignment notes, must accompany the returns to the Chief Accountant's Office (Audit Section).

Stations must, as far as possible, exercise a check as to the purpose for which the materials, etc., conveyed under this arrangement are used, as it covers only the articles to be used in connection with the repair and testing of weighing machines upon the Company's system.

The "Contract" Accounts must be kept separate from the accounts for traffic dealt with for Messrs. Pooley & Son in their ordinary business, and the latter should be collected from the firm.

STATION INSTRUCTIONS.

MAINTENANCE OF WEIGHING MACHINES, WEIGHBRIDGES, ETC.—

- Station Records.** 19. A memorandum book must be kept containing a list of the dates of verification and, in the case of Weighing Bridges, the names of the persons who are visitors or complainants by the Inspector of Weighing Machines and traders.
- Spare Parts.** 20. All parts of weighing machines repaired in the course of business are the property of the contractors, but parts replaced in the course of repair by the Company's servants are to remain the property of the Company and are to be sent to the General Stores, Swindon. Persons who are not authorised to take away any parts taken away by Messrs. Pool & Sons are liable for the purposes of this rule under the last-mentioned category are returned to the Company at Swindon.

HIRE OF RUGS AND PILLOWS.

- Hire of Rugs and Pillows to Passengers.** Men appointed to take charge of stock of articles.
- Number of Rugs and Pillows issued to each Station.
1. RUGS FASTENED WITH PAPER BANDS, and PILLOWS IN PAPER BAGS, may be hired at certain Stations at a fixed charge of one shilling per article.
 2. Men appointed to take charge of the rugs and pillows at the stations referred to in the last paragraph, will be notified by the Station Master, for the proper care and issue of the stock under their charge.
 3. The rug and pillow hiring stations, and the authorised stocks at each station are as shown below:—

Station.	Authorised Stock.			Station.	Authorised Stock.		
	Rugs.	Pillows	Pillow Slips.		Rugs.	Pillows	Pillow Slips.
Banbury ..	6	6	12	Paddington ..	117	117	362
Birmingham ..	12	12	12	Penzance ..	12	24	48
Birkenhead ..	6	6	12	Plymouth (N.R.) ..	25	50	90
Bristol (T.M.) ..	20	30	100	(Millbay) ..	10	20	24
Cardiff (Gen.) ..	23	22	44	Reading ..	6	6	12
Exeter (St. David's) ..	6	12	24	Shrewsbury ..	12	12	24
Fishguard H'br. ..	32	70	120	Swansea (High St.) ..	4	4	10
Newport ..	6	6	12	Torquay ..	4	6	12
Newton Abbot ..	6	6	12	Truro ..	14	24	48

Tickets in connection with the hire of rugs and pillows.

4. With the hiring of a rug or pillow a ticket must be issued. Tickets are in three sections and must be dealt with as follows:—

(a) RECEIPT FOR PASSENGER

OBVERSE G.W.R. No. (1843) PASSENGER'S TICKET PAID FOR HIRE OF RUG - 1/- PILLOW - 1/- SP. A. NOTICE In order to ensure the provision of clean Rugs and Pillows, Passengers are requested to report to the Station Master any case where the paper bands round Rugs or the paper bags in which Pillows are enclosed have been broken (SEE BACK)	REVERSE. Passengers are requested to kindly place the articles in the Carriage Rack when leaving the Train.
--	---

This must show exactly what articles have been issued, i.e. if a rug is supplied the word **PILLOW** should be crossed through in pencil, and *vice versa* if a pillow only is supplied. If two or more of the same article are issued a receipt must be given in respect of each.

STATION INSTRUCTIONS.

HIRE OF RUGS AND PILLOWS—Continued.

(b) TICKET FOR GUARD.

OBVERSE.	REVERSE
<p>G.W.R. No. (1843)</p> <p>GUARD'S TICKET FOR HIRE OF RUGS AND PILLOWS.</p> <p>Date: 19</p> <p>No. of Rugs: 1</p> <p>No. of Pillows: 1</p> <p>SEE BACK</p>	<p>SPECIAL INSTRUCTIONS.</p> <p>This Ticket must be given to the Guard who, at the completion of his journey and as the Passenger has alighted at an intermediate station, must hand it to the Guard working the train forward or to the Guard working the train backward, as the case may be, in which the Passenger is travelling.</p> <p>When a Guard is relieved en route he must hand this ticket to the Guard working the train forward and indicate the compartment in which the Passenger is travelling.</p> <p>In the case of Passengers completing their journey at an intermediate station or changing to another train in order to reach their destination, the Guard must hand this ticket to the Station Master or Inspector at the station at which the Passenger alights.</p>

This must be made out immediately a rug or pillow is issued to a passenger, and must be handed to the Guard working the train forward or to the Guard working the train backward, as the case may be, in which the Passenger is travelling. Tickets must be handed to the Guard, who will deal with them in accordance with the instructions shown on the back of the ticket.

When a passenger with a rug or pillow, or both, changes at an intermediate station into another train in order to reach his destination, the Station Master or Inspector to whom the Guard hands this ticket must give it to the Guard working the train by which the passenger is going forward and indicate the compartment where the articles are in use.

(c). COUNTERFOIL FOR ISSUING STATION. This must be carefully made out at the same time as the Guard's ticket, double-sided carbon paper being used.

THE RUGS, PILLOWS AND PILLOW SLIPS ARE BRANDED "G.W.R." AND ARE COMMON USER TO THE STATIONS CONCERNED.

A linen tab is attached to the rugs and pillows worded "Traffic Department Stock. To be returned to nearest hiring station," and in the event of any of these articles finding their way to Departments other than the Traffic Department they must be dealt with accordingly. The hiring stations are shewn in paragraph 3, page 316.

6. At Stations where men are appointed to collect Rugs and Pillows they must be held in stock at the hiring station, and also particulars of the compartments in which the Rugs and Pillows are to be found. AT OTHER STATIONS THE GUARD MUST HAND THE TICKETS TO THE STATION MASTER OR INSPECTOR, WHO WILL BE HELD RESPONSIBLE FOR THE COLLECTION AND DISPOSAL OF THE ARTICLES.

7. After rugs and pillows have been collected at the passengers' destination stations, they must be dealt with as follows:—

If destination station is a Rug Hiring Station—to be taken into stock.

If destination station is NOT a Rug Hiring Station—to be sent to the nearest Rug and Pillow Hiring Station by the next train, securely packed in paper and booked as a parcel.

8. Station Masters must take a muster of the stock of rugs, pillows and pillow slips at the hiring stations, and enter the particulars on the Muster Book. The Muster Book must be handed to the Stores Van Attendant who will make up the authorised stocks at stations reporting shortages. Any articles in excess of the authorised stock must be handed to the Stores Van Attendant, from whom a signature must be obtained.

Guard's ticket to show the destination of the Passenger

Collection of Rugs and Pillows.

Disposal of Articles after 50

Maintenance of stocks at Hiring Stations

STATION INSTRUCTIONS.

GREAT WESTERN RAILWAY.

(5633)

WEEKLY RETURN OF RUGS, PILLOWS, AND PILLOW SLIPS.

..... Station. ....		..... 19.....	
	Number on hand. *	Number in excess of authorised stock.	Number short of authorised stock.
RUGS			
PILLOWS ..			
PILLOW SLIPS			

* The "Number on hand" to include soiled or damaged articles on hand for exchange.

The Stores Superintendent,
SWINDON,
G.W.R.

.....
Signature of Station Master.

Station Masters must also examine the stocks of rugs, pillows and pillow slips at their stations on the 1st of each month to ascertain whether the articles are in good condition, clean and well aired.

Exchange of
soiled and
damaged
articles.

9. Soiled or damaged articles should be securely tied in bundles, each bundle bearing label No. 5621-2 properly completed (except at Stations where the Stores Van does not call in which case label No. 5621-3 should be used), and tendered to the Stores Van Attendant, who will exchange for clean articles. A separate label must be used for each class of article. In the event of its being impracticable, due to depletion of stock, to exchange articles handed in, arrangements will be made to forward clean articles as early as possible. If exchanges or additional supplies are required before the Stores Van is due, delivery will be made from Swindon by passenger train service upon the receipt of a telegram, and the soiled articles should be returned to the General Storekeeper, Swindon, by the same service, carefully packed (in hampers if available), under cover of washing advice No. 5148 duly completed. This practice must not be adopted unless absolutely essential.

Enquiries respecting errors or discrepancies in the exchange of articles must be addressed to the General Storekeeper, Swindon.

Rugs and
Pillows held at
Swindon for
special require-
ments.

10. A small stock of rugs and pillows branded "Return to G.W.R. General Stores, Swindon," and numbered consecutively, is held at Swindon for special requirements. Stations requiring rugs and pillows to meet temporary requirements (such as football excursions, outings, etc.) should make application to their Divisional Superintendent, giving as much notice as possible. If approved, the Divisional Superintendent will pass the application forward to the Superintendent of the Line, who will make the necessary arrangements. After use, the articles should be collected and returned to the General Stores, Swindon, immediately, an advice of despatch being sent under separate cover.

Supplies of
labels and
Forms to be
used.

11. Supplies of the weekly return form (specimen above) and also label No. 5621-2 can be obtained from the Stores Van Attendants. Supplies of label No. 5621-3 and washing advice No. 5148 can be obtained by written application to the General Storekeeper, Swindon, and stations at which the Stores Van does not call can also obtain supplies of the weekly return form from Swindon.

Receipts.

12. THE RECEIPTS FOR THE HIRE OF THE ARTICLES MUST BE ACCOUNTED FOR THROUGH THE "PASSENGER GENERAL ACCOUNT, No. 874."

Paragraph 7

After the line "Castings give shape marks and weight where possible," add "Articles of a specialised nature.—Dimensions shape identification marks colour, material, etc., also the trade name if known should be given, and if the article is of such a specialised nature that the name would be unlikely to give a true idea of its outward appearance, or if the trade name is likely to be misleading, a sketch should be provided."

(G.A.15—12/44 C.P.50221)

STATION INSTRUCTIONS.

GENERAL INSTRUCTIONS TO BE OBSERVED IN DEALING WITH LOST AND UNCLAIMED LUGGAGE AND PARCELS.

The arrangements for dealing with lost and unclaimed property and parcels are as follow:—

1. The Lost Property Depots of the Railway Companies are situated at the following stations, to which, according to the circumstances of the case, enquiries must be sent:—

C.L.C.	Liverpool (Central).
East Kent	Shepherdswell.
Great Northern (Ireland) ..	Belfast (for Northern Ireland Stations).
	Dublin (Amiens Street) for Irish Free State Stations.
Great Southern (Ireland) ..	Dublin (Kingsbridge).
	Dublin (Broadstone).
	Dublin (Westland Row).
Great Western	Paddington.
Kent & East Sussex	Raydon.
L. & N.E.	Liverpool Street.
	Marxbone.
	Kings Cross.
	Edinburgh (Waverley).
	York.
Continental Light	Liverpool Street.
L.M. & S.	Dorby.
	Glasgow (Central).
N.C.C. Section	Belfast (York Road).
L.P.T.B.	200, Baker Street, London, W.
M. & C.N. Joint	King's Lynn.
S. & D. Joint	Bath.
Southern	Waterloo.
L. & N.E. and L.M. & S. Joint ..	Aberdeen.
	Leeds (New).

2. Searching of Premises. Station Masters or Parcels Agents must inspect parcels offices, cloak room, etc., daily, to see that no articles remain on hand irregularly or unreported.

3. Unclaimed Articles to be Taken to Lost Property Office. Articles (excluding unaddressed parcels) to be taken to the Lost Property Office. Unaddressed book parcels must be dealt with by the Parcels Department.

4. Recording of Unclaimed Articles. To be kept in the station Lost Property Office and in the Lost Property Register No. 2158 or in a foolscap book ruled for the purpose. The register should be opened by a responsible person, and the owners communicated with if an address is found.

5. Reporting Unclaimed Articles. Particulars of unclaimed property must be reported to the Station Master or Parcels Agent, 2158, and must be forwarded so as to reach Paddington not later than 11.0 a.m. the day following the finding of the articles.

6. Reporting Luggage, Parcels, &c., Addressed "Till Called For." Booked luggage and parcels which have not been claimed or delivered more than a week, and all consignments which will not be delivered owing to the address label being lost, or which are found on the carriage, must be reported to the Lost Property Depot, Paddington, on Form 2158.

7. Reporting Found Luggage, Parcels, &c., and reported in progressive number order on the station register, a new series of numbers being commenced on January 1st in each year.

8. Description of Found Luggage and Parcels. To assist identification a full and explicit description which may help to distinguish them, and, where possible, the contents, which may appear thereon, and also the following special features:—

parcels should be distinguished as white, brown, or newspaper, as the case may be, and contents stated where possible.

Portmanteaux, Bags, Baskets, Boxes, Bundles, Cases, &c.—Give colour, material, and any other marks which may help to distinguish them, and, where possible, the contents.

Valises, Luggage.—Give regimental numbers or other marks.

Books.—Give duplicate numbers and any speciality in contents.

Ropes and Tyres.—Give full description, make and number, also particulars of any

and weight where possible.
stated, whether on platform or elsewhere, and if found in train,

STATION INSTRUCTIONS.

LOST AND UNCLAIMED LUGGAGE AND PARCELS—Continued.

8. Disposal of Unclaimed Property.—Unclaimed property (except booked parcels and booked luggage bearing addresses, and perishables, must at the expiration of 3 days be forwarded to the Lost Property Depot, Paddington, and bear label No. 272 properly filled in.

Articles referred to in C.T. Reg. 6 must be sent to the Lost Property Depot, Paddington, at the expiration of 1 month and be duly labelled with Label 22.

9. Cloak Room Deposits Unclaimed.—Articles not arriving on haul on a given Railway, the owner or whosoever known must be reported to the Station Master at the Lost Property Depot, Paddington, on full particulars, including date of arrival, bearing, etc. Articles not claimed within six months must be opened by the Station Master and a receipt be put by him in the presence of a third party, and full particulars of contents stated. If a value can be found, the owner should be communicated with, but if not, the contents of the articles received within a week, the packages together with a list of the contents, must be sent to the Lost Property Depot, Paddington.

10. Perishables.—Perishable articles on hand without account returned or refused, must be dealt with in accordance with R.C.H., C.T. Regulation No. 57.

11. Clearance of Charges.—No arrival charges are to be raised on unclaimed property forwarded to the Lost Property Depot, Paddington. When a consignment of unclaimed property is sent, the particulars must be submitted to the Divisional Superintendent or District Traffic Manager.

12. Money, etc., Found.—Cash, Rings and Treasures, Notes, Passes, Orders, Stamps, etc., found and not claimed can be stored up to a year, and a receipt given for the same. After one year the empty receptacles to be dealt with as unclaimed property. A report to be sent on the day of sending to the Divisional Superintendent or District Traffic Manager.

13. Articles of Value.—Jewellery, watches, gold glasses, and other articles of value when sent to the Lost Property Depot must be treated as "Values."

14. Articles Sent for Inspection.—Articles sent to a station for inspection and not claimed within 48 hours must be forwarded to the Lost Property Depot, Paddington, with a receipt, and forwarding station advised. If received from another railway, it must be returned to that Company.

15. Articles to be Protected when Sent to Depot, Etc.—Articles sent to the Lost Property Depot or from one station to another, must be packed in boxes, or other strong and secure containers, and packed in condition. Where parcels are packed in the packages, the ways be packed in them.

16. Luggage Miscarried.—When luggage is lost or addressed to a station is received at another, the railway must advise by wire the other railway concerned.

17. Articles Reported and Subsequently Claimed by Owner.—Articles previously reported and subsequently delivered to the railway must be posted in a bag marked "Returned to Owner." Other stations for lost property must be reported to the Lost Property Depot as early as possible on the "Disposal Return" and the following particulars must be given:

Progressive number under which reported.

Date found.

Description of article.

How disposed of.

18. Naval Seamen's Luggage.—Naval and Royal Marine Kit (knapsacks, chests, hammocks, etc.) found in Railway Carriages, etc., must be sent to the nearest station where the goods are shown, and the following particulars must be given: (a) Name of the owner, (b) Address, (c) Rank, (d) Service, (e) Date of arrival, (f) Date of departure, (g) Date of arrival at the depot, (h) Date of departure from the depot, (i) Date of arrival at the depot, (j) Date of departure from the depot, (k) Date of arrival at the depot, (l) Date of departure from the depot, (m) Date of arrival at the depot, (n) Date of departure from the depot, (o) Date of arrival at the depot, (p) Date of departure from the depot, (q) Date of arrival at the depot, (r) Date of departure from the depot, (s) Date of arrival at the depot, (t) Date of departure from the depot, (u) Date of arrival at the depot, (v) Date of departure from the depot, (w) Date of arrival at the depot, (x) Date of departure from the depot, (y) Date of arrival at the depot, (z) Date of departure from the depot.

Naval stores, of nets and restorers, must be sent to the nearest station where the goods are shown, and the following particulars must be given: (a) Name of the owner, (b) Address, (c) Rank, (d) Service, (e) Date of arrival, (f) Date of departure, (g) Date of arrival at the depot, (h) Date of departure from the depot, (i) Date of arrival at the depot, (j) Date of departure from the depot, (k) Date of arrival at the depot, (l) Date of departure from the depot, (m) Date of arrival at the depot, (n) Date of departure from the depot, (o) Date of arrival at the depot, (p) Date of departure from the depot, (q) Date of arrival at the depot, (r) Date of departure from the depot, (s) Date of arrival at the depot, (t) Date of departure from the depot, (u) Date of arrival at the depot, (v) Date of departure from the depot, (w) Date of arrival at the depot, (x) Date of departure from the depot, (y) Date of arrival at the depot, (z) Date of departure from the depot.

An advice note, particulars of the articles together with any markings thereon, must be sent to the Commanding Officer at the Naval or Royal Marine Depot to which any articles are sent (see below).

The usual Lost Luggage Charges must be made for articles of Naval and Royal Marine equipment, etc., found on the Railways. Each consignment must be entered with charges at the authorised Naval Stores rate.

The following to be substituted for paragraph 19:—

19. Military and Air Force Equipment.—Unclaimed articles of military equipment must be forwarded to Greenford Station for collection by the C.O.O. Greenford.

Articles of R.A.F. equipment must be sent to the Central Depository at the R.A.F. Station, Hornchurch, Essex.

Particulars on Form 2158 must be sent to the Lost Property Depot, Paddington, in all cases.

(G.A.19-10/48. C.S.—C.R./0.)

Unlabelled articles bearing an indication of the owner's Port Division must be disposed of as under —

Articles marked.		To be sent to:
*"C," "Ch," or "Ch.X"	.. ..	The Commodore, R.N. Barracks, Chatham.
*"P," "Po," or "Po.X"	.. ..	The Commodore, R.N. Barracks, Portsmouth.
*"D" or "Dev"	.. ..	The Commodore, R.N. Barracks, Devonport.
"L.T."	.. ..	The Commodore, Patrol Service Central Depot, Lowestoft.
"R"	.. ..	The Superintendent of Boom Defence, E.M. Boom Dept., Rosyth.
"F" or "FX"	.. ..	The Commodore, R.N. Barracks, Lee-on-Solent.
"T124" or "T124X"	.. ..	The Commanding Officer, H.M.S. "Mersey," Liverpool, 3.
"T124T"	.. ..	Commanding Officer, H.M. Rescue Tug "Minona," Campbeltown.
"R.M.E."	.. ..	The Commandant, Eastney Barracks, Southsea, Hants.
"Ply.," "Ply.X" or "Aux."	.. ..	The Commandant, R.M. Barracks, Plymouth.
"Ex."	.. ..	The Commandant, R.M. Depot, Lympstone, Devon.
"R.M.B." or "R.M.B.X."	.. ..	The Commandant, R.N. School of Music, Scarborough, Yorks.
"Depot"	.. ..	The Commandant, R.M. Holding Battalion, R.N. Barracks, Deal.

*—Articles of Royal Marine kit marked (1) "Ch.," or "Ch.X," (2) "Po.," or "Po.X," and (3) "D" must be sent to R.N. Barracks, Chatham, Portsmouth and Devonport respectively.

(G.A.12, 4 (3, C.R.1113.)

New Paragraph **29. Official Documents.**

Official Government Documents, Service Identity Documents, or Government Passes, found in trains or on the Company's property, should be dealt with as follows :—

Confidential official documents must be passed to the Civil Police.

Service Identity Documents must be passed to the R.T.O. (where available).

Government passes must be passed to the Establishment Officer of the Department concerned.

(G.A.15—12/44 C.R.)

STATION INSTRUCTIONS.

INSTRUCTIONS AS TO TRACING MISSING LUGGAGE AND PARCELS.

20. Enquiries for Missing Luggage. In all cases of complaint of loss of articles a full description of the extent of appearance with marks or initials and a general description of contents of the missing package must be obtained and registered at once. When luggage is concerned in addition to the particulars of the journey and train, it should be ascertained where the package was last seen and what persons and addresses including particulars of initials were at it. It should be shown that it was not found.

21. Steps to be Taken when Valuable Luggage Lost. Full particulars should be wired immediately to the Divisional Superintendent or District Traffic Manager and to the Lost Property Depot, Paddington. The Police Department must also be advised.

22. Periodical Enquiries at Hotels, Police Stations, Etc. Special enquiry for missing packages must be made at the hotels, police stations, steamship, dock and cartage offices, goods depots, etc., in the neighbourhood on the last weekday of each month, and a certificate sent to the Divisional Superintendent the day following that this has been done with full particulars of any articles which may have been found.

23. Articles Lost whilst in Passenger's Own Care. Enquiries for articles lost whilst in the passenger's own care should be made on the appropriate form and passengers must be informed that telegraphic enquiries can only be made at their expense unless the Company's staff are at fault.

24. Luggage Not Found Day Following Its Loss. If luggage is not found the day following its loss, Form No. 1 must be filled up and sent to the Divisional Superintendent or District Traffic Manager, with full particulars of the steps taken to recover the property, and the result.

A copy of Form No. 1 must also be sent to the forwarding station to ascertain whether the luggage has actually left the owner's place of residence and to find out if it has been deposited in any place. Enquiries must be made at the starting station with a view to ascertaining how the missing luggage was dealt with.

25. Enquiries for Missing Parcels.—Persons complaining of loss of parcels must be asked to give—

A full description of the package and list of contents.

Name and address of sender and date of dispatch.

Enquiries should be made immediately on enquiry note (Form No. 1) to the Lost Property Depot, Paddington, and other likely points.

If the enquiry is not successful within a week, full particulars must be sent to the Divisional Superintendent or District Traffic Manager.

26. Two or More Stations in One Town. Where there are two or more stations in a town, a statement of the enquiry should be made with the forwarding station, missing or found luggage.

27. Enquiries at Destination. Messages should be received by destination who should be carefully examined as to the loss of the parcel. If the parcel is found, the enquiry should be closed. If not found, the enquiry should be continued. If the parcel is found, the enquiry should be closed. If not found, the enquiry should be continued. If the parcel is found, the enquiry should be closed. If not found, the enquiry should be continued.

28. Stations Responsible for Enquiry. The destination station is responsible for making enquiry for missing parcels. If the enquiry is made at an intermediate or junction station, a note must be made and the destination station advised.

In the case of parcels for a particular destination, the forwarding station will be responsible for making enquiry. The forwarding station is responsible for tracing forward parcels, but this does not relieve destination station from the necessity for making enquiries at the Lost Property Office and other likely points when complaint is made by the consignee.

CONVEYANCE OF LETTERS BY TRAIN.

The attention of Station Masters and others concerned is specially called to the necessity of vigilance to prevent letters, not exempted from the exclusive privilege of His Majesty's Postmaster General as conferred by Act of Parliament being conveyed over the Railway otherwise than through the Post.

The only letters that may be conveyed by train are official communications relating to the business of the Company. Railway Letters, carried in connection with the Post Office, which must be signed by all concerned in the same manner as Insured Parcels, and certain exemptions, including the following:—

1. Letter or Despatch bags for which the Company make a contract charge for conveyance by train between Traders' Head Offices and their Branch Depots, Works, Collieries, etc.

2. Letters sent by Traders and Carting Agents to Station Agents, and to their own Agents at the different Stations, when relating to the disposal of goods carried, or about to be carried over the Line.

3. Letters or Invoices, sent by Traders with, and relating to, goods and which are to be delivered with the goods for the consignee.

STATION INSTRUCTIONS.

CONVEYANCE OF LETTERS BY TRAIN—Continued.

4. Letters sent to and from the Weighing Machine Contractors on matters relating to the Company's Machines.

5. Letters sent to and from the Company's Sack Contractors respecting the hire of Sacks for Grain, Seed or other traffic conveyed over the Line.

Contravention of the law upon this subject renders the offending person liable, in the event of prosecution, to heavy cumulative penalties, and may place the Company in a like position.

Instances have come under the notice of the Company of members of the Staff sending unauthorised communications by train, and making improper use of the staffery of the Company in connection therewith. If any Servant of the Company should become aware of irregularities of his class, or are being committed, it will be his duty to bring the matter to the immediate knowledge of his superior Officer.

FREIGHT CHARGES ON TRAFFIC CONVEYED IN CONNECTION WITH COMPANY'S NEW WORKS, Etc., LET TO CONTRACTORS.

Contractors materials, etc., passing in connection with contracts let by the Company to private firms, must be properly consigned and charged invoices issued.

Where a Contractor is given an "Occupation" of a section of the Line, a record must be taken of the materials moved from point to point.

At both forwarding and receiving points the staff concerned must exercise every precaution to ensure that traffic passing is duly recorded by consignment note and invoice in respect of all traffic either in Company's or Contractor's trucks and worked either by Company's or Contractor's engines.

Guards must record upon their journals particulars of traffic dealt with at Contractor's Sidings or Depots situated beyond the environments of Stations in connection with special "Contracts" or "Occupations", and the exact points between which the traffic was worked, the numbers and owners of the trucks conveyed, together with an approximate idea as to the quantity and contents thereof. A duplicate of this journal to be furnished to the District Goods Manager in whose district the traffic originated, who must satisfy himself that the traffic has been properly delivered and charged.

Agents in charge of Stations adjacent to these working Sidings or Depots must satisfy themselves as to the delivery of the arrangements concerned at the forwarding point, and if necessary, arrange for vehicles to be provided at the Siding or Depot to record the necessary particulars of all services which the Company may render in connection with such "Contract" or "Occupation" may be duly charged for.

G.P.O. and Railtex PARCELS.

1. Insured or Value Parcels, Railway Letters, carried in connection with the Post Office, and "News Intelligence" Packets and News Photographs for Reproduction, must always be sent registered for insurance provided for the purpose by the Post Office, and must be taken to the Parcel Offices at Stations, who will be responsible for forwarding the parcels and safe custody until they are transferred to the Guards of the trains by which they are intended to be forwarded.

If Porter, or other person, may be employed to deliver the parcels, they will also be held responsible for the parcels being taken to the forwarding station, and the parcels must be taken to the forwarding station by the G.P.O. way (H) to the Guards and obtain their signatures. Each parcel must be signed for separately.

For "Value" letters must be accompanied by a guide waybill and dealt with in the same way as value parcels.

The time of the forwarding station and the date must always be shown upon value envelopes and parcels, in order that a statement of date may be kept a proper record of receipt.

Guards must give special attention to the safe custody of such consignments, and wherever practicable lock them up. Before parting with them they must obtain the signature of a responsible official at the junction or destination station, in the case of traffic for other companies' lines or steamers, at the point where the traffic is handed over.

2. Ordinary Parcels. All cases of damage, loose packing or appearance of loss must be noted by Guards in their Train Books, entered on the waybill when such are issued, and particulars reported to the Divisional Superintendent on Form 734. Before handing such parcels to Parcel Porters, attention must be called to their condition, and Parcel Porters must note all particulars in case of claim or enquiry.

3. Parcels handed out of Trains by Passengers or Parcels handed into Trains to Passengers. Passengers carrying parcels or market goods and handing them out of the trains to the Company's Staff or others, with instructions to deliver them to the consignees, or to await a signal for delivery at the Station, must be charged for such parcels at the ordinary rate applicable to all traffic which is not company's. In the same manner parcels or market goods handed into passengers at intermediate stations must be charged for.

4. Handling of Parcel Traffic. Complaints are frequently received of damage in transit to parcels and perishable traffic due to lack of care in handling.

All concerned must see that traffic is dealt with carefully and Staff at transfer points and destination stations are specially reminded of the necessity for keeping traffic off wet platforms and out of the rain.

5. Newspapers for Use of Staff. Small parcels of newspapers sent by Messrs. Wymann & Sons to stations for use of Staff must be stamped.

BULLION, SPECIE AND ARTICLES OF EXCEPTIONAL VALUE BY PASSENGER TRAIN.

These instructions do not apply to traffic conveyed in Bullion Vans, in respect of which special instructions are in operation.

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CONDITIONS OF ACCEPTANCE.

Consignments of Bullion, Specie and other articles included in the Carriers' Act known to be of greater value than £25 must not be accepted for conveyance unless :—

1. The value is declared and the Executive's charge for insurance is paid :

or

2. The sender fills in and signs the special Consignment Note and Declaration (Form 894).

Station Masters and others at forwarding stations are reminded that, in the exercise of their functions, they consider it desirable to inspect consignments before insurance to ascertain if the goods are in accordance with the declaration, in good condition and well packed, they have the right to do so.

Reasonable notice is required before acceptance of any consignment declared to be of a value exceeding £500. Station Masters or other persons in charge must immediately communicate (by telephone if necessary) full particulars to the Divisional Superintendent or District Traffic Manager when advice is received of such consignments, whether insured or not.

The conditions laid down under Regulations 64, 82, 83 and 84, Coaching Arrangements Book January, 1935, edition) must also be borne in mind.

—INSURED CONSIGNMENTS EXCEEDING £1,000 IN VALUE.

Arrangements must be made, unless otherwise specially agreed, for a responsible caretaker to accompany insured consignments exceeding £1,000 in value from the starting point, or transfer station if from another Region, until delivered to the consignee, or, if for another Region, until transfer to that Region has taken place.

The caretaker must travel with the guard in the brake van, and during the whole journey keep the consignments under his personal care. Before parting with them he must obtain the signature of the consignee or of a responsible official of the Region taking forward.

Transfer points and destination stations must be advised by telephone or telegram, so that any assistance necessary may be given to the caretaker.

Note.—In quoting services preference should be given to express trains, regard being also had to the desirability of arranging an early return home for the caretaker.

—INSURED CONSIGNMENTS OF £1,000 IN VALUE OR LESS, OR UNINSURED CONSIGNMENTS OVER £25 IN VALUE IN RESPECT OF WHICH FORM 894 IS SIGNED.

Insured Consignments of £1,000 or less in value and uninsured consignments in respect of which the special Consignment Note and Declaration (Form 894) has been signed should, as far as possible, be sent by express trains, and must be handed to the guard personally, and his signature obtained. But a caretaker in ordinary circumstances need not be provided. Guards must give special attention to the safe custody of such consignments, and wherever practicable lock them up. Before parting with them they must obtain the signature of a responsible official at the junction or destination station, or, in the case of traffic for other Regions, at the point where the traffic is handed over.

Transfer points, destination stations, and such intermediate stations as considered necessary, must be advised by telephone or telegram, and steps taken to protect the traffic at such stations.

—TRAFFIC FROM OR TO OTHER REGIONS.

Where consignments are handed to the Western Region by employees of other Regions for transfer to or over the Western Region, similar precautions must be taken from the transfer point to destination, or to the point of transfer with the Region taking forward.

5.—SAFE CUSTODY AT FORWARDING, TRANSFER AND RECEIVING STATIONS.

During the time valuable consignments are at forwarding, transfer or destination stations they must be placed in the custody of a trustworthy person, who will be held responsible for their safety.

Upon receipt of the advice from the sending station the receiving station must advise the consignees in order to arrange delivery immediately on arrival.

Where such consignments have to be kept on hand over-night they should be locked in the safe, the night staff (where employed) told to keep special observation, and the police warned when considered necessary. The Divisional Superintendent or District Traffic Manager must be consulted in case of difficulty.

Where consignments are tendered for Banks and other business premises which would in all probability be closed at the time of arrival at destination senders should be asked to communicate with consignees so that if practicable someone may be available to receive and sign for the traffic.

6.—SEA TRANSIT.

FISHGUARD AND ROSSLARE, AND FISHGUARD AND WATERFORD.

Valuable consignments for transit to Eire via Ross are must be handed over by a trustworthy person to the ticket collector of the steamer, who must immediately lock them up in the Purser's safe, and report particulars to the Chief Officer. The latter official will be responsible for seeing that such consignments are duly cleared, and on arrival at Rosslare the ticket collector must, before parting with them, obtain a signature from the Official to whom they are handed.

In the case of the direct Waterford service such consignments must be handed to the Chief Officer of the vessel, who, after giving a signature in acknowledgment, will place them in the ship's safe, and not part with them at the Irish port until he has obtained the signature of the Official to whom he hands them.

Consignments from Eire to England must be similarly safeguarded.

It will not be necessary for a Western Region caretaker (where one is provided) in charge of a consignment to Eire to travel beyond Fishguard, but he must personally place the consignment in the hands of the proper ship's officer and obtain his signature.

CHANNEL ISLANDS.

Consignments of bullion, specie, value parcels or articles of exceptional value must be stowed in the ship's Parcel Room, by order of the supervision of the Supercargo. Only small consignments consisting of a few boxes or packages can be stowed, and in the event of a consignment of any magnitude being presented for transport, prior advice must be sent to the Quay Superintendent, Weymouth Quay, by telephone or telegram, in order that such special arrangements as may be necessary can be made.

This Parcel Room is in charge of the Supercargo who must retain the keys in his custody and must personally supervise the stowing or landing of any of the articles enumerated above. The duty must not be delegated to another person.

Value parcels, etc., from shore must be signed for by the Supercargo on board ship, and in the reverse direction, by the person ashore authorised to receive and deal with them.

Mail Bags and value parcels are in all cases to be examined on receipt to verify the fact that the seals are intact.

It will not be necessary for a Western Region caretaker (where one is provided) in charge of a consignment to the Channel Islands to travel beyond Weymouth, but he must personally place the consignment in the hands of the Supercargo and obtain his signature.

7.—PRECAUTIONS AT DESTINATION.

Where it is observed that packages appear to be in loose condition or in any case where considered desirable the caretaker or a responsible local representative of the Executive should be present at the unpacking of insured articles.

(G A 19—10 48, C S—F 73278 181 H)



STATION INSTRUCTIONS.

CHARGING OF PARCELS TRAFFIC.

Charges on Parcel Traffic. Miscellaneous Traffic, with certain exceptions, the case of Fish Traffic, parcels on Swindon and Reading, and Live Stock from sales, must be prepaid, and except in the case of parcels which are charged to be sent, the charges must be accounted for by means of the proper receipts, and before being moved to the traffic, must be properly cancelled by means of the rubber cancelling stamp supplied for the purpose.

In the case of Ice, Sacks and Baskets covered with canvas upon which stamps will not adhere, tie-on labels must be used to which the stamps can be affixed.

Parcel stamps represent cash, and every one must be taken to ensure their safe custody.

All persons handling stamped traffic are required to notice whether the stamps are properly cancelled. In cases where this has been neglected, stamps are to be cancelled at once, and the irregularity reported to the Divisional Superintendent or District Traffic Manager, giving the number of the stamp and the name of station at fault.

All Traffic must be recorded by the Forwarding Station before despatch, and by the Receiving Station before delivery.

Receiving Stations must be careful to see that the stamps affixed represent the correct carriage charges from the Station from which the goods are sent, and must be made with regard to New Rates, and also that the stamps are correctly taken down on arrival of the train.

Parcels of various kinds, such as those of the following description, are stamped with a rubber stamp "Ledger Account."

Miscellaneous Traffic, such as parcels, boxes, etc., from London, Liverpool, Manchester, etc., to Swindon, and from Swindon to London, Liverpool, Manchester, etc., are stamped with a rubber stamp "Parcel Stamps not affixed, except in the case of parcels sent from the following stations, and the Staff of Forwarding and Receiving Stations must be careful to see that the stamps are duly sent forward with the traffic.

Label "A" must be attached to all consignments to which stamps showing Sending Station and route are not affixed.

All Traffic should be clearly addressed and bear the name of the Station to which it is intended to be conveyed. If a parcel is addressed to a place at which there is no railway station, the sender must be requested to give the name of the station to which it is to be forwarded.

The attention of all concerned is drawn to the necessity for seeing that all Parcel Stamps are properly cancelled. Parcel Handlers, Receiving Clerks, and other persons before they are despatched, and also the Staff of Forwarding, Parcel Porters and others concerned are particularly requested to give this matter special attention.

Ledger Labeled Traffic.—In the case of certain traffic, special Ledger Labels are used in connection with the parcels. If parcels are received bearing Ledger Labels for a particular station, the particulars must be reported to the Divisional Superintendent or District Traffic Manager.

LIGHT TARPAULIN SHEETS FOR PROTECTION OF PARCELS ON PLATFORMS.

Light Tarpaulin Sheets are supplied to various stations for the purpose of protecting parcels from inclement weather when being conveyed on barrows to and from the trains.

It should be taken to see the sheets are kept in a ready state of repair for immediate use, and that they are properly used for the purpose for which they are provided. When not in use the sheets must be carefully folded and kept in the Parcels Office.

PLATFORM TROLLIES FORWARDED TO SWINDON STORES.

When platform trollies are forwarded to Swindon Stores an advice must be sent to the Stores Superintendent stating the number and type of trollies forwarded, reason for return, date and train despatched, and the number of the truck into which loaded.

A label must be attached to each trolley showing sending Station and reason for returning to Swindon, also a memo-waybill or invoice must accompany all trollies.

MILK TRAFFIC.

Having regard to the highly perishable nature of this commodity and the severe road conditions now obtaining, it is the Company's policy that Milk Traffic should be afforded the best possible facilities, special attention being paid to the working of natural empty churns. If any difficulties are experienced in this connection, the Divisional Superintendent or District Traffic Manager must be notified.

When churns are received at a station, they should be either taken direct to a Milk Van, or when one is provided, or placed, as far as possible, upon trolleys, which should be placed in a position on the platform as nearly as can be judged, opposite the spot where the churns are to be loaded with milk. In the event of there not being a sufficient number of trolleys, the churns must be placed on the platform, in such a position as will cause the least delay in loading into the Milk Van. In either case the milk should be placed as far as possible from the platform.

Care must be taken to stand the trollies so that they will be well clear of the open platform during and outgoing Trains.

STATION INSTRUCTIONS.

MILK TRAFFIC—Continued.

In loading Milk great care must be taken to keep together in one part of the van all churns from one Station, so that the Staff at the receiving and transfer Stations may be in a position to re-load and unload the Churns.

In accordance with the conditions upon which Milk is conveyed at the reduced rate at Owners' Risk, the men in the employ of the senders and consignees of the Milk must assist in the loading and unloading of the churns.

In the event of the van accommodation provided in the Train for the conveyance of Milk being considered by the Station Staff to be at any time insufficient or if the Milk to be dispatched from the Station is sufficient to fully load a Milk Truck, the attention of the Divisional Superintendent and the Traffic Manager must at once be directed to the matter, so that revised arrangements may be made if necessary.

It is found that "Through" milk vehicles are often not fully labelled on Both Sides, with the result that delays and wrong loading arise, causing the milk being loaded on wrong vehicles, and the much to station. The fact that stations depotting "Through" milk vehicles must be carefully observed, that these are fully labelled on both sides, and any omission in this respect must be observed by the staff at stations where the vehicle must be immediately reported to the Divisional Superintendent.

Cases have come under notice where milk churns not bearing the Senders' label or a message are being forwarded, and considerable inconvenience has arisen. Consequently, the Staff at all stations must see that all churns are properly labelled, and the Senders' attention must be specially drawn to cases where this is not done, and to any case where milk is consigned to a destination without bearing a Consignee's name-plate for another.

Instances of mislabelling of vehicles and light loading of milk trucks have been observed, and great care must be exercised by all to avoid such cases occurring.

Trucks used for the conveyance of First Class passengers must not be used for the conveyance of Milk Traffic. Milk Trucks should be kept in regular working for Milk Traffic only.

Station Masters, Porters, and others are responsible for seeing that Milk Trucks are properly cleaned before being loaded with Milk Traffic.

Other descriptions of traffic must not be loaded on top of the Milk Churns.

It is essential that the goods for a given destination shall be put in the same vehicle with the milk, so that the staff can apply to the milk wags, or to the staff at the destination, to ensure the most important particulars being shown.

Milk traffic must not be taken from Senders' consignments with any other goods.

A milk wagon must be serially and progressively numbered, a fresh number being commenced each month.

It is of the utmost importance that milk wags be forwarded by the same train as the milk, which the sender and forwarders must see that the wags are correctly labelled, and that they are only put out with the milk at transfer points and also that they are not put under the wheels of the churns.

Complaints are frequently made that Milk is short in quantity at destination stations, and must therefore be checked before delivery. It is essential to see that the quantity is correct, and to allow for the weight of the contents of the churns, and any shortage must be made up by putting out to Sender so that the necessary alteration may be made at the consignment note before the milk is forwarded. In the case of scaled churns the weight of the churn must be clearly stamped on the label, and the contents may be arrived at by working out the being allowed for each Imperial gallon of milk, plus the weight of the churn.

Should a complaint be made in respect of any particular consignment, that Sender's traffic must be specially tested daily for a couple of weeks, and the result noted for reference.

Unclean and empty milk churns must not be allowed to stand about at the stations without any effort being made to find a owner. When found or found a yard or farm, or put out at station in error, immediate steps must be taken to ascertain the owner, or the correct destination station, and the empty churns must be at once disposed of accordingly.

Milk churns must be examined to see that they are in good order before they are accepted for conveyance, so as to prevent possible injury to the staff owing to cans with defective handles, &c.

INSTRUCTIONS TO BE OBSERVED IN DEALING WITH THE RECEIPT, LABELLING, FORWARDING AND DELIVERY OF PASSENGERS' LUGGAGE, ALSO WITH LOST AND FOUND LUGGAGE AND PARCELS.

4A12

Receipt, Forwarding and Delivery of Luggage.

Acceptance and Labelling of Luggage.

1. Passengers are required to attach securely to each article of luggage a label with the owner's name and destination clearly written thereon, and to see it properly labelled by the Company's staff. No article must be accepted unless this condition is complied with.

All luggage other than such hand luggage as may be retained by the passengers under their own care, must be labelled to the station to which tickets have been taken by the staff appointed for this work.

STATION INSTRUCTIONS.

RECEIPT, LABELLING, FORWARDING AND DELIVERY OF PASSENGERS' LUGGAGE, ETC. *Continued.*

In all cases where there are alternative routes, the label must show by which route the passenger is travelling. Before affixing the new label, all old labels, including address labels, must be removed or unmistakably defaced. Special care should be taken in this respect in dealing with commercial travellers' luggage.

2. Duplicate labels, which must always be used, are provided for sailors' luggage, Sailors' Luggage, one part being given to the passenger, and the other tied to the article.

3. Cycles must not be accepted unless plainly addressed, and in order to meet any cases where passengers have not provided themselves with labels, a supply must be kept by the luggage labellers.

4. THE COMPANY'S STAFF ARE NOT PERMITTED UNDER ANY CIRCUMSTANCES TO TAKE CHARGE OF LUGGAGE FOR ANY PASSENGER EXCEPT IN THE CASE OF TAKE-UP LUGGAGE, OR AT ONCE PLACING IT IN A COMPARTMENT, ON A CAB OR OTHER VEHICLE, IN CHARGE OF THE OWNER, OR DEPOSITING IT IN THE CLOAK ROOM. WHEN LUGGAGE HAS TO BE DEPOSITED IN THE CLOAK ROOM, THE OWNER MUST BE REQUESTED TO ACCOMPANY IT AND RECEIVE A CLOAK ROOM TICKET.

5. Care must be exercised to ensure luggage being stowed in the prescribed positions, and Guards are required to see that it is placed ready for unloading at the proper stations.

6. Before luggage is handed over at the destination station, the staff must require passengers to identify their property, and when engaging cabs must not place anything in or on the vehicle, without first noting the number or being otherwise able to identify it, unless the owner is in attendance to see personally to the safety of his or her property.

7. Tickets for cycles, perambulators, mail carts, &c., and the passenger's portion of the duplicate label used for sailors' or registered luggage must be collected before the articles are given up.

Guards should in all cases be advised of unclaimed articles handed out of trains by passengers, or taken out by the Company's servants, and should make a note of the fact. A signature must be given for articles found in Restaurant Cars and handed to the Traffic Department staff, and for articles handed to the Lost Property Office by guards or porters.

The Guard's attention should be directed by the Platform staff to luggage out of repair or in any way damaged.

On arrival of trains at terminal stations, the Guards must ascertain whether the whole of the luggage is claimed or not; and call the attention of the Station Master or person in charge to any irregularity, luggage out of repair, or in any way damaged.

8. Any luggage not claimed must be taken immediately to the Lost Property Office, and full particulars of the train by which it was received, &c., recorded.

REGULATIONS BEARING UPON CLAIMS ON COACHING TRAFFIC.
DISPOSAL OF CONSIGNMENTS, SALVAGE, ETC.

1. When consignments are lost, pilfered, or damaged, the destination station must advise the sending station immediately. The receipt of a consignment, the sending station must make the necessary inquiries forthwith, but the destination station must also make inquiries as may be necessary.

2. A note must be made and initialed by the writer on consignment notes or waybills, where such a note is given in case of pilferage, loss or damage, etc., coming under the notice of the staff, and, where known, the cause must be given. When made on waybills, the name of the station where the loss or damage is discovered must be shown.

At stations, transfer or otherwise, where discovery of loss, pilferage or damage is made, the facts must be reported to the Station Master or other person in charge of the Parcels Department, who must at once advise the forwarding and destination stations.

All such cases discovered by Guards or Travelling Ticket Collectors, either on the journey or at stations, should be reported on the special "Bad Order" form, No. 534, which must be attached to the journal.

3. In cases of serious damage to valuable traffic conveyed at Company's risk, the services of an expert should be obtained at once, so that reliable information may be obtained of the nature and extent of the damage, and the approximate amount of liability, if any.

4. The instructions as to the weighing of traffic at both the forwarding* and receiving stations must be strictly carried out. Particular care must be exercised in weighing packages of tobacco, butter, sausages, rabbits, or other commodities specially liable to loss by pilferage.

* A Receiving Office where parcels are booked to be regarded as a station

STATION INSTRUCTIONS

CLAIMS ON COACHING TRAFFIC, ETC. *Continued.*

Notice or receipt
of . . .

5. When notice of claim is given, or a claim is made, the Station Master must, when practicable, inspect the consignment, check the quantity, ascertain if the packing was sufficient and proper, and the extent of damage or loss, and see that the amount claimed is reasonable. The invoice from the sender to the customer should be inspected where possible.

Liability not to
be admitted

6. In the course of preliminary inquiry, or upon receipt of a claim, no admission must be made as to the Company's liability, nor until proper inquiries have been made and the Company's position determined.

Salvage

7. When a claim is made for the full value of a damaged consignment and the salvage is said to be worthless, claimant should be requested not to destroy or otherwise dispose of it, as, in the event of the Company admitting liability, they may require to take possession of the salvage.

Responsibility for
dealing with
Claims.

8. A claim made at either the original forwarding or the destination station must be dealt with by the station receiving it. Claims made upon another Company in which this Company is intermediate must be dealt with by the station where the transfer is made to this Company.

In the case of traffic from a foreign line to a Great Western station, the claim should be dealt with by the destination station.

Advice to Police
Department.

9. The Company's Police Department must be advised of all cases of total loss, or pilferage as laid down in the General Manager's Circular No. 2,696, dated 1st May, 1922.

Inquiry into
Claims

10. Claims must be inquired into immediately by means of Report Form (3476), and otherwise as may be necessary. The following points must be specially observed :—

(a) The checking of delivery sheets for proof of delivery (including the number of parcels for each consignee) by a competent and responsible person.

(b) "No trace" replies must be signed by a responsible person.

(c) Possible misdeliveries to branches of Co-operative Stores, multiple shops, or other wrong consignees, must be looked for.

Both the FORWARDING and the RECEIVING stations must fully and accurately record on the form the facts relating to the transaction. Special care must be taken to state the nature, extent and cause of the damage or loss (if known), and other grounds of claim (if any), also whether the amount is reasonable. Any other information likely to be useful should also be given, with an expression of opinion from the ascertained facts by the Station Master or Chief Parcels Clerk.

In cases of delay, the proper time of delivery and the actual time must be given on the report. In cases of pilferage, particulars of any evidence of theft and the time of delivery and time at which complaint is made must be furnished. The date of first complaint must in all cases be shown in the space provided on the claims form.

In the case of claims upon Milk traffic, it should be stated whether the churns were plated or labelled in accordance with the conditions of carriage. Where labels are used a description of the label must be given.

Claims made by
Managers of
Branch shops.

11. Claims received from Managers of Branches of multiple shop firms should be settled through the Head Offices of the Firms, and not through the Branch Managers.

Claims to
be dealt with
promptly.

12. Prompt dealing with claims is essential, not only to enable the facts of a case to be ascertained, but to avoid annoyance to traders. If any difficulty is encountered in obtaining quick replies from stations, the Divisional Superintendent or District Traffic Manager should be advised without delay.

Rates to be
quoted in claim
reports

13. When reporting claims on traffic consigned at Owner's risk, or by traders who have signed a General Request for the traffic to be conveyed at the Owner's risk, the O.R. and C.R. rates must be shown in the report, and any discrepancy between the O.R. and the rate charged must be explained.

Prevention of
Damage, etc.

14. It is of the first importance to traders and the Company that traffic should be carried free from damage, loss, etc., as far as possible. In all cases the causes of damage, loss, etc., such as improper handling, opportunities for theft, or pilferage, etc., must be thoroughly investigated, and statements obtained from all concerned, and such measures as may be necessary to remove the causes promptly taken. In the event of claims on any particular traffic, or from certain traders, being numerous and out of proportion to the receipts, or of repeated cases of loss or pilferage on particular sections of the line, special representations should be made to the Divisional Superintendent or District Traffic Manager.

STATION INSTRUCTIONS.

CLAIMS ON COACHING TRAFFIC, ETC.—Continued.

15. Subject to limitations mentioned hereafter, Station Masters are authorised to settle claims on behalf of the Company, including FORTY POUNDS (£40), made on traffic conveyed at Company's risk, where, after exhaustive inquiry, there is no doubt:—

- (1) as to the liability of the Company;
- (2) of the reasonableness of the claim; and
- (3) that no vital principle is involved.

Station Masters to settle claims including £22.

16. This authority does not extend to:—

- (a) Claims on traffic charged and conveyed at Owner's risk rate—such claims having to be submitted to the Joint Claims Committee for decision.
- (b) Claims on articles and goods comprised in the Carriers Act, 1830, viz. — Gold or Silver Coin, Gold or Silver in a manufactured or unmanufactured state, Precious Stones, Jewellery, Watches, Clocks, or Timepieces of any description, Trinkets, Bills, Bank-notes, Orders, Notes, or Securities for Payment of Money, English or Foreign Stamps, Maps, Writings, Title Deeds, Paintings, Engravings, Pictures, Gold or Silver Plate or Plated Articles, Glass, China, Furs, or Lace.
- (c) Claims in respect of street or other accidents involving personal injury or damage to private property.
- (d) Claims upon Horses, Cattle, and other traffic, of which the Company are not common carriers by passenger train.

Limitations of Station Masters' authority.

17. Immediately a claim is paid, a Compensation Voucher (202) is to be prepared by the Station Master, and sent, with all papers, to the Divisional Superintendent or District Traffic Manager for signature, one copy being retained by the latter as a record.

Compensation Vouchers

In cases where claims are sent by one Divisional Superintendent or District Traffic Manager to another for settlement, the Divisional Superintendent or District Traffic Manager paying the claim must prepare the Compensation voucher.

In cases where claims are sent by one Divisional Superintendent to a station in another Division for settlement, the station paying the claim must prepare the compensation voucher and send it to the Superintendent from whom the claim was received for certification.

18. Divisional Superintendents or District Traffic Managers are authorised to settle claims on coaching traffic, Local and Through, conveyed at CR rates, and claims arising out of street accidents not involving personal injury, provided that a liability of the Company or Companies interested is clearly established, and (b) that no vital question of principle is involved, and (c) that the amount paid does not exceed TWENTY-FIVE POUNDS (£25).

Divisional Superintendents to settle claims on coaching traffic, Local and Through, conveyed at CR rates, and claims arising out of street accidents not involving personal injury, provided that a liability of the Company or Companies interested is clearly established, and (b) that no vital question of principle is involved, and (c) that the amount paid does not exceed TWENTY-FIVE POUNDS (£25).

The following claims must be reported by the Divisional Superintendent or District Traffic Manager as shown:—

at of

1. To the Commercial Superintendent.

- (a) Claims for payment of sums exceeding TWENTY-FIVE POUNDS (£25);
- (b) Claims for submission to the Claims Committee.

and

or when the latter is taken credit for through the Daily Cash Account (110).

Voucher to be sent to Cash Account (110)

22. A Return of Local Traffic Compensation to be prepared on Form 257 and forwarded by the Divisional Superintendent or District Traffic Manager to the Superintendent of the Line not later than the 8th day of each month.

Local Traffic Compensation Return of compensation.

23. *Perishable Traffic.* Efforts should in all cases be made to induce Consignees to accept traffic which has been delayed, damaged, or pilfered in transit, but if negotiations fail and senders' instructions cannot be obtained, traffic of an unimportant character should be sold to the best advantage under the personal sanction of the Station Master.

Station Master

Perishable traffic must not be sold to the Company's staff if better prices can be obtained elsewhere.

Monthly Return of compensation.

1. The following claims must be reported by the Divisional Superintendent or District Traffic Manager as shewn :—

1 To the Commercial Superintendent.

- (a) Claims for payment of sums exceeding TWENTY-FIVE POUNDS (£25) ;
- (b) Claims for submission to the Claims Committee ;
- (c) Claims in respect of Cattle, Sheep and other animals killed or injured whilst trespassing on the line ;
- (d) Claims raising questions of principle ; and
- (e) Claims on Livestock, and on insured Carrier's Act traffic.

2 To the Operating Superintendent

- (i) Claims in respect of street or other accidents involving personal injury ;
- (i) Claims arising out of collisions between locomotives or rolling stock and privately owned vehicles.

(G A 23—7 49. C.S —C.P.)

STATION INSTRUCTIONS.

CLAIMS ON COACHING TRAFFIC, ETC.—*Continued.*

Before large or valuable consignments of perishable traffic are disposed of the circumstances must be telegraphed to the Divisional Superintendent or District Traffic Manager whenever possible and authority obtained as to the disposal of the traffic.

Particular attention is drawn to the Standard Terms and Conditions on this point.

Other Traffic—Not Perishable.—When traffic not of a perishable nature is refused by the Consignee the Sender must be advised, and if no instructions can be obtained, notice should be given sender and consignee that it remains on hand at their sole risk and the facts reported to the Divisional Superintendent or District Traffic Manager. If there are special features in connection with its manufacture or nature which render it desirable that it should be disposed of locally in the district concerned, arrangements may be made to sell the same under the authority of the Divisional Superintendent or District Traffic Manager, the Assistant Superintendent or Manager, or, in their absence, the Chief Clerk.

Whole proceeds of Sales to be paid in.

24. The proceeds of all sales by or on behalf of the Company must be paid in on the day of receipt through the Daily Cash Account (110), to the credit of Parcels Compensation, no part thereof to be applied to the reduction of claims.

Compensation Vouchers must be issued for the full amounts paid in settlement of claims, and reference must be made thereon to the date of the Cash Account (110) in which the amount of the salvage credit appears.

Proceeds of Sales to be advised monthly.

25. Particulars of the amounts paid in, through the Cash Account, to the credit of Parcels Compensation, must be forwarded by the Station Master to the Divisional Superintendent or District Traffic Manager as they arise and a copy of the advice sent to the Police Department, Paddington. Divisional Superintendents must advise the Chief Accountant of such amounts at the close of each month.

REGULATIONS BEARING UPON CLAIMS ON MERCHANDISE TRAFFIC,
DISPOSAL OF REFUSED GOODS, SALVAGE, ETC.

Acceptance of Traffic from Traders and others.

1. Persons receiving traffic from traders, the public generally, or from other Rail or Road Carriers, must carefully examine same at time of tender. If any packages are not in proper state, the attention of the sender, or carriers or their representatives must be called to their condition. If the defect be not remedied, the goods must be signed for with a qualifying remark describing the state of the goods, and a similar remark must be made upon the consignment note. When packages are repaired, record must be made when signing for the goods and on the consignment note, thus: "Package repaired, condition of contents not known."

When Goods are Lost.

2. When goods are lost, the station to which they are invoiced must so advise the station at which the goods were last transhipped or transferred as well as the invoicing station immediately. On receipt of such advice, the last station from which the goods are alleged to have been forwarded (whether it be the transfer or original forwarding station) must make the necessary inquiries for the missing goods.

NOTE.—(i) See R.C.H. Regulation re Reporting the Loss of Goods within 24 hours after the loss has been ascertained.

(ii) See General Manager's Circular 2696, of 1st May, 1932, re Reporting Goods Stolen to Special Police Department.

When Goods are Damaged or Pilfered.

3. Checkers, Loaders, and all others concerned must record upon the consignment notes or invoices, and in their note books, all cases of Pilferage, Loss or Damage, etc., and the cause of the damage (whether due to pilferage, bad loading, improper shunting, or as the case may be), in such manner as to convey a correct idea of what was amiss; the remarks must be initialled by the writer, and, when made on invoices, the name of the station where made must be written against them.

4. All cases of Damage or Pilferage of serious nature, or of Bad Loading, must be reported to the forwarding and last transfer stations.

5. In cases of serious damage to Goods, the services of one of the Company's experts should be obtained immediately, so that whilst the facts are fresh the expert may obtain such evidence as may clear the Company from liability or enable him to advise what ought to be done so as to reduce the monetary loss to whomsoever may be concerned and also to suggest, when possible, measures for preventing similar damage.

STATION INSTRUCTIONS.

CLAIMS ON MERCHANDISE TRAFFIC, ETC.—Continued.

6. Damages of comparatively unimportant character need not be reported to either the forwarding or transfer station, whether local or foreign, excepting when the Manchester Ship Canal Company is concerned, which Company must be advised of all damages (excepting those only discovered by consignees after delivery) within 24 hours after they have been ascertained (vide R.C.H. Regulation, 158, as amended July, 1930).

7. Packages of Wines, Spirits, Cigarette, Tobacco, Tea, Silks, Boots, Shoes or other goods specially liable to loss by Pilferage, Leakage, etc., must be accurately weighed at both forwarding and receiving stations, and in the case of "Through" traffic, at stations of transfer from one Company to another also. In many cases, the weight is the only means of determining whether loss has taken place in transit.

8. In the event of loss by leakage or destruction of wines, spirits, tobacco, or other datable merchandise, the facts must be at once reported to the nearest Inland Revenue Officer, so that he may testify as to the circumstances and loss, otherwise the Company will be unable to obtain a certificate of the duty. When a leakage is discovered, within a wagon, lorry, or other vehicle, the vehicle concerned must be detained for the Officer's inspection, in order that he may satisfy himself in regard to any evidence which may exist as to the extent of the leakage. In the absence of an Inland Revenue Officer, a Railway Police Officer or, if one is not available, a Public Police Officer, should be called in as a witness. The case must also be reported immediately to headquarters. The Agent or person in charge must take prompt steps to prevent further loss, and to make certain that no part of the merchandise passes into consumption. The names of all persons present when such damages are discovered must be recorded, so that, if required, their evidence may be obtained.

NOTE.—See the General Manager's Circular 2696, 1st May, 1922, re Reporting Pilferages to Special Police Department.

Method of dealing with Goods Compensation Claims.

9. When notice of damage or loss is received, the Goods Agent or Station Master must, by every practicable means, ascertain the nature and extent of the damage or loss, and take every reasonable means to see that the claim is correctly stated, that the amount claimed is in accordance with the terms of sender's invoice to his customer, and record particulars on Goods Inspection Report (Form 5198).

10. With respect to the merchandise and receiving the account for Inland or loss, the Company must be made that the Company's position can be determined.

11. When a claim represents a full value of damage to merchandise, an liability is to be worth, the claim should be full not to destroy or otherwise dispose of the goods, as in the event of the Company admitting liability, they may require to take possession of the salvage.

12. A Station receiving a claim if it be either the original Forwarding, or the RECEIVING (i.e.—destination) Station, must deal with it.

13. When a claim is received, it must be enquired into immediately by means of the approved Goods Claims Report Form (1396) and otherwise as may be necessary.

14. Both the Forwarding and Receiving Stations must fully and accurately ascertain and record on the form the facts relating to the transaction, and special care must be taken to state the nature, extent, and particularly the cause of the damage, loss, or of whatever the claim may be for, whether the amount is reasonable or not, and any other useful information; and reference to the correspondence must be made upon the original invoice and upon the copy thereof.

15. Subject to the exceptions recorded in the Note to this Clause, when dealing with Net Claims (i.e.—Claims less salvage or other credits) not exceeding £7 for Damage, Pilferage, or Partial Loss (i.e.—Loss of part of the contents of a package) on "THROUGH" TRAFFIC forwarded to or received from Stations on Railways in Great Britain, it will not be necessary to communicate with Intermediate Stations or Companies concerning such claims; it will only be necessary for the Terminal Station or Company to communicate with the other Terminal Station or Company to ascertain the facts and determine the extent of the Carrier's liability (if any) in reference thereto, because all such compensation payments not exceeding £7 will be scheduled for through mileage division irrespective of the point at which the damage, pilferage, or partial loss arose.

NOTE. Claims arising from accidents to trains, or fire, or claims in which Railways in Ireland, and Steamship Companies are concerned are excepted from this agreement.

STATION INSTRUCTIONS.

CLAIMS ON MERCHANDISE TRAFFIC, ETC.—*Continued.*

When dealing with claims on "Through" traffic, other than those referred to in the preceding paragraph, it will be necessary for the Station or Goods Manager dealing with the claims to communicate with the Intermediate Stations and to act according to the requirements of each case as heretofore.

16. Promptness in dealing with claims being essential to the obtainment of reliable information and the furtherance of the Company's interests with traders, if any difficulty is encountered in obtaining prompt replies from stations, the attention of your District Goods Manager must be directed to the irregularity.

Rates to be
quoted when re-
porting claims
Prevention of
Claims.

17. When reporting claims on traffic consigned for conveyance at Owners' Risk, the O.R. rate, C.R. rate and rate charged must be quoted in the report.

18. It is of the highest importance to traders and the Company that goods should be carried as free from damage, loss, etc., as possible; that the cause of damage, loss, etc., should be duly and accurately recorded; that such measures as are necessary to prevent the recurrence of those causes should be promptly taken; and that whenever it is found that claims on any particular traffic or from certain traders are numerous, and the percentage of claims to the receipts on the traffic is high, special representations should be made to your superior officer.

Powers of Goods
Agents and
Station Masters
in reference to
settlement of
Claims.

19. Subject to certain limitations, mentioned in the "Notes" below (see Clause 2), Goods Agents and Station Masters are authorised to settle claims up to £100 (Shillings) in value on Goods, etc., CONVEYED AT CARRIERS' RISK in cases where, after full enquiry has been made, there is no doubt as to the liability of the Company or Companies interested, or of the reasonableness of the claims, and that no principle is involved.

Care must be used in the exercise of this authority, as if found to have been mis-used it may be withdrawn.

20. Immediately the settlement of a claim is made, the whole of the papers, including the claim and receipt together with the report of the facts, must be sent by Letter to District Stations to the Chief Goods Manager and by other Stations to their District Managers who, if satisfied, will issue compensation vouchers for the amount paid. Those Stations being authorised so to do will issue and send compensation vouchers with the papers.

NOTES.—THIS AUTHORITY IS SUBJECT TO THE FOLLOWING RESERVATIONS:—

(a) *Railway Clearing House Regulation 159, 1926 Edition, stipulates, in reference to all claims on Goods Traffic forwarded from G.W.R. STATIONS to Stations on other Railways, that the concurrence of the other Terminal Company to the proposed settlement shall be obtained first.*

(b) (i) *Claims on Owner's Risk traffic and claims on goods not properly protected by packing, whether or not the consignment note is so endorsed.*

(ii) *Claims for delay.*

(iii) *Claims on Live Stock.*

(iv) *Claims on Carriers' Act Goods.*

(v) *Claims by Commission Salesmen on goods sent to them to sell on commission.*

(vi) *Claims for street or other accidents involving injury to persons or damage to private property.*

Must not be entertained without special authority.

(c) *Claims on CARRIERS' ACT GOODS; claims on STATUTORY and claims in respect of street or other accidents involving damage to private property, must not be entertained without special authority.*

Powers of
District Goods
Managers in
reference to
Claims

21. District Goods Managers are empowered to settle claims on Merchandise and Live Stock (Traffic "Local" and "Through") excepting claims which have to be submitted to the Joint Claims Committee, and claims involving personal injury for such sums as, after due inquiry, it is clearly established the Company or Companies interested are liable to pay, provided that no important question of principle is involved, and that no payment exceeds twenty five pounds (£25).

What Claims, etc.,
are to be
reported to
Chief Goods
Manager.

22. Claims which have to be submitted to the Joint Claims Committee; claims involving personal injury or questions of principle, and claims involving payments exceeding twenty five pounds (£25), must be reported by the District Goods Manager to the Chief Goods Manager upon the approved Claims Report Forms numbered (2437), (2439), (2956).

Receipts for
Claims.

23. Whenever several "Local" traffic claims by one trader are settled at one time, the multiple claim receipt forms 4176 A providing for 10 claims, or 4176 providing for 20 claims, should be used. These forms may also be utilised for foreign

STATION INSTRUCTIONS.

CLAIMS ON MERCHANDISE TRAFFIC, ETC.—Continued.

claims, but when this is done a separate form must be used for the items with each constituent Company of another group. When claims are settled individually receipts must be obtained on the claims and must specify the amount paid in settlement.

24. Goods Compensation payments must be cleared by Local Goods Compensation Voucher Form 457, by R. H. Goods Compensation Voucher (Form 549), or Cartage Compensation Voucher (2498), as the circumstances require.

Company of
Goods Compensation
voucher payment

25. The Claim and Receipt must, in every instance, accompany the Compensation Voucher when the latter is taken credit for through the Daily Cash Account (110).

Attachment of
Claim, Receipt
and Compensation
Voucher to Daily
Cash Account (110).

26. Carting Agents, Road Carriers (e.g. Pickfords, Carter Paterson, Sutton and others), Railway, Shipping and Dock Companies, have to bear certain agreed proportions of claims on merchandise in connection with the services of collection or delivery performed by them. In the case of Railway Companies the agreed proportions must be sent, under Rule 1 to the Railway Clearing House for apportionment in the usual way. In the case of other carriers the agreed proportions must be collected and paid into the credit of Goods Compensation.

Cartage Agents,
Road Carriers
&c., proportion of
claims.

27. When settling claims for loss from or damage to goods carted by Senders, Consignees, or their Agents to or from the Railway Station within the vicinity of Senders and or Consignees premises which pass all hands without remark the deductions as agreed with the Mutual House Association must be made from the net loss.

If goods are conveyed by Senders, Consignees, or their Agents greater distances than are indicated in the foregoing paragraph and there is no evidence to show where the loss or damage occurred, instructions must be obtained from the District Manager.

28. An Abstract of Goods Compensation, prepared on Form (2577a), must be forwarded by the District Goods Manager to the Chief Goods Manager not later than on the fourth day of each month.

A abstract of Goods
Compensation to
be sent to Chief
Goods Manager
monthly

Disposal of Refused Goods, Salvage, etc. Perishable Goods.

29. If perishable goods have been damaged, pilfered, or delayed in transit and consignees refuse to receive them, efforts should be made to induce the consignees to accept the goods and deal with them promptly and to the best advantage on behalf of whom it may concern.

Disposal of
damaged goods
Salvage &c.

30. If the consignees decline to do this, and senders' instructions cannot be obtained without risk of further deterioration of the goods, goods of an important character should be sold to the best advantage, under the personal sanction of the Goods Agent or Station Master.

31. Goods must not be sold to the Company's staff if better prices can be otherwise obtained.

32. BEFORE IMPORTANT CONSIGNMENTS OF PERISHABLE GOODS ARE DESPOSED OF, the circumstances must be immediately reported to the District Goods Manager, by telegraph or telephone who will give his assent to the course proposed to be adopted or other instructions after consulting the Salvage Department.

Other Goods—Not Perishable.

33. When goods of a non-perishable character, are refused by the consignee, or owing to any other cause, and full and complete instructions cannot be obtained from the consignee, or when there are special features in connection with the nature of the goods, or when the goods are so valuable that they should be disposed of by the District Goods Manager, or when it is desirable to telegraph or telegram to District Goods Manager who will give instructions after consulting the Salvage Department. If the special features do not obtain, the goods must be sent to the Salvage Department, Park Royal, and full particulars of the goods must be shown on Form 475 which must be despatched to that Department on the same date. Every package and article forwarded to the Salvage Department, Park Royal, must bear Label No. 1093 properly filled up and securely fastened.

STATION INSTRUCTIONS.

CLAIMS ON MERCHANDISE TRAFFIC, ETC.—Continued.

- NOTES: (i) See also Standard Terms and Conditions A.16, B.15 and C.15.
 (ii) The foregoing instructions, re "Other Goods not Perishable," do not apply to such goods received from other Carriers when the compensation payable thereon has to be shared by such other Carriers. In such cases, arrangements must be made with the other Carriers concerned before any sale is effected.

While proceeds
of sales to be
paid in

34. The proceeds of all sales of goods by or on behalf of the Company be at once paid into the Daily Cash Account (110) to the credit of the Compensation; no part thereof may be applied in reduction of claims. Compensation Vouchers must be issued for the full amounts paid in settlement of the claims for such goods, and reference must be made thereon to the date of the Cash Account (110), in which the amount of the sales credit appears.

Statement of
Sales, to be re-
corded monthly

35. Particulars on Form 2190 of the amounts paid in, through the Cash Account, to the credit of the Compensation must be forwarded by the Station Agent on the 1st of each month to his District Manager, and by the latter to the Chief Goods Manager on the 4th of the month.

GOODS NOT TO HAND AND GOODS RECEIVED WITHOUT ACCOUNT.

Failures to
connect "Goods
not to hand"

The Company incur serious monetary loss owing to failures or delay in connecting goods received without account with the consignments to which they belong. The goods largely concerned are those forwarded by the senders either unaddressed or under mark and those which from various causes have lost their address labels.

These failures and delays are largely contributed to by inadequate and incorrect information being given in official communications concerning such goods. Unless the goods required or the goods on hand without account are promptly reported and correctly and adequately described connections cannot be properly made.

Station Agents will be held responsible for seeing that the following instructions are properly carried out.

Goods not to hand

Goods not to hand
to be recorded.

1. Full particulars of goods not to hand must be recorded in the "Goods not Received as Invoiced" Book (2021).

Efforts must
be made to
trace them.

2. Every effort must be promptly made to trace missing goods. "Goods not to hand" advice (Form 197) must be issued to the invoicing station and to a tracing station, and this advice must be followed up by further enquiries as each case demands, enquiry forms (2319) and (4079) being used as may be necessary.

Shipment, Val-
ues, to be traced
for enquiries.

3. When goods not to hand are urgently required for Shipment or otherwise or for Parcel Post, Air Mail, or Parcel Post, the tracing station must be used for enquiries.

It is particularly
important that
the best and fullest
description of missing
goods and the marks
they bear should be
given in all tracing
communications.

4. It is particularly important that the best and fullest description of missing goods and the marks they bear should be given in all tracing communications.

On receipt of "Not to
Hand" advice, it is the
duty of the tracing
station to satisfy itself
as to whether the goods
are reported by any
station as received
without account.

5. On receipt of "Not to Hand" advice, it is the duty of the tracing station to satisfy itself as to whether the goods are reported by any station as received without account. It is the duty of the last station from which the goods are alleged to have been forwarded (whether it be the transfer or original forwarding station) to also make the necessary enquiries for the missing goods, and to give prompt and complete replies to the communications about them.

Supplementary
enquiries.

6. All enquiries of the Salvage Department for missing goods must be made by Form 2319.

Goods received without account.

Goods received at
G.W. Stations
must also be recorded
in the "Goods Received
Unentered Book" (2020).
If the goods are damaged
record must be made
accordingly.

7. Full particulars of GOODS RECEIVED WITHOUT ACCOUNT, including goods found or lost, must be entered in the "Goods Received Unentered Book" (2020). If the goods are damaged record must be made accordingly.

8. Goods advised to G.W. Stations as received at Foreign Stations without account must also be recorded in the "Goods Received Unentered Book" (2020), or in the "Goods Reported Unentered Book" (216) where kept, but these entries must be distinguished from the entries of goods received at the Recording Station without account. (See also Clause 16.)

Efforts to enable
extra owners of
goods.

9. Every effort must be promptly made to trace the owners of goods received without account. "Received Unentered" form 381 must be issued, and if necessary the telegraph or telephone must be used, and these enquiries must be followed by such other enquiries as each case demands.

STATION INSTRUCTIONS.

GOODS NOT TO HAND AND GOODS RECEIVED WITHOUT ACCOUNT -Continued.

10 It is very important that the best and fullest information respecting goods received without account should be given, the packages and their contents and the marks they bear should be clearly described.

11 Stations must forward to the Salvage Department immediately with enquiries as to the disposal of goods received without account, the full description of the goods and marks they bear.

12 Valuable Goods, and Goods evidently intended for Shipment, the owners of which cannot be traced promptly, must be reported to the Salvage Department by wire.

13 Perishable Goods must also be reported to the Salvage Department by wire (See Instructions 16 to 19 as to the disposal of Perishable Goods received without account).

14 When Dangerous Goods, *e.g.* EXPLOSIVES, INFLAMMABLE OILS, MATCHES, etc., are received without account the station they were received from, and when known the original forwarding station, must be so advised by wire, and if disposal instructions are not obtained immediately the District Manager must be applied to by wire for his instructions.

Full description of the goods and of the marks thereon must also be telegraphed to the Salvage Department, Park Royal.

Dangerous goods must not be sent to the Salvage Department without special authority from that department.

The General Regulations contained in the "Special Classification of Explosives and Other Dangerous Goods" Section of the General Railway Classification of Goods must be observed.

Each Station must forward to the Salvage Department, daily, the "RETURN OF GOODS RECEIVED WITHOUT ACCOUNT" form 410, on which nothing is to be reported.

This return must contain full and correct particulars of:

Perishable, dangerous, and valuable Goods received without account on the day the return is issued.

Goods not perishable received without account and which have been on hand three working days awaiting disposal instructions.

NOTE.—The instructions apply to all goods including those received in trucks without labels.

Unaddressed goods received in excess of invoiced quantities in through trucks or otherwise with the following exceptions:—

(a) Flour from Barry; District Goods Manager, Cardiff, will give instructions for disposal.

(b) Continental Iron from South Wales Ports; Forwarding Station will give instructions for disposal.

(c) Traffic from Avonmouth Docks; District Goods Manager, Bristol, will give instructions for disposal.

(d) Traffic to or from Manchester Ship Canal; in addition to usual reports, advices of all excesses or shortages to be sent to District Goods Manager, Liverpool, who will give instructions for disposal of excesses.

Goods Found on Line.

NOTE.—For instructions to Permanent Way men respecting "Goods found on line," see page 289.

Goods advised to the reporting stations as received at Foreign Stations without account.

The Return must also contain the best and fullest description of the goods, and the marks they bear must be given, and when the written description is inadequate a sketch of the articles should be sent.

The entries on the return must be numbered consecutively commencing No. 1 on 1st January in each year.

Each return must bear the last "entry number" shown on the preceding return. "Nil" returns must not be issued.

Disposal of Perishable Goods received without account.

16 If the owners of Perishable Goods received without account cannot be promptly ascertained and they would involve loss by deterioration, goods of an unimportant character should be sold to the best advantage under the personal sanction of the Goods Agent or Station Master.

17 Goods must not be sold to the Company's staff if better prices can be otherwise obtained.

18 Before important consignments of Perishable Goods are disposed of the circumstances must be reported to the District Manager by telegraph or telephone, who will give his consent to the course proposed to be adopted or other instructions after consulting with the Salvage Department.

STATION INSTRUCTIONS.

GOODS NOT TO HAND AND GOODS RECEIVED WITHOUT ACCOUNT

Butter, Cheese,
Margarine, Bacon
Lard, etc

19. Butter, Cheese, Margarine, Bacon, Lard, and similar goods, sold locally if the condition of the goods permits, must have disposal instructions not received within three days from the date of receipt. Goods must be forwarded to the Salvage Department, Park Royal, invoiced free.

Disposal of Non-perishable Goods received without account.

Non-Perishable
goods received
without account.

20. If disposal instructions for non-perishable goods received without account are not received within fourteen days from the day of receipt the goods must be forwarded to the Salvage Department, Park Royal, invoiced free, subject to the following exception:—

EXCEPTION.—*Timber, Iron, Stone, and other goods in large quantities, very bulky articles (e.g., sheets of wool), and empties, must not be forwarded to the Salvage Department until authority from that Department has been received, but disposal instructions must be regularly pressed for until obtained.*

Returns and Labelling of Goods despatched to Salvage Department.

Returns and label-
ling of goods for-
warded to Salvage
Department, Park
Royal.

21. When goods are despatched to the Salvage Department, Park Royal, full particulars of them must be duly and properly given on Form 677, which must be forwarded to that Department on the same day as the goods are despatched.

22. Every package and article forwarded to the Salvage Department must bear label (No. 1033) properly filled up and securely fastened to the package or article.

Disposal of Proceeds of Sale of Goods.

23. The proceeds of an sale of goods by or on behalf of the Company must be at once paid into the Daily Cash Account (110), to the credit of Goods Compensation; no part thereof may be applied in reduction of claims.

24. Particulars on Form 2190 of the amounts paid in, through the Cash Account, to the Credit of Goods Compensation, must be forwarded by the Station Agent on the 1st of each month, to his District Manager, and by the latter to the Chief Goods Manager on the 4th of the month.

25. Foreign Ropes, etc., on hand without account.—Other Companies' Ropes, Chains, Sockets, etc., on hand without account, are forwarding Stations unknown must be forwarded immediately to the Salvage Department, Park Royal, advised and labelled in accordance with Instructions 21 and 22.

INSTRUCTIONS TO BE OBSERVED IN DEALING WITH THE RECEIPT STORAGE, AND DELIVERY OF ARTICLES DEPOSITED IN CLOAK ROOMS.

Charges to be
made.

1. The charges to be made for Cloak Room deposits are as follows:—

(a)		(b)	
Bags.		Bass Viola.	
Baskets.		Bath Chairs.	
Boxes.		Bicycles (ordinary).	
Bundles.		Cash Registers.	
Cases.		Hand Carts.	
Coats.		Harp.	
Hat-boxes.		Ice Cream Carts.	
Hucksters' Luggage.		Ice Cream Freezers.	
Mail Carts, Folding (folded).		Invalid Chairs.	
Packmen's Luggage.		Mail Carts (children's).	
Parcels.		Organs (street).	
Portmanteaux.		Perambulators.	
Rugs.		Pianos (street).	
Sewing Machines, hand.		Scissor Grinders' Machines.	
Typewriting Machines.		Sewing Machines, treadle.	
Umbrellas.		Side Cars.	
Walking Sticks.		Violoncellos.	
If removed on day of deposit or next day		If removed on day of deposit or next day	
Each article.	For each other day than the days of deposit and removal	Each article.	For each other day than the days of deposit and removal.
1d. 4 th	1d. (min.) 2d.	1d. 1 st	1d. 1 st

STATION INSTRUCTIONS.

ARTICLES DEPOSITED IN CLOAK ROOMS—*Continued.*

(c)		(d)	
Motor Cycles and Motor Scooters, uncharged with electricity, gas, oil, or other inflammable liquid or vapour (declaration to be signed by depositor). Tricycles.		Bicycles with more seats than one. Harmoniums. Pianos (other than street).	
If removed on day of deposit or next day.	For each other day than the days of deposit and removal.	If removed on day of deposit or next day.	For each other day than the days of deposit and removal.
Each article.	Each article.	Each article.	Each article.
6d. 11 ^{de}	4 ^{de} (max. 6d.)	1s. 2 ^{de}	6d. 4 ^{de}

Notes. — (i.) Sunday is not to be counted in the period, except when the articles are deposited or taken out on that day.

(ii.) The following are charged half rate, minimum deposit charge, 2d. per package.

Commercial Travellers' luggage (including Bicycles with handle-bars, pedals and saddles removed).

Personal luggage of passengers engaged in Theatrical profession, and Luggage of Music Hall Artists (vide C.A.B. Reg. 40).

Luggage of Lecturers (vide C.A.B. Reg. 22).

(iii.) When the length and girth together of any article—

Exceed 15 feet, and do not exceed 25 feet.... Double charge is made.

Exceed 25 feet Treble charge is made.

(iv.) Packages containing Liquid Air; Cylinders containing Compressed or Liquefied Gases; Motor Cycles and Motor Scooters charged with electricity, gas, oil or other inflammable liquid or vapour, are not accepted for deposit in Cloak Rooms.

2. No article shall be accepted for deposit in the Company's Cloak Rooms unless it is properly packed and labelled. In special cases where this may be waived, Form No. 4172 must be used. Any articles found on the platforms must be treated as unclaimed and the Lost Property fee charged.

Every article accepted for storage in a Cloak Room must have either the usual Cloak Room numbered label or a Warehouse label affixed.

The name of the depositor is to be recorded in the space provided on the ticket, which must be initialled by the person issuing it.

3. No package of an offensive, dangerous, or otherwise objectionable character is to be accepted for storage in Cloak Rooms.

Live animals or birds taken charge of in exceptional circumstances are accepted at the Owner's risk only and the Cloak Room ticket should be endorsed accordingly.

4. If any article of any description being tendered to the Cloak Room, is found to be locked, the depositor's attention should be called to it with a request to unlock it, and if, from any cause, he should omit to do so, it will be locked, Owner's risk, with the depositor should

Articles to bear either a Cloak Room or Warehouse label

Offensive and dangerous articles, and live animals and birds,

Packages to be locked or unlocked.

5. Tickets in rolls, printed in distinctive colours and in face-values of 2d., 3d., 4d., 6d., 9d., 1s., 1s. 3d., and 1s. 6d., will be supplied as the existing stocks of tickets are exhausted, and the values must be requisitioned to meet the general requirements.

Cloak Room tickets and cash.

Pads of face value tickets will be supplied to stations not having a large issue.

When a charge of more than the highest value of ticket in use is due on deposit, two or more tickets to cover the total must be issued. Each ticket is provided with numbered labels, and when more than three articles are deposited under one ticket, label 522 must be used.

STATION INSTRUCTIONS.

ARTICLES DEPOSITED IN CLOAK ROOMS—*Continued.*

Supplies must be ordered on Form 5746 from the Accountant (Audit Section), the closing number of the stock on hand being stated. Particulars of tickets received must be entered on the first page (inside cover) of the proof book.

The proof book (5063) of value tickets must be kept and made up daily and totalled monthly. The daily total should agree with the cash remitted.

A Return showing the commencing and closing numbers and amount of each value used must be sent to the Accountants Office (Audit Section) each month with the Parcels Accounts. The total of the Return is to be carried to Column "D" of the Parcels General Account 109.

All Cloak Room receipts must be paid in daily, and at stations where books remain in use the person receiving the cash must initial the counterfoil of the last ticket issued and insert the amount thereon.

Tickets to be filled up in ink, and cancelled, with date stamp.

6. Each ticket issued must be clearly filled up in ink, and, when handed back by the holder, be cancelled with the station date stamp and warehouse receipt, if any, as per clause 7, collected. The person delivering the package should sign or initial the ticket.

Surrendered Cloak Room tickets must be withdrawn from the Cloak Rooms each morning by the Station Master or other responsible person deposited by the District Stationmaster, and a record made by him of those not surrendered at expiration of period allowed by terms of deposit. If not already done corresponding entries must be made in the Warehouse Register either by or under the direct supervision of the Station Master or deputy referred to.

In no circumstances must the name of the depositor on a cloak room ticket be altered. If an error is made, the ticket must be cancelled and attached with a corrected complete, in support.

Warehouse Charges.

7. After expiry of the second day of deposit, warehouse rent must be charged at the rates set out in clause 1, and the amount accounted for as Warehouse revenue, Warehouse receipt (4472) being issued in every case.

No extra charge is made when Commercial Travellers' baggage is deposited after Friday 12 noon and taken out on the following Monday, nor when deposited after 5.0 a.m. on the Tuesday before Good Friday or the Tuesday before the latter falls on a Friday, or after 5.0 a.m. on the Friday before Whit Monday and August Bank Holiday and taken out on the following Tuesday.

"To Pay" Deposits.

8. When an article for which a Cloak Room ticket has not been issued, or a package left to be called for by a person other than the depositor, is claimed, the applicant should be required to give satisfactory proof of ownership. Any fees chargeable must then be collected and a Warehouse receipt (4472) issued.

Register of Warehouse Charges.

9. A book, ruled as shown below, must be kept in each Cloak Room, and all articles that have been on hand over two days (or beyond the time allowed in the case of Commercial Travellers) entered therein daily. When the articles are claimed the Warehouse charges which have accrued since date of entry must be collected and Warehouse receipt (4472) issued.

Printed books (5780) are stocked for issue to stations where the entries are numerous.

No. of Cloak Room Ticket.	No. of Articles.	Date Deposited.	Date Delivered.	Days Chargeable.	Warehouse Receipt Form 4472	Amount.	Signature of Person Delivering Articles.
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STATION INSTRUCTIONS.

ARTICLES DEPOSITED IN CLOAK ROOMS—Continued.

Supervision of Cloak Rooms.

18. The Cloak Room tickets and the Warehouse Charges register must be checked by the Station Master or some responsible person deputed by him at least once a week and the general conduct of the Cloak Room business should receive the personal supervision of the Station Master as far as possible.

Articles not
claimed.

19 Any article remaining on hand in the Cloak Room more than a month after deposit must be reported to the Lost Property Office and Divisional Superintendent or District Traffic Manager, full particulars being given.

Any article not claimed within 6 months must be opened by a responsible person in the presence of another, and if the owner's name and address can be ascertained, instructions for disposal should be asked for. If no address can be found or no reply is received to an application within a month, the article must be sent to the Lost Property Depot, Paddington, a full list of the contents of all packages being kept.

Lost Luggage charges.

20. Cask Room in kets must not be drawn to cover loss incurred for the loss of passengers' lost luggage. Such amounts must be paid in separately, and, if collected, at the Station at which the article is found, Warehouse receipt (4472) must be issued.

TRAINMEN'S INTERVALS FOR REST.

With the exceptions which follow, the roster must provide for a period of 12 hours rest being shown in the case of a recruit, after a tour when at the place of duty. In all cases a minimum of 11 hours is to be allowed from the time of signing off for one turn of duty to signing on for the next turn on duty.

Exceptions :—

(4) The rest interval a rail worker shall not be withheld, when it is necessary to can men out for other causes, such as breakdown, illness, fire, etc. It is not intended to vary the agreed arrangements in regard to the intervals of rest between train duties.

(b) The two variables entered for rest may be reduced to eight hours by mutual agreement in the case of a working month of eight hours when contracting terms of day at the week is

4. The non-employees interviewed do not necessarily be with full in respect of the main working race training, excluding that is, or other similar situation. In such cases it is agreed that a minimum of seven hours rest only need be given where the outward working has not exceeded seven hours. In such cases non-employees released from work as soon as possible after completion of the return journey.

When the return train arrives late at its destination, it is on that the third cannot work the return train unless he resumes duty with less than the required interval for rest. Arrangements must be made, such as making the third man a large order to put the guard on to a later train, or to put the train back so as to enable the forward or towards to have the proper interval for rest. It will be necessary however for the third man going off duty to see the person making the arrangements, or receive from him written instructions as to the time duty is to be resumed.

With the foregoing background, the parties to the present dispute may be said to have made by both D. 1. art. 10.

When trains are put on, a card must be sent to the terminus, and principal stations on the route, giving particulars of the arrangements.

GUARDS AND ENGINEMEN TRAVELLING FROM POINT TO POINT TO RELIEVE TRAINMEN.
AND TRAINMEN PROCEEDING TO THEIR HOMES AFTER BEING RELIEVED *en route* FOR
REST, ETC.

Tramway's Order 7 in No. 670 is to be read as the authority for guards in Union R.R. to
 travel as passenger or freight train, with the stipulation that the freight train and any ticket on
case of an order for it.

When a mail is forwarded, the envelope must be stamped with the date and station to which it is forwarded. It must also be stamped with the name of the person to whom it is forwarded. The envelope must be sealed and the seal must be signed by the person who forwards it. The envelope must be forwarded to the Superintendent of the Division in which the mail is forwarded, and not to the Add. Office with the collected tickets.

In the event of a train being made up for a man to travel the first portion of a journey by passenger train and the remainder by freight train, the form must be completed at the end of the journey and attached by the guard in charge of the freight train traveling to the terminal or through station rendered to the Superintendent of the Division in which the man leaves the train, and the Superintendent receiving it must forward it to the Superintendent in whose division the man is stationed.

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STATION INSTRUCTIONS.

DUTIES WHICH MUST NOT BE ALLOTTED TO LADS.

Working signals or points.	Handsignalling.
Working block telegraph instruments.	Acting as pilotman for single line working.
Roping vehicles.	Dealing with Train Staffs, Tokens or Tablets.
Coupling and uncoupling vehicles.	Carrying full sacks of corn, flour, milk churns, &c.
Shunting.	Getting on tops of carriages.
Look out men.	Starting engines of road motor vehicles.

REFRESHMENT ROOMS AT STATIONS.

At Stations where the Refreshment Rooms are held by tenants.

The Station Masters to see that:—

The rooms are properly conducted, and to bring under notice any complaints made respecting them.

A printed copy of the authorised tariff of charges is suspended in a conspicuous position in each of the public rooms and is the only tariff exhibited.

Each room is kept neat and clean.

At Stations where the Refreshment Rooms are under the Company's own management.

The Station Masters to keep the rooms under general observation, and to call the attention of the Board of Directors to any complaint or any other matter affecting them, which it may be desirable to bring under notice.

At all Stations where there are Refreshment Rooms.

The staff to co-operate with, and assist, the persons in charge of the refreshment rooms in any measures that may from time to time be necessary to preserve order. Any persons evidently under the influence of drink or known to be habitual drunkards must, while at the station, be kept under observation, and, as far as possible, prevented from entering the refreshment rooms, especially as, in the event of a prosecution for public drunkenness on a railway, the responsibility of proving that all reasonable steps were taken to prevent drunkenness is placed, by the Licensing Act of 1902, upon the licensee holder.

SUPPLY OF LUNCHEON AND TEA BASKETS AND CUPS OF TEA TO PASSENGERS IN THE TRAINS.

1. Breakfast, Luncheon and Tea Baskets. Baskets containing Breakfast, Hot or Cold Luncheon or Dinner or Tea are obtainable at the Refreshment Rooms of the following Stations:—

†Aberystwyth	†Hereford	†Shrewsbury
Banbury	Kidderminster	*Slough
*Barmouth Junction	†Leamington Spa	Stourbridge Junction
†Barn	†Maidenhead	Stratford-on-Avon
†Birmingham (Snow Hill)	Malvern, Great	†Swansea, High Street.
†Bristol (Temple Meads)	*Moat Lane	†Swindon
*Bristol (Stapleton Road)	†Neath	†Taunton
*Builth Road	Newbury	Torquay
†Cardiff (General)	†Newport (Mon.)	Trowbridge
†Cardiff (Queen Street)	†Newton Abbot	†Truro
†Carmarthen	†Oswestry	Wellington (Salop)
Chester	†Oxford	†Welshpool
Chippenharn	†Paddington	†Westbury (Wilts)
Corwen	Penzance	Weston super-Mare
Craven Arms	†Plymouth (Millbay)	*Weymouth Quay
Didcot	†Plymouth (North Road)	*Windsor
Dudley	†Pontypool Road	†Wolverhampton
†Exeter (St. David's)	†Reading	Worcester (Shrub Hill)
*Fishguard Harbour	Ruabon	Wrexham
†Gloucester		

† Hot Luncheons supplied. * Tea Baskets only.

Hot Breakfast baskets can be supplied at all stations except those marked *

The charges for Breakfast, Luncheon and Tea Baskets are as shown below:—

Breakfast Baskets. —Eggs and Bacon (or Cold Ham), Bread, Butter, Preserves, Tea,	s. d.
.. .. .	3 6
Luncheon or Dinner Baskets. Meat (Hot or Cold), (Roast or Pressed Beef, &c.), Bread,	
.. .. .	3 0
Cold Chicken and Ham, Bread, Butter, Cheese, Salad, &c.	3 6
Chop or Steak, Bread, Butter, Cheese, &c.	3 6
Tea Baskets. —Pot of Tea, &c., Bread and Butter, Cake or Bun and Fruit. Per person	1 3

STATION INSTRUCTIONS.

LUNCHEON AND TEA BASKETS, ETC.—*Continued.*

2. When empty baskets are handed out by passengers, the persons receiving them must take care of themselves that the fittings—a list of which is given on a card in the inside of the baskets—are complete, and if any article should be missing they must, before the train leaves the station, report it to the Station Master or person in charge.

3. All such baskets handed to the Company's Servants by passengers must be taken immediately into the parcel office by the person receiving them, and booked free of charge by next train to the station marked upon them. In the case of Guards, however, whose time does not permit of their taking the basket to the parcel office, they must immediately deliver it to a member of the Station Staff, who must take it there at once.

Luncheon and Tea Baskets, or cups and saucers, removed from carriages by searchers or others must be taken immediately to the parcel office, or deposited in a special receptacle when provided for the purpose.

4. At Junction stations where through carriages (whether belonging to this Company or to other Companies) are taken on by, or transferred to, the trains of other Companies, such a service must be made in the through carriages as the circumstances and admit, with the object of returning Luncheon and Tea Baskets, or cups and saucers, and, if practicable, this must be done by a particular member of the Staff as part of his duties.

5. In addition to Tea Baskets, cups of tea, &c., are supplied on the platform at the following stations, and passengers are allowed to take the cups and saucers into the carriage with them:

Aberystwyth	Hereford	Reading
Afonwen	Leamington Spa	Ruabon
Banbury	Most Lane	Shrewsbury
Bath	Neath	Swansea
Birmingham	Newbury	Swindon
Bristol	Newport	Taunton
Cardiff (General)	Newton Abbot	Trowbridge
Carmarthen	Oswestry	Truro
Chester	Oxford	Welshpool
Chippenharn	Paddington	Westbury (Wilts)
Didcot	Plymouth (Mill Bay)	Weston-super-Mare
Dovey Junction	Plymouth (North Road)	Wolverhampton
Exeter	Pontypool Road	Wrexham
Gloucester		

6. The Staff must remove cups, saucers, &c., from carriages after passengers have finished with them, and at those stations where the Rooms are near the terminus of the line, they must take them to the Refreshment Rooms, or place them in the basket provided on the platform for their reception, which at stations where the Refreshment Rooms are situated is to be taken to them; must be promptly taken to the parcel office, where they must be returned to the Refreshment Stores, Paddington.

7. To prevent damage to crockery during transit, baskets are worked under the platform at stations for this purpose, and every attention must be given to the proper working of this basket.

8. To prevent extensive loss of crockery, Station Masters must see that the baskets are returned in due season to the collection of empty Luncheon and Tea Baskets and cups and saucers, and that no delay takes place in returning them to the Refreshment Rooms.

9. It is hoped that the Staff generally will take an interest in these arrangements, which are made for the comfort and convenience of the travelling public, and also see that these instructions are properly observed, reporting any irregularity which may come to their notice.

10. Folding tables. For the convenience of First-Class Passengers, small folding tables for luncheon or other purposes are provided for hire at the stations at which Luncheon and Tea Baskets are supplied, and also at Llanrwst. Passengers requiring Luncheon Baskets should be asked, or they also require a Folding Table, and if so the words "Folding Table" should be added to the telegram. Folding tables are also provided in certain cases for the use of invalid passengers or special parties, first or third class, and in such cases authority for the hire thereof must be obtained from the Divisional Superintendent.

A charge of 1/- is made for the use of a folding table.

ADVERTISING ARRANGEMENTS.

1. Posting of Bills on Station Boards, &c. — Only posters authorized for exhibition should be displayed at stations, and Station Masters should report the circumstances to the Divisional Superintendent or District Traffic Manager if posters reach their stations through unauthorised channels.

Current time bills and posters affecting each station must be posted neatly on the station boards on the line of sight and renewed as soon as they become damaged, illegible, or begin to look shabby. A red line must be ruled on each bill under the name of the Station and the times applying to it.

Slips issued extending the period for which bills are dated, or announcing alterations in the services, must be promptly and neatly pasted upon the bills to which they refer.

STATION INSTRUCTIONS.

ADVERTISING ARRANGEMENTS—Continued.

Great Western Pictorial Posters should occupy prominent positions, and should be renewed as soon as they become faded, dirty or damaged. No pictorial poster must be displayed for any longer than two months.

All out-of-date bills must be promptly washed off the boards. Station Masters must see that this is done and that current bills are properly displayed.

The best positions on boards should be retained for the Company's own advertisements, and other posters authorised for display should be confined to less important positions.

Posters advertising Excursion and Cheap Ticket arrangements should be displayed upon boards in positions where they are most likely to be seen by the travelling public. At some stations Special Notices are applied with one panel for a bill and a black panel for the notices. At other stations black boards or black paper bills are provided for Cheap Notices. Special attention should be given to this valuable form of publicity.

Where boards are exhibited in situations at a distance from the stations, e.g. at Hotels, Institutes or on Company's property away from stations, special steps must be taken to ensure posting with up-to-date bills.

The attention of the Divisional Superintendent should be called to all boards needing repair or repainting.

Poster boards and poster board sites at stations are sometimes required for the purposes of the Company's Advertising business, either temporarily or permanently, and when this is the case all concerned are requested to review the proposals made at the most sympathetic and practical point of view with the necessity for adequately advertising the Company's excursions.

2. Handbills, Pamphlets, &c. All Excursion and other handbills, pamphlets, &c. should be strong and long-lasting. Bills or other places at the stations most frequented by the public should be examined regularly to ensure no out-of-date matter being displayed.

In the event of the display of posters, handbills, pamphlets or other advertising matter being too large, or too numerous, the Divisional Superintendent should be notified of the facts.

The distribution of advertising matter is very important, and Station Masters should personally interest themselves in it and lists should be drawn up for each station providing for the distribution of:—

(a) Race, Football and other sporting trip bills mainly to public houses, sporting clubs, works, workmen's clubs, &c., and a few copies to Libraries, Hotels, Institutes, Post Offices, &c.

(b) Ordinary day and half-day trip bills mainly to Libraries, Hotel, Institutes, Post Offices, Works, Newspapers, &c., and a few to public houses, sporting and workmen's clubs, &c.

Outlying villages and neighbouring towns should be specially considered. In some cases, bills may have to be sent by post. Now that so many places are kept open late hours, and the Rail Companies, a very much wider field is opened up for the distribution of excursion literature, and the Railway and Station Masters should make the most of these services, and when Rail excursions are arranged for such times as make it quite practicable for day passengers to travel at home by the excursions, tickets should be sent to every suitable place along the road service route.

At seaside and holiday resorts, generally, the distribution lists require to be of a still more extensive character and should comprise all hotels, boarding houses, apartments, clubs, &c. in addition to the places in clauses (a) and (b). Even in small houses accommodating only two or three holiday makers at a time, the publicity is valuable. The visitors at these addresses are just the class likely to make trips to neighbouring places if the facilities are convenient.

Tourist and Holiday Ticket Pamphlets and general descriptive literature issued by the Company gratuitously, must be distributed as widely as possible in all quarters where the benefit may be expected in the Company's interest.

Opportunity should be taken of the delivery of parcels to place a few handbills or a pamphlet or two under the string, &c.

3. Advertising in Waiting Rooms, Booking Halls, &c. Attention should be given to the doing all possible to keep Waiting Rooms, Booking Halls, &c., as tidy as possible. All shabby notices of the G.W.R. advertisements and old untidy Shipping Co's advertisements should be reported to the Divisional Superintendent who will, after consulting with the Publicity Committee, take steps to disposal.

4. Gratuitous Time Tables. (Large Edition only.) The gratuitous time tables must be delivered to the Hotels, Firms, Clubs, &c., on the agreed list as soon as possible after receipt at the stations. In no case should the delivery be delayed after the date each time table comes into operation.

If the supply is not received wholly or in part at least 10 days before the date of operation, the attention of the Divisional Superintendent or District Traffic Manager is to be drawn to the matter.

Opportunity should be taken of the distribution of gratuitous time tables to include local cheap tickets bills or pamphlets.

Gratuitous time tables sent to stations in addressed wrappers must be sent equally promptly.

STATION INSTRUCTIONS.

COMMERCIAL ADVERTISING ARRANGEMENTS.

All communications relating to commercial advertising arrangements should be addressed to the Commercial Advertising Agent, Paddington Station, W.2.

Six canvassing areas are each operated by a responsible representative, with Headquarters as under :—

Paddington	..	Station.
Birmingham	..	Divisional Superintendent's Office.
Gloucester	..	" " " "
Cardiff	..	" " " "
Bristol	..	" " " "
Plymouth	..	District Traffic Manager's Office.

The Company's Billposting Staff is responsible for the display, erection and proper maintenance of all Commercial advertisements.

The duties of the Representatives and Billposting Staff are controlled direct from Head Office.

All sites on and within the immediate precincts of a station are considered "free" for letting for general advertising matter, but Station Masters will be consulted when necessary before a special site is definitely let.

Proposals for sites on the Company's buildings or land in the occupation or maintenance of another Department will be referred to the Officer concerned for agreement.

Station Masters must see that all Commercial advertisements exhibited at stations are in good condition, and to advise the Commercial Advertising Agent when "renewals" or other attention is necessary.

It is very important that no delay should occur to bills, boards and other material of the Department during transit, and Station Masters and others concerned must carefully watch this.

SALE OF NEWSPAPERS, TOBACCO AND CONFECTIONERY AT STATIONS.

Messrs. Wyman & Sons Ltd. have the right to sell (other than by automatic machines) newspapers, books, periodicals, pamphlets, prints and stationery, and other articles as may be subsequently agreed with the Company and which will be notified from time to time.

The Contractors are required to keep the bookstalls clean and tidy; no trade advertisements must be exhibited thereon without the Company's consent, and contents bill spaces should not bear announcements other than those of the newspapers, etc., on sale at the bookstall. The newsboys employed by the Contractors are expected to be neat in appearance, and all persons employed by the Contractors must not in any way obstruct or impede the servants of the Company in the performance of their duties, but obey all reasonable orders or instructions of the Company's Station Masters. In the event of misconduct or refusal to obey such orders or instructions, the Station Master or other authorised Official may withdraw from any person in the employ of the Contractors the right of access to stations to which they otherwise are entitled under the terms of the contract.

The Contractors' stock in trade forwarded by or for them to the Company's stations should be charged at the ordinary rate, but unsold newspapers and other "returns" after having been exhibited for sale will be conveyed free on the Company's system. The Company have the right to inspect such parcels of "returns."

At stations where no bookstalls are provided, the Contractors may, subject to the Company's approval, arrange with a local newsvendor for the sale of newspapers, etc.

Members of the Contractors' staff not holding passes or season tickets must pay ordinary fare.

The attention of Station Masters and others in authority is directed to the necessity for ensuring that the terms of the contract are observed by Messrs. Wyman & Sons Ltd., and any breach of the arrangement should be reported.

Particular care and attention must be exercised by the Guards and others in dealing with the parcels of newspapers, etc., in transit, and at stations, in order that they may arrive safely and in good condition, at their respective destinations.

Trestles or other temporary stands upon which newspapers are sorted must be removed from the platforms each day before the busy traffic commences.

Station Masters must satisfy themselves that adequate arrangements are in force for the regular disposal of waste paper, which must not be allowed to accumulate at the bookstalls, particularly at the back or on the top.

In some cases outsiders have been permitted to sell race-cards on the platforms at the Company's stations. This is an infringement of the rights of the Bookstall Contractors, and none but Messrs. Wyman & Sons' employees must be allowed to sell the cards on the Company's premises.

Instances have also occurred where platform barrows have been allowed to run against the bookstalls, and in consequence a considerable amount of damage has been caused, which, with ordinary care, could easily have been avoided. The attention of the staff concerned is specially drawn to the matter, and serious notice will be taken of any irregularity that may be discovered.

A certificate must be sent by Station Masters or others concerned to their Divisional Superintendent or District Traffic Manager on December 15th of each year that these regulations are being strictly carried out.

STATION INSTRUCTIONS.

SALE OF NEWSPAPERS, TOBACCO AND CONFECTIONERY AT STATIONS—*Continued.*

Sale of Tobacco and Confectionery.

Messrs. Wyman & Sons are permitted to sell confectionery and tobacco at certain stations, which are advised as and when agreed.

At all other stations where the sale of confectionery, tobacco and cigarettes, etc., is undertaken by the Company by means of trolleys or basket-boys under the control of the Refreshment Department, Station Masters should exercise supervision over the boys when on duty and, at Stations where there are no refreshment rooms, sign them on and off; receive and pay in cash; and sign the boys' daily time sheets and pay their wages. The paybills will be prepared by the Refreshment Rooms Department, who will also be responsible for checking the stocks. Cupboards to contain the stock sold from trolleys and baskets are provided by the Refreshment Rooms Department, and Station Masters must take all possible steps to prevent damage or pilferage.

RAILWAY SKETCHING.

Occasions frequently arise when a descriptive report dealing with the locality of a particular occurrence, detailing shunting or other operations, outlining suggested structural alterations, or describing some other matter pertaining to the railway may be made far more intelligible if accompanied by a rough sketch. It will be found that very little skill is required so long as the recognised way of depicting railway objects is followed, and the diagrams given on the following page may be taken as examples of the correct way of showing these.

	Fig. No.		Fig. No.
Audible signalling ramp	14	Lamp-post	11
Barrow Crossing	1	Level crossing	3
Bay line	2	Load gauge	11
Boundaries	12	Locking bar	9
Bridges	1, 3, 6	Mile posts	15
Buildings, station	1	Overbridge	5
Carriage landing	10	Parish boundary	12
Catch-point	9	Platforms	1, 2
Cattle-pens	10	Points	9
Company's boundary	12	Point discs	15
Compound points	9	Ramp	1
Crossing, barrow	1	Scissors crossing	8
Crossing (level) gates	3	Sidings	9
Cross-over road	7	Signal-box	3, 14
Cross-over roads, scissors	8	Signals	14, 15
Cut lines	3, 4	Steps	1
Cutting	16	Stop block	13
Dead end	13	Stop, wheel	13
Discs, point	15	Subway	1
Distances	14	Telegraph pole	11
Ditch and hedge	12	Track circuit	15
Doors	1	Track circuit indicator	14
Double compound points	9	Track weighbridge	11
Embankment	16	Tunnel	4
Engine pit	1	Turntable	11
Facing point locking bar	9	Underbridge	6
Fences, &c.	12	Urinals	1
Footbridge	1	Veranda coverings	1
Gantry	15	Wall	12
Gate, level crossing	3	Water column	11
Gate, platform	1	Wheel stop	13
Gate, siding	11	Wicket	3
Goods shed	10		
Gradient Posts	15		
Hedge and ditch	12		

STATION INSTRUCTIONS.
RAILWAY SKETCHING—Continued.

Fig. 1.

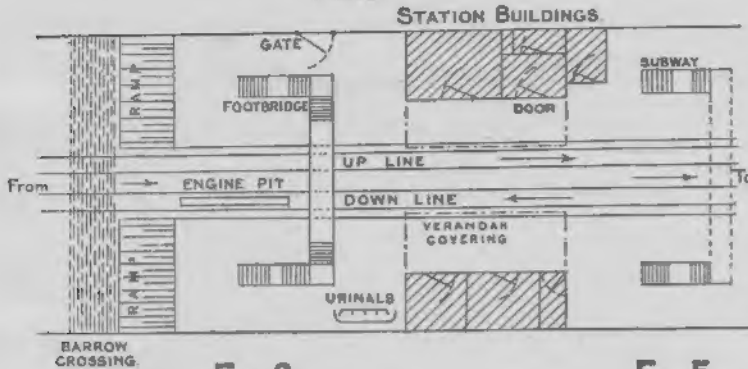


Fig. 3.

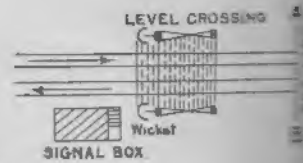


Fig. 2.

GROUND PLAN OF STATION

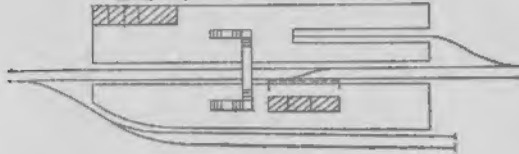


Fig. 5.

OVERBRIDGE

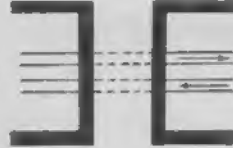


Fig. 6.

UNDERBRIDGE

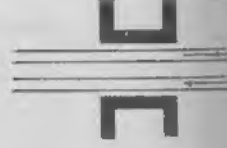


Fig. 7.

CROSSOVER ROAD

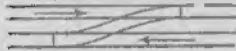


Fig. 8.

SCISSORS CROSSING



Fig. 9.

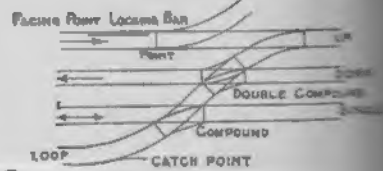


Fig. 10.

GOODS SHED

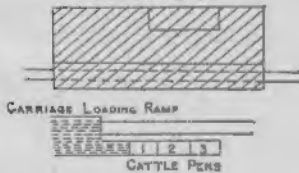


Fig. 11.

TRUCK WEIGHBRIDGE

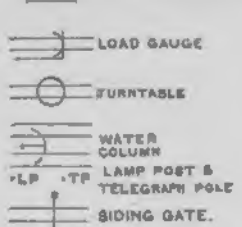


Fig. 12.

FENCES ETC.



Fig. 13.

STOP BLOCK

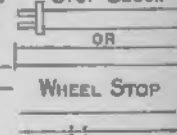


Fig. 14.

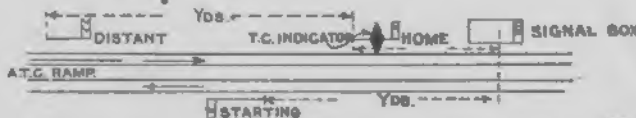
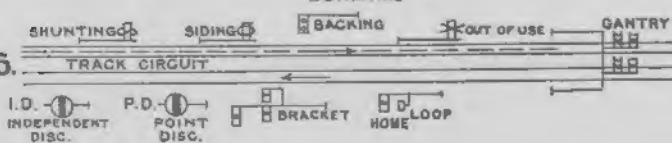


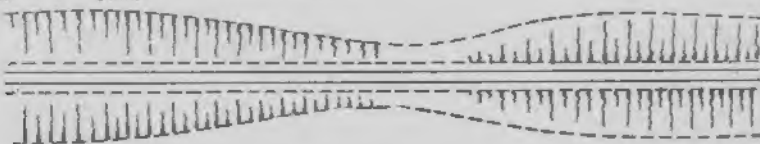
Fig. 15.



118 M.P.

MILE POST

GRADIENT POST



CUTTING

Fig. 16.

EMBANKMENT.

